



City of Santa Fe

# AGENDA

BICYCLE AND PEDESTRIAN  
ADVISORY COMMITTEE  
AUGUST 10, 2023  
5:30 PM  
CITY COUNCIL CHAMBERS,  
CITY HALL  
200 LINCOLN AVENUE

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## PROCEDURES FOR BICYCLE AND PEDESTRIANS ADVISORY COMMITTEE MEETING

**Written Public Comment:** Members of the public may submit written comments on legislation by clicking on the comment bubble to the right of the meeting on the public portal at <https://santafe.primegov.com/public/portal> three hours prior to the start of the meeting.

The agenda and packet for the meeting will be posted at <https://santafe.primegov.com/public/portal>.

1. **CALL TO ORDER**
2. **ROLL CALL**
3. **APPROVAL OF AGENDA**
4. **APPROVAL OF MINUTES**

- a. July 13, 2023 minutes

5. **COMMUNICATIONS FROM THE PUBLIC**

6. **COMMUNICATIONS FROM OTHER AGENCIES**

- a. Santa Fe Metropolitan Planning Organization (Erick Aune, AICP, Director and Leah Yngve, Senior Transportation Planner)
- b. Santa Fe Conservation Trust (Tim Rogers, Trails Program Manager)
- c. Public Safety Committee (Khalil Spencer, BPAC Vice-Chair)

7. **DISCUSSION AND POSSIBLE ACTION ITEMS**

- a. Appointments to BPAC Subcommittees
  - 1) Angela Bordegaray – BPAC Member, Promotion Education and Programming Subcommittee
  - 2) Gary Schiffmiller – Citizen Member, Technical Review Subcommittee
  - 3) Tim Rogers – Stepping down as Citizen Member of Technical Review Subcommittee
- b. Constituent Request for Pedestrian/bicycle Crossing Improvements to the River Trail Crossing at W. Alameda/St. Francis (Abigail Wolaver)
- c. Presentation on Traffic Signals and Safety Design and Updates on City Traffic Operations Activities including Maintenance (Mike Dalmolin, City Traffic Operations Engineer)
- d. Comments on the draft Bill 2023-24 – Defining Electric Unicycles (Yolanda Eisenstein, Policy, Planning and Law Subcommittee)  
A Bill Amending SFCC 1987, Section 23 5.1 to Include a Definition for Unicycles; Amending the Uniform Traffic Ordinance (UTO), Exhibit A to Chapter 24 by Creating a New Section 12-1-86.5 to Add a Definition for “Unicycle” and “Electric Unicycle”, Amending Section 12-1-40 to Exclude Electric Unicycles from the Definition of Motor Vehicles, Amending Section 12-1-88 to include Electric Unicycles in the Definition of Vehicle

and Amending Articles 8-1 Through 8-16 and 12-10-3.1.

- e. Request to Adopt the Amended BPAC FY2023-2033 Strategic Plan (Yolanda Eisenstein, Policy, Planning and Law Subcommittee Chair)
- f. Electric Bicycles Safety Issues (Judith Gabriele, Promotions, Education and Programming Subcommittee Chair)

**8. SUBCOMMITTEE COMMUNICATIONS**

- a. Policy, Planning and Law (Yolanda Eisenstein, Chair)
- b. Promotions, Education and Programming (Judith Gabriele, Chair)
  - BPAC Presentation to the Governing Body
- c. Technical Review (Ben Pingilley, Chair)
  - Paseo Del Sol Extension 30% Design Review
- d. Bike Friendly Community Application Task Force (Steven Pilcher, Chair)

**9. MATTERS FROM STAFF**

**10. MATTERS FROM THE COMMITTEE**

**11. MATTERS FROM THE CHAIR**

**12. NEXT MEETING: Thursday, September 14, 2023**

**13. ADJOURN**

**Persons with disabilities in need of accommodations, contact the City Clerk's office at 955-6521, five (5) working days prior to meeting date.**



# MINUTES

BICYCLE AND PEDESTRIAN  
ADVISORY COMMITTEE  
JULY 13, 2023  
5:30 PM  
ROUNDHOUSE ROOM, MARKET  
STATION  
500 MARKET STREET, SUITE  
200

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## PROCEDURES FOR BICYCLE AND PEDESTRIANS ADVISORY COMMITTEE MEETING

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### 1. CALL TO ORDER

The meeting was called to order at 5:31pm by Chair Michael Garcia.

### 2. ROLL CALL

**Members Present:**

Member Erik Aaboe, County Representative  
Member Angela Bordegaray, District 4  
Member Yolanda Eisenstein, District 1  
Member Judith Gabriele, District 2  
Member Daniel Painter, District 4  
Member Steven Pilcher, District 3  
Vice-Chair Khalil Spencer, District 1  
Chair Michael Garcia, City Councilor, District 2

**Members Excused:**

Member Tony Gerlicz, District 2  
Member Ben Pingilley, District 3

**Others Attending:**

Romella Glorioso-Moss, Staff Liaison  
Lucrecia Diaz, Clerk

3. **APPROVAL OF AGENDA**

- a. **MOTION:** Chair Garcia moved, seconded by Member Gabriele to remove item 6.a (Communications from Santa Fe Planning Organization) from the Agenda.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Aaboe, Member Eisenstein, Member Gabriele, Member Painter, Member Pilcher, Member Spencer, Chair Garcia

**Against:** None

**Abstain:** None

- b. **MOTION:** Member Spencer moved, seconded by Member Gabriele to replace Asst. City Atty. Kevin Nault's name with Member Judith Gabriele's name as per City Attorney's email to Staff Liaison.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Aaboe, Member Eisenstein, Member Gabriele, Member Painter, Member Pilcher, Member Spencer, Chair Garcia

**Against:** None

**Abstain:** None

- c. **MOTION:** Member Aaboe moved, seconded by Member Pilcher to approve the Agenda as amended.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Aaboe, Member Eisenstein, Member Gabriele, Member Painter, Member Pilcher Member Spencer, Chair Garcia

**Against:** None

**Abstain:** None

#### 4. APPROVAL OF MINUTES

**MOTION:** Member Pilcher moved, seconded by Member Painter to approve the June 8, 2023 minutes as amended. Amendment is to include Member Painter’s name in “Roll Call Votes – For” on pp. 5 and 6.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Gabriele, Member Painter, Member Pilcher, Member Spencer, Chair Garcia

**Against:** None

**Abstain:** Member Aaboe, Member Eisenstein

#### 5. COMMUNICATIONS FROM THE PUBLIC

- a. Hubert van Hecke  
871 Don Cubero, Santa Fe, NM

As you know, I am not a fan of so called bike lanes that consist simply of little white lines and I realize it’s hard to retrofit all Streets. However, what bothers me is that in new developments, I see the same little white lines. I wondered at what level of the City we can change the rules, recommendation, requirements such as a new street goes in higher priorities are given to bicycle safety, so we don’t end up with just another piece of asphalt and little white lines. We know if you start with a blank slot you have lots of options to do a proper bike paths, etc. It doesn’t seem to happen. I wondered if this committee has jurisdiction over that or I hope we can work to change the codes at the right level or department such that proper bike paths become the default.

Chair Garcia: Little by little, improvements will be made due to the implementation of Complete Streets Resolution.

Staff Liaison Glorioso-Moss: Public Works Roadway and Trails is reconstructing a number of roads/streets to make them safer. Reconstruction

includes the addition of bike lanes, buffered bike lanes, ADA compliant sidewalks, continental crosswalks, and narrowing travel lanes. Currently, protected bike lanes have not yet become the standard design for bike lanes due to maintenance issues.

- b. Gary Schiffmiller, Santa Fe, NM  
Policy, Planning and Law Subcommittee Member

Question and comments about bike lane signs and I have noticed some going up and I think that is great. My question is how is it determined, where are they going to go and how is it prioritized? I have noticed some signs that are really useful and noticed some in places, where no bicyclist would ever take the lane and they exist in places, they really shouldn't. It seems to me that they should be put anywhere we have sharrows.

Chair Garcia: Will ask Public Works Department Director.

## 6. **COMMUNICATIONS FROM OTHER AGENCIES**

- a. Santa Fe Metropolitan Planning Organization (Erick Aune, AICP, SFMPO Director)

[Removed from the Agenda]

- b) Santa Fe Conservation Trust (Tim Rogers, Trails Program Manager)

[Mr. Rogers did not attend this meeting.]

- c) Public Safety Committee (Khalil Spencer, BPAC Vice-Chair)

## 7. **DISCUSSION AND POSSIBLE ACTION ITEMS**

- a. Introduction of Ms. Angela Bordegaray, new BPAC member representing D4 followed by introductions of all BPAC members and staff.

**MOTION:** Chair Garcia moved, seconded by Member Painter to table this agenda item until later.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Aaboe, Member Eisenstein, Member Gabriele, Member Painter, Member Pilcher, Member Spencer, Chair Garcia

**Against:** None

**Abstain:** None

b. City Trails Maintenance Plan FY 2024, Parks and Open Space Presentation  
(Scott Overlie, Project Administrator)

Scott Overlie: Good evening Chair Garcia and other committee members. I am here to give you a brief presentation I guess of sorts, where we are at with maintenance of our trails. We have engaged on getting some quotes for crack seal work and repairs. We are currently in that process, we started it about a month ago and we are hoping to have it wrapped up and have a schedule figured out, on how we are going to proceed on this.

As you may know, we had an assessment that was done last year and that is broken out into priorities. We have some trails, that are high priority dropping down to I wouldn't say low but definitely lower priority. We are everting looking at the Rail Trail, as that as shown from the reports having the most and then boils down Tierra Contenta Trails, there are also inner City trails that are also within that report. That's where we are really focusing our work load. So, that's where we are at hoping to potentially engage with some of this maintenance, maybe this fall, maybe sooner, if possible. One thing we are up against though, company's or vendor's that the City have licenses they are really busy. We are trying to make it work with them, that's really our primary contact is with those vendors because they are the ones that supply maintenance that we need on specifically on the asphalt trails. So yeah, that's where we are at.

Member Gabriele: I'm very excited to hear about the Rail Trail. Are those frost heaves, how can those cracks form? What happened?

Scott Overlie: They're most likely frost heaves and the age of asphalt and deferred maintenance, where we haven't had the chance to get back, they tend to keep moving and as years go by and we have heat cycles and cold cycles the asphalt starts to shrink a bit and that's what we starting to see. On the edges that's what we are starting to see, where it falls off it's basically the the same kind of principle. We get moisture that gets underneath it.

Member Gabriele: Any of the planning, include plowing these trails in the winter time?

Scott Overlie: That I don't know, I will get back to you on that.

Member Spencer: The cracks are wide on the River Trail and Casa Solana, that are too wide for crack seal.

Scott Overlie: We will need to mill them out.

Member Eisenstein: Are all of them up for repair or any of the trails being resurfaced?

Scott Overlie: Right now we are not looking to resurface because we don't have the funding for that, however if there are particular areas you are aware of, let us know, we can take a look at it. It probably is already on the list, it comes down to prioritization.

Member Pilcher: As more trails get built do you have automatically have budget maintenance?

Scott Overlie: We have been requesting, we don't automatically have that.

a. Introduction of Ms. Angela Bordegaray, new BPAC member representing D4 followed by introductions of all BPAC members and staff.

**MOTION:** Chair Garcia moved, seconded by Member Painter to bring Agenda Item #7.a off the table.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Aaboe, Member Bordegaray, Member Eisenstein, Member Gabriele, Member Painter, Member Pilcher, Member Spencer, Chair Garcia

**Against:** None

**Abstain:** None

BPAC Members introduced themselves.

c. City Ordinance 2023-4 "Electric Bicycles" vis-à-vis NM SB69 "Electric-Assisted Bicycle Definition" (Judith Gabriele, PEP Subcommittee Chair)

Chair Garcia read Asst. City Atty. Kevin Nault's e-mail sent July 13 at 5:35 pm as follows:

*"In lieu of appearing at a public meeting, I'm sending this e-mail to inform you that the City Attorney's Office has reviewed the State's new statute regarding restrictions on electric bikes on paths and, as permitted in that statute, the City's ordinance arguably permits level 2 and 3 electric bicycles on bike paths like the Rail Trail. That said, the Advisory Committee may advise the*

*Governing Body to make that permission more explicit for the sake of clarity, if it wishes to do so.”*

At the recommendation of Member Eisenstein, Policy, Planning and Law Subcommittee Chair, no further action by BPAC needs to be made.

d. CONSIDERATION OF A BILL 2023-24 (Councilor Lindell, Councilor Cassutt, Councilor M. Garcia)

A Bill Amending SFCC 1987, Section 23 5.1 to Include a Definition for Unicycles; Amending the Uniform Traffic Ordinance (UTO), Exhibit A to Chapter 24 by Creating a New Section 12-1-86.5 to Add a Definition for “Unicycle” and “Electric Unicycle”, Amending Section 12-1-40 to Exclude Electric Unicycles from the Definition of Motor Vehicles, Amending Section 12-1-88 to Include Electric Unicycles in the Definition of Vehicle, and Amending Articles 8-1 Through 8-16 and 12-10-3.1. (Romella Glorioso-Moss, rsglorioso-moss@santafenm.gov, 505-955-6623)

**COMMITTEE REVIEW:**

Governing Body: (Introduction): 07/12/2023

Bicycling and Pedestrian Advisory Committee: 07/13/2023

Governing Body: (Public Comment) 07/26/2023

Quality of Life Committee: 08/02/2023

Public Works and Utilities Committee: 08/07/2023

Governing Body: 08/09/2023

**MOTION:** Member Eisenstein moved, seconded by Member Spencer that given the fact that electric unicycles are not human-powered devices, their accessibility at indoor facilities such as shopping malls, and the lack of classifications, we recommend further research before passing a bill on electric unicycles. And the rapidly changing availability of other micro-mobility devices adds complexity and warrants additional consideration.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Member Aaboe, Member Bordgegaray, Member Gabriele, Member Painter, Member Pilcher, Member Spencer, Chair Garcia

**Against:** None

**Abstain:** None

Chair Garcia will bring this up at the Quality of Life Committee and Public Works and Utilities Committee meetings (of which he is a member of) scheduled on August 2, 2023 and August 7, 2023 respectively.

e. Update on City's Bike Friendly Community (Silver-level) Renewal (Steven Pilcher, Task Force Chair)

Sean Moody, new Project Administrator in Complete Streets – Roadways and Trails section introduced himself. He is tasked to help the Task Force to complete City's application. His contacts were emailed to Task Force members.

f. BPAC Presentation to Governing Body (Judith Gabriele, Promotions, Education and Programming Subcommittee Chair)

Member Gabrielle discussed what she thinks needed to be included in her 5-10 minute presentation.

g. BPAC Strategic Plan FY2023-2033 Review/Update (Yolanda Eisenstein, Policy, Planning and Law Subcommittee Chair)

July 13, 2023 Updates/Amendments:

- 1) Cover Page – updated FY24 Members' Names
- 2) Page 16, Action 1, Task 1.1. – added "*Work ongoing with MPO*"
- 3) Page 17, Tasks 1.3 and 2.1 – added "Ongoing"
- 4) Page 18, Task 2.2 – added "Ongoing"
- 5) Page 18, Task 2.3 – added "*Submit for legislative review for August/September submission.*"
- 6) Page 19, Task 2.4 – added "*Complete Streets adopted by the city. Work ongoing.*"
- 7) Page 19, Task 2.5 – added "*The City doesn't anticipate a review of the Traffic Calming Program for at least two years. Also, Complete Streets will impact the program.*"
- 8) Page 20, Task 2.6 – added "Ongoing"
- 9) Page 22, Tasks 3.1 through 3.3 – added "Ongoing"
- 10) Page 23, Tasks 3.4 and 3.5 – added "Ongoing"

h. Arroyo De Los Chamisos Crossing (Ben Pingilley, Technical Review Subcommittee Chair)

Member Pingilley was absent so Member Painter reported on the changes on the plan sets.

i. Complete Streets FY 2024 Projects (Romella Glorioso-Moss, BPAC Staff Liaison)

Staff Liaison Glorioso-Moss reported that construction of Agua Fria-South Meadows Intersection Improvements will start in early August. Also, Guadalupe Bridge and Street Reconstruction and Canada Rincon Trail will start construction late August to early September. At the preconstruction meeting with NMDOT and FHWA, held on July 20 at NMDOT District 5 Office, both Contractors (GM Emulsion and H.O. Construction) were asked to resubmit/ amend their construction schedules. The City has yet to issue a PO (Purchase Order).

8. **MATTERS FROM STAFF**

9. **MATTERS FROM THE COMMITTEE**

10. **MATTERS FROM THE CHAIR**

Looking forward to the presentation of Member Gabrielle to the Governing Body (GB) on July 26, 2023 at 5 pm. Invited all BPAC members as well as Subcommittee Citizen Members to the GB meeting. He will acknowledge everyone. Appreciates all the work of the Committee.

11. **NEXT MEETING: Thursday, August 10, 2023**

12. **ADJOURN 7:50 pm**

*Lucrecia Diaz*

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Lucrecia Diaz, Clerk

*Romella Glorioso-Moss*

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Romella Glorioso-Moss, Staff Liaison

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Councilor Michael Garcia, Chair





# Minutes 07-13-2023

Final Audit Report

2023-08-04

Created:	2023-08-04
By:	Lucrecia Diaz (ldiaz@santafenm.gov)
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**BICYCLING AND PEDESTRIAN ADVISORY COMMITTEE  
SUBCOMMITTEE APPOINTMENTS  
FY 2024**

**A) Planning, Policy & Law Subcommittee**

1. BPAC Members:
  - a. Yolanda Eisenstein, Chair (appointed October 8, 2020)
  - b. Steven Pilcher (appointed October 13, 2022)
  - c. Khalil Spencer (appointed June 8, 2023)
  - d. VACANT
2. Members-at-Large
  - a. Lara Miller (appointed October 8, 2020)
  - b. Gary Schiffmiller (appointed October 8, 2020)
  - c. Bill Adrian (appointed October 13, 2022)

**B) Promotion, Education & Programming Subcommittee**

1. BPAC Members
  - a. Judith Gabriele, Chair (appointed August 12, 2021)
  - b. Tony Gerlicz (appointed October 14, 2021)
  - c. VACANT
  - d. VACANT
2. Members-at-Large
  - a. Rachel Wexler (appointed October 8, 2020)
  - b. Gabriel Gaarden (appointed March 10, 2022)
  - c. Jon Pyle (appointed March 10, 2022)

**C) Technical Review Subcommittee**

1. BPAC Members
  - a. Ben Pingilly, Chair (appointed September 9, 2021)
  - b. Erik Aaboe (appointed June 8, 2023)
  - c. Daniel Painter (appointed June 8, 2023)
  - d. VACANT
2. Members-at-Large
  - a. Nathan Lemons (appointed July 8, 2021)
  - b. Tim Rogers (appointed September 9, 2021)
  - c. VACANT

**NOTE: TO AVOID ROLLING QUORUM, NO BPAC MEMBER SHALL BE A MEMBER OF MORE THAN ONE SUBCOMMITTEE.**

2021 Sharrows Maintenance Priority List

LOCATION/ NO. of SHARROWS INSTALLED	PRIORITY for MAINTENANCE/ REPLACEMENT				COMMENTS/ QUESTIONS
	IMMEDI- ATELY	WITHIN A YEAR	SOME- TIME NEXT YEAR	NOT A PRIORITY	
Henry Lynch (10)				X	Sharrows OK; How many people cycle on the road?
Grant Avenue (10) <b>(Completed June 2022)</b>	X				From Marcy to Federal
Palace Avenue (29)				X	Sharrows OK
East and West Alameda (49)	X				Most, if not all need to be replaced
Old Santa Fe Trail (27)			X		Between Paseo de Peralta and Old Pecos Trail Are sharrows needed - How many riders? We don't know.
Camino De Monte Sol (9)					Closed – unable to access. Are sharrows needed? We don't know.
Camino Cabra (6) <b>(Completed June 2022)</b>	X				Up to Rio Grande School
Camino De Los Montoyas (15)	X				A few on CDLM and on the short segment of Buckman Rd. Important to connect to La Tierra Trails. Need signs stating bike may use full lane.
Artist Rd. (5)	X				
Lower Gonzales Rd. (3)				X	
Upper Gonzales (4)				X	
Second St. (4)	X				
Galisteo St. (8)	X				
Marcy St. (12)		X			
Baca (22)	X				
Don Gaspar (1)			X		
Pacheco St. (9)	X				How many riders? Connectivity/ proximity to rail trail, but no easy access between

LOCATION/ NO. of SHARROWS INSTALLED	PRIORITY for MAINTENANCE/ REPLACEMENT				
	IMMEDI- ATELY	WITHIN A YEAR	SOME- TIME NEXT YEAR	NOT A PRIORITY	COMMENTS/ QUESTIONS
					the two.
<b>Paseo De Peralta (32)</b>				<b>X</b>	How many riders? Doesn't feel safe to ride on so should there be sharrows?
<b>Guadalupe St. (13)</b>	<b>X</b>				
<b>North Paseo De Peralta (10)</b>				<b>X</b>	How many riders? Doesn't feel safe to ride on
<b>Osage (6)</b>	<b>X</b>				Existing Sharrows are OK; maybe could use more. This is a priority if more are added.
<b>Cordova Rd. (1)</b>				<b>X</b>	Not appropriate for sharrows
<b>Wagon Rd. (4)</b>				<b>X</b>	
<b>Jaguar Drive (13)</b>				<b>X</b>	Could use some signs supplementing the sharrows. Most sharrows OK; a couple partially patched over
<b>Governor Miles (1)</b>				<b>X</b>	Not appropriate for sharrows; there's plenty of room for bicycles
<b>Washington (2)</b>		<b>X</b>			From Federal to Marcy

underscored material = new  
[bracketed material] = delete

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**CITY OF SANTA FE, NEW MEXICO**

**BILL NO. 2023-24**

**INTRODUCED BY:**

Councilor Signe Lindell

Councilor Jamie Cassutt

Councilor Michael Garcia

**A BILL**

**AMENDING SFCC 1987, SECTION 23-5.1 TO INCLUDE A DEFINITION FOR UNICYCLES; AMENDING THE UNIFORM TRAFFIC ORDINANCE (UTO), EXHIBIT A TO CHAPTER 24 BY CREATING A NEW SECTION 12-1-86.5 TO ADD A DEFINITION FOR “UNICYCLE” AND “ELECTRIC UNICYCLE, AMENDING SECTION 12-1-40 TO EXCLUDE ELECTRIC UNICYCLES FROM THE DEFINITION OF MOTOR VEHICLES, AMENDING SECTION 12-1-88 TO INCLUDE ELECTRIC UNICYCLES IN THE DEFINITION OF VEHICLE, AND AMENDING ARTICLE 8, SECTIONS 12-8-1 THROUGH 12-8-16 AND SECTION 12-10-3.1 TO APPLY THE RULES OF OPERATION OF BICYCLES TO UNICYCLES TO THE EXTENT FEASIBLE AND TO PROHIBIT ANYONE UNDER THE AGE OF SIXTEEN FROM RIDING A CLASS TWO ELECTRIC UNICYCLE.**

**BE IT ORDAINED BY THE GOVERNING BODY OF THE CITY OF SANTA FE:**

**Section 1. Section 23-5.1 of SFCC 1987 (being Ord. No. 2023-4 , § 9) is hereby ordained to read as follows:**

1 **23-5.1 - Definitions.**

2 As used in this chapter:

3 A. **Artist/artisan** means a person who designs and creates arts or crafts.

4 B. **Artwork** means an object for sale by an artist/artisan.

5 C. **Bicycle** means:

6 (1) every device propelled by human power, upon which any person may ride,  
7 having two or more wheels, except scooters, skateboards, and similar devices; and

8 (2) every device propelled upon which a person may ride that is equipped with  
9 two or three wheels, pedals for human propulsion, and a motor of no more than seven  
10 hundred fifty (750) watts whose maximum speed is no more than twenty-eight (28) mph.

11 D. **Canyon Road periphery area** means an area bounded by Paseo de Peralta on the  
12 west, the southern right-of-way line of East Alameda Street on the north, East Palace  
13 Avenue and Camino del Monte Sol on the east, and Canyon Road and Acequia Madre  
14 Street on the south. This area includes both sides of the boundary streets, except along the  
15 southern right-of-way line of East Alameda Street. (Editor's Note: A copy of the Canyon  
16 Road periphery area can be found at the end of this chapter.)

17 E. **Collective** means a nonprofit organization that shares the use of the license among  
18 three (3) to seven (7) nonrelated artists/artisans.

19 F. **Collective license** means a Plaza Park artist/artisan license issued to a collective.

20 G. **Commercial use** means any function or event at which goods, artwork, foodstuffs,  
21 merchandise of any kind are offered for sale.

22 H. **Community Days Festival** means a city sponsored event promoting the programs  
23 and services offered by local nonprofit organizations, performances by multicultural,  
24 youth, and school groups and celebrating the diversity of Santa Fe.

25 I. **Findings** mean mechanical fittings in a great variety, either commercially

1 available or handmade, used to hold jewelry to the clothing or the person, i.e., ear wires,  
2 screw backs, broach pins, chains, gallery bezel wire or hollow beads.

3 J. **Handmade** means the design and creation of the artwork from raw materials and  
4 its finishing and decoration were accomplished by hand labor and by manually controlled  
5 methods that permit the maker to control and vary the construction shape, design and/or  
6 finish of each individual piece, but does not exclude the use of findings, hand tools and  
7 equipment nor does it exclude the hand assembly of premanufactured components in a  
8 creative manner.

9 K. **Immediate family** means the spouse, children and stepchildren of a Plaza Park  
10 artist/artisan. The city may require proof of relationship for family members.

11 L. **Individual license** means a Plaza Park artist/artisan license issued to an individual  
12 artist/artisan licensee pursuant to the provisions of subsection 23-5.3 SFCC 1987.

13 M. **Machine made** means the producing or reproducing of artwork in mass production  
14 by mechanical processes such as stamping, blanking, weaving, molds or offset printing and  
15 other printing methods that allow mass production except as allowed by these regulations.

16 N. **Major commercial event** means any commercial use for which the entire Plaza is  
17 used.

18 O. **Minor rework** means taking a machine made or mass-produced item and making  
19 only minor additions or changes to the item.

20 P. **Noncommercial use** means any function or event at which nothing is offered for  
21 sale and at which no money or other valuable consideration is exchanged for goods or  
22 services. This use includes, but is not limited to, patriotic observances, political rallies,  
23 televised news media events and motion picture filming activities.

24 Q. **Nonprofit organization** means an organization that is registered as a New Mexico  
25 nonprofit organization with the New Mexico Public Regulatory Commission.

1 R. **Original** means the single, one (1) of a kind, master that can be used as the model  
2 for a reproduction.

3 S. **Plaza** means that inclusive area bounded on the north by the north curblin of  
4 Palace Avenue running in front of the Palace of the Governors, and on all other sides by  
5 the facades of the buildings, excluding portals, or property lines lying to the east, south and  
6 west of the Plaza Park as outlined in the map prepared by the city, labeled "Plaza Park  
7 Map." (Editor's Note: The Plaza Park Map may be found at the end of this chapter.)

8 T. **Plaza Park** means that inclusive area bounded by the inside back of the curb  
9 surrounding the Plaza as outlined in the map prepared by the city, labeled "Plaza Park  
10 Map." (Editor's Note: The Plaza Park Map may be found at the end of this chapter.)

11 U. **Plaza Park artist/artisan** means an artist/artisan or an artist/artisan represented  
12 by a collective issued a Plaza Park artist/artisan license.

13 V. **Plaza Park artist/artisan license** means a license issued to an artist/artisan by the  
14 city for the purpose of commercial sales on the Plaza Park.

15 W. **Plaza periphery area** means an area outside the Plaza Park, not including the  
16 Plaza Park, bounded by Sandoval and Grant Streets on the west, Alameda Street on the  
17 south, Paseo de Peralta on the east and Marcy Street on the north. This area includes both  
18 sides of the boundary streets.

19 X. **Plaza pushcart vendor** means a vendor who uses a non-permanent, non-  
20 motorized cart, barrow or unit that can be pushed by hand for the purpose of selling food  
21 products and is authorized by the city to be located in the Plaza Park, the Plaza, excluding  
22 the Plaza Park, or at any location in the Plaza periphery area.

23 Y. **Portrait artist** means a person who creates portraits, cartoons, caricatures, or  
24 silhouettes.

25 Z. **Portrait artist license** means a Plaza Park artist/artisan license issued to a portrait

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- artist.
- AA. **Pushcart** means a non-permanent, non-motorized cart, barrow or unit that can be pushed by hand.
- BB. **Raw materials** mean any material that can be converted by manufacture or processing or a combination of manufacture and processing into a new and useful product.
- CC. **Recreational object** means any object that can be thrown, kicked or struck, including, but not limited to, balls, frisbees or any object used in game or sport.
- DD. **Reproduction** means the production of multiples of a single piece, the original or master, applicable to each medium.
- EE. **Resident of Santa Fe county** means a person whose primary residence is in Santa Fe county. Residency shall be established by any standard identification that proves residency such as a driver's license, passport or voter registration, utility bills or other documentation.
- FF. **Roller skates or in-line skates** means a shoe with a set of wheels attached for skating over a flat surface.
- GG. **Rotating license** means a Plaza Park artist/artisan license issued to seven (7) artists/artisans to be used on a rotating basis.
- HH. **Service animal** means any animal trained to assist mobility of impaired, blind, or deaf people or trained for and actively employed by a police department.
- II. **Skateboard** means a narrow board about two feet (2') long mounted on roller skate wheels.
- JJ. **Special event** means an event that is permitted by the city pursuant to subsection 18-8.9 SFCC 1987, Section 23-4 SFCC 1987, subsection 23-5.2 SFCC 1987 or resolution of the governing body.
- KK. "Unicycle" means:

1           (1) Every device propelled by human power, upon which any person may ride,  
2           having one wheel, pedals, and a saddle; and

3           (2) Every device upon which a person may ride that is equipped with one wheel  
4           and a motor of no more than seven hundred fifty (750) watts that has a maximum  
5           speed of no more than twenty-eight (28) miles per hour and which includes any of  
6           the following additional characteristics, or combination thereof: stationary foot  
7           plates, pedals, a saddle, handlebars.

8           **Section 2.     [NEW MATERIAL] a new Section 12-1-86.5 to Exhibit A of Chapter**  
9           **24 (Uniform Traffic Ordinance) of SFCC 1987 is hereby ordained to read as follows:**

10          **12-1-86.5     UNICYCLE.** “Unicycle” means

11           A. Every device propelled by human power, upon which any person may ride, having  
12           one wheel, pedals, and a saddle; and

13           B. Every device upon which a person may ride that is equipped with one wheel and a  
14           motor of no more than seven hundred fifty (750) watts and that has any of the  
15           following additional characteristics, or combination thereof: stationary foot plates,  
16           pedals, a saddle, handlebars. These devices are categorized into two classes:

17                   (1)     Class 1 electric unicycles are electric unicycles equipped with a motor  
18                   that may or may not provide pedal assistance and may reach the speed of up to  
19                   twenty miles per hour (20 mph).

20                   (2)     Class 2 electric unicycles are electric unicycles equipped with a motor  
21                   that may or may not provide pedal assistance and may reach the speed of up to  
22                   twenty-eight miles per hour (28 mph).

23           C. Devices with motors outputting more than 750 watts or that can achieve a  
24           maximum speed of more than 28 mph are motor vehicles.

1 D. Unicycles described in paragraph B are also called "electric unicycles." \*

2 **Section 3. Section 12-1-40 to Exhibit A of Chapter 24 (Uniform Traffic**  
3 **Ordinance) of SFCC 1987 (being Ord. No. 2023-4 , § 2) is hereby ordained to read as follows:**

4 **12-1-40 MOTOR VEHICLE.** "Motor Vehicle" means every vehicle ~~that [which]~~ is self-  
5 propelled and every vehicle, except electric bicycles and electric unicycles, that [which] is  
6 propelled by electric power obtained from batteries, [~~except electric bicycles~~], or from overhead  
7 trolley wires, but not operated upon rails; but for the purposes of the Mandatory Financial  
8 Responsibility Act [66-5-201 to 66-5-239 NMSA 1978], "motor vehicle" does not include  
9 "special mobile equipment." (66-1-4.11 NMSA 1978)

10 **Section 4. Section 12-1-88 to Exhibit A of Chapter 24 (Uniform Traffic**  
11 **Ordinance) of SFCC 1987 (being Ord. No. 2023-4 , § 3) is hereby ordained to read as follows:**

12 **12-1-88 VEHICLE.** "Vehicle" means every device in, upon, or by which, any person or  
13 property is or may be transported or drawn upon a street, including any frame, chassis, body, or  
14 unitized frame and body of any vehicle or motor vehicle, except devices moved by human power  
15 or used exclusively upon stationary rails or tracks. Electric bicycles and electric unicycles are  
16 vehicles. (66-1-4.19 NMSA 1978)

17 **Section 5. The title of Article VIII to Exhibit A of Chapter 24 (Uniform Traffic**  
18 **Ordinance) of SFCC 1987 (being Ord. No. 2023-4) is hereby ordained to read as follows:**

19 **ARTICLE VIII – OPERATION OF BICYCLES AND UNICYCLES**

20 **Section 6. Section 12-6-2.16 to Exhibit A of Chapter 24 (Uniform Traffic**  
21 **Ordinance) of SFCC 1987 is hereby ordained to read as follows:**

22 **12-6-2.16 RESTRICTIONS ON USE OF CONTROLLED-ACCESS STREETS.**

23 A. No pedestrian, bicycle, unicycle, or other non-motorized traffic shall use any controlled-  
24 access street, but notwithstanding this provision, drivers of vehicles using the controlled-

1 access street are not relieved of responsibility for exercising due care.

2 B. In addition to the provisions of paragraph A, the local governing body may, by ordinance,  
3 regulate or prohibit the use of any controlled-access street within its jurisdiction by any  
4 class or kind of traffic which is found to be incompatible with the normal and safe  
5 movement of traffic. (66-7-321 NMSA 1978)

6 C. No driver shall stop a vehicle upon any controlled-access street for the purpose of taking  
7 on or discharging passengers, freight or merchandise.

8 D. The administrator has the authority to erect and maintain official traffic-control devices on  
9 the controlled-access street on which the restrictions are applicable, and when such traffic-  
10 control devices are in place, no person shall disobey the restrictions stated on the devices.  
11 (66-7-321 NMSA 1978)

12 **Section 7. Section 12-8-1 to Exhibit A of Chapter 24 (Uniform Traffic**  
13 **Ordinance) of SFCC 1987 (being Ord. No. 2023-4, § 4) is hereby ordained to read as follows:**

14 **12-8-1 EFFECT OF REGULATIONS.**

15 A. It is a penalty assessment misdemeanor for a person to ~~be~~perform any act forbidden  
16 or fail to perform any act required by Sections 12-8-1 through 12-8-23 of the Uniform  
17 Traffic Ordinance.

18 B. No person shall authorize or permit any child to violate any of the provisions of this  
19 ordinance nor shall any person permit any child under the age of sixteen (16) to  
20 operate a Class 3 electric bicycle or a Class 2 electric unicycle. A person under the age  
21 of sixteen (16) may ride as a passenger on a Class 3 electric bicycle that is designed to  
22 accommodate passengers.

23 C. These regulations applicable to bicycles and unicycles shall apply whenever  
24 a bicycle or unicycle is operated upon any street, bicycle lane, or path subject to existing laws and  
25 those exceptions stated herein.

1 D. These provisions apply to the operation of bicycles, ~~and~~ electric bicycles, unicycles,  
2 and electric unicycles in the city of Santa Fe. (66-3-701 NMSA 1978)

3 **Section 8. Section 12-8-2 to Exhibit A of Chapter 24 (Uniform Traffic**  
4 **Ordinance) of SFCC 1987 (being Ord. No. 2011-23, § 8) is hereby ordained to read as follows:**

5 **12-8-2 TRAFFIC ORDINANCE APPLIES TO PERSONS RIDING.**

6 A. Operators of bicycles and unicycles have the same rights as operators of motor vehicles in  
7 the use of streets, highways and roadways within the city, except as otherwise specifically  
8 provided herein.

9 B. Every person riding a bicycle or unicycle upon a roadway, street or highway shall be  
10 subject to all the duties applicable to the drivers of motor vehicles, except as otherwise  
11 expressly provided in this Traffic Ordinance and except as to those provisions of laws and  
12 ordinances which by their nature can have no applications; and each such person shall be  
13 subject to the same provisions and sections of this Traffic Ordinance to which a motorist  
14 is subject within Sections 12-8-1 through 12-8-21. (66-3-702 NMSA 1978)

15 **Section 9. Section 12-8-4 to Exhibit A of Chapter 24 (Uniform Traffic**  
16 **Ordinance) of SFCC 1987 (being Ord. No. 2023-7, Exh. A) is hereby ordained to read as**  
17 **follows:**

18 **12-8-4 CLINGING TO VEHICLES**

19 No person riding upon any bicycle, unicycle, coaster, roller skates, sled or toy vehicle shall attach  
20 the same or themselves to any vehicle upon a street. (66-3-704 NMSA 1978)

21 **Section 10. Section 12-8-7 to Exhibit A of Chapter 24 (Uniform Traffic**  
22 **Ordinance) of SFCC 1987 (being Ord. No. 2011-23, § 11) is hereby ordained to read as**  
23 **follows:**

24 **12-8-7 LAMPS AND OTHER EQUIPMENT ON BICYCLES AND UNICYCLES.**

25 A. Every bicycle or unicycle when in use at nighttime shall be equipped with a lamp on the

1 front which shall emit a white light visible from a distance of at least five hundred feet to  
2 the front and with a red reflector on the rear which shall be visible from all distances from  
3 fifty feet to three hundred feet to the rear when directly in front of lawful upper beams of  
4 head lamps on a motor vehicle. A lamp emitting a red light visible from a distance of five  
5 hundred feet to the rear may be used in addition to the red reflector. A lamp or light emitting  
6 white light and attached to the helmet of the bicyclist or unicyclist may also be used in lieu  
7 of a light attached to the bicycle or unicycle.

8 B. Every bicycle or unicycle shall be equipped with a brake or other means which will enable  
9 the operator to bring the bicycle or unicycle promptly to a stop on dry, level, clean  
10 pavement. (66-3-707 NMSA 1978)

11 **Section 11. Section 12-8-8 to Exhibit A of Chapter 24 (Uniform Traffic Ordinance**  
12 **of SFCC 1987 (being Ord. No. 2011-23, § 12) is hereby ordained to read as follows:**

13 **12-8-8 OBEDIENCE TO TRAFFIC-CONTROL DEVICES.**

14 A. Any person operating a bicycle or unicycle shall obey the instructions of official traffic-  
15 control devices applicable to vehicles, unless otherwise directed by a police officer or  
16 unless a less stringent requirement specifically applicable to bicycles or unicycles applies.

17 B. Whenever authorized signs are erected indicating that no right or left or U-turn is permitted,  
18 no person operating a bicycle or unicycle shall disobey the direction of any such sign, except  
19 where such person dismounts from the bicycle or unicycle to make any such turn, in which  
20 event the person shall then obey the regulations applicable to pedestrians. [ ( ) \* [ ] ]

21 **Section 12. Section of 12-8-9 to Exhibit A of Chapter 24 (Uniform Traffic**  
22 **Ordinance) of SFCC 1987 (being Ord. No. 2011-23, § 13) is hereby ordained to read as**  
23 **follows:**

24 **12-8-9 PARKING OF BICYCLE OR UNICYCLE.**

25 A bicycle or unicycle may be parked in a manner that does not impede movement of pedestrians or

1 other traffic. [←]\* [→]

2 **Section 13. Section of 12-8-10 to Exhibit A of Chapter 24 (Uniform Traffic**  
3 **Ordinance) of SFCC 1987 (being Ord. No. 2023-4 , § 5) is hereby ordained to read as follows:**

4 **12-8-10 SPEED.**

5 A. No person shall operate a bicycle or unicycle at a speed greater than is reasonable  
6 and prudent under the conditions then existing. [←]\*[→]

7 B. Where riding on sidewalks is allowed, bicycles and unicycles shall be limited to  
8 ten (10) miles per hour.

9 **Section 14. Section of 12-8-11 to Exhibit A of Chapter 24 (Uniform Traffic**  
10 **Ordinance of SFCC 1987 (being Ord. No. 2011-23, §§ 14, 15) is hereby ordained to read as**  
11 **follows:**

12 **12-8-11 RIDING ON PROHIBITED STREETS OR CONTROLLED ACCESS.**

13 No person shall ride a bicycle or unicycle either on any street or path where signs have been  
14 erected by the city, which prohibit the use of the street or path to bicycles or unicycles.

15 **Section 15. Section of 12-8-12 to Exhibit A of Chapter 24 (Uniform Traffic**  
16 **Ordinance) of SFCC 1987 (being Ord. No. 2011-23, §§ 16, 17) is hereby ordained to read as**  
17 **follows:**

18 **12-8-12 OPERATION IN BICYCLE LANE.**

19 A. Where the lane designated by markings on the pavement for the exclusive use of  
20 bicyclists provides a minimum of four feet of rideable space, a bicyclist or unicyclist  
21 having entered such a lane shall endeavor to maintain the lane, except:

22 (1) At intersections; or

23 (2) To pass a slower bicyclist or unicyclist, or to avoid parked cars or obstacles.

24 B. A bicyclist or unicyclist may leave the bicycle lane between intersections in order to  
25 make a U-turn, or left-hand turn where such a turn is permissible for vehicular traffic, or

1 to turn into driveways.\*

2 **Section 16. Section of 12-8-13 to Exhibit A of Chapter 24 (Uniform Traffic**  
3 **Ordinance of SFCC 1987 (being Ord. No. 2011-23, §§ 18, 19) is hereby ordained to read as**  
4 **follows:**

5 **12-8-13 DIRECTION OF TRAVEL IN BICYCLE LANE.**

6 No person shall ride or operate a bicycle or unicycle within a bicycle lane or on the roadway in  
7 any direction except that permitted of vehicular traffic traveling on the same side of the roadway;  
8 provided, that bicycles and unicyles may proceed either way along a lane where two-way  
9 bicycle traffic is so designated. [ ←]\*[ →]

10 **Section 17. Section of 12-8-14 to Exhibit A of Chapter 24 (Uniform Traffic**  
11 **Ordinance of SFCC 1987 (being Ord. No. 2011-23, §§ 20, 21) is hereby ordained to read as**  
12 **follows:**

13 **12-8-14 POSITION ON THE ROADWAY.**

14 A. If a right vehicle lane available for traffic is wide enough to be safely shared with  
15 overtaking vehicles and a bike lane is not present, a bicycle or unicycle shall be ridden far  
16 enough to the right in said lane to facilitate such overtaking movements unless other  
17 conditions make it unsafe to do so.

18 B. Exceptions to driving bicycles and unicyles on the right vehicle lane:

19 (1) When reasonably necessary to avoid conditions including, but not limited to, fixed or  
20 moving objects, parked or moving vehicles, bicycles, unicyles, pedestrians, animals,  
21 or surface hazards.

22 (2) To overtake vehicles making right-hand turns at intersections.

23 (3) A bicyclist or unicyclist may move out of their lane to overtake and pass another  
24 vehicle, unicycle or bicycle.

25 (4) When preparing for a left turn at an intersection, when making a U turn or turning

1 into a private road or driveway.

2 (5) Upon a one-way road with two or more marked traffic lanes the bicyclist or  
3 unicyclist may ride in the center of the most left hand lane for a reasonable distance  
4 to complete a left hand turn.

5 (6) If the right vehicle lane available for traffic is not wide enough to be safely shared  
6 with overtaking vehicles or if passing is otherwise unsafe, a bicycle or unicycle may  
7 be ridden far enough to the left to temporarily control the lane. Bicyclists and  
8 unicyclists must move to the right to allow vehicles to overtake at the earliest and  
9 safest location. At no point is a bicyclist or unicyclist required to ride less than three  
10 feet from any objects or curb on the right.

11 (7) If a roadway is marked with a sharrow, a bicycle or unicycle can travel in the center  
12 of the lane and should move right when it is safe to do so.

13 (8) During organized bicycling and unicycling races. [-( ]\*[ ] )

14 **Section 18. Section 12-8-15 to Exhibit A of Chapter 24 (Uniform Traffic**  
15 **Ordinance of SFCC 1987 (being Ord. No. 2023-4, § 6) is hereby ordained to read as follows:**

16 **12-8-15 RIDING ON SIDEWALKS.**

- 17 A. No person shall ride a bicycle or unicycle on any sidewalk when signs are posted  
18 prohibiting the riding of bicycles or unicycles on the sidewalk.
- 19 B. If the bicyclist or unicyclist dismounts, the bicyclist or unicyclist is subject to the laws  
20 that apply to pedestrians.
- 21 C. Whenever any person is riding a bicycle or unicycle upon a sidewalk the person shall  
22 yield the right of way to any pedestrian.

23 **Section 19. Section 12-8-16 to Exhibit A of Chapter 24 (Uniform Traffic**  
24 **Ordinance) of SFCC 1987 (being Ord. No. 2023-7, Exh. A) is hereby ordained to read as**  
25 **follows:**

1 **12-8-16 TURNING AND HAND SIGNALS.**

2 A. While riding a bicycle or unicycle in traffic, the bicyclist or unicyclist shall make sure  
3 that their movement can be made safely and shall give a signal by hand in the same  
4 manner as hand signals are given by motorists to indicate the direction in which they  
5 intend to proceed, except that when signaling to make a right turn, a bicyclist or  
6 unicyclist may do so by extending the right arm.

7 B. A signal by hand and arm need not be given if the hand is needed in the control or  
8 operation of the bicycle or unicycle. \*

9 **Section 20. Section 12-10-3.1 to Exhibit A of Chapter 24 (Uniform Traffic**  
10 **Ordinance) of SFCC 1987 is hereby ordained to read as follows:**

11 **12-10-3.1 RESTRICTIONS UPON USE OF STREETS BY CERTAIN VEHICLES**

12 A. The administrator may determine and designate those heavily traveled streets upon which  
13 shall be prohibited the use of the street by motor driven cycles, bicycles, unicycles, horse-  
14 drawn vehicles or other non-motorized traffic and shall erect appropriate signs giving  
15 notice thereof.

16 B. When signs are erected giving notice of the restrictions, no person shall disobey the  
17 restrictions stated on the signs. [← ]\*[→]

18 **PASSED, APPROVED, and ADOPTED** this \_\_\_\_\_day of \_\_\_\_\_, 2023.

19 APPROVED AS TO FORM:

20   
21 [Erin McSherry \(Jul 7, 2023 15:01 MDT\)](#)

22 ERIN K. McSHERRY, CITY ATTORNEY

23  
24  
25 *Legislation/2023/Bills/Electric Unicycle Definition*

# BICYCLE AND PEDESTRIAN ADVISORY COMMITTEE (BPAC)

## STRATEGIC PLAN (FY2023-2033)

### MEMBERS

COUNCILOR MICHAEL GARCIA, MPA, CHAIR, DISTRICT 2

KHALIL SPENCER, PhD, VICE CHAIR, DISTRICT 1

YOLANDA EISENSTEIN, Esq., POLICY, PLANNING, & LAW SUBCOMMITTEE CHAIR, DISTRICT 1

JUDITH GABRIELE, MPH, PROMOTION, EDUCATION & PROGRAMMING SUBCOMMITTEE CHAIR, DISTRICT 2

TONY GERLICZ, DISTRICT 2

BEN PINGILLEY, TECHNICAL REVIEW SUBCOMMITTEE CHAIR, DISTRICT 3

ANGELA BORDEGARAY, DISTRICT 4

ERIK AABOE, COUNTY REPRESENTATIVE

### SUBCOMMITTEE CITIZEN MEMBERS

POLICY, PLANNING & LAW: BILL ADRIAN & LARA MILLER

PROMOTION, EDUCATION & PROGRAMMING: GABRIEL GAARDEN , JON PYLE & RACHEL WEXLER

TECHNICAL REVIEW: NATHAN LEMONS, GARY SCHIFFMILLER & TIM ROGERS

### STAFF LIAISON

ROMELLA GLORIOSO-MOSS, PhD, AICP

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## INTRODUCTION

The Bicycling and Pedestrian Advisory Committee (BPAC) was first established on September 24, 2003 as the Bicycle and Trails Advisory Committee (BTAC) via Resolution No. 2003-87. It was renamed on January 27, 2021 BPAC via Resolution No. 2021-8 (Appendix A). It's an advisory committee of the City of Santa Fe's Public Works Department, Complete Streets Division.

As articulated in Resolution 2021-8, *“The purpose of the Bicycling and Pedestrian Advisory Committee is to provide input and advice that supports the ongoing development and maintenance of a transportation infrastructure that makes bicycling and walking in the City of Santa Fe safe, equitable, viable, and comfortable modes of transportation, commuting, and recreation. This includes the responsibility to deliberate on City projects, plans, and policies that impact both on road and off-road bicycle and pedestrian infrastructure, and to advise the Governing Body on such matters.”*

BPAC consists of ten members: one of whom is a member of the City Council who serves as its Chair, along with nine members of the public, eight of whom are City residents and one who may be a County resident. They are appointed by the Mayor for two-year staggered terms for no longer than six consecutive years. They represent diverse interests among recreationists, youth and neighborhood groups, commuters, pedestrian, and bicyclists.

Currently, BPAC has three subcommittees appointed by the Chair whose memberships consist of four BPAC members and three members of the public. These three subcommittees are: (1) Policy, Planning and Law (“PPL”); (2) Promotion, Education and Programming (“PEP”); and, (3) Technical Review (“TR”). Each subcommittee is chaired by a BPAC member.

This Strategic Plan developed for Fiscal Years 2023 through 2033 was initiated in January 2022 by the Policy, Planning, and Law subcommittee. It's the first strategic plan developed by the Committee in recognition that a long-term view and a workable strategy and action plan are needed to achieve its vision, mission and objectives. Ten years was chosen as the time-frame for the plan since the majority of roadways and trails projects take an average of ten years from project development through construction. Moreover, the federal *Infrastructure Investment and Jobs Act* – a \$1.2 trillion funding mechanism for infrastructure, which the City is relying on for much of the design and construction of transportation alternative infrastructure coincides with the time-frame of this strategic plan. The methodology used for developing the plan follows the Multiple Scenario Strategic Planning Process (“MSSP”) described in Appendix B.

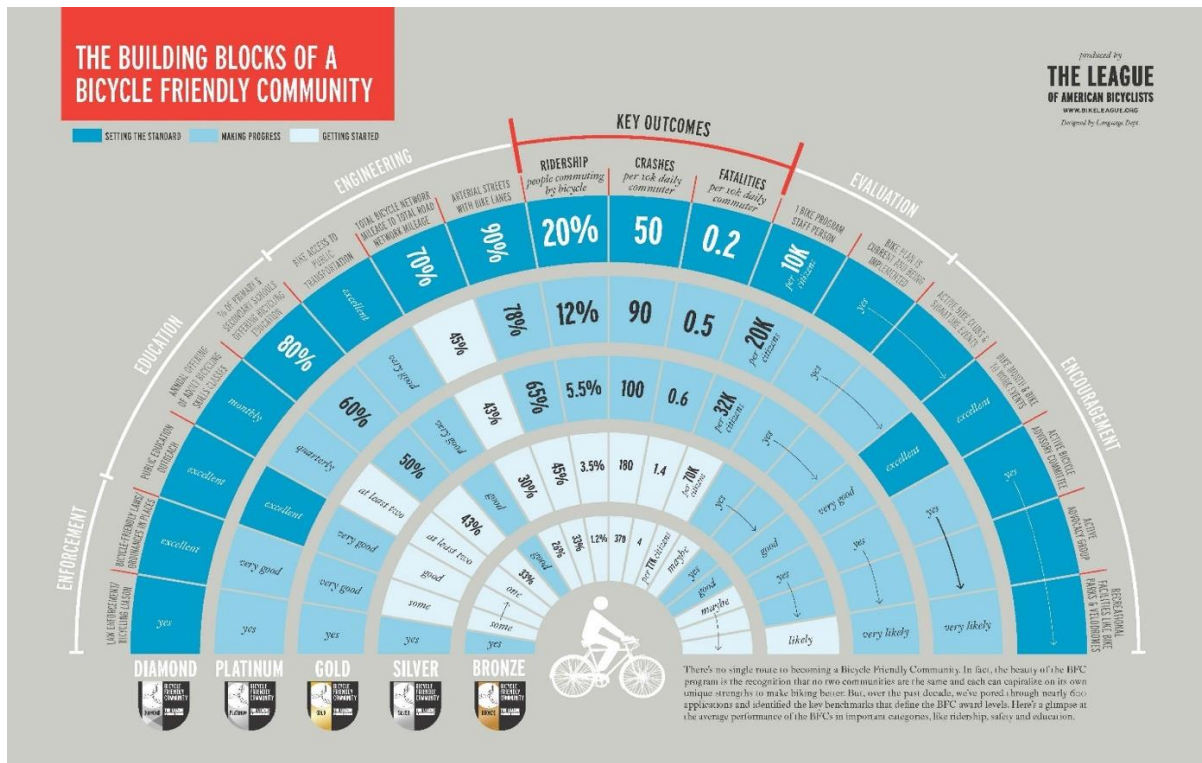
In more recent years, BPAC has partnered with a few organizations such as the Santa Fe Metropolitan Planning Organization, Santa Fe Conservation Trust, City of Santa Fe Public Safety Committee and the Santa Fe Safe Route to Schools Program, which they can rely on to implement parts of the Strategic Plan.

It is also important to note that the survey undertaken in October, 2021 conducted by the Promotion, Education and Programming subcommittee, has been a rich resource for formulating the strategy and action plan. Full survey results are attached as Appendix C.

**VISION – A COMMUNITY’S DESIRED IMAGE OF THE FUTURE**

By 2050, the City of Santa Fe has achieved the League of American Bicyclists Diamond-level Bike Friendly Community designation where 20% of Santa Feans are commuting by bicycle with 0.2 fatalities and 50 crashes per 10,000 daily commuters. This is achieved through following the Complete Streets design guidelines resulting in 90% of Santa Fe arterial streets having bicycle lanes. See Fig. 1, *The Building Blocks of A Bicycle Friendly Community*, The League of American Bicyclists.

**Figure 1: The Building Blocks of A Bicycle Friendly Community**



**MISSION STATEMENT**

Ensure that bicycling and walking in the City of Santa Fe are safe, equitable, viable, and comfortable modes of transportation, commuting, and recreation. Within 10 years, by the end of this planning period (FY2033), the City of Santa Fe has successfully obtained the League of American Bicyclists Gold-level Bicycle Friendly Community designation, increasing City’s bicycle ridership from 1% to 5.5%; and 65% of City’s arterial streets have bicycle lanes. See Fig. 1, *The Building Blocks of A Bicycle Friendly Community*, The League of American Bicyclists.

## DUTIES & RESPONSIBILITIES

Per Section 3 of Resolution 2021-8 the duties and responsibilities of Committee Members are:

- a) Assist in the prioritization of bicycle and pedestrian infrastructure projects to be completed using city, state, and federal funds, through the development of the Santa Fe Metropolitan Planning Organization (“SFMPO”) Bicycle Master Plan and the SFMPO Pedestrian Master plan;
- b) Review preliminary designs for new Public Works projects involving public roadways and trails funded out of city, state, or federal sources to ensure designs comply with the City's commitment to make bicycling and walking safe, equitable, viable, and comfortable modes of transportation. Design plan reviews shall take place at 30% design and before plans are presented to the public for comments or submitted for review to the New Mexico Department of Transportation (“NMDOT”);
- c) Advise on policies, programs, and ordinances as they relate to bicycle and pedestrian infrastructure safety, design, construction, and operation and maintenance;
- d) Develop, review, and advise on media and educational campaigns providing information and promoting bicycle- and pedestrian-related activities and education;
- e) Work with other agencies for the enhancement of city and county trail systems;
- f) Review and recommend updates to the Bicycle Master Plan, Pedestrian Master Plan, Metropolitan Transportation Plan, Parks Master Plan related to trails, and the City’s Land Use Development Code as they pertain to bicycle and pedestrian infrastructure and associated regulations;
- g) Seek funding from city, state, and federal sources to implement the City-led projects identified in the SFMPO’s 2019 Bicycle Master Plan and 2020 Metropolitan Transportation Plan and consider reoccurring funding sources from the City to be applied to the implementation of policies, programs, and other projects that are supported by the plans;
- h) Pursue the League of American Bicyclists Diamond-level Bicycle Friendly Community designation for the City of Santa Fe, as well as any other local, state, or national awards or designations that the City deems worthy of pursuit;
- i) Advise on policies, projects, ordinances, and funding as they relate to bicycling and walking as safe, equitable, viable, and comfortable modes of transportation; and
- j) Educate the public on the work of the Committee.

**EXTERNAL ANALYSIS: FORMULATING ALTERNATIVE FUTURES (2023-2033)**

As described in Appendix B, the next step in the MSSP is the External Analysis referring to the formulation of multiple scenarios that begins with the identification of key opportunities and threats that BPAC has no control over, but would likely impact BPAC’s mission achievement. These opportunities and threats, called *key decision factors*, are listed in Table 1 below, and are categorized as socio-cultural, economic, political, technological/infrastructure and environmental.

**Table 1: Key Decision Factors (Opportunities and Threats)**

Key Decision Factors (Opportunities and Threats)	Socio-Cultural (S) Economic (Eco) Political (P) Technological or Infrastructure (TI) Environmental (Env)	Opportunity (+) Threat (-)
1) Federal funding (Infrastructure and Investment Jobs Act)	Eco	+
2) Numerous organizations with bicycle and pedestrian focus (SFMPO, Santa Fe Conservation Trust, Earth Care, Bike Santa Fe, Chainbreaker Collective, Local Bike Shops, etc.)	S, Env	+
3) Supportive City Council	P	+
4) League of American Bicyclist Silver Level Bike Friendly Community Designation	S	+
5) High fuel prices, which means more income for the state but which makes driving less affordable to much of the public.	Eco	+/-
6) E-bikes (game changer especially for older adults and longer trips, but expensive. However prices will come down)	TI, Eco, Env	+
7) Aging population (decreasing stamina for bicycling; older drivers have higher accident rates (crashes and fatality) compared to younger drivers; no longer active in the workforce, therefore bicycle/walk for recreation; superior purchasing power)	S, Eco, Env	+/-

Key Decision Factors (Opportunities and Threats)	Socio-Cultural (S) Economic (Eco) Political (P) Technological or Infrastructure (TI) Environmental (Env)	Opportunity (+) Threat (-)
8) No grants funding for maintenance of trails, sidewalks, roads/streets, etc. Funded via Gross Receipts Tax (“GRT”) or operational funds therefore many trails, roadways, sidewalks, etc. are not in good condition.	Eco, P	-
9) Limited connectivity particularly in Districts 3 and 4.	TI	-
10) Unsafe roads to bicycle and walk	TI	-
11) Sprawl/Land use pattern – long distances to bicycle/walk to work, school, shopping or entertainment	S, TI	-
12) Insufficient resource for effective traffic enforcement	Eco	-
13) Strong car culture/ weak bicycle culture	S	-
14) Lack of educational/ environmental awareness	S, Eco, Env	-
15) Less bicycle and pedestrian infrastructure (especially bicycle trails) in Southwest of the City where growth is the fastest and poverty rate the highest.	S, Eco, TI	-
16) Competing priorities	Eco, P	-
17) Open space vs housing (rise of gated communities where wealthy people live in neighborhoods with excellent, well-maintained streets, sidewalks, trails, open space and parks)	S, Eco	-
18) Tourists’ needs and wants vs residents’ needs and wants	S, Eco, P	-/+
19) Lack of pro-bicycle and pedestrian policy implementation (i.e. Complete Streets, Smart Growth, Infill)	S, P	-
20) Lack of political will	S, P	-

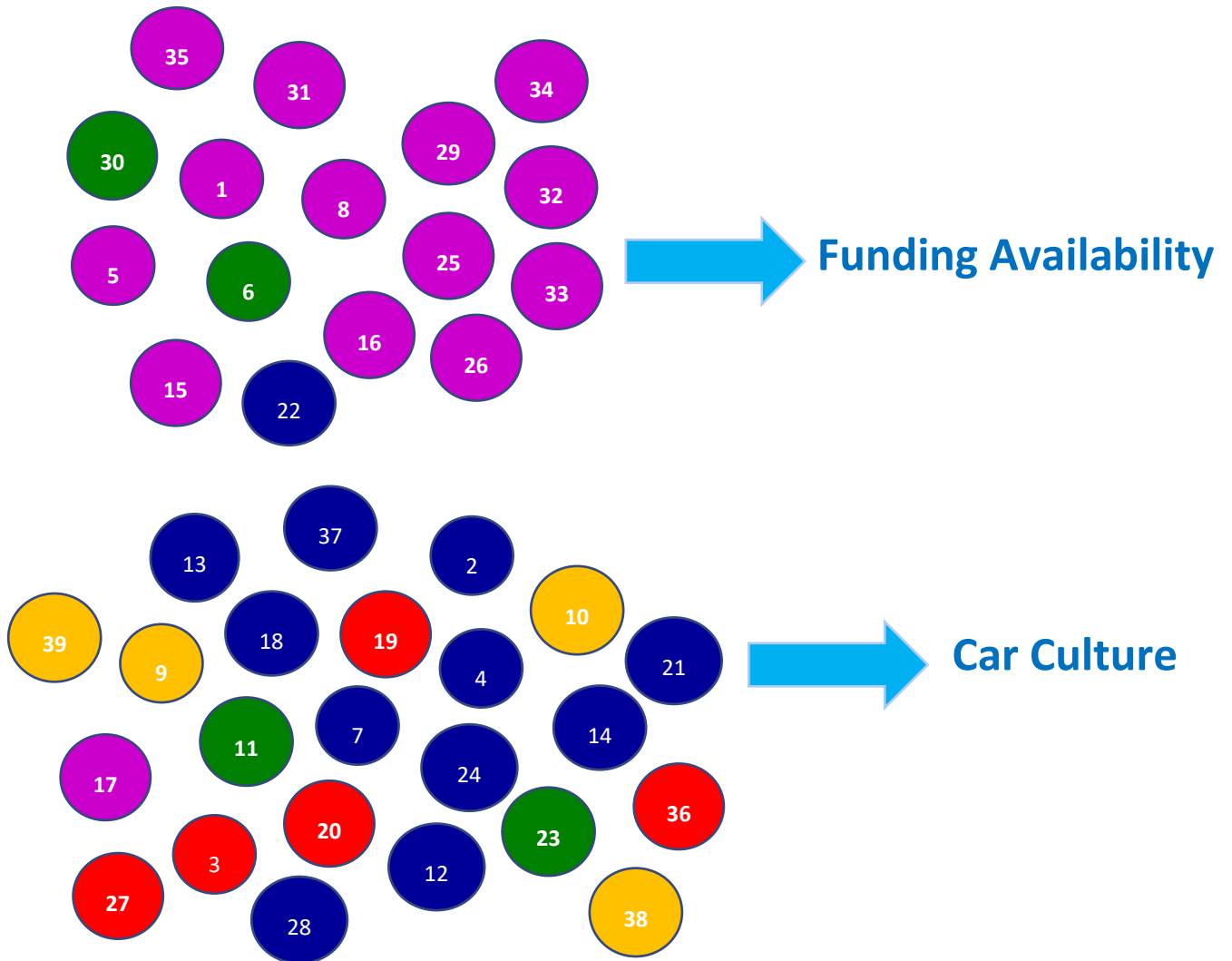
Key Decision Factors (Opportunities and Threats)	Socio-Cultural (S) Economic (Eco) Political (P) Technological or Infrastructure (TI) Environmental (Env)	Opportunity (+) Threat (-)
21) High crashes and fatality (State level particularly)	TI	-
22) Infrastructure that favors cars over bicycles and pedestrians.	S, TI	-
23) Sidewalks are too narrow and inappropriate for bicycles, but often the only safe route; majority are not ADA compliant; majority of sidewalks are not well maintained because maintenance is the responsibility of homeowners.	TI, S	-
24) Stigma towards people riding public transit; therefore unwilling to make first and last mile (FLM) of the trip via bicycle or walking.	S	-
25) More funding for roadways compared to bicycle trails.	Eco	-
26) High real estate value downtown where work is more available.	Eco	-
27) Lack of recognition of the work of BPAC	S, P	-
28) Differing goals between design engineers who have an interest in moving motor vehicles quickly and transportation planners who encourage multiple modes of travel sharing facilities; engineers currently have more influence in project outcomes.	S, TI	-
29) High cost of living	Eco	-/+
30) Undependable public transit system – running too hot/ too cold; not enough drivers; buses not in good condition; routes too long and not direct to places of work, lack of a grid system that makes buses efficient.	S	-
31) High inflation rate	Eco	-/+
32) Looming economic recession (may be global due to Russian-Ukraine war)	Eco	-/+

Key Decision Factors (Opportunities and Threats)	Socio-Cultural (S) Economic (Eco) Political (P) Technological or Infrastructure (TI) Environmental (Env)	Opportunity (+) Threat (-)
33) Perceived high cost of construction and maintenance of bicycle and pedestrian infrastructure per user compared to cost of roadways per motor vehicles user resulting in lower Cost-Benefit Ratio for walking/bicycling investments.	Eco, P	-
34) Local funding such as GRT, and Roads Impact Fees collected to fund roads and trails infrastructure all-time high	Eco	+
35) Reducing climate change impacts and increasing resiliency and adaptation (high cost with huge impact to economy; how committed is City government to Carbon Neutrality by 2040; is 2040 too late?).	Env	-/+
36) Will state or Federal government pass law that will force a shift to emission-free vehicles (e-vehicles, bicycles, mass transit), or will resource depletion do the same, thus controlling events?	P, Env	-+/
37) BPAC Survey shows that Santa Feans bicycle and walk for recreation (exercise and entertainment); not to work, school or shopping.	S, TI	-
38) BPAC survey indicates that 72% of Santa Feans surveyed don't bicycle or walk to work, school, etc. because bicycle and pedestrian facilities are "inconvenient to access, not in their neighborhood, hard to get to, and/or they don't go where they want to go." While 23% of survey respondents stated "they don't feel safe."	TI	-

Key Decision Factors (Opportunities and Threats)	Socio-Cultural (S) Economic (Eco) Political (P) Technological or Infrastructure (TI) Environmental (Env)	Opportunity (+) Threat (-)
39) BPAC survey respondents identified the following infrastructure is needed in Santa Fe to increase bicycling and walking: <ul style="list-style-type: none"> <li>a. Physically separated/protected bike lane (90%)</li> <li>b. Easily navigable bikeways to the southside (74%)</li> <li>c. Streets closed to motorized traffic/cycling with pedestrian access only (61%)</li> <li>d. Universal user activated blinking stop signals at crosswalks (58%)</li> <li>e. Bike boxes (designated space for bikes at intersections to direct them through the intersection) (53%)</li> </ul>	TI	-

The next step in MSSP is to coalesce or cluster the 39 key decision factors identified in Table 1 above into two or three groups which are called *societal driving forces*. As shown in Figure 1, the key decision factors coalesced into two (2) societal driving forces, namely: 1) availability of funding from federal, state and City to build safe, accessible, and convenient bicycle and pedestrian infrastructure; and 2) prevailing car culture defined as a way of life characterized by a prioritization of cars over other modes of transportation, and excessive use of or reliance on motor vehicles in American society. This is in part, a result of land use and transportation decisions that often lock society into an auto-dependent paradigm, and in part, due to society’s higher valuing of convenience, speed and social status over equity and environmental considerations.

Figure 2: Clustering of Key Decision Factors into Societal Driving Forces

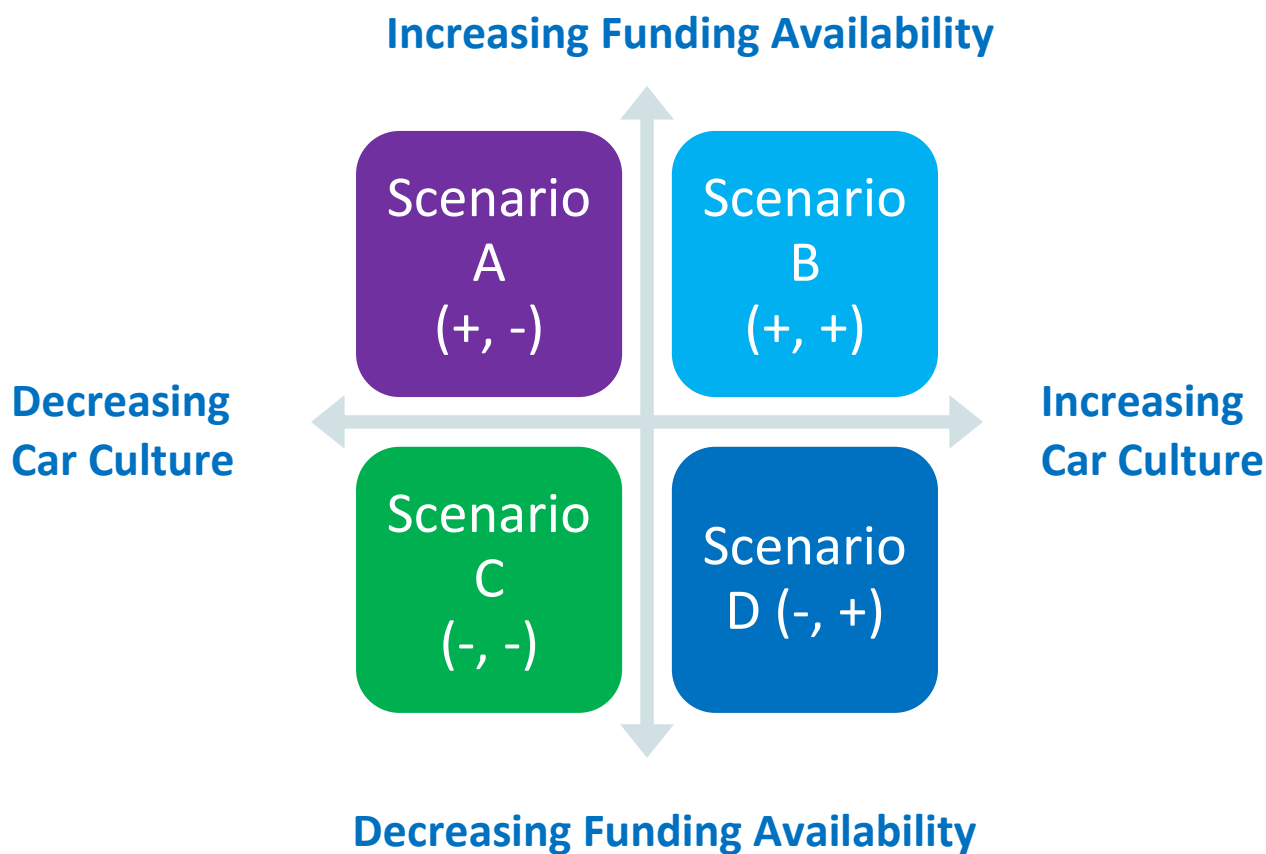


LEGEND:

- 2 Socio-cultural
- 8 Economic
- 20 Political
- 10 Technological/ Infrastructure
- 23 Environmental

The next step in MSSP’s External Analysis is the formulation of alternative future *scenarios*. This step is done by first making the societal driving forces neutral, or non-directional, then positioning them in axial relationship, or what is called *scenario formats*. Their number will depend on the number of societal driving forces. For example, two societal driving forces generate four scenario formats as shown in Figure 2 below.

**Figure 3: Scenario Formats**



The combination of the two societal driving forces forms the scenario logics that will define the characteristics of each scenario. For example, in Figure 3, Scenario B has increasing (+) *Funding Availability* and increasing (+) *Car Culture*. With the logics of Scenario B (+,+), one expects that the percentage of the total federal, state and city funding allocated for multi-modal transportation especially for bicycle and pedestrian infrastructure is much higher in Scenario B compared to Scenario C and D with decreasing (-) *Funding Availability* but much less than Scenario A where Society’s valuing of *Car Culture* is decreasing (-). See Figure 3 for key characteristics of each scenario.

Figure 4: Scenarios Key Characteristics (2023-2033)

Scenario Key Indicators*	Scenario A “Our Vision” (+, -)	Scenario B “Our Planning Period Mission” (+, +)	Scenario C (-, -)	Scenario D “Most Likely Future to Unfold” (-, +)
% of the total federal, state and city funding allocated for multi-modal transportation especially bicycle and pedestrian infrastructure	15%	5%	2%	0.5%
% of City population bicycling or walking to work, school, shop or recreation	20%	5.5%	3.5%	1%
Economic Recession	Moderate	No	Long & Deep	Short
Global Climate change impacts	Very Low	Moderate	Low	Unabated
Connectivity	Excellent	Moderate	Poor	Very Poor
Total bicycle & pedestrian network mileage to total road network mileage	70%	30%	15%	5%
Arterial streets with bicycle lanes and ADA compliant sidewalks	90%	45%	15%	5%
Bike access to public transportation	Excellent	Average	Above Average	Poor

\* For bicycling key indicators, we used the League of American Bicyclists’ Bicycle Friendly Community criteria for the 5 designations (Bronze through Diamond).

After we describe the characteristics of each scenario, we then choose the *most likely scenario* to unfold in the future (2023-2033) to guide the strategy and action plan. For this planning period, BPAC thinks Scenario D will unfold – the future BPAC needs to contend or to plan for.

**INTERNAL ANALYSIS: BPAC'S STRENGTHS & WEAKNESSES**

This analysis involves the identification and evaluation of BPAC's Strengths and Weaknesses for achieving its mission. They are usually identified in terms of human, financial, physical, informational, and temporal resources. In contrast with the Opportunities and Threats identified in the External Analysis section, BPAC has control over its own Strengths and Weaknesses enumerated below. However, they are not listed in order of importance.

**STRENGTHS**

- Committed group to achieve the mission and purpose
- Members have relevant experience and knowledge
- Diverse skill set
- Mandate is documented through a resolution
- Members are active
- Cohesive / unified view of mission
- Staggered terms
- Unlike other Advisory Committees, BPAC is chaired by a City Councilor
- BPAC has a seat on Public Safety Committee, which is also chaired by a City Councilor
- Has long-time, well-established partnerships with SFMPO and SFCT
- City is Silver-level Bicycle Friendly Community through BPAC
- Member – League of American Bicyclists

**WEAKNESSES**

- Little direct power to mandate meaningful changes; is dependent on others in government (Public Works, SFMPO)
- Lack of diversity - gender, racial, some districts not represented, etc.
- Lack of volunteers
- Silos of committees – except for Public Safety Committee, BPAC does not interact or coordinate their activities with other City Committees or Commissions such as Public Transit or Planning Commission
- Long meetings - dissuading participation or discouraging volunteers
- Limited Staff support – only support from Staff Liaison. No Stenographer; no full-time bike and ped coordinator with Civil Engineering or similar degree to provide clout
- No strategic plan
- Duties & responsibilities in the resolution exceed what members have the power to do due to lack of resources: funding and staff
- Lack of clarity in members' mind – is BPAC a City Advisory Committee or Roadway and Trails Advisory Committee? Is there a difference? Is BPAC advising the City or Public Works Department?
- Where does the responsibility for City's bicycle and pedestrian planning lie? Is it with BPAC, SFMPO, or Public Works Department's Roadway and Trails? What is the process for prioritizing projects?

## SWOT ANALYSIS – KEY ISSUES IDENTIFIED

This step brings together the separately identified strengths (*S*) and weaknesses (*W*) of BPAC to achieve the mission in the context of Scenario D – the most likely future to unfold in FY2023-2033– opportunities (*O*) and threats (*T*) for mission achievement. This will result in identification of *key issues* that need to be addressed for achieving the mission.

Evaluating the Opportunities and Threats of the Most Likely Future (see Scenario D, Figure 4 above) against BPAC’s Strengths and Weaknesses (see Table 1 above), the following key issues must be addressed in the Strategy and Action Plan.

- Shift the prevailing car culture towards a more multi-modal transportation culture. Included in this shift is finding a way to a) reduce stigma towards people who ride public transit; and 2) create a community where walking and bicycling live safely and comfortably with faster, longer-distance modes.
- Integrate Land Use and Transportation Planning to create bicycle and pedestrian-friendly communities that will result in connected neighborhoods and places with accessible, convenient, and safe bicycle and pedestrian facilities.
- Develop a meaningful public involvement process in road and street design that includes respect for all user input into the project. The enabling Resolution authorizes review and input when plans are at 30% and final design stages. Involvement at the conceptual stage for both the public and BPAC will result in designs that better accommodate all users.
- Need for paradigm shifts including:
  - a. from roadways to streets. Roads are thoroughfares designed to speed travel between two points, streets have homes or shops on both sides to facilitate public interaction;
  - b. moving cars to moving people so all modes have equal value;
  - c. limiting choice to multiple choice;
  - d. from traffic signals to roundabouts;
  - e. street landscaping important component of Complete Streets;
  - f. counting all trips; not only vehicles miles traveled (VMT); and
  - g. from Gray Infrastructure to Green Infrastructure.

## THE STRATEGY FOR CHARTING A DIFFERENT FUTURE

Shift City policies, programs and practices including those of Land Use and Public Works, toward bicycle and pedestrian friendly infrastructure to help people feel safe and more comfortable about traveling by bicycle or walking with other traffic. Substantial increases in bicycling and walking require an integrated package of many different, complementary interventions, including infrastructure provisions and pro-bicycle and pedestrian programs, supportive land use planning, and restrictions on car use. Both the Strategy and the Action Plan below need top-down buy-in from all levels of city government.

**ACTION PLAN (FY2023-2033)**

The Action Plan developed to implement the strategy is a three-pronged approach that addresses distance; providing appropriate bicycle and pedestrian infrastructure; and the integration of public transport, education and marketing programs. Although the Strategy developed is for 10 years, the Action Plan below is focused on the first five years of strategy implementation (2023-2027).

The implementation of this Action Plan will be led by one of the three BPAC Subcommittees:

**Policy, Planning & Law (PPL)** – Ensures that the City of Santa Fe’s plans, policies, and laws support BPAC’s purpose and the City’s goal to make bicycling and walking safe, equitable, viable, and comfortable.

**Promotion, Education & Programming (PEP)** – Develops, reviews, and advises on communications campaigns that inform and educate Santa Fe residents and visitors on the bicycle and walking rules and regulations, BPAC, relevant events, safety, and other communications that support BPAC’s purpose.

**Technical Review (TR)** – Ensures that Santa Fe’s infrastructure projects include critical components that contribute to the safety, accessibility, connectivity and equity of bicyclists and pedestrians.

**ACTION 1:** *Formulate policies, programs and practices that would make travel distances to work, school, or shopping easily to moderately bikeable/walkable.*

**Task 1.1:** Work with Land Use Department in Code Rewrite. Advocate for bicycle and pedestrian-friendly land use codes. Examples are:

- a. cluster commercial and residential development in higher density centers, rather than extended in linear strips along roads;
- b. restrict development of neighborhood commercial areas to a pedestrian scale and design;
- c. encourage grid design in new residential developments or require interconnection of cul-de-sac neighborhoods with “cut-through” non-motorized access public rights-of-way to allow safe, access along streets within and between neighborhoods;
- d. coordinate land use decisions with existing and planned public transportation services and the needs for non-motorized access; and
- e. using the permitting process, require bicycle parking at all commercial establishments.

**Rationale:** Through appropriate land use codes, the design and location of residential development, retail, commercial and institutional buildings can provide an environment that is safe and convenient for pedestrian and bicycle travel, and give people shorter travel distances between origins and destinations.

**Complete By:** December 2026

**Lead Subcommittee:** PPL (Coordinate work with Land Use Department Senior Planner Daniel Alvarado, AICP, [djalvarado@santafenm.gov](mailto:djalvarado@santafenm.gov), (505) 955-6670, and Jeanne Wolfenbarger.)

**July 13, 2023: Work ongoing with MPO.**

**Task 1.3:** Assess bicycle and pedestrian connectivity defined as the ability to reach important destinations, such as workplaces, schools, grocery stores, and banks, via pathways or roads with low vehicle volumes and low speed limits. Include in analysis first and last mile connectivity defined as the beginning or end of an individual trip made primarily by public transportation.

Based on connectivity analysis, develop a bicycling and pedestrian network that will provide bicycle and pedestrian facilities that allow immediate, low-stress access to the street network that do not require cyclists and pedestrians to use links that exceed their tolerance for traffic stress, and that do not involve an undue level of detour.

Prioritize projects for funding and implementation that improve connectivity within and between neighborhoods.

**Rationale:** Connectivity is perhaps the most critical aspect of a bicycling network and should feature prominently in network planning. A bicycle and pedestrian network that has high connectivity should not be incoherent – it should not have areas disconnected from others, or the connecting route should not be so circuitous that most people would consider them effectively unconnected – thereby discouraging people to bicycle or walk.

**Complete By:** December 2026

**Lead Subcommittee:** TR with PEP (Work with SFMPO Officer, Erick Aune, AICP, [ajaune@santafenm.gov](mailto:ajaune@santafenm.gov), (505) 330-8483.)

**July 13, 2023: Ongoing**

**ACTION 2:** *Assist Public Works and Land Use Departments in Ensuring that the Design, Maintenance & Construction of all City streets including private ones, roadways, and trails have appropriate bicycle and pedestrian facilities, meaning they are safe, equitable, viable and comfortable.*

**Task 2.1:** Develop a user-friendly checklist for reviewing roadway and trails design and construction plans based on Complete Streets Design Principles and Vision Zero.

**Rationale:** Both Complete Streets and Vision Zero Policies assert that good roadway and street design can positively influence the behaviors of all users and should be the first step toward goals to change behavior and enhance safety.

If a user-friendly checklist has been developed, then reviewing plans won't be as daunting a task for BPAC members.

**Complete By:** December 2023

**Lead Subcommittee:** TR

**July 13, 2023: Ongoing**

**Task 2.2:** Develop relationships with relevant City staff to encourage more diverse input into the conceptual designs of new or upgraded developments and transportation facilities. Urge City leadership to develop procedures that include and encourage neighbors and interested parties, including BPAC, in meaningful participation during the early stages of projects.

**Rationale:** Early public participation will result in outcomes that better reflect the priorities and interests of the community. Most neighborhood opposition to projects include concerns that traffic will increase and make their neighborhoods less safe. By involving neighborhood and bike/pedestrian advocates early on, and throughout the process of project design, the resulting project will likely be welcomed, rather than opposed, by the Community.

**Complete By:** Continuous

**Lead Subcommittee:** PEP

**Public Works:** Complete Streets: Regina Wheeler, Department Director  
([rawheeler@santafenm.gov](mailto:rawheeler@santafenm.gov), (505) 955-6622)

**Land Use:** Jason Kluck, Department Director ([jmkluck@santafenm.gov](mailto:jmkluck@santafenm.gov), (505) 955-6729)

**SFMPO:** Erick Aune, SFMPO Officer ([ejaune@santafenm.gov](mailto:ejaune@santafenm.gov), (505) 330-8483)

**July 13, 2023: Ongoing**

**Task 2.3:** Facilitate the approval and implementation of the *Inspection and Maintenance Plan for On-road Bicycle and Pedestrian Infrastructure* Resolution.

**Rationale:** Annual and routine maintenance of on-road bicycle and pedestrian infrastructure such as street sweeping, pavement markings, traffic signals, and wayfinding signage are critical components to safety of vulnerable road users: children, older adults, people who walk and bike, people with disability and homeless people. Yet, the City of Santa Fe does not have a Maintenance Plan/Program. Therefore maintenance is usually underfunded.

**Complete By:** June 2023

**Lead Subcommittee:** PPL (If needed, work with City's Legal Services Staff Liaison, Jesse Guillen, [jbguillen@santafenm.gov](mailto:jbguillen@santafenm.gov), 505-955-6518.)

**July 13, 2023: Submit for legislative review for August/September submission.**

**Task 2.4** Facilitate the adoption and codification of Complete Streets.

**Rationale:** In the US, in the decade between 2009 and 2018, traffic deaths for motor vehicle occupants declined by 1%, but pedestrian deaths increased by 53%, and deaths among cyclists increased by 36%. The statistics for the City of Santa Fe follows the national data.

Complete Streets aims at reducing and eventually eliminating all serious injuries and deaths related to traffic crashes and the transportation system. Complete Streets proponents argue that our roads are currently built to optimize motor vehicles' speed and convenience over safety. Complete Streets asserts that traffic deaths are preventable when streets and roads are re/designed to anticipate user error and protect all users, especially the most vulnerable.

**Complete By:** December 2026

**Lead Subcommittee:** PPL (Work with: a) SFMPO Officer, Erick Aune, AICP, [ejayne@santafenm.gov](mailto:ejayne@santafenm.gov), (505) 330-8483; and b) Land Use Department Senior Planner Daniel Alvarado, AICP, [djalvarado@santafenm.gov](mailto:djalvarado@santafenm.gov), (505) 955-6670.

**July 13, 2023: Complete Streets adopted by the city. Work ongoing.**

**Task 2.5:** Update the City's Traffic Calming Program

**Rationale:** The last time the City's Traffic Calming Program was updated was in 2013. Since then, technologies, design standards, policies, and social behaviors and conditions have significantly changed but the program has not changed and implementation by the citizen is difficult. No programs or plans should be static.

**Complete By:** TBD

**Lead Subcommittee:** TR & PPL (Work with: a) City Traffic Engineer Jeanne Wolfenbarger, PE, [jawolfenbarger@santafenm.gov](mailto:jawolfenbarger@santafenm.gov); b) Traffic Calming Consultant Leroy Pacheco, PE, [engineer@leroypacheco.com](mailto:engineer@leroypacheco.com), (505) 423-4068; and c) SFMPO Officer, Erick Aune, AICP, [ejayne@santafenm.gov](mailto:ejayne@santafenm.gov), (505) 330-8483.)

**July 13, 2023: The City doesn't anticipate a review of the Traffic Calming Program for at least two years. Also, Complete Streets will impact the program.**

**Task 2.6:** Assist the SFMPO in prioritizing bicycle and pedestrian on-road and off-road projects that support safety, equity, access, connectivity, and convenience. See Table 2 for List of Projects prioritized by BPAC since June 2020.

**Rationale:** Although SFMPO has an excellent Metropolitan Transportation Plan, and has developed a list of project priorities for the City of Santa Fe, BPAC being the City's Advisory Committee with members from each 4 Council Districts and County resident has better connection with City constituents, and therefore can represent better Constituents' wants and needs.

**Complete By:** Continuous

**Lead Subcommittee:** TR

**July 13, 2023: Ongoing**

Table 2: BPAC's Approved Priority Projects (2020 – present)

**A) Roadway and Sidewalk Improvements:**

- 1) Bishop's Lodge Road Reconstruction – Project Termini: Paseo de Peralta to City/County Boundary (2.8 mi). Engineering design funded via Transportation Project Fund (\$800,000) and Road Impact Fees (\$327,000) for a total of \$1,127,000. Design phase started January 1, 2022.
- 2) Buckman Road Pavement Rehabilitation – Project Termini: Cattle guard to Camino de las Crucitas (1.32 mi). Engineering design funded via Local Government Road Fund (\$73,000). Cooperative Agreement not yet executed.
- 3) Add curb cut/bulb out at the end of Acequia Trail and Montezuma Avenue.

**B) Trail Improvements**

- 1) Acequia Trail - Otowi to La Cieneguita via Maclovía and Hermanos Rodríguez Parks with connection to Cielo Vista Park. ~\$1,650,000
- 2) Tierra Contenta Arroyo Chamiso Trail – design: Along South Meadows and to Camino Entrada, via school crossing and city-owned corridor to Camino Estrada (design) ~\$425,000
- 3) NM Central Rail Trail - Pinon Elementary School to Pueblos del Sol Trails - Safe Routes to School. ~\$650,000
- 4) MID-TOWN CAMPUS TRAIL: Using east boundary easement, from Siringo to driveway near St. M's Dr., w/connections to LaFarge Library, Middle School, St. Michael's Village W. shopping center. ~ \$1,400,000
- 5) Arroyo Chamisos Trail Extension Linking East and West Zia and connect to Botolph Road or Gail Ryba Trail ~ \$1,500,000
- 6) Mutt-Nelson Rd Trail to connect to Tierra Contenta Trail & SWAN Park
- 7) Nina Otero SRTS Trail, Tierra Contenta (Added 2/9/23)

**C. Signage/Striping Improvements**

- 1) Wayfinding Project (Phase 1-3) (Phase 1 for implementation FY2023)
- 2) Sharrows Maintenance (Submitted to Traffic Operations Manager; for implementation FY2023)
- 3) Striping on Calle Mejia (Submitted to Traffic Operations Manager and City Traffic Engineer for evaluation)
- 4) Tierra Contenta Wayfinding (To be constructed with Tierra Contenta Trail Extension from Buffalo Grass to South Meadows in FY2023)
- 5) Striping on Paseo de Peralta on Canyon Rd (To be submitted to City Traffic Engineer and Traffic Operations Manager for evaluation)

**ACTION 3:** *Integrate public transport, education, and marketing programs.*

**Task 3.1:** Obtain media coverage in news outlets; propose to the *Santa Fe New Mexican* and other news outlets to include a regular column on biking and walking in Santa Fe; and increase visibility through social media outlets.

**Rationale:** Can reach wider audience, educate on the work of BPAC.

**Complete By:** Ongoing

**Lead Subcommittee:** PEP

**July 13, 2023: Ongoing**

**Task 3.2:** Lobby to have input, and/or a seat at the table of other key City Committees/Commissions, particularly on Mayor’s Committee on Disability, Parks and Trails, Planning Commission, Public Transit and Recreation.

**Rationale:** BPAC’s achievement of its mission, in part, depends on the activities/ work of other City Committees/Commissions. However, currently, BPAC has only a seat on Public Safety Committee.

**Complete By:** June 2027

**Lead Subcommittee:** PEP

**July 13, 2023: Ongoing**

**Task 3.3:** Develop partnerships with other government entities, NGOs, and private entities to overcome lack of financial and human resources. See Table 3 for list of entities to partner with.

**Rationale:** The City cannot afford to support all its Committees with Staff and funding.

**Complete By:** June 2027

**Lead Subcommittee:** PEP

**July 13, 2023: Ongoing**

**Task 3.4:** Develop promotional or marketing program with Santa Fe Trails, Rail Runner, and North Central Regional Transit District focusing on First and Last Mile (FLM).

**Rationale:** The neighborhoods in the City are quite fragmented with low connectivity and equity. Many are dependent on cars. Although this condition can be corrected via road reconstructions, land use policy implementation such as Smart Growth, the most likely scenario indicates otherwise. There are neither resources nor desire to change this condition. Further, a huge percentage of work force live outside Santa Fe such as Española, Albuquerque, Bernalillo, and Rio Rancho. Therefore, BPAC needs to focus on encouraging more people to use Public transportation in tandem with bicycling/walking the first and last mile.

**Complete By:** June 2027

**Lead Subcommittee:** PEP (Need to work with Santa Fe Trails Division Director Thomas Martinez, [tamartinez@santafenm.gov](mailto:tamartinez@santafenm.gov), (505) 955-2006; and Michaela Beggins, Communications Manager, (505) 469-0946.)

**July 13, 2023: Ongoing**

**Task 3.5:** Develop key information needed for providing safe, equitable, viable and comfortable bicycle and pedestrian infrastructure via surveys or other means.

**Rationale:** To implement this Action Plan effectively, BPAC will need information from residents and visitors alike. Examples are information on how do people think about connectivity or equity; what do they think about the quality of our streets, roadways and trails; what do they want to see; what services do they need; etc.

**Complete By:** Continuous

**Lead Subcommittee:** PEP (Work with SFMPO Officer, Erick Aune, AICP, [ejaune@santafenm.gov](mailto:ejaune@santafenm.gov), (505) 330-8483.)

**July 13, 2023: Ongoing**

**Table 3: List of Possible Partner Organizations**

**Santa Fe Organizations/Entities**

- Santa Fe Public Schools
- Santa Fe Conservation Trust
- Chainbreaker
- Railyard Park Conservancy
- Santa Fe Community College
- Christus Community Health
- Santa Fe Community Foundation
- Girls, Inc.
- Earthcare
- Homewise
- NM Techworks
- Institute of American Indian Arts

**Retail**

- BTI (Bicycle Technologies International)
- Sirius Cycle
- Broken Spoke
- Second Street Brewery
- REI

**Biking Groups**

- Bike Santa Fe
- Santa Fe Fat Tire Society
- Seniors on Bikes (SOBs)

**Hiking Groups**

- 

**Events**

- Santa Fe Century

**Other**

- Food Depot
- AARP
- The Green Chamber of Commerce

## CONCLUSION

This Plan was developed by the BPAC Policy, Planning, and Law Subcommittee with input from and the approval of the entire BPAC Committee, facilitated and drafted by Staff Liaison Romella Glorioso-Moss. It is intended to be a dynamic document that changes with a changing Santa Fe and its citizens.

BPAC Subcommittees will work to implement the Plan and report to the full committee as appropriate. BPAC will officially review the Plan twice a year, revising as tasks are accomplished, goals are reached, and new ones added. As BPAC members term out and new ones join, the transitions will be transparent, as the Plan will be a major driver of the work of the committee.

# APPENDIXES

A.City of Santa Fe Resolution No. 2021-8

## B. Multiple Scenario Strategic Planning Process (MSSP)



# **BICYCLING AND PEDESTRIAN ADVISORY COMMITTEE (BPAC)**

**JUDITH GABRIELE, SUBCOMMITTEE CHAIR  
PROMOTION, EDUCATION & PROGRAMMING  
(PEP) SUBCOMMITTEE**

# PURPOSE OF THE BPAC

- **Provide input and advice**
- **Deliberate on city projects, plans, and policies**
- **Advise the Governing Body**

# BPAC Strategic Plan 2023-2033

**Vision: By 2050, the City of Santa Fe has achieved the League of American Bicyclists Diamond-level Bike Friendly Community designation.**

- **20% of Santa Feans commute by bicycle**
- **0.2 fatalities and 50 crashes per 10,000 daily commuters**
- **90% of Santa Fe arterial streets have bicycle lanes**

# BPAC Strategic Plan 2023-2033

**Mission: Bicycling and walking in the City of Santa Fe are safe, equitable, viable, and comfortable modes of transportation, commuting, and recreation.**

- **By FY2033, Santa Fe has successfully obtained the League of American Bicyclists Gold-level Bicycle Friendly Community designation**
- **Bicycle ridership up from 1% to 5.5%**
- **65% of City's arterial streets have bicycle lanes**

# BPAC ACCOMPLISHMENTS

## STRATEGIC PLAN ACTION 1: FORMULATE POLICIES, PROGRAMS, AND PRACTICES

Strategic Plan Task	BPAC Accomplishment
<b>E-Bikes</b>	Amended the City's Unified Traffic Ordinance (UTO) to include e-bike regulations. City approved February 2023.

# **BPAC ACCOMPLISHMENTS**

## **STRATEGIC PLAN ACTION 2: ASSIST PUBLIC WORKS AND LAND USE DEPARTMENTS**

<b>Strategic Plan Task</b>	<b>BPAC Accomplishment</b>
<b>Input into Design</b>	<b>Provided technical review of 12 Complete Streets projects</b>
<b>Maintenance Plan</b>	<b>Developed resolution regarding maintenance and installation of bicycle/pedestrian facilities</b>

# BPAC ACCOMPLISHMENTS

## STRATEGIC PLAN ACTION 3: INTEGRATE PUBLIC TRANSPORT, EDUCATION, AND MARKETING PROGRAMS

Strategic Plan Task	BPAC Accomplishment
Media	Conducted radio interview during Bike Month
Partnerships	Conducted outreach at Santa Fe Century; Participated in District 4 survey and outreach meetings and Bike Santa Fe's re-launch
Data	Partnered with MPO to conduct survey to assess who is using bike trails and for what; used this data for strategic planning

## COMING DOWN THE ROAD...

- Renewing Santa Fe's Silver-level Bike Friendly Community Designation
- Developing outreach and education campaigns about Santa Fe and NM E-bike regulations
- Researching the impacts and policy implications of other motorized vehicles
- Passing the Maintenance Resolution and identifying resources to support the ongoing maintenance of bicycling and pedestrian facilities.

# QUESTIONS/COMMENTS



## Contact Information:

Planning, Policy and Law Subcommittee: Yolanda Eisenstein at [yeisenstein@icloud.com](mailto:yeisenstein@icloud.com)

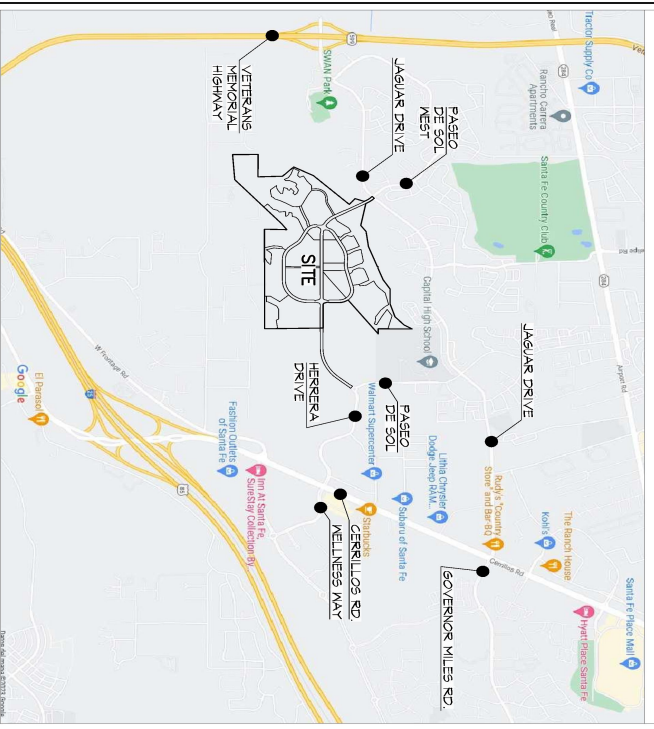
Promotion, Education and Programming Subcommittee: Judith Gabriele at [judegabe@gmail.com](mailto:judegabe@gmail.com)

Technical Review Subcommittee: Ben Pingilley at [ben.pingilley@gmail.com](mailto:ben.pingilley@gmail.com)

# TERRA CONTENTA PHASE 3 A

## 30% SUBMITTAL

SANTA FE, NM 87507  
SECTION 13 & 14, T 16 N, R 8 E, N.M.P.M.



VICINITY MAP  
NOT TO SCALE

**SHEET LIST**

- 1 COVER SHEET - VICINITY MAP
- 2 SITE PLAN
- 3 PRELIMINARY SUBDIVISION PLAT - NOT INCLUDED
- 4A-4D CERTIFIED TOPOGRAPHY - DRAFT
- 5 BOUNDARY SURVEY
- 6A SLOPE ANALYSIS
- 6B EXISTING CONDITIONS & SOIL MAP - NOT INCLUDED
- 7 ROAD SECTIONS
- 8A-8C ROAD PLANS
- 9A-9K ROAD PLANS & PROFILES
- 10A-10B TRAFFIC CIRCLE DETAILS
- 11 ADA PLANS AND DETAILS - NOT INCLUDED
- 12A-12E SIGNAGE & STRIPING PLANS - NOT INCLUDED
- 13 TERRAIN MANAGEMENT PLANS - NOT INCLUDED
- 14 GRADING DETAILS - NOT INCLUDED
- 15 MASTER UTILITY PLAN
- 16A-16D WATER PLANS
- 17 FIRE PROTECTION PLAN
- 18A-18F SAG PLANS & PROFILES
- 19 DRY UTILITY PLAN - NOT INCLUDED
- 20 SEDIMENT CONTROL PLAN - NOT INCLUDED
- 21A-21B GENERAL CONSTRUCTION REQUIREMENT
- 22 SUMMARY OF QUANTITIES
- A-F STANDARD DETAILS FOR ROADS, DRAINAGE AND WASTEWATER IMPROVEMENTS - NOT INCLUDED
- L LANDSCAPE PLANS
- IR IRRIGATION PLANS - NOT INCLUDED
- E STREET LIGHT PLANS - NOT INCLUDED

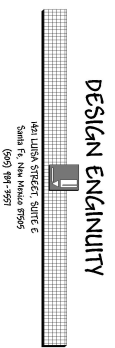
**OWNERS:**



**PLANNER:**



**CIVIL ENGINEERING:**



**SURVEYING:**

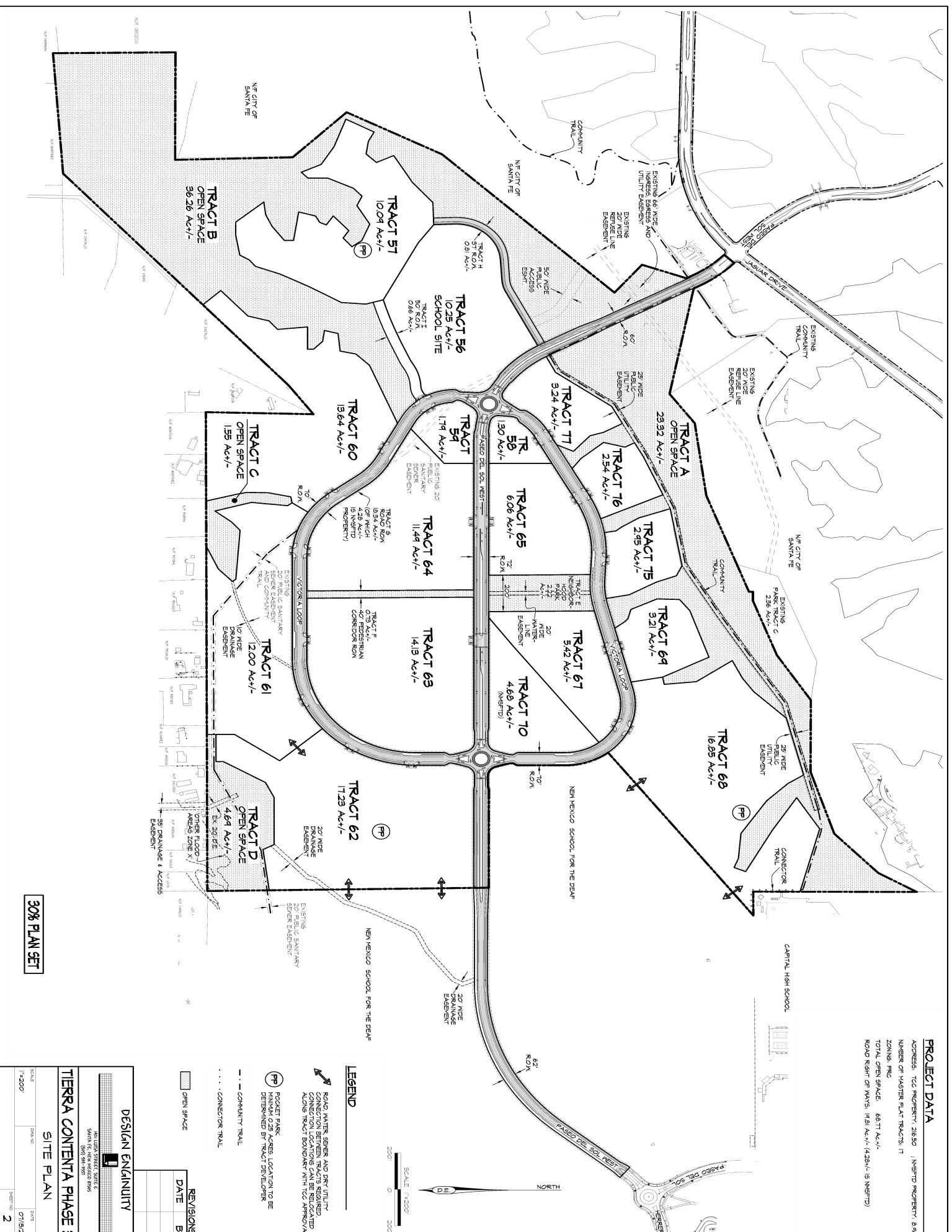
**SANTA FE SURVEYING COMPANY**  
 P.O. BOX 2395, SANTA FE, NM 87504, 120 LUISA ST. SUITE 8  
 PHONE: (505)-992-3211, MOBILE: (505)-640-2659, FAX: (505)-992-2905

**JULY 2023**

REVISIONS			
DATE	BY	DATE	BY

ARJCK14-PROJECTS\TERRA\_CONTENTA\DWG\2023-07-06 TCS.dwg, COVER, 7/6/2023 8:53:11 PM

**PROJECT DATA**  
 ADDRESS: TCC PROPERTY 20,830 ; INHERIT PROPERTY 9,66  
 NUMBER OF MASTER PLAN TRACTS: 17  
 ZONING: PFC  
 TOTAL OPEN SPACE: 68,71 AC +/-  
 ROAD RIGHT OF WAYS: 19,89 AC +/- (4,284' - 19 INHERIT)



30% PLAN SET

**DESIGN ENGINTEER**

**TERRA CONTENTA PHASE 3A**

**SITE PLAN**

SCALE: 1" = 200'

DATE: 07/19/2023

BY: [Signature]

REVISIONS:

NO.	DATE	BY
1		

**LEGEND**

- ROAD WATER SEWER AND PER/UTILITY CONNECTION LOCATIONS CAN BE RELOCATED ALONG TRACT BOUNDARY WITH TCC APPROVAL
- PROJECT PARK (PP) MINIMUM 0.25 ACRES LOCATION TO BE DETERMINED BY TRACT DEVELOPER
- COMMUNITY TRAIL
- CONNECTOR TRAIL
- OPEN SPACE

SCALE: 1"=200'

200' 0' 200'

NORTH

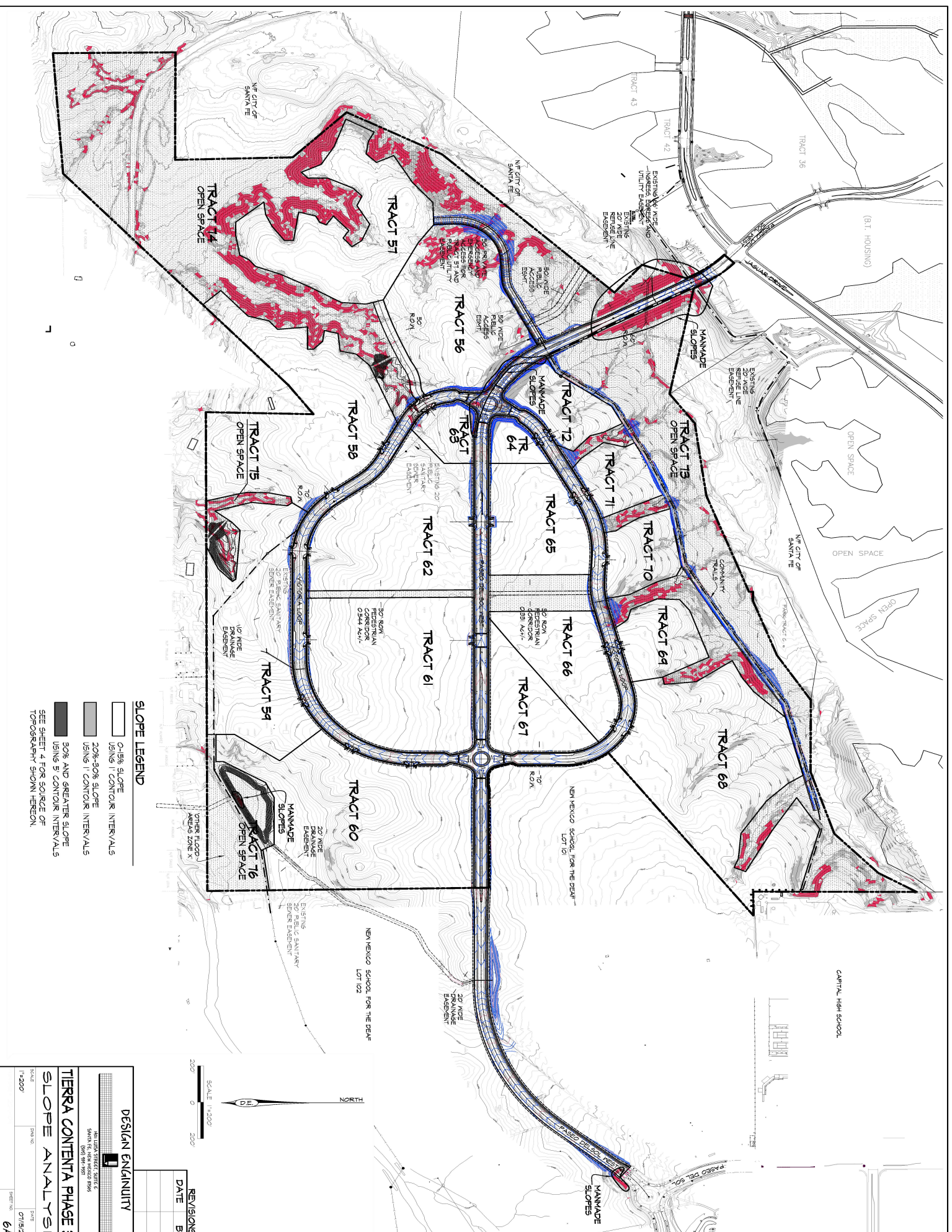






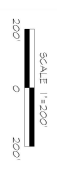






**SLOPE LEGEND**

- 0-15% SLOPE USING 1' CONTOUR INTERVALS
  - 20%-50% SLOPE USING 1' CONTOUR INTERVALS
  - 50% AND GREATER SLOPE USING 5' CONTOUR INTERVALS
- SEE SHEET 4 FOR SOURCE OF TOPOGRAPHY SHOWN HEREON



**DESIGN ENGINTEER**

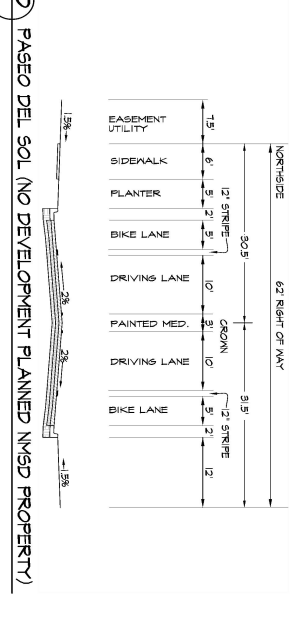
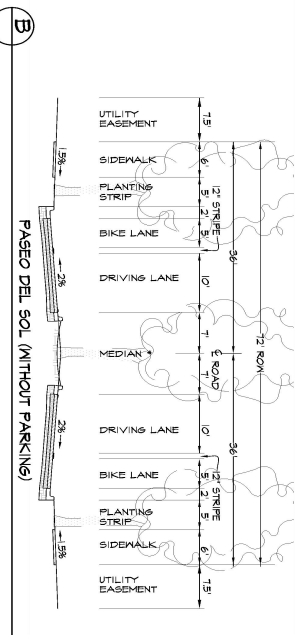
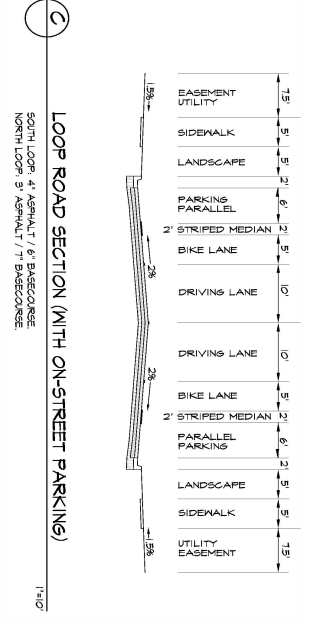
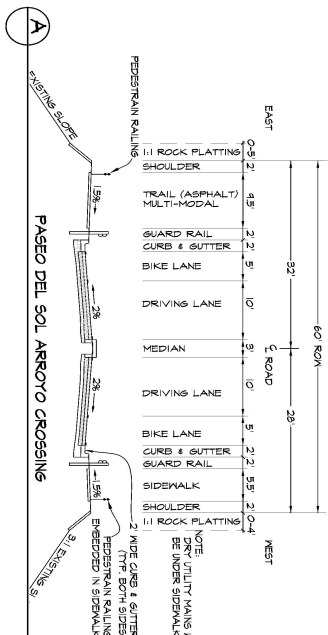
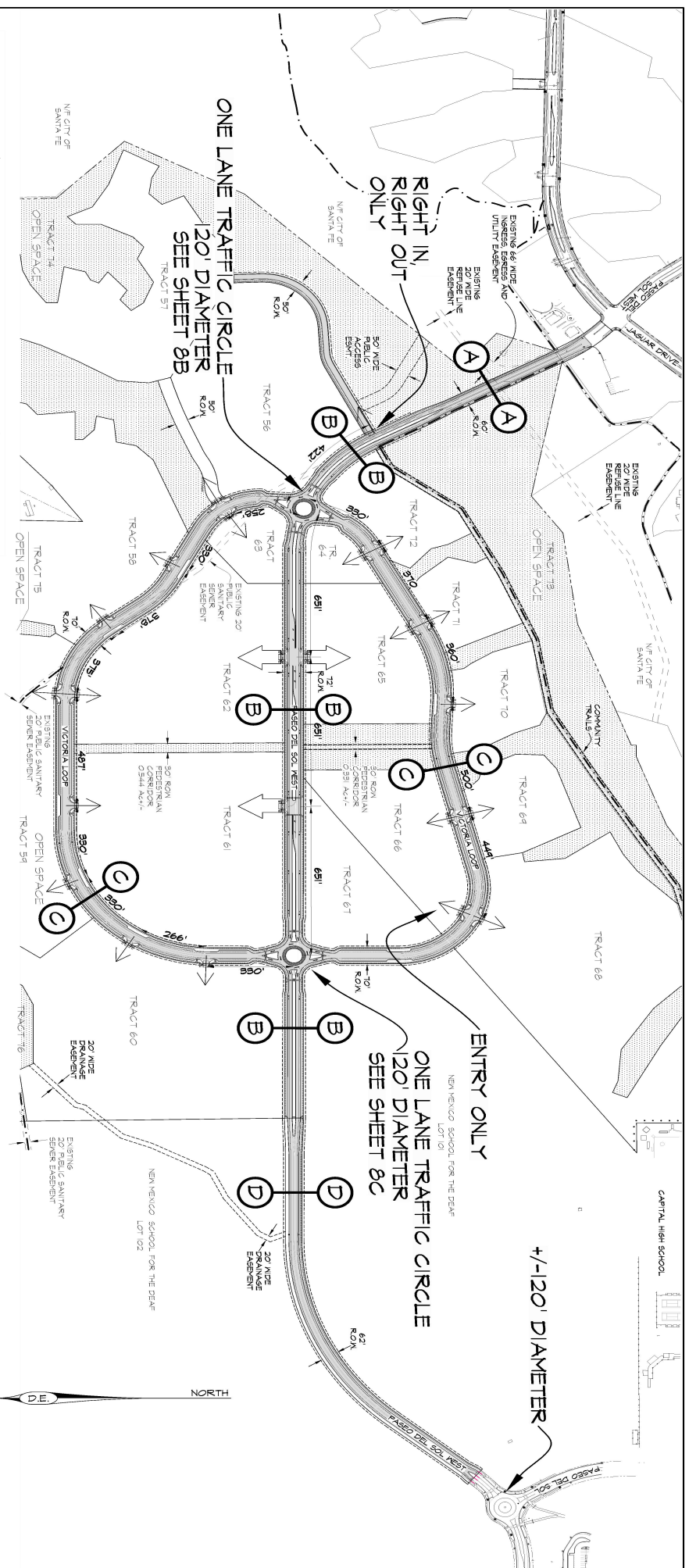
441 TULSA STREET, SUITE 6  
 SANTA FE, NEW MEXICO 87505  
 (505) 988-9997

**TERRERA CONTENTIA PHASE 3A  
 SLOPE ANALYSIS**

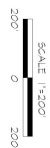
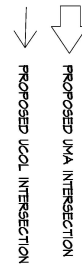
SCALE	DATE
1"=1200'	07/19/2023
BY	6A

REVISIONS	DATE	BY





**SANM: RECOMMENDED INTERSECTION SPACING, UNSIGNALIZED**  
 UVA: 90-50 MPH - 660 FEET WITH MEDIAN  
 UCL: 90 MPH - 930 FEET



**DESIGN ENGINEER**

441 TWIN STREET, SUITE 6  
 SAN MATEO, CALIFORNIA 94401  
 650.981.9977

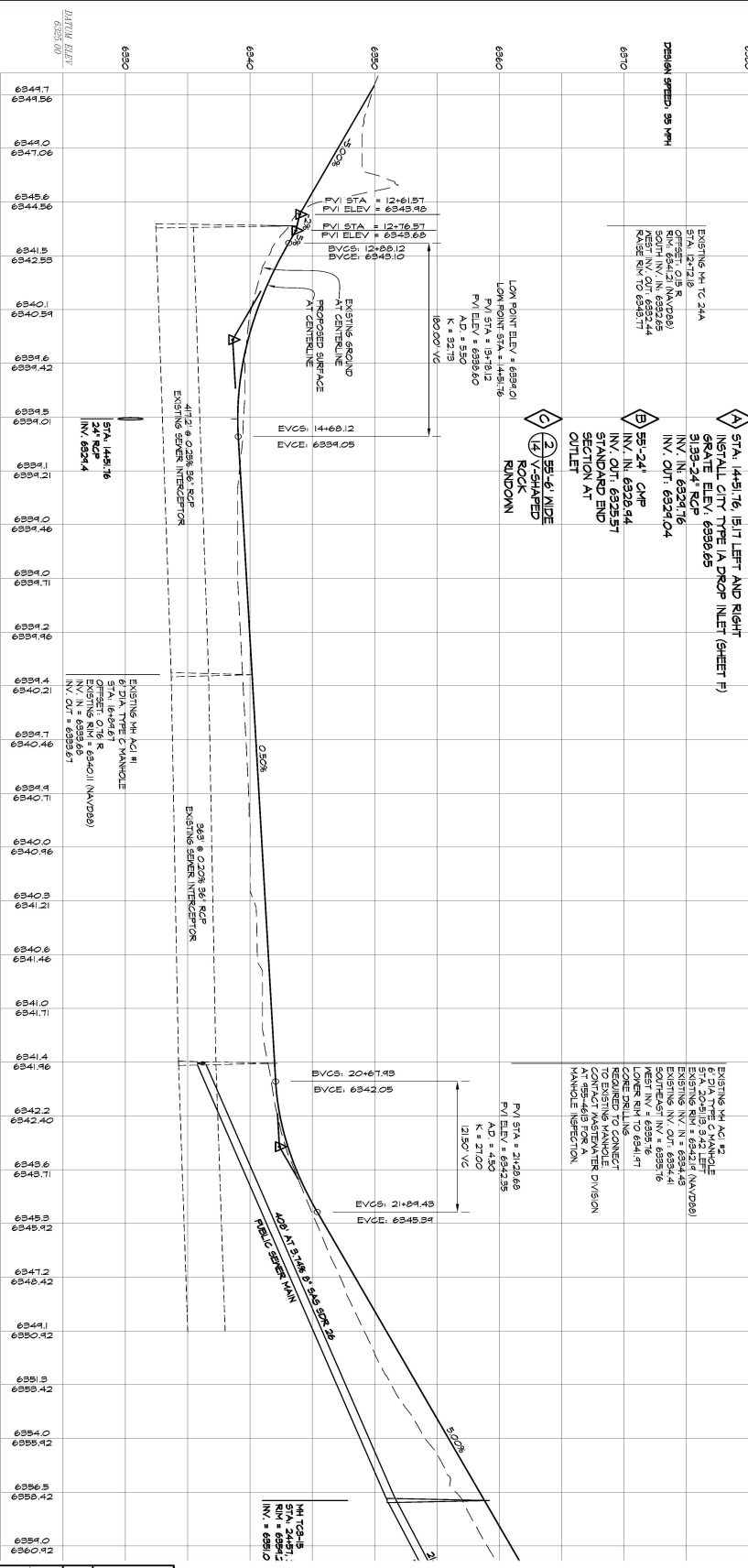
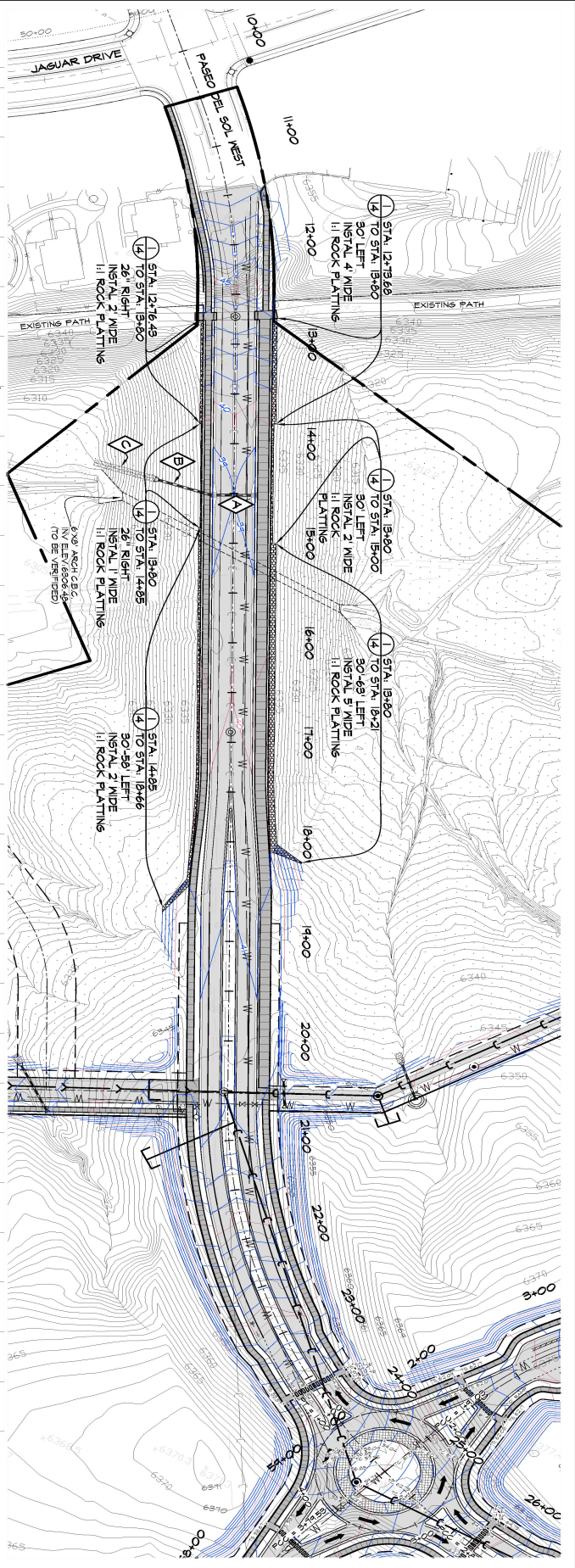
**TERRERA**

ROAD PLAN  
 DATE: 07/19/2023  
 BY: SA

REVISIONS	
DATE	BY







EXISTING H/L TO ACU #2  
 STA. 20+51.5 TO 21+00  
 EXISTING R/W = 6342.4 (NWD288)  
 EXISTING INV. OUT. 6344.4  
 SOUTHEAST INV. = 6335.6  
 WEST INV. = 6335.6  
 REQUIRED TO CONNECT TO EXISTING MANHOLE DIVISION AT 20+46.8 FOR A MANHOLE INSPECTION

LOW POINT STA. = 6339.01  
 PVI STA. = 13+12.12  
 PVI AD. = 55.0  
 K = 32.75  
 160.00' VC

PVI STA. = 21+28.68  
 PVI AD. = 45.0  
 K = 21.00  
 121.50' VC

EXISTING H/L ACU #1  
 STA. 13+77.7 TO 14+00  
 EXISTING R/W = 6340.1 (NWD288)  
 EXISTING INV. OUT. 6342.1  
 INV. OUT. = 6332.51

STA. 14+51.76  
 INV. 6331.4



SCALE:  
 HORIZONTAL: 1" = 50'  
 VERTICAL: 1" = 5'

- LEGEND:**
- 66 GRADE BREAK
  - TC TOP OF CURB ELEVATION
  - TA TOP OF ASPHALT ELEVATION
  - FL ELEVATION AT FLOW LINE
  - IN INVERT AT INLET
  - IO INVERT AT OUTLET
  - TS TOP OF SUBPAVK
- NOTES:**
1. GRADE ELEVATIONS PROVIDED AT FACE
  2. OF STRUCTURES MEASURED TO EDGE
  3. OF STRUCTURES CLOSEST TO CENTERLINE

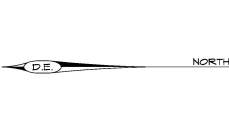
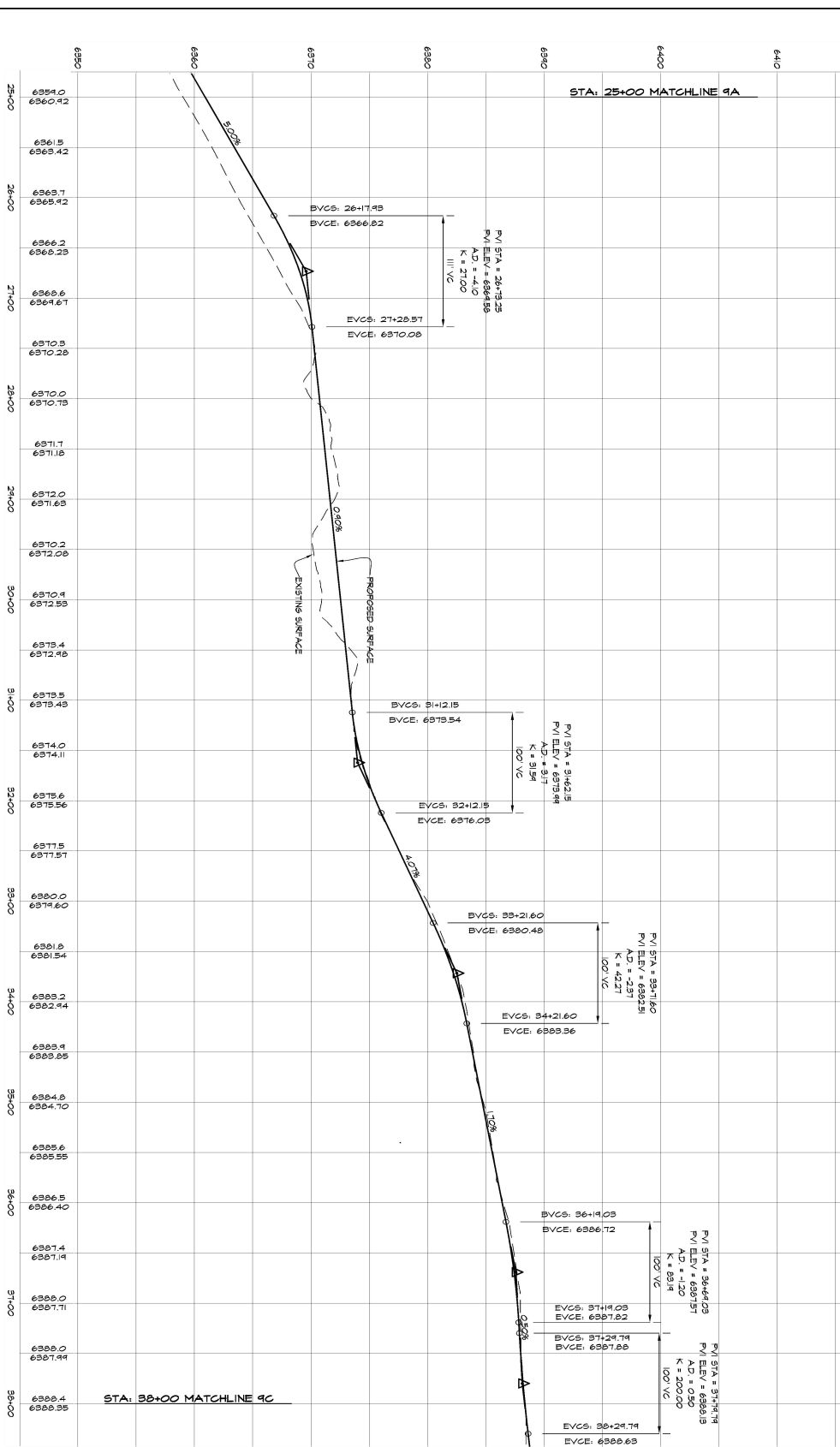
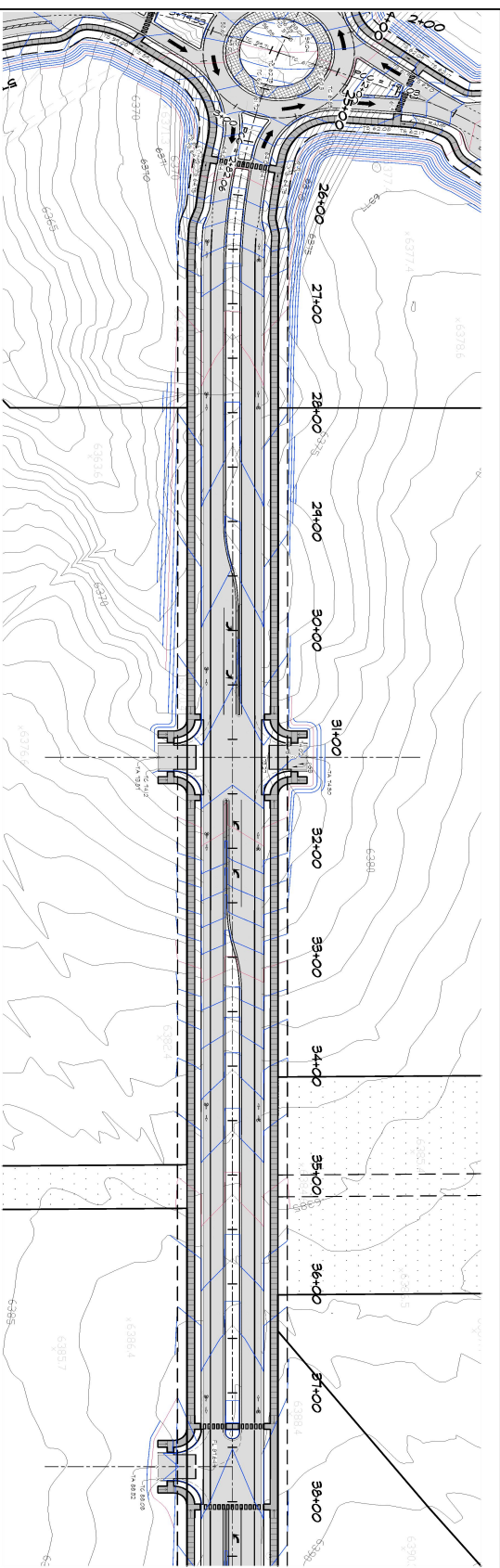


DESIGN ENGINTEERY

341 TULLIN STREET, SUITE 100  
 SAN FRANCISCO, CA 94102  
 PHONE: 415.774.9977

TERRA ENGINTEERY  
 PASO DEL SOL PLAN AND PROFILE

NO.	DATE	BY
1	07/15/2023	BA



**DESIGN ENGINEER**

4611 LUNA STREET, SUITE 100  
SAN JOSE, CALIFORNIA 95128  
PHONE: (408) 998-9997

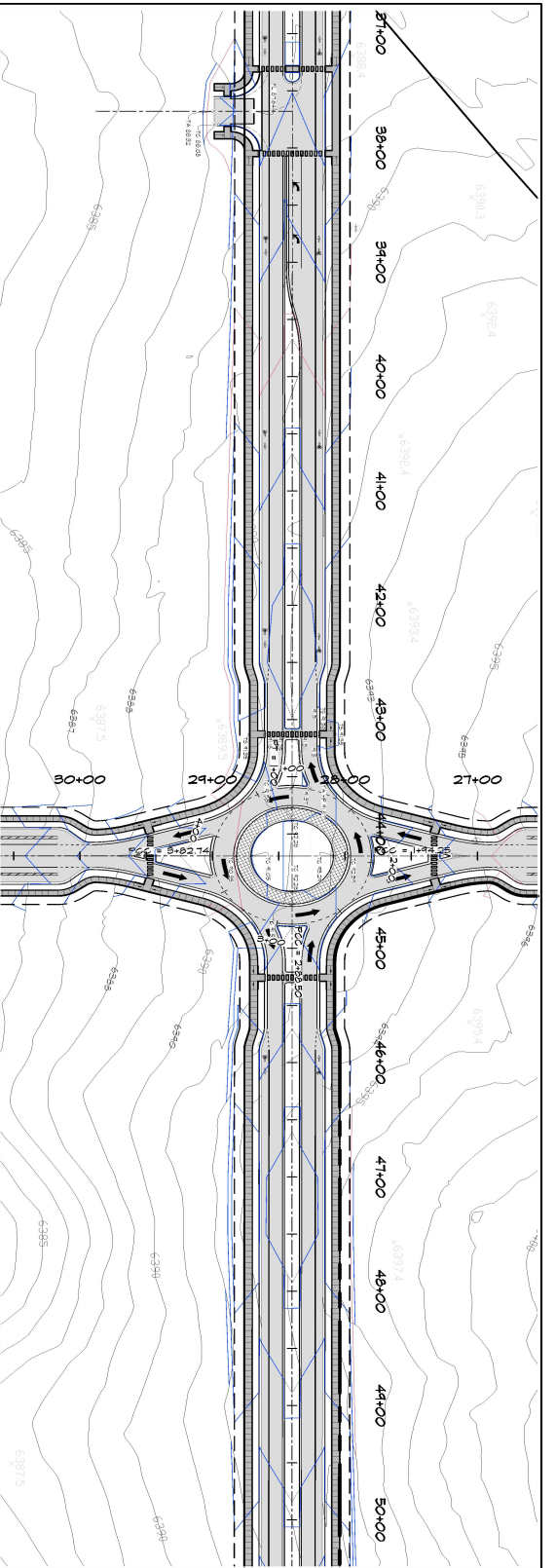
**PHASE 3 PASEO DEL SOL**

**PASEO DEL SOL PLAN AND PROFILE**

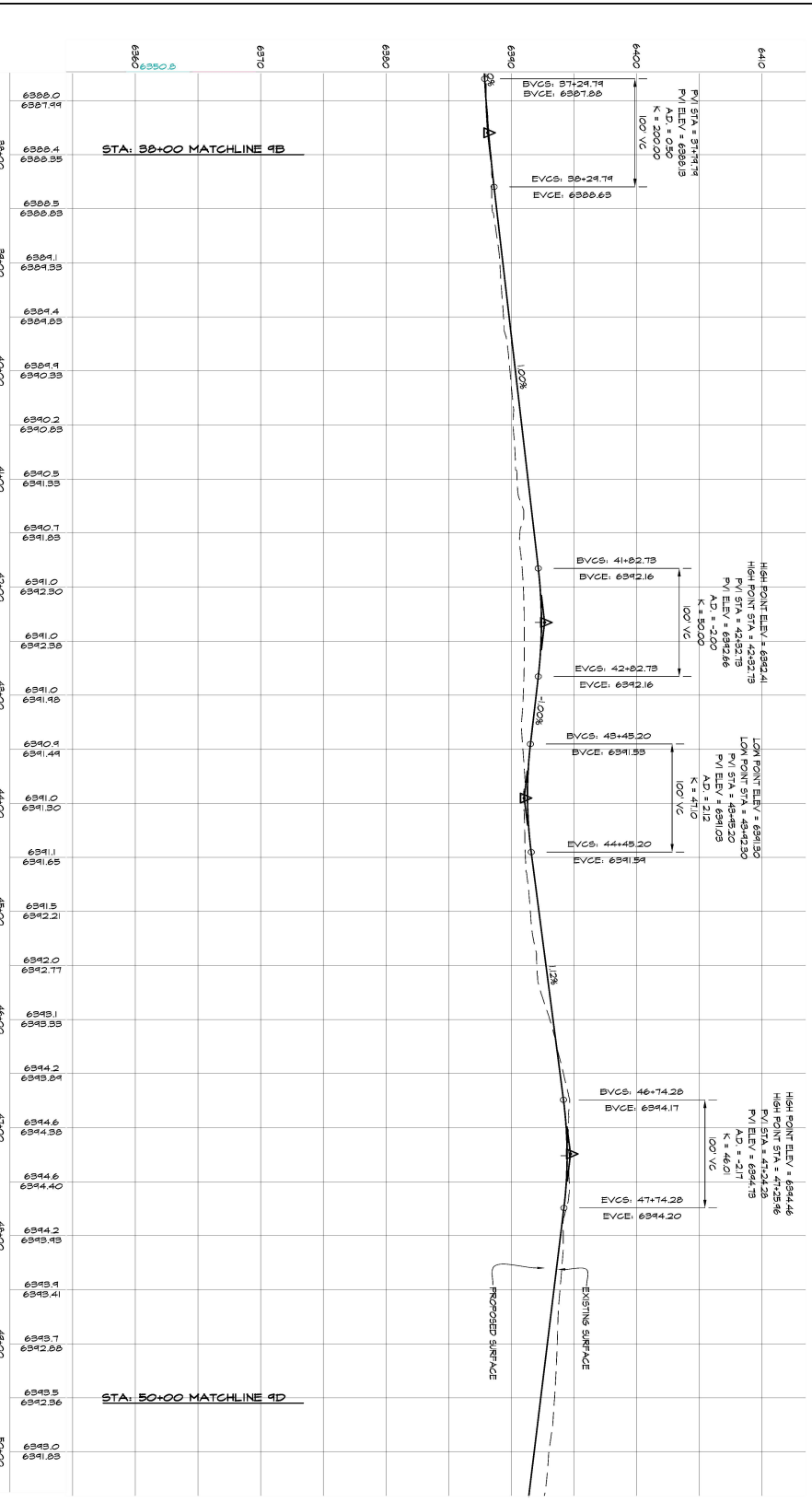
DATE: 07/15/2023

BY: [Signature]

07/15/2023



SCALE:  
HORIZONTAL: 1" = 50'  
VERTICAL: 1" = 5'



**DESIGN ENGINEER**

461 UTAH STREET, SUITE 100  
SALT LAKE CITY, UT 84103  
PHONE: 801.961.9977

**TERRA CONTENTA PHASE 3A**

PASEO DEL SOL PLAN AND PROFILE

SCALE: 1"=50'

DATE: 07/15/2023

BY: [Signature]

**LEGEND:**

- 66 GRADE BREAK
- TC TOP OF CURB ELEVATION
- TA TOP OF ASPHALT ELEVATION
- FL ELEVATION AT FLOW LINE
- II INVERT AT INLET
- IO INVERT AT OUTLET
- TS TOP OF SUBPAVK

**NOTES:**

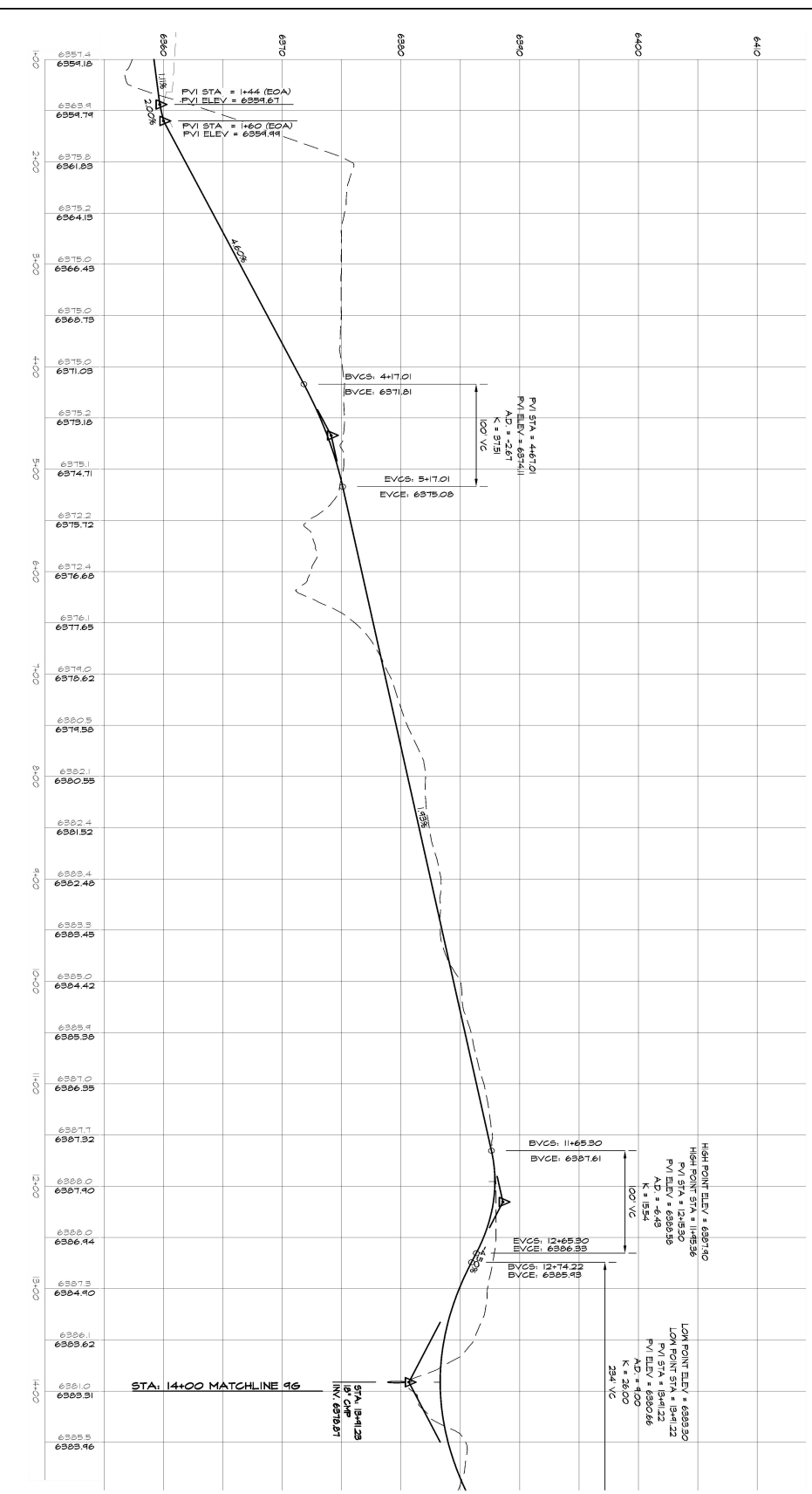
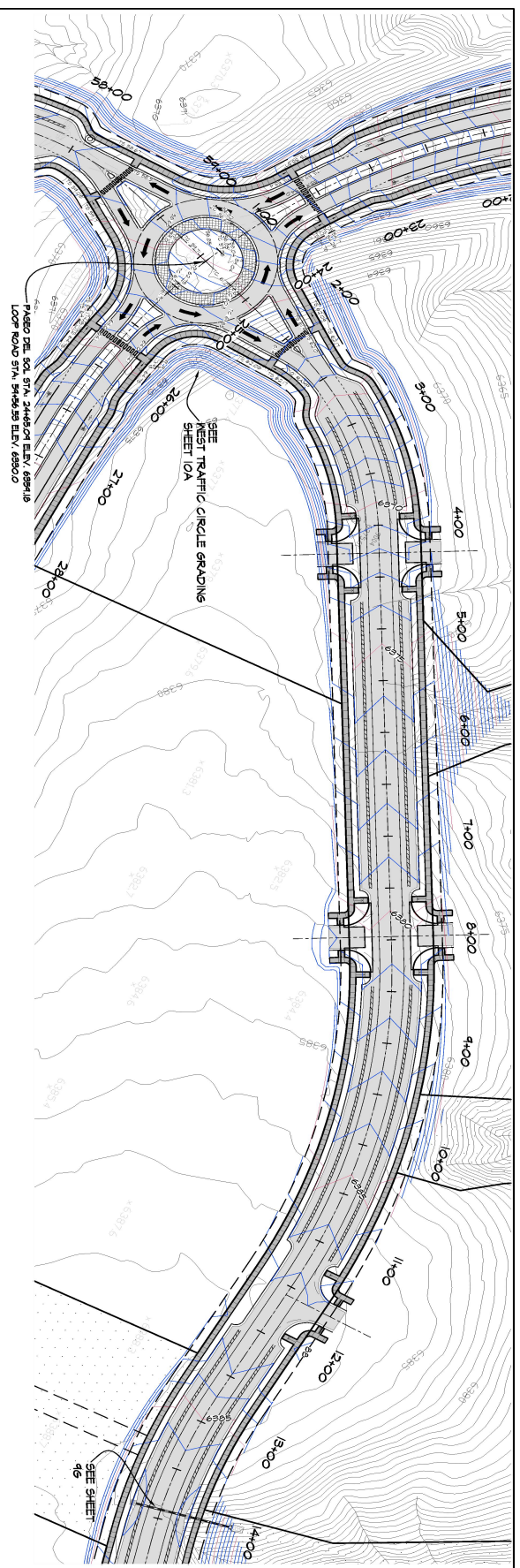
1. GRADE ELEVATIONS PROVIDED AT FACE OF CURB
2. GRADE ELEVATIONS MEASURED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE

**REVISIONS**

DATE	BY







- LEGEND:**
- 66 GRADE BREAK
  - 70 TOP OF CURB ELEVATION
  - 7A TOP OF ASPHALT ELEVATION
  - 7B ELEVATION AT LOW LINE
  - 11 INVERT AT INLET
  - 10 INVERT AT OUTLET
  - 13 TOP OF SIDEWALK
- NOTES:**
- 1 EXISTING ELEVATIONS PROVIDED AT FACE OF STRUCTURES
  - 2 OFFSET DISTANCES MEASURED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE

SCALE  
HORIZONTAL 1" = 30'  
VERTICAL 1" = 3'

**DESIGN ENGINTEER**

REVISIONS

DATE	BY

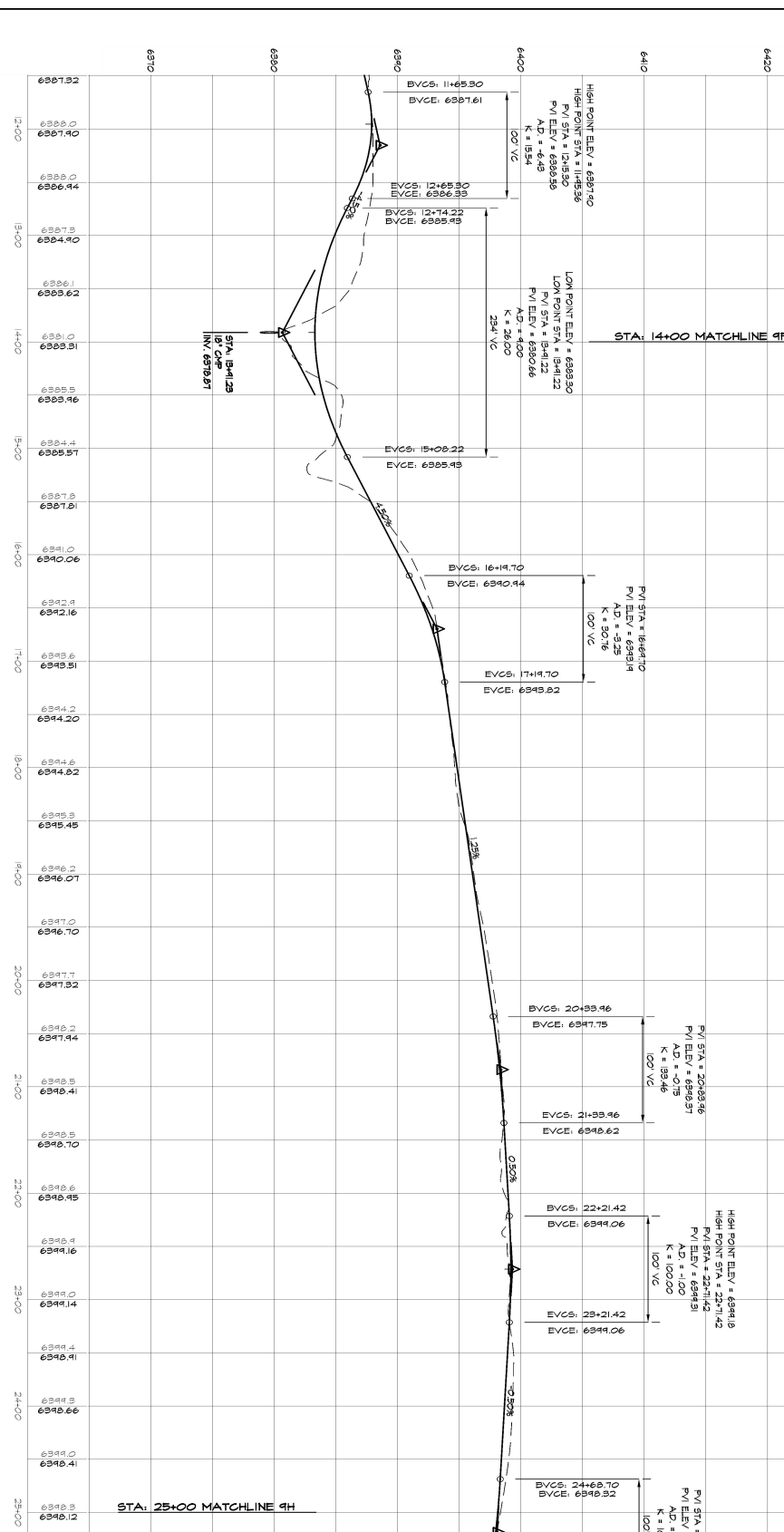
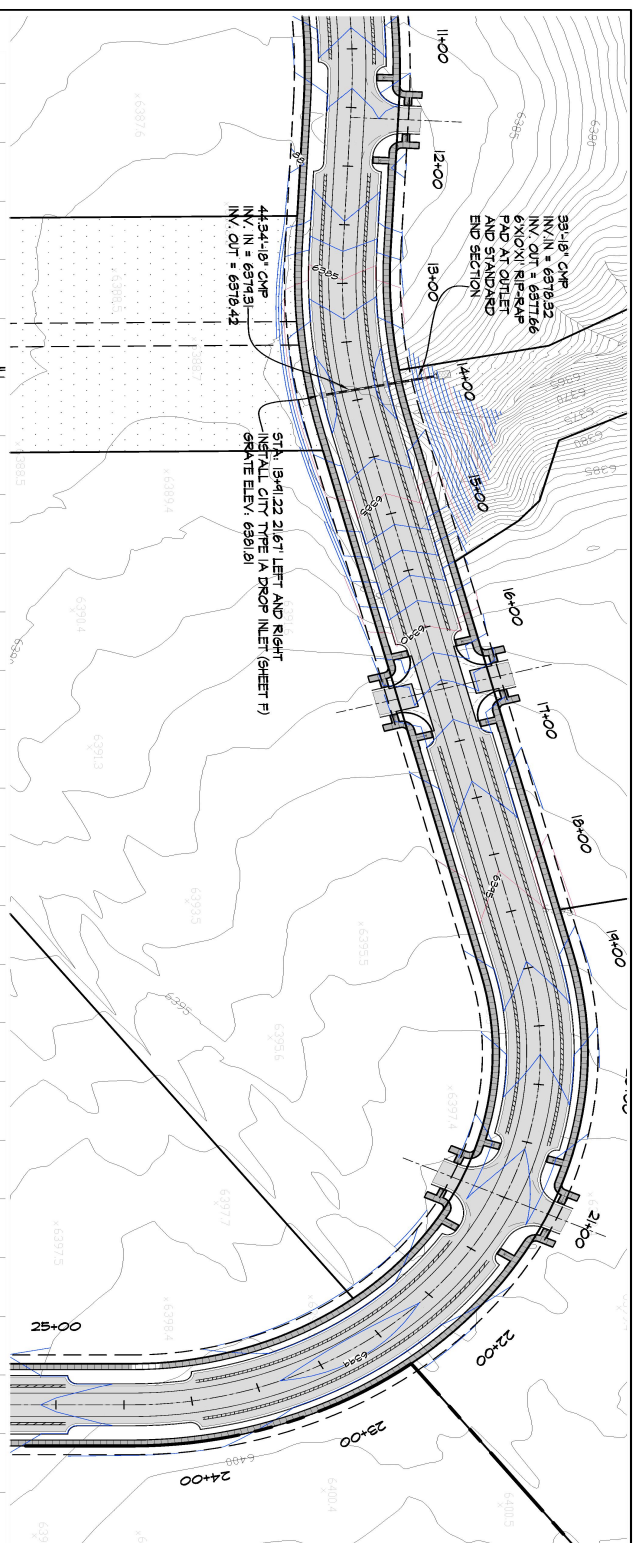
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**LOOP ROAD PLAN AND PROFILE**

SCALE: 1" = 30'

DATE: 07/15/2023

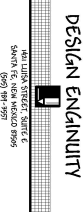
BY: [Signature]



- LEGEND:**
- 66 GRADE BREAK
  - TC TOP OF CURB ELEVATION
  - TA TOP OF ASPHALT ELEVATION
  - FL ELEVATION AT FLOW LINE
  - II INVERT AT INLET
  - IO INVERT AT OUTLET
  - TS TOP OF SUBPAVK
- NOTES:**
- 1 GRADE ELEVATIONS PROVIDED AT FACE OF CURB
  - 2 GRADE ELEVATIONS MEASURED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE

**REVISIONS**

DATE	BY



**DESIGN ENGIENUITY**

441 TULLIN STREET, SUITE 6  
SAN JOSE, CALIFORNIA 95128  
(408) 981-9977

**TERRA ENGIENUITY**

**LOOP ROAD PLAN AND PROFILE**

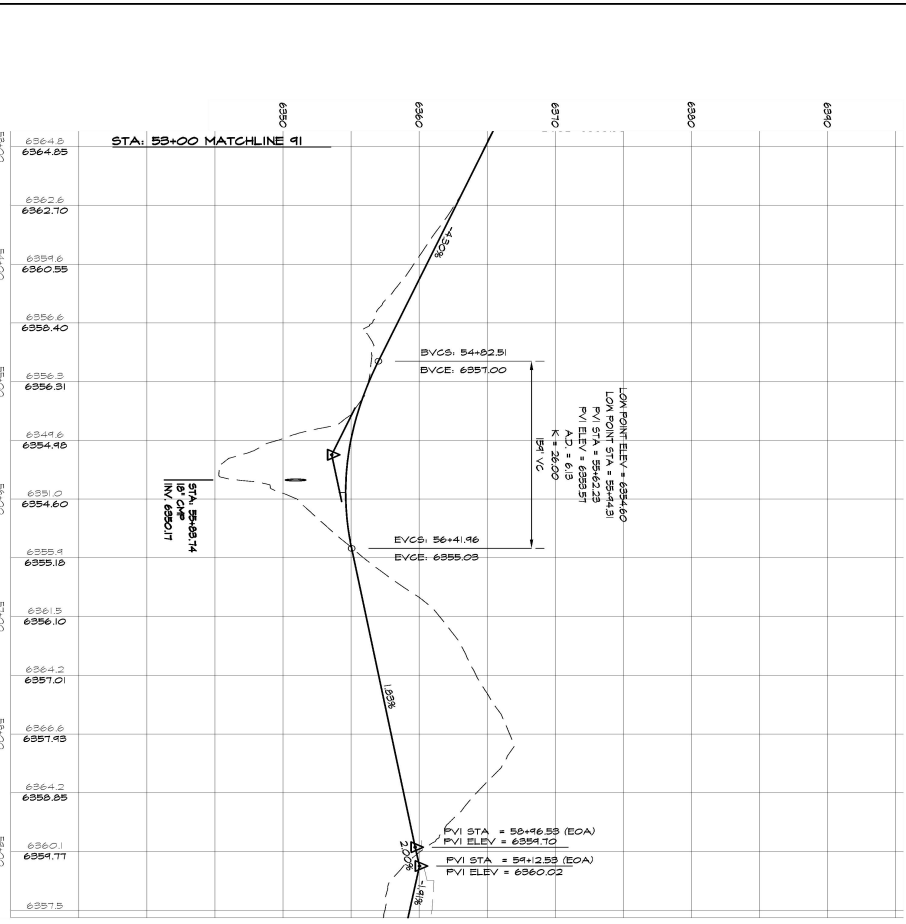
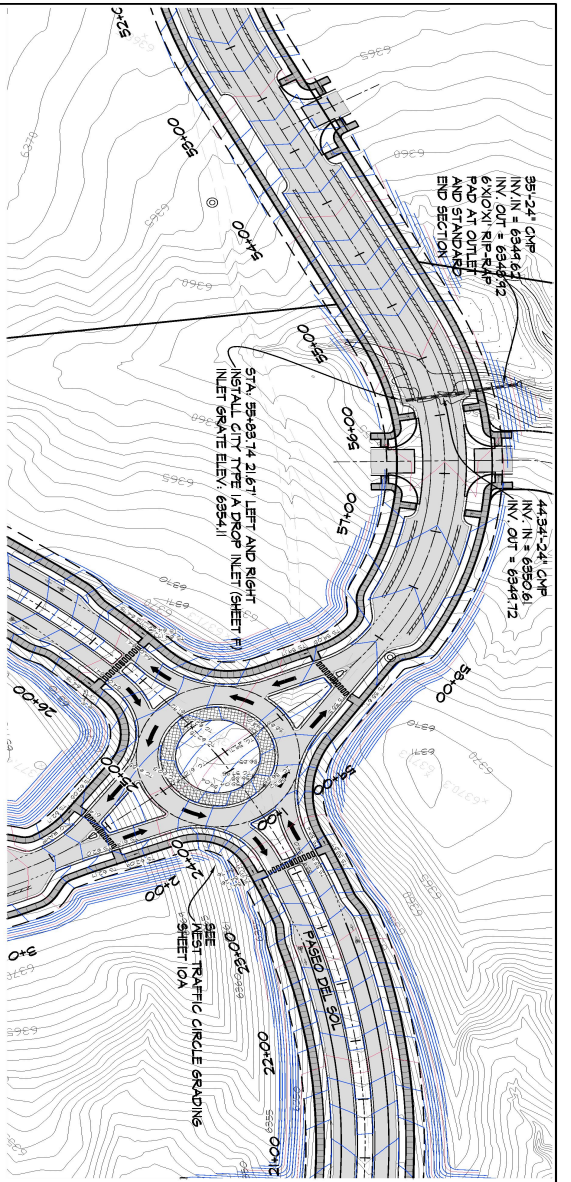
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DATE: 07/18/2023

SHEET NO. 45





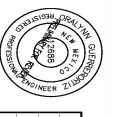


**LEGEND:**

- 66 GRADE BREAK
- TC TOP OF CURB ELEVATION
- TA TOP OF ASPHALT ELEVATION
- FL ELEVATION AT FLOW LINE
- II INVERT AT INLET
- IO INVERT AT OUTLET
- TS TOP OF SIDEWALK

**NOTES:**

1. GRADE ELEVATIONS PROVIDED AT FACE OF CURB
2. OFFSET DISTANCES MEASURED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE

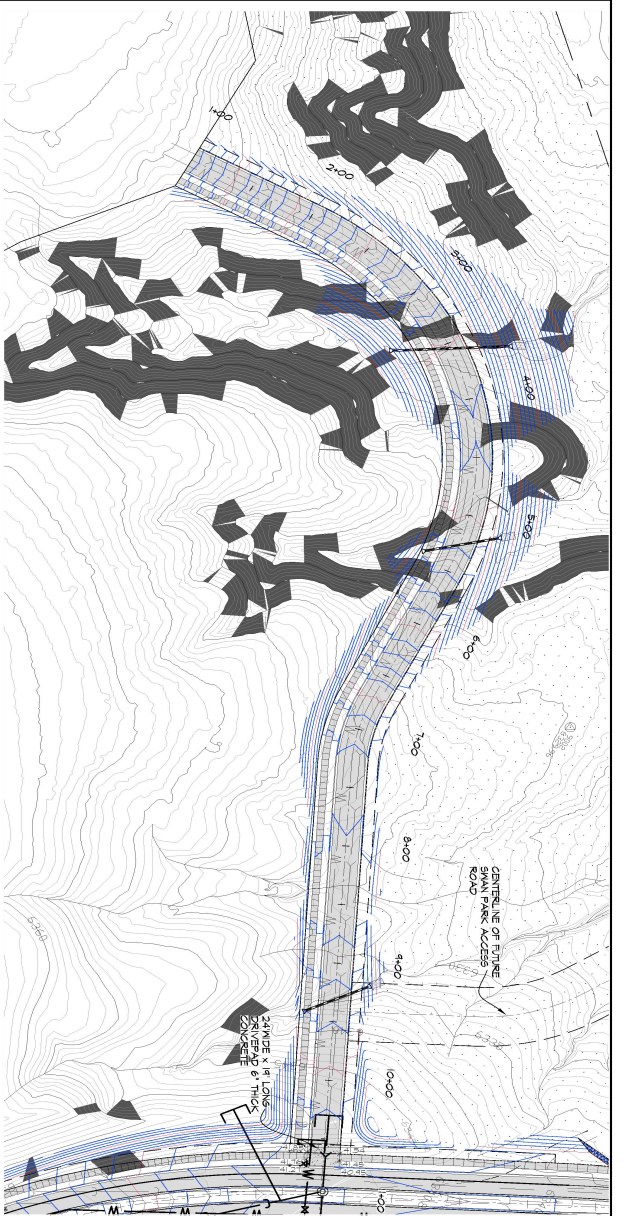


**DESIGN ENGINEER**

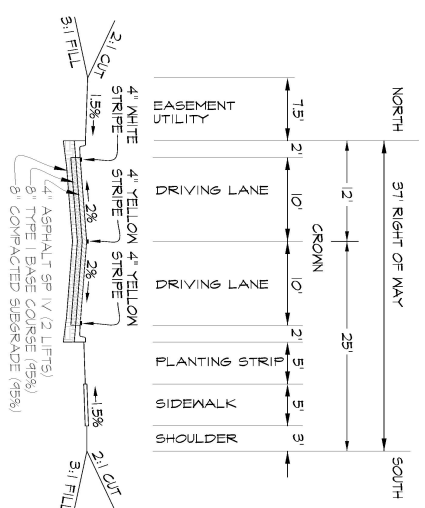
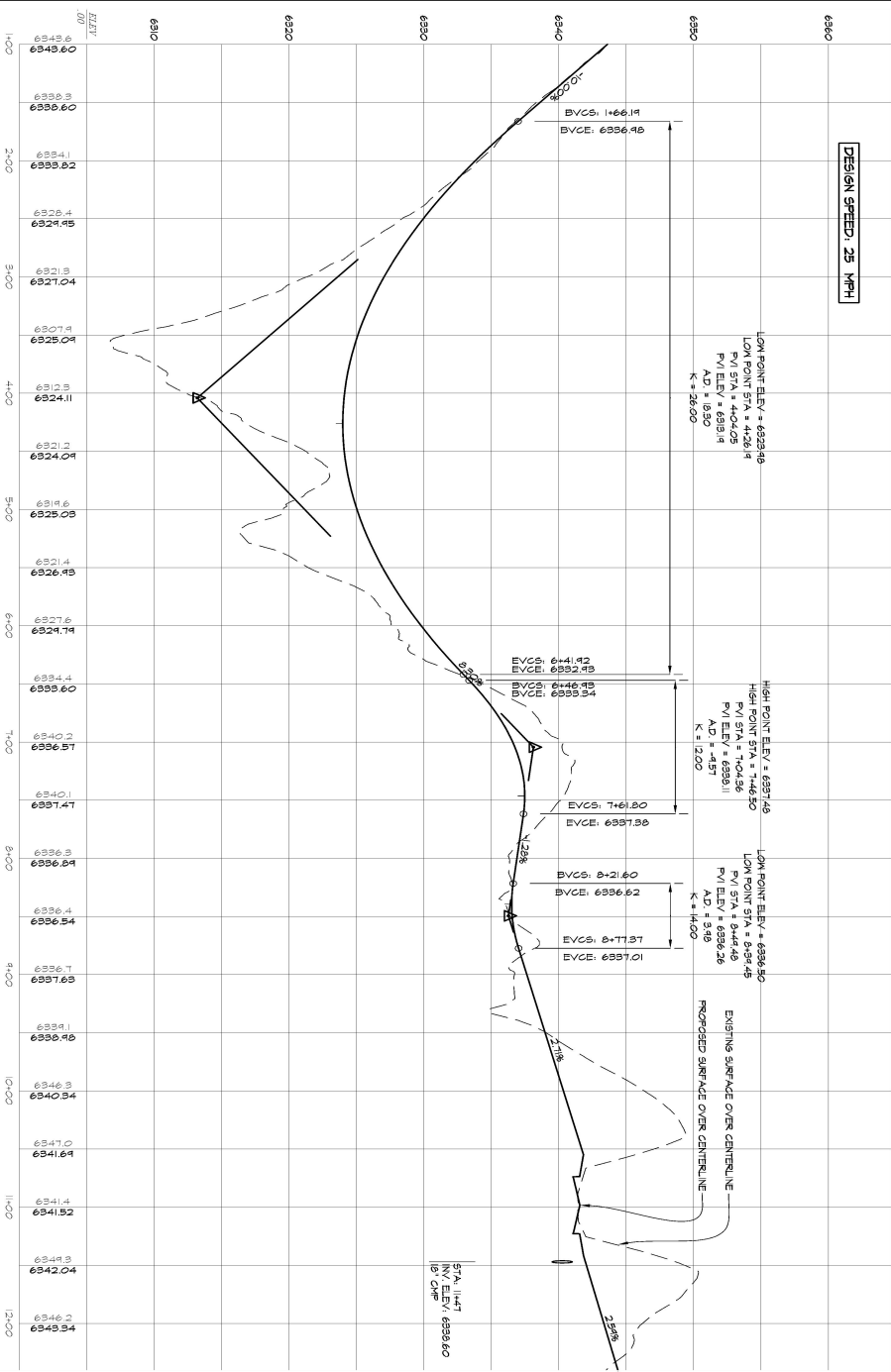
ALLIANCE STREET, SUITE 6  
SAN JOSE, CALIFORNIA 95128  
PHONE: 408.998.9977

**TERRA CONTENTA PHASE 3A**  
**LOOP ROAD PLAN AND PROFILE**

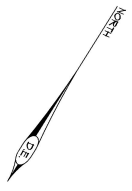
SCALE	DATE	BY
1"=50'	07/18/2023	BJ



DESIGN SPEED: 25 MPH



**TRACT 56/57 NORTH ACCES ROAD SECTION**  
 NOTE: ROAD DESIGN IS FOR PLANNING PURPOSES AND WILL NOT BE CONSTRUCTED AS PART OF THE T23 BACKBONE INFRASTRUCTURE



SCALE: HORIZONTAL 1" = 50'  
 VERTICAL 1" = 5'

- LEGEND:**
- GP GRADE BREAK
  - TC TOP OF CURB ELEVATION
  - TA TOP OF ASPHALT ELEVATION
  - FL ELEVATION AT FLOWLINE
  - II INVERT AT INLET
  - IO INVERT AT OUTLET
  - TS TOP OF SIDEWALK
  - 30% 30% GREATER SLOPE
  - BRSD 30% OR GREATER SLOPE BRSD ON 2% CONDITIONS

- NOTES:**
1. OFFSET ELEVATIONS PROVIDED AT FACE
  2. OFFSET DISTANCES RELATED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE

30% PLAN SET



DESIGN ENGINEER

TERRA CONTENTA PHASE 3A  
 TRACT 56/57 NORTH ACCESS ROAD RMP

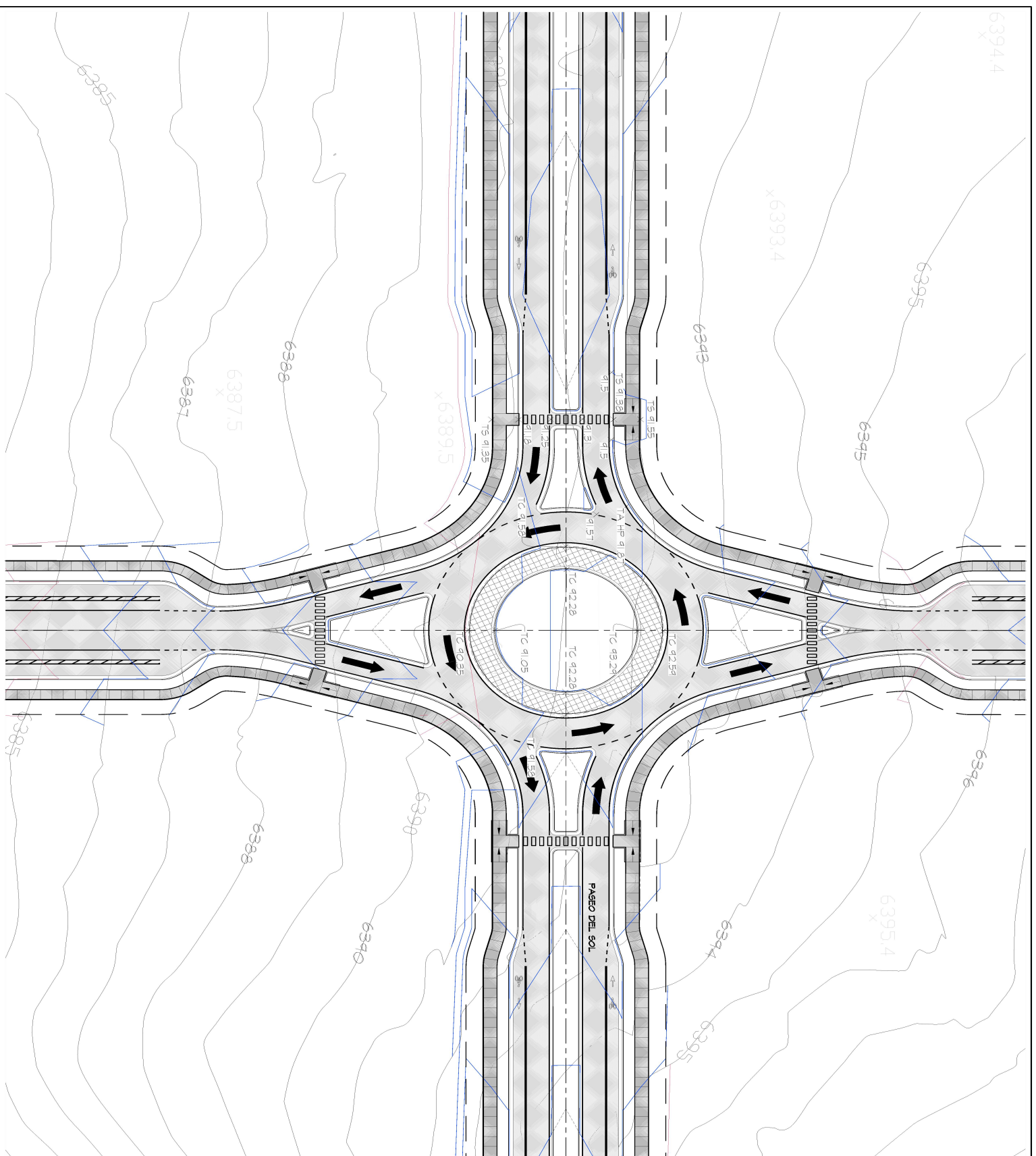
REVISIONS	DATE	BY

SCALE: 1" = 50'

DATE: 07/15/2023

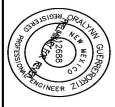
BY: RK





NORTH  
  
 SCALE:  
 HORIZONTAL: 1" = 20'

- LEGEND:**
- 66 GRADE BREAK
  - TC TOP OF CURB ELEVATION
  - TA TOP OF ASPHALT ELEVATION
  - FL ELEVATION AT FLOW LINE
  - II INVERT AT INLET
  - IO INVERT AT OUTLET
  - TS TOP OF SUBPAVK
- NOTES:**
1. GRADE ELEVATIONS PROVIDED AT FACE OF CURB
  2. STAKE POSITIONS MEASURED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE



**DESIGN ENGINEER**

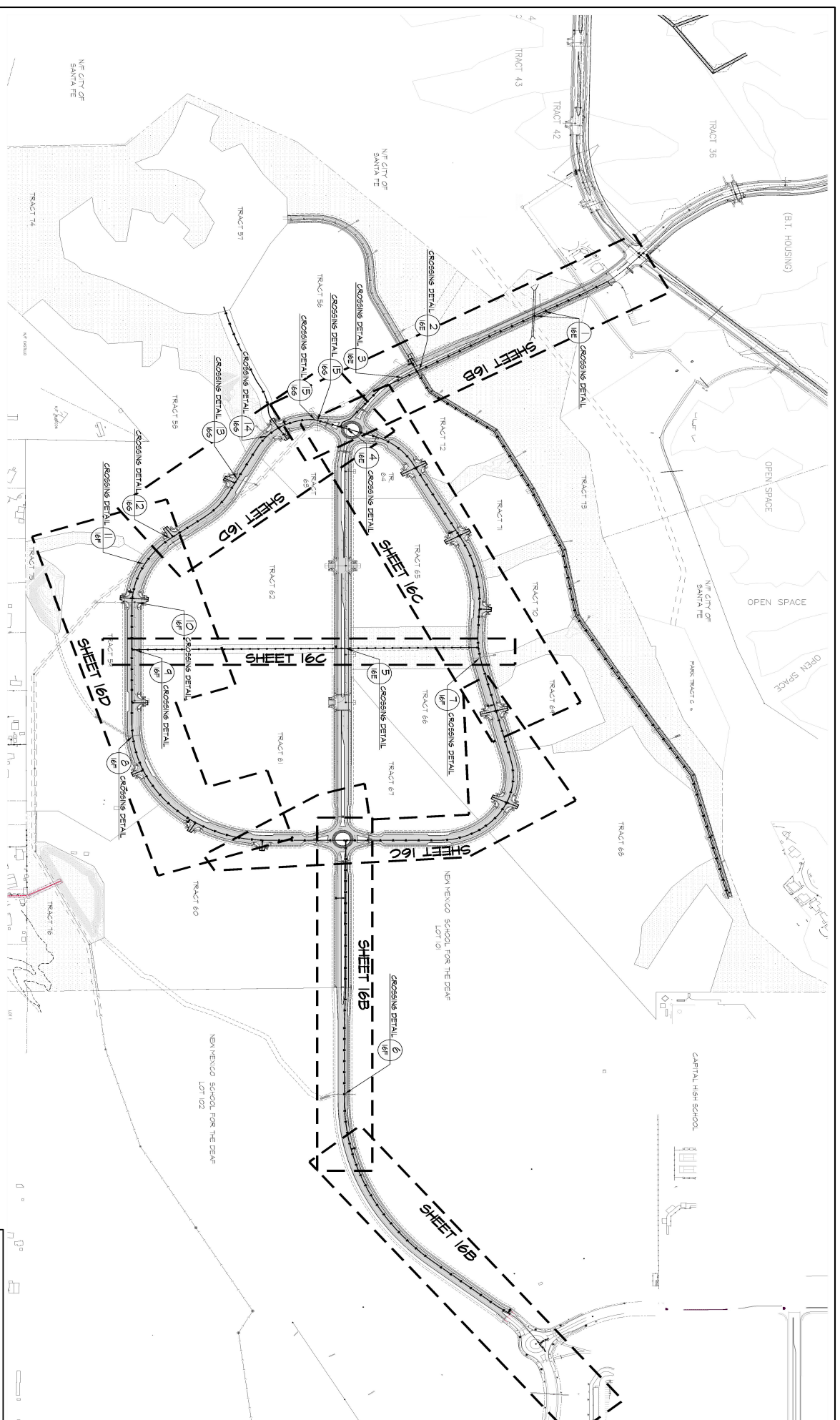
461 LINDEN STREET, SUITE 100  
 SAN ANTONIO, TEXAS 78204  
 PHONE: (214) 343-9977  
 FAX: (214) 343-9978

**TIERRA CONTENTA PHASE 3A**

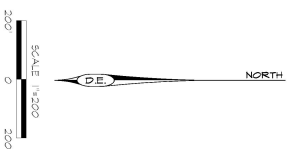
**EAST TRAFFIC CIRCLE GRADING**

SCALE: 1" = 20'  
 SHEET NO. 07/18/2023  
 DATE: 07/18/2023  
 BY: [Signature]  
 DATE: [Blank]  
 BY: [Blank]





I:\PROJECTS\BENTONIA\CONTRACTING\2023\07-08 TCS.dwg, WATER, 7/19/2023 4:02:15 PM



**RECORDED MAPPING**

INITIALS	DATE
AS BUILT	
VALVE VIEWS	
GIS MAPPING	
INSPECTOR APPROVED	

**30% PLAN SET**

DATE	BY	REVISIONS

**DESIGN ENGINEER**

MELISSA SHAW, Suite 6, Santa Fe, New Mexico  
(505) 966-9999

**PHASE 3 PASEO DEL SOL**

**WATER SHEET INDEX**

SCALE: 1"=50'

DATE: 07/15/2023

SHEET NO: 16A

DATE: 07/15/2023

APPROVED

CITY OF SANTA FE WATER DIVISION

SANTA FE FIRE DEPARTMENT

TIGAN ROE

SECTION: SEC 13 & 14

DATE: 2023

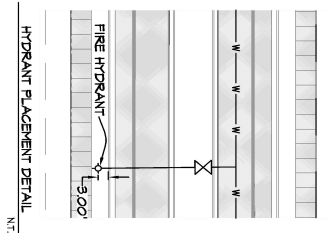
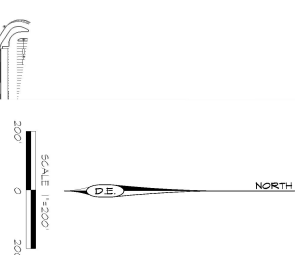
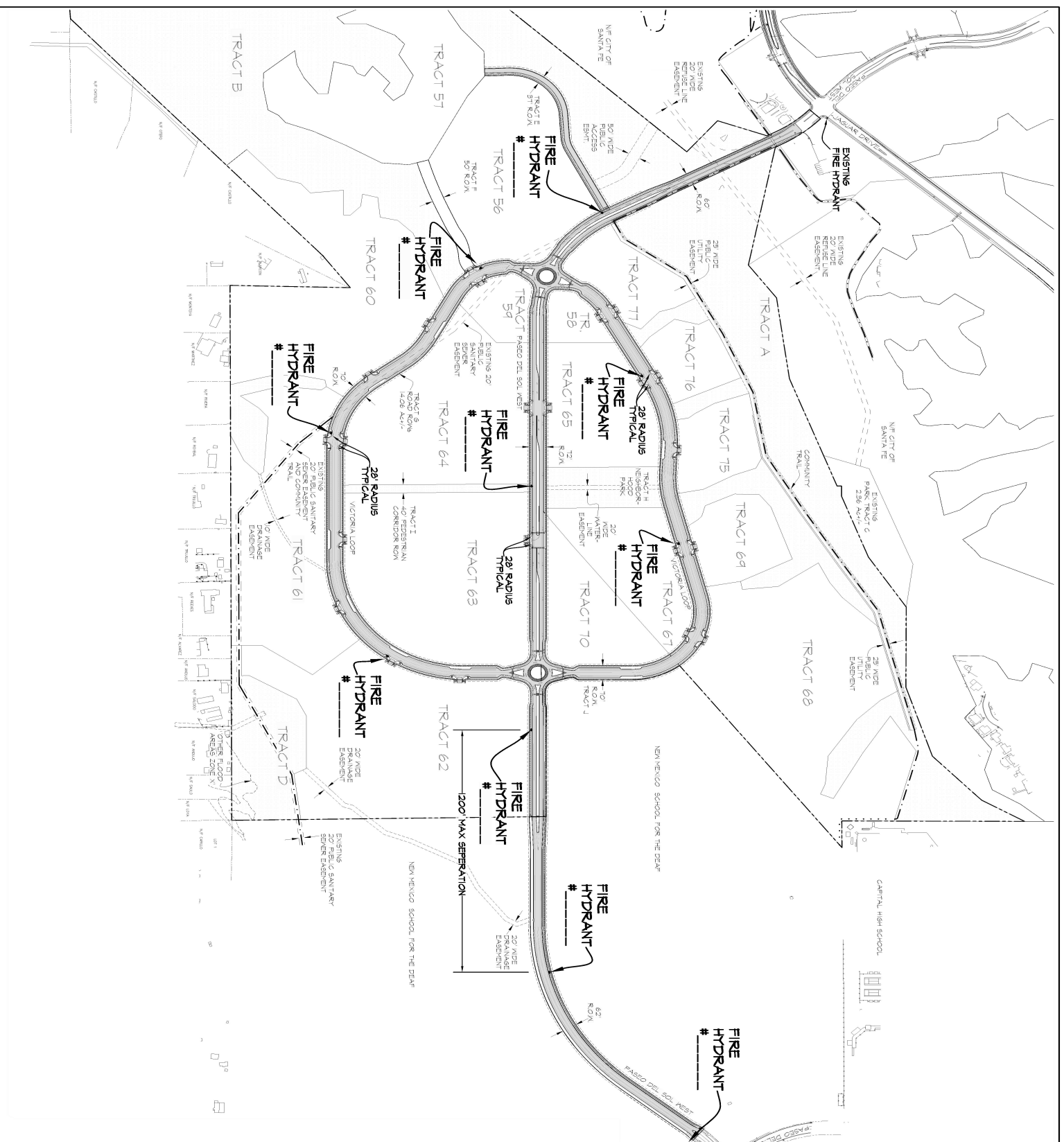
INSPECTOR

(7 OF 7)









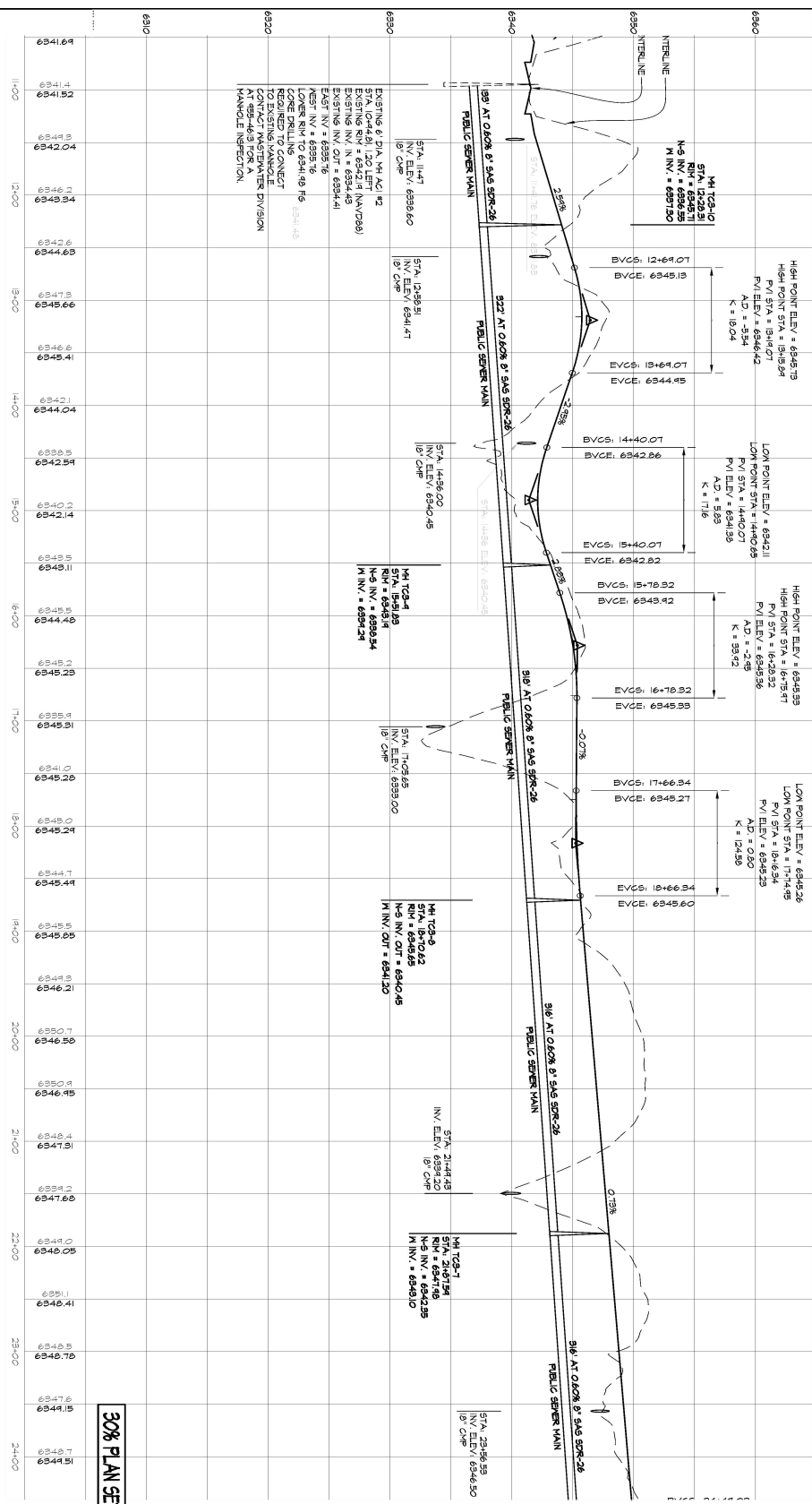
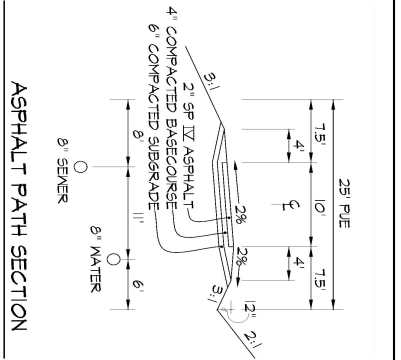
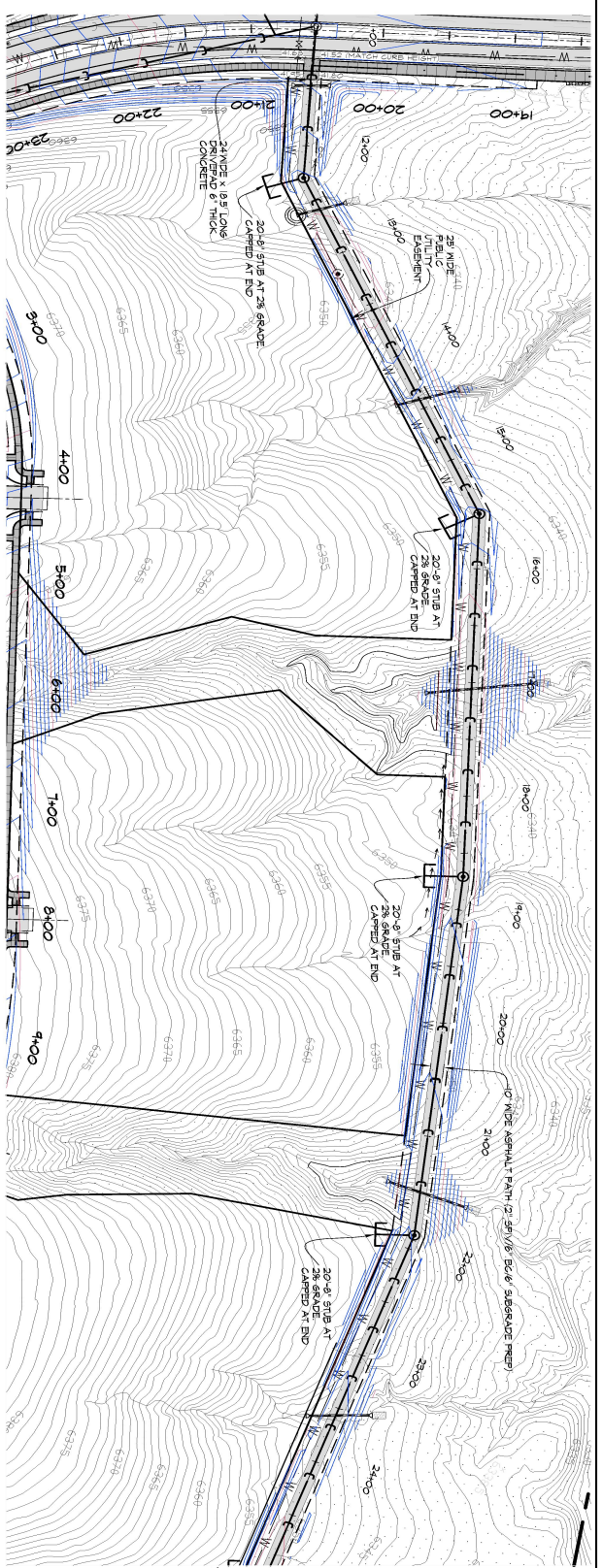
- NOTES:  
 1. ALL ROAD GRADES ARE 0% OR LESS  
 2. ALL ROAD RADII ARE 20' OR GREATER

**30% PLAN SET**

DESIGN ENGINEER	
DATE	BY

**TIERRA CONTENITA PHASE 3A**  
**FIRE PROTECTION PLAN**

SCALE: 1"=1200'  
 DATE: 07/19/2023  
 SHEET NO: 17



**DESIGN ENGINEERING**

**TERRA CONTENTA PHASE 3A**

**SEGMENT 5AS AND ROAD PLAN & PROFILE**

SCALE: 1"=50'

DATE: 07/15/2023

BY: BA

REVISIONS:

NO.	DATE	BY	DESCRIPTION
1			

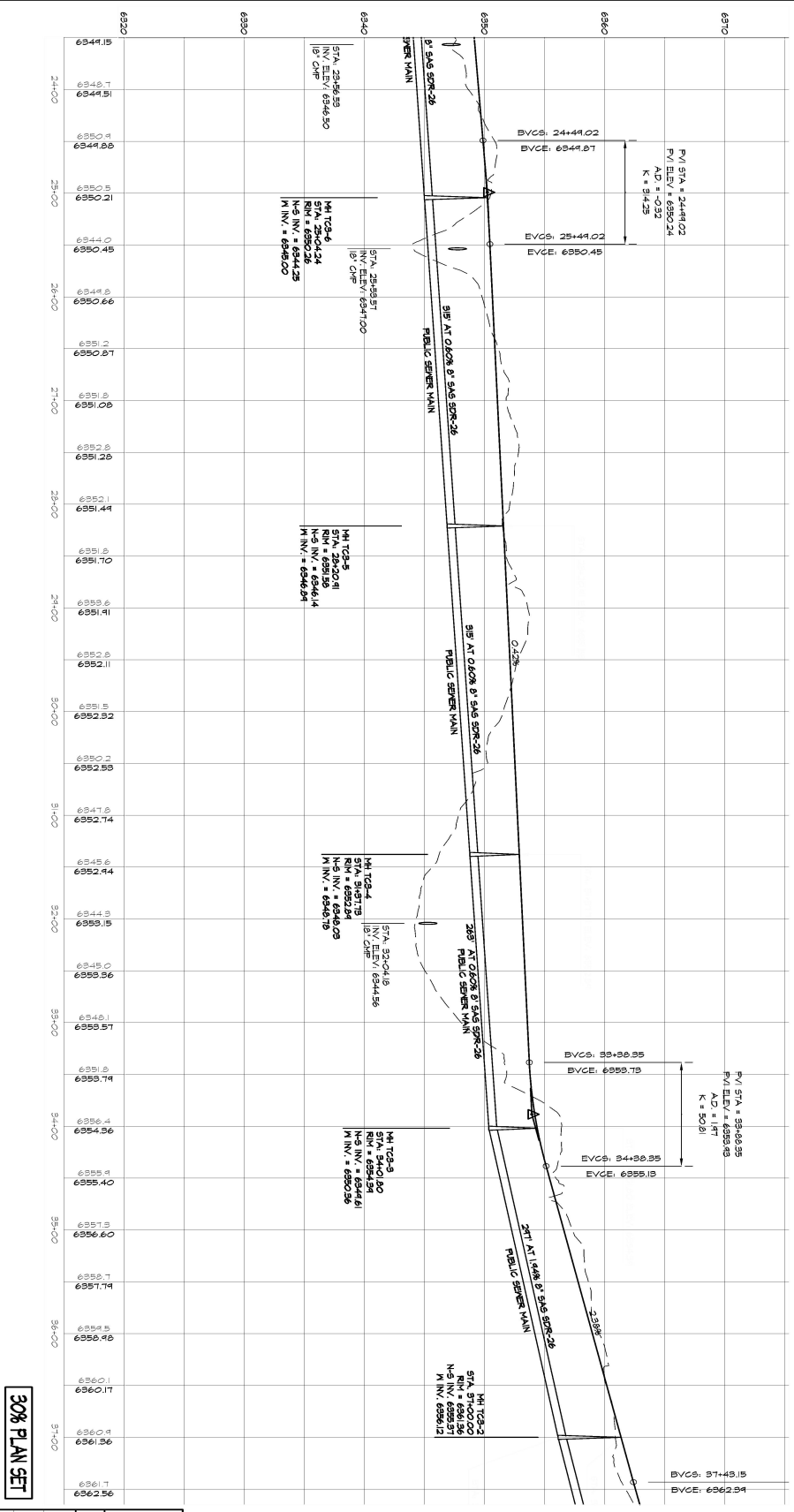
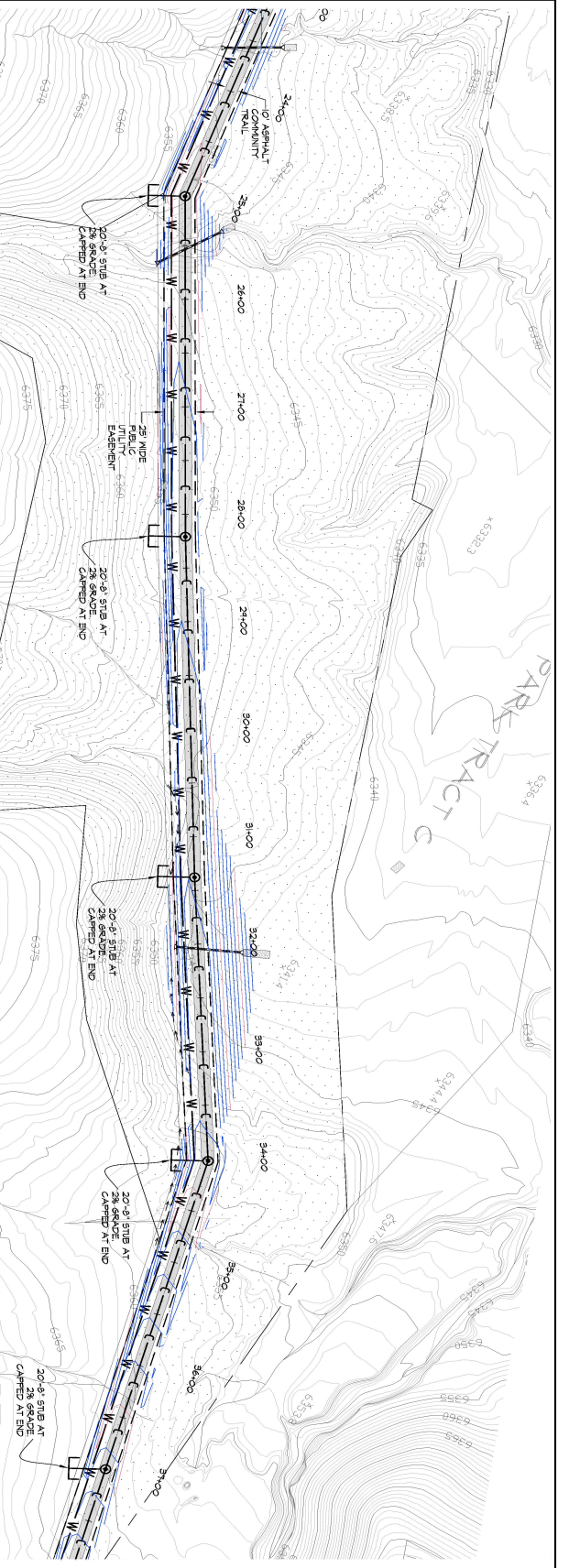
**LEGEND:**

- 66 GRADE BREAK
- TC TOP OF CURB ELEVATION
- TA TOP OF ASPHALT ELEVATION
- FL ELEVATION AT FLOW LINE
- II INVERT AT INLET
- IO INVERT AT OUTLET
- TS TOP OF SUBGRADE

**NOTES:**

- GRADE ELEVATIONS PROVIDED AT FACE
- EXISTING ELEVATIONS MEASURED TO EDGE OF STRUCTURE CLOSEST TO CENTERLINE

**SCALE:**  
HORIZONTAL: 1" = 50'  
VERTICAL: 1" = 5'



**30% PLAN SET**

**TERRA CONTENTA PHASE 3A**  
SEGMENT 1 SAS AND ROAD PLAN & PROFILE

SCALE: 1"=50'

DATE: 07/19/2023

BY: [Signature]

**DESIGN ENGINTEER**

**REVISIONS**

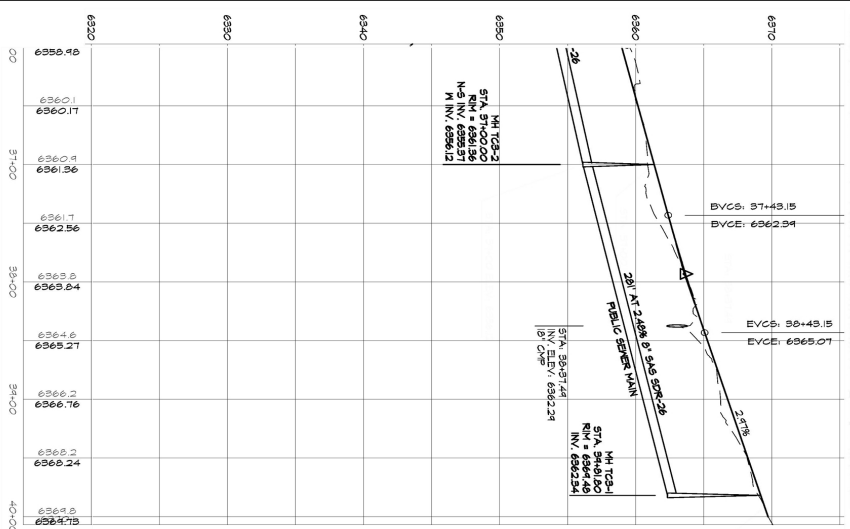
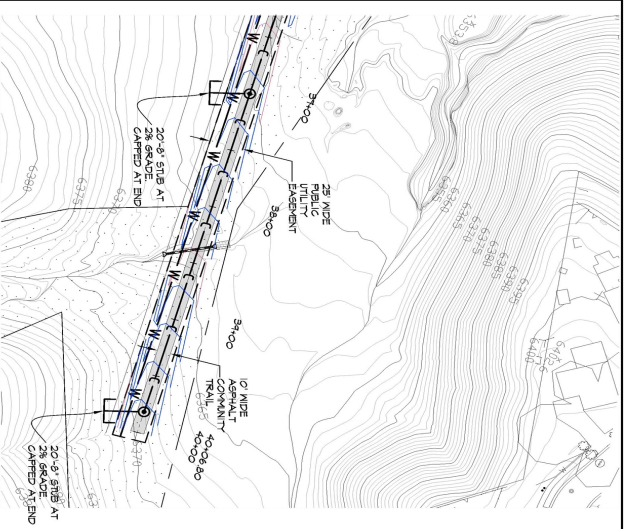
NO.	DATE	BY	DESCRIPTION
1			

**LEGEND:**

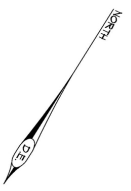
- 66 GRADE BREAK
- TC TOP OF CURB ELEVATION
- TA TOP OF ASPHALT ELEVATION
- FL ELEVATION AT FLOW LINE
- II INVERT AT INLET
- IO INVERT AT OUTLET
- TS TOP OF SIDEWALK

**SCALE:**  
HORIZONTAL: 1" = 50'  
VERTICAL: 1" = 5'

**30% PLAN SET**

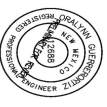


STATION	ELEVATION
6320+00	6359.49
6320+10	6359.17
6320+20	6359.36
6320+30	6359.11
6320+40	6362.56
6320+50	6363.0
6320+60	6363.84
6320+70	6364.6
6320+80	6365.27
6320+90	6366.1
6321+00	6366.76
6321+10	6366.24
6321+20	6366.9
6321+30	6367.4



SCALE:  
HORIZONTAL: 1" = 50'  
VERTICAL: 1" = 5'

- LEGEND:**
- 66 GRADE BREAK
  - TC TOP OF CURB ELEVATION
  - TA TOP OF ASPHALT ELEVATION
  - FL ELEVATION AT FLOW LINE
  - II INVERT AT INLET
  - IO INVERT AT OUTLET
  - TS TOP OF SUBPAVK



DESIGN ENGINEER

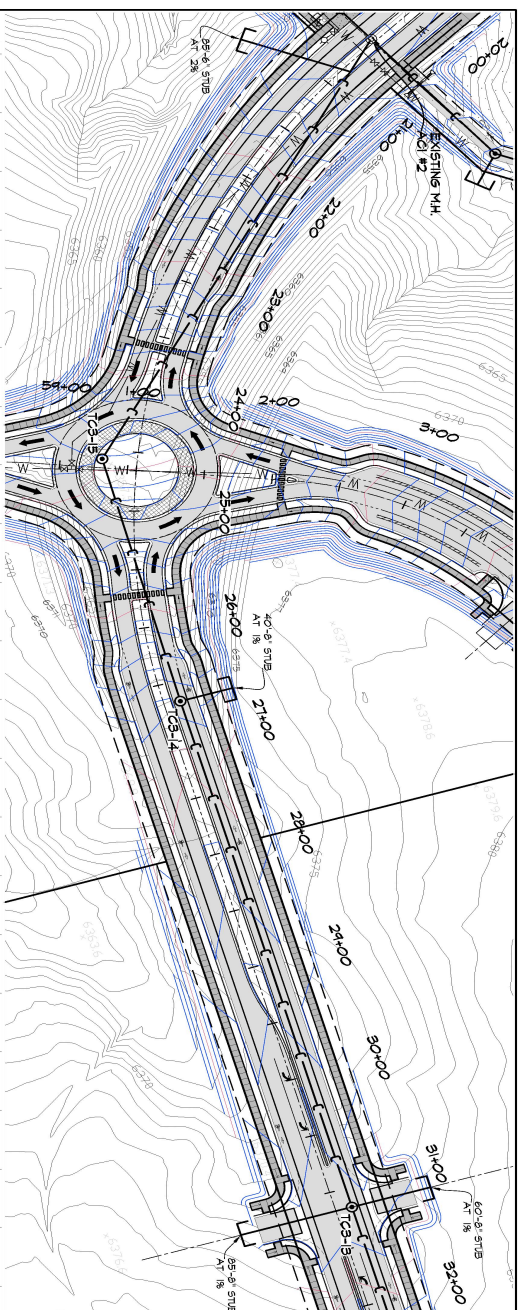
ALLIANCE STREET SURVEY & MAPPING  
600 W. 9th Street  
Reno, NV 89502

**TERRA CONTENTA PHASE 3A**

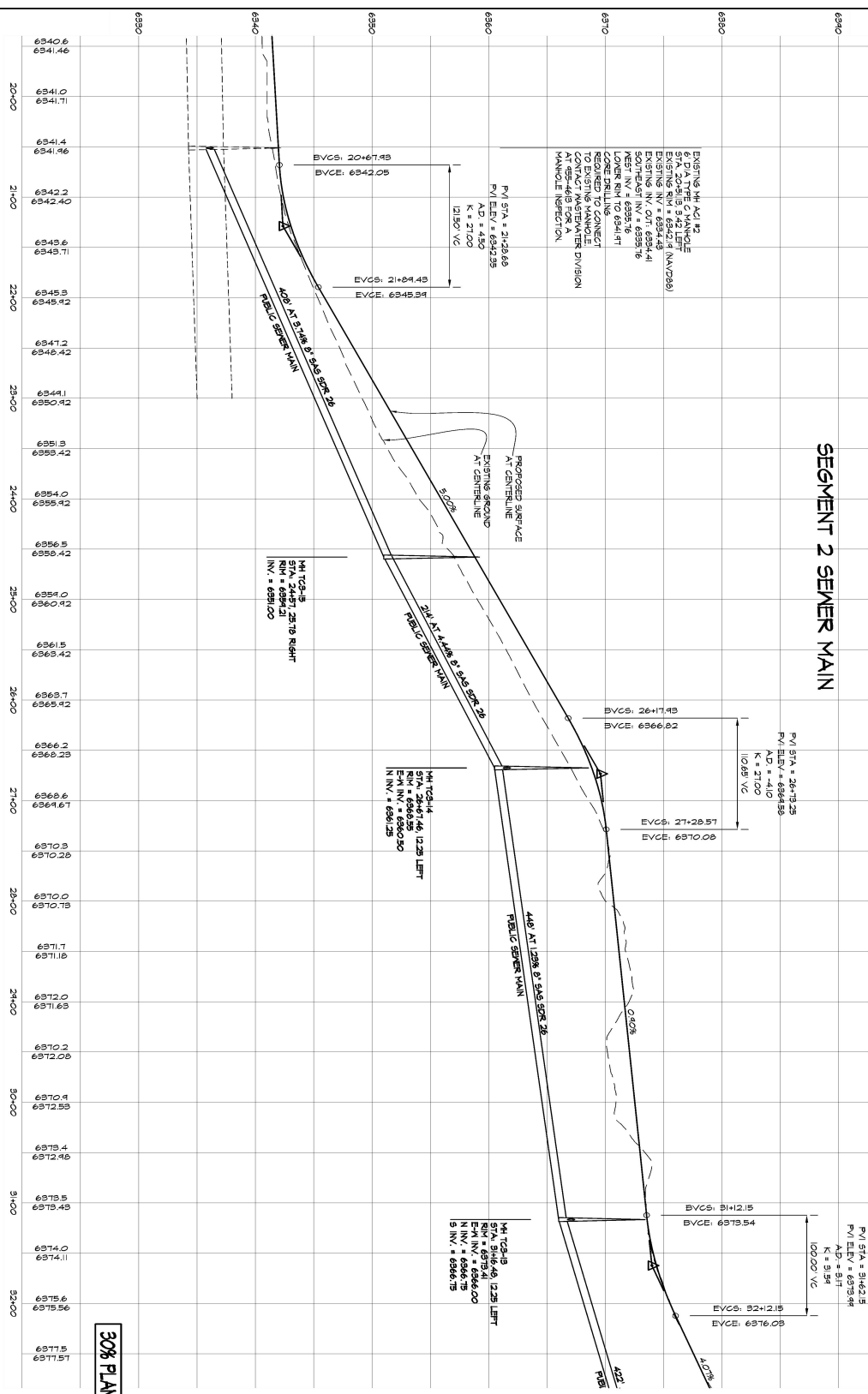
**SEGMENT 1 SAS AND ROAD PLAN & PROFILE**

SCALE	DATE	BY
1" = 50'	07/18/2023	IBC

30% PLAN SET



**SEGMENT 2 SEWER MAIN**



- LEGEND**
- ⊙ EXISTING SANITARY SEWER MANHOLE
  - ⊙ NEW SANITARY SEWER MANHOLE
  - EXISTING PUBLIC GRAVITY SEWER MAIN SIZE AS NOTED
  - NEW 12" PUBLIC GRAVITY SEWER MAIN SIZE AS NOTED
  - EXISTING 12" PUBLIC WATER LINE
  - NEW FIRE HYDRANT
  - NEW 12" PUBLIC WATERLINE UNLESS OTHERWISE NOTED
  - NEW WATER GATE VALVE SIZE AS NOTED
  - ⊙ NEW SANITARY SEWER MANHOLE
  - NEW 8" PUBLIC GRAVITY SEWER MAIN
  - LOW PRESSURE GAS
  - FLUSHING HYDRANT AND GATE VALVE
  - CAP
  - ⊙ AIR VACUUM RELIEF VALVE

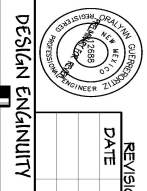
EXISTING MH MANHOLE STA. 20+51.3 3'x4' LEFT  
 EXISTING R/W = 65+12.8 (NAD283)  
 EXISTING INV. OUT. 65+41.4  
 SCOUT EAST INV. = 65+31.6  
 WEST INV. = 65+31.6  
 VERT. CURVE DATA: 12+31.41  
 REQUIRED TO CONNECT TO EXISTING MANHOLE AT 65+46.8 FOR A MANHOLE INSPECTION

PROPOSED SURFACE AT CENTERLINE  
 EXISTING GROUND AT CENTERLINE

PROPOSED SURFACE AT CENTERLINE  
 EXISTING GROUND AT CENTERLINE

SCALE:  
 HORIZONTAL: 1" = 50'  
 VERTICAL: 1" = 5'

**30% PLAN SET**

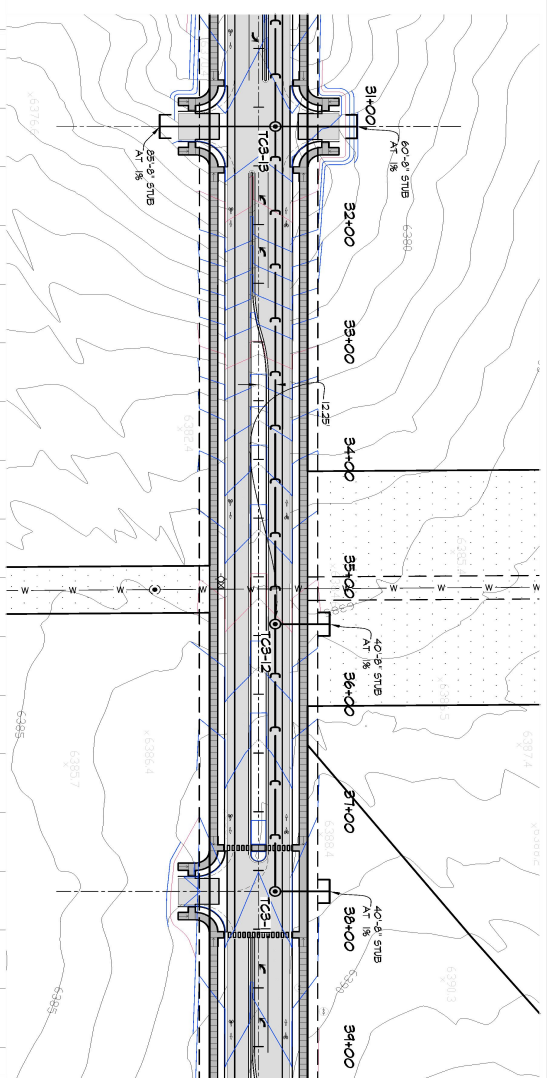


**DESIGN ENGINEER**

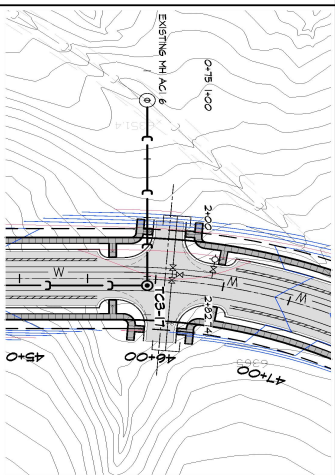
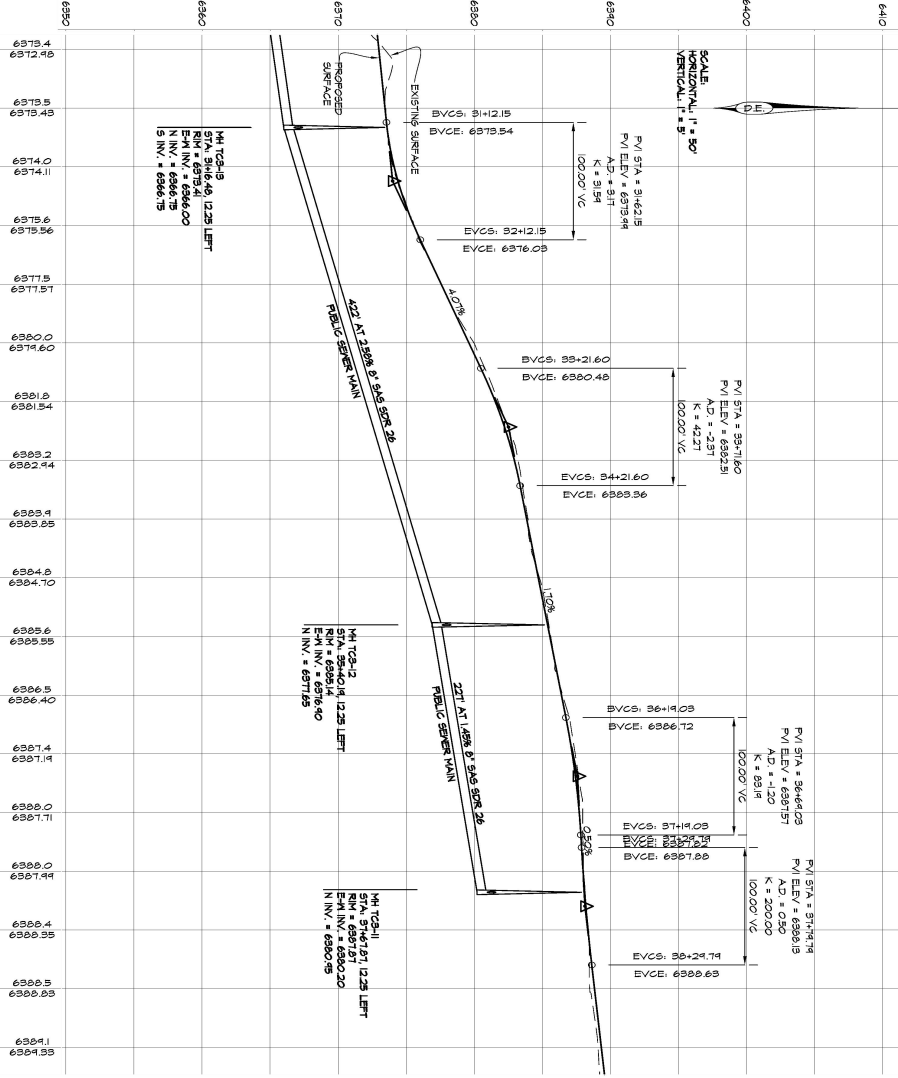
REVISIONS	DATE	BY

**TERRA CONTENTA PHASE 3A**  
**SEGMENT 2 SAS PLAN AND PROFILE**

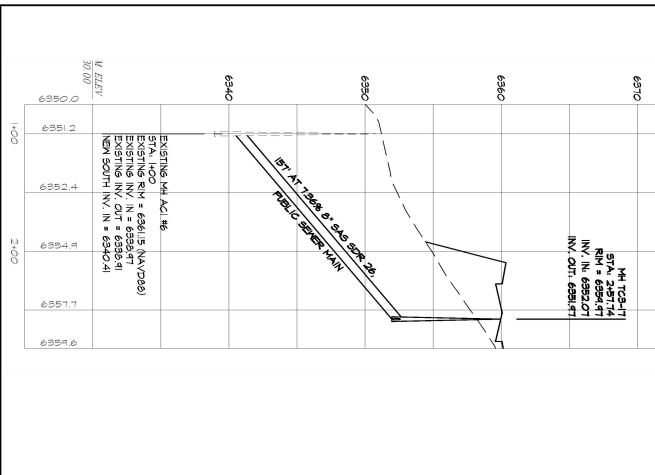
DATE: 07/15/2023  
 BY: BPO



SEGMENT 2 SEWER MAIN

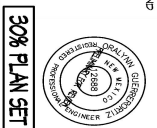


SEGMENT 3 SEWER MAIN



LEGEND

- ⊙ EXISTING SANITARY SEWER MANHOLE
- ⊙ NEW SANITARY SEWER MANHOLE
- EXISTING PUBLIC GRAVITY NOTED
- NEW PUBLIC GRAVITY
- EXISTING 12" PUBLIC WATER LINE
- NEW 12" PUBLIC WATER
- EXISTING 12" PUBLIC WATER
- NEW 12" PUBLIC WATERLINE
- EXISTING HYDRANT AND FIRE VALVE
- NEW HYDRANT AND FIRE VALVE
- EXISTING HYDRANT AND FIRE VALVE
- NEW HYDRANT AND FIRE VALVE
- EXISTING VACUUM RELIEF VALVE
- NEW VACUUM RELIEF VALVE
- EXISTING AIR VALVE
- NEW AIR VALVE



**308 PLAN SET**

**DESIGN ENGINTEER**

**PHASE 3 PASO DEL SOL**

**SEGMENT 2 SAS PLAN AND PROFILE**

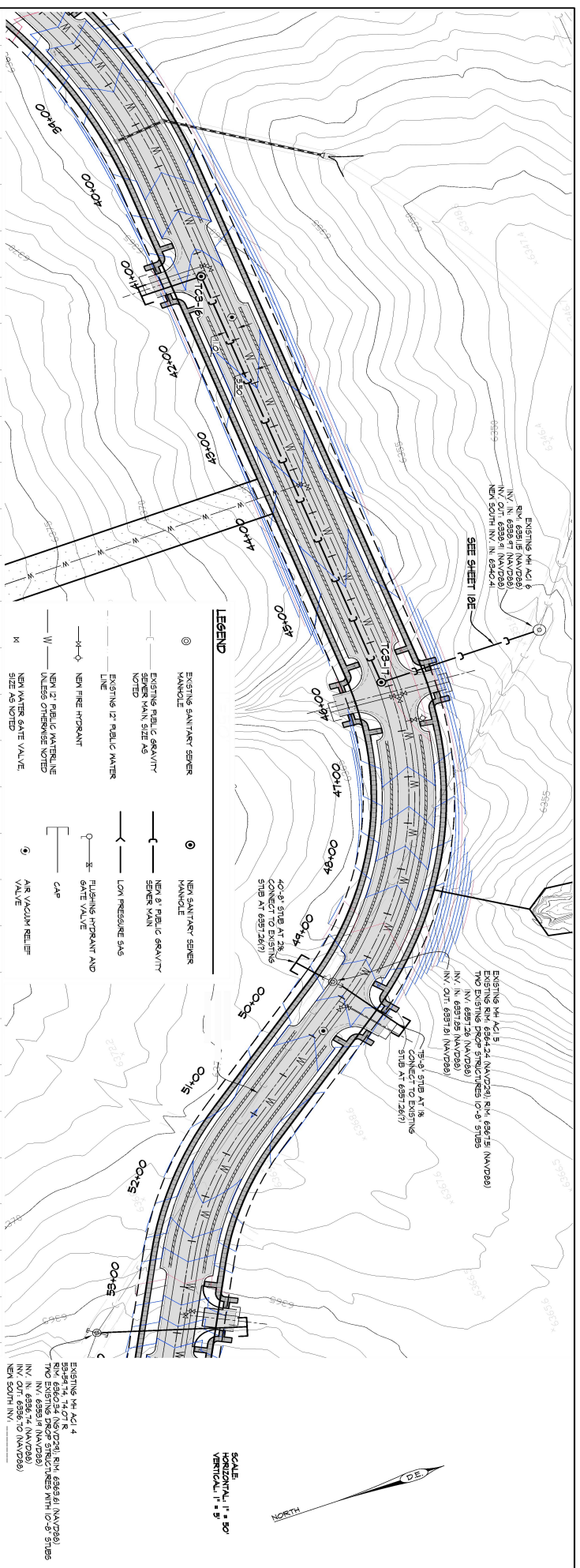
1411 WILSON STREET, SUITE C  
SAN ANTONIO, TEXAS 78205  
512.381.9977

REVISIONS	DATE	BY

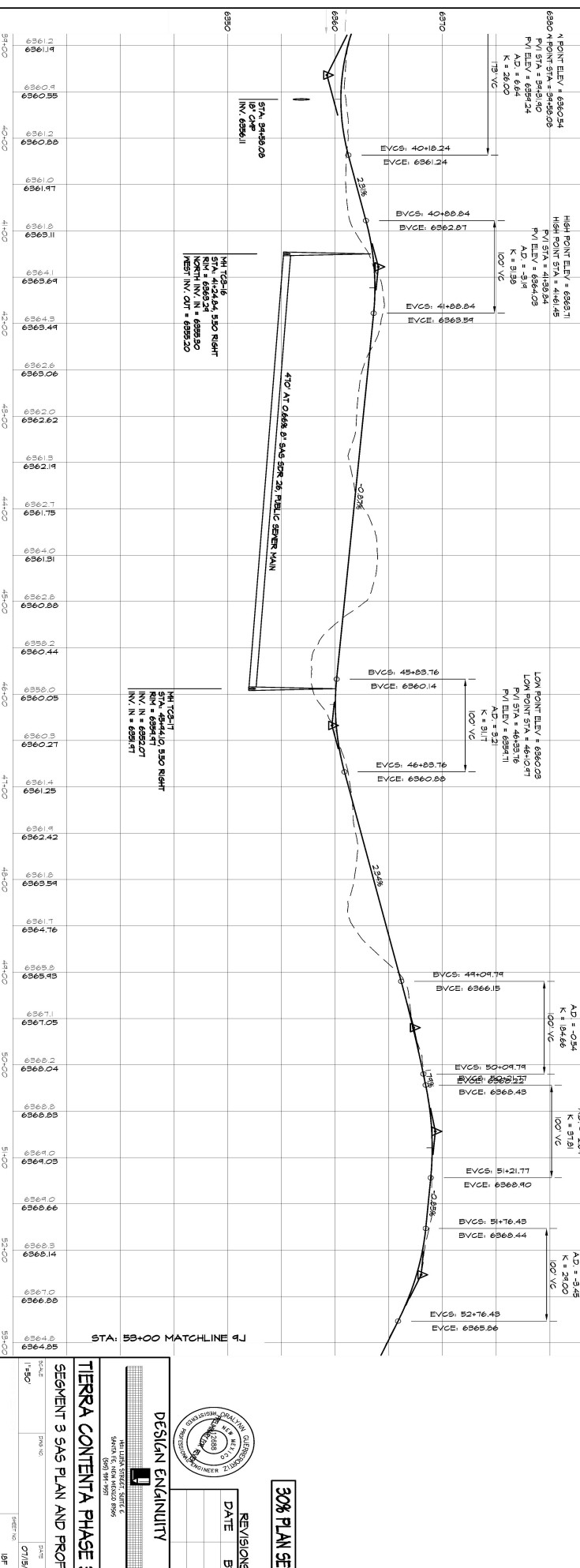
SCALE: 1" = 50'

DATE: 07/15/2023

BY: [Signature]



**SEGMENT 3 SEWER MAIN**



STA. 53+00 MATCHLINE 41

**DESIGN ENGINTEER**

30% PLAN SET

DATE \_\_\_\_\_ BY \_\_\_\_\_

**TERRA CONTENTA PHASE 3A**

SEGMENT 3 SAS PLAN AND PROFILE

SCALE: HORIZONTAL 1" = 50'

VERTICAL 1" = 5'

DATE: 07/15/2023

BY: BPR





ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
1	Grading, Paving and Concrete	AC	27.64
2	Clear & Grub, clearing of any trash	CV	0
3	Earthwork Cut	CF	0
4	Earthwork Fill	CF	0
5	Earthwork Import	CF	0
6	Final grading of ponds	EA	0
7	Water Development Water Coat	LS	1
8	Water Development Water Coat	SV	614.8
9	Gravel gravel basecourse	SV	0
10	Gravel gravel basecourse	SV	50.91
11	Gravel gravel basecourse	SV	48.97
12	Gravel gravel basecourse	SV	785.56
13	Gravel gravel basecourse	SV	22.57
14	Gravel gravel basecourse	SV	62.51
15	Gravel gravel basecourse	SV	19.85
16	Gravel gravel basecourse	SV	11.71
17	Gravel gravel basecourse	SV	11.71
18	Gravel gravel basecourse	SV	11.71
19	Gravel gravel basecourse	SV	11.71
20	Gravel gravel basecourse	SV	11.71
21	Gravel gravel basecourse	SV	11.71
22	Gravel gravel basecourse	SV	11.71
23	Gravel gravel basecourse	SV	11.71
24	Gravel gravel basecourse	SV	11.71
25	Gravel gravel basecourse	SV	11.71
26	Gravel gravel basecourse	SV	11.71
27	Gravel gravel basecourse	SV	11.71
28	Gravel gravel basecourse	SV	11.71
29	Gravel gravel basecourse	SV	11.71
30	Gravel gravel basecourse	SV	11.71
31	Gravel gravel basecourse	SV	11.71
32	Gravel gravel basecourse	SV	11.71
33	Gravel gravel basecourse	SV	11.71
34	Gravel gravel basecourse	SV	11.71
35	Gravel gravel basecourse	SV	11.71
36	Gravel gravel basecourse	SV	11.71
37	Gravel gravel basecourse	SV	11.71
38	Gravel gravel basecourse	SV	11.71
39	Gravel gravel basecourse	SV	11.71
40	Gravel gravel basecourse	SV	11.71
41	Gravel gravel basecourse	SV	11.71
42	Gravel gravel basecourse	SV	11.71
43	Gravel gravel basecourse	SV	11.71

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
72	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	383
73	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
74	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	1130.88
75	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
76	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
77	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
78	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	10
79	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
80	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	44
81	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	10
82	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	20
83	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	12
84	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
85	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
86	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
87	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
88	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
89	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
90	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
91	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
92	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
93	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
94	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2
95	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	2

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
138	As-Built Plans for Project (8%)	LS	1
139	Project Construction Staking (8%)	LS	1
140	Project Traffic Control (8%)	LS	1
141	Project Testing (8%)	LS	1
142	Construction Inspection and Management (8%)	LS	1

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
143	Retaining Walls	SF	0

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
1	Clear & Grub, clearing of any trash	CV	0
2	Earthwork Cut	CF	0
3	Earthwork Fill	CF	0
4	Earthwork Import	CF	0
5	Final grading of ponds	EA	0
6	Water Development Water Coat	LS	1
7	Water Development Water Coat	SV	614.8
8	Gravel gravel basecourse	SV	0
9	Gravel gravel basecourse	SV	50.91
10	Gravel gravel basecourse	SV	48.97
11	Gravel gravel basecourse	SV	785.56
12	Gravel gravel basecourse	SV	22.57
13	Gravel gravel basecourse	SV	62.51
14	Gravel gravel basecourse	SV	19.85
15	Gravel gravel basecourse	SV	11.71
16	Gravel gravel basecourse	SV	11.71
17	Gravel gravel basecourse	SV	11.71
18	Gravel gravel basecourse	SV	11.71
19	Gravel gravel basecourse	SV	11.71
20	Gravel gravel basecourse	SV	11.71
21	Gravel gravel basecourse	SV	11.71
22	Gravel gravel basecourse	SV	11.71
23	Gravel gravel basecourse	SV	11.71
24	Gravel gravel basecourse	SV	11.71
25	Gravel gravel basecourse	SV	11.71
26	Gravel gravel basecourse	SV	11.71
27	Gravel gravel basecourse	SV	11.71
28	Gravel gravel basecourse	SV	11.71
29	Gravel gravel basecourse	SV	11.71
30	Gravel gravel basecourse	SV	11.71
31	Gravel gravel basecourse	SV	11.71
32	Gravel gravel basecourse	SV	11.71
33	Gravel gravel basecourse	SV	11.71
34	Gravel gravel basecourse	SV	11.71
35	Gravel gravel basecourse	SV	11.71
36	Gravel gravel basecourse	SV	11.71
37	Gravel gravel basecourse	SV	11.71
38	Gravel gravel basecourse	SV	11.71
39	Gravel gravel basecourse	SV	11.71
40	Gravel gravel basecourse	SV	11.71
41	Gravel gravel basecourse	SV	11.71
42	Gravel gravel basecourse	SV	11.71
43	Gravel gravel basecourse	SV	11.71

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
96	TR8 S/S 6-10' Deep SDR 35, inc. video taping and pressure testing lines	LF	3804
97	TR8 S/S 10-18' Deep SDR 35, inc. video taping and pressure testing lines	LF	0
98	TR8 S/S 18-30' Deep SDR 35, inc. video taping and pressure testing lines	EA	12
99	TR8 S/S 30-40' Deep SDR 35, inc. video taping and pressure testing lines	EA	1
100	TR8 S/S 40-50' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
101	TR8 S/S 50-60' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
102	TR8 S/S 60-70' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
103	TR8 S/S 70-80' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
104	TR8 S/S 80-90' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
105	TR8 S/S 90-100' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
106	TR8 S/S 100-110' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
107	TR8 S/S 110-120' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
108	TR8 S/S 120-130' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
109	TR8 S/S 130-140' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
110	TR8 S/S 140-150' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
111	TR8 S/S 150-160' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
112	TR8 S/S 160-170' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
113	TR8 S/S 170-180' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
114	TR8 S/S 180-190' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
115	TR8 S/S 190-200' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
116	TR8 S/S 200-210' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
117	TR8 S/S 210-220' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
118	TR8 S/S 220-230' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
119	TR8 S/S 230-240' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
120	TR8 S/S 240-250' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
121	TR8 S/S 250-260' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
122	TR8 S/S 260-270' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
123	TR8 S/S 270-280' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
124	TR8 S/S 280-290' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
125	TR8 S/S 290-300' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
126	TR8 S/S 300-310' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
127	TR8 S/S 310-320' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
128	TR8 S/S 320-330' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
129	TR8 S/S 330-340' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
130	TR8 S/S 340-350' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
131	TR8 S/S 350-360' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
132	TR8 S/S 360-370' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
133	TR8 S/S 370-380' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
134	TR8 S/S 380-390' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
135	TR8 S/S 390-400' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
136	TR8 S/S 400-410' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
137	TR8 S/S 410-420' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
138	TR8 S/S 420-430' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
139	TR8 S/S 430-440' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
140	TR8 S/S 440-450' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
141	TR8 S/S 450-460' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
142	TR8 S/S 460-470' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
143	TR8 S/S 470-480' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
144	TR8 S/S 480-490' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
145	TR8 S/S 490-500' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
146	TR8 S/S 500-510' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
147	TR8 S/S 510-520' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
148	TR8 S/S 520-530' Deep SDR 35, inc. video taping and pressure testing lines	EA	2
149	TR8 S/S 530-540' Deep SDR 35, inc. video taping and pressure testing lines	EA	2

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
144	Street Light System (35-40' poles, LED fixture 400W HPS Equivalent, incl	LS	1
145	Dry Utility Main TR8	LF	12035
146	Gas	LS	1
147	Electricity	LS	1
148	Telephone	LS	1
149	Cable	LS	1

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
143	Retaining Walls	SF	0

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
44	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
45	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
46	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	27.72
47	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
48	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	2
49	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	238
50	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	31
51	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	4
52	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
53	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	0
54	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
55	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
56	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	0
57	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	0
58	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	10
59	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	10
60	1/2" x 3/4" x 1/8" w/ restriants and fittings	SV	42.22
61	1/2" x 3/4" x 1/8" w/ restriants and fittings	EA	4
62	1/2" x 3/4" x 1/8" w/ restriants and fittings	LF	0
63	1/2" x 3/4" x 1/8" w/ restriants and fittings	SV	3

ITEM NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY
107	Trees - Large Deciduous (12-17" 3" caliper)	EA	0
108	Trees - Large Evergreen (8-10' DBH)	EA	0
109	Small Trees / Shrubs (1.5' gal)	EA	0
110	Shrubs (1.5' gal)	EA	0
111	Shrubs (1.5' gal)	EA	0
112	Shrubs (1.5' gal)	EA	0
113	Shrubs (1.5' gal)	EA	0
114	Shrubs (1.5' gal)	EA	0
115	Shrubs (1.5' gal)	EA	0
116	Shrubs (1.5' gal)	EA	0
117	Shrubs (1.5' gal)	EA	0
118	Shrubs (1.5' gal)	EA	0
119	Shrubs (1.5' gal)	EA	0
120	Shrubs (1.5' gal)	EA	0
121	Shrubs (1.5' gal)	EA	0
122	Shrubs (1.5' gal)	EA	0
123	Shrubs (1.5' gal)	EA	0
124	Shrubs (1.5' gal)	EA	0
125	Shrubs (1.5' gal)	EA	0
126	Shrubs (1.5' gal)	EA	0
127	Shrubs (1.5' gal)	EA	0
128	Shrubs (1.5' gal)	EA	0
129	Shrubs (1.5' gal)	EA	0
130	Shrubs (1.5' gal)	EA	0
131	Shrubs (1.5' gal)	EA	0</

Form: 23870

Santa Fe

Started at: 7/21/2023 05:32 PM - Finalized at: N/A

#### Page: Application Tips

- **A word version of this application is available for download, at [bit.ly/BFCappFall2023](https://bit.ly/BFCappFall2023) (<https://bit.ly/BFCappFall2023>) (last updated 5/17/23)** - that document highlights all new and updated questions on the application since the previous round, and can also be useful to review all the dependent fields that are hidden on the online application until their corresponding parent answer option is selected. **\*Applicants are STRONGLY encouraged to download the latest version of this document when each new round opens!\***
- Find a Glossary of Terms used in the BFC Application here (<https://docs.google.com/spreadsheets/d/11K31XfewKWUCVr6BuLJvooO8A7JKfUziGBj0hzxqXjE/edit#gid=0>). If you come across a term on the application that is not listed on the glossary that you have questions about, please email [bfa@bikeleague.org](mailto:bfa@bikeleague.org).
- **The Bicycle Friendly Community online application now allows multiple user accounts to collaborate** on a single application! To add a collaborator, click on "Manage Collaborators" in the upper right-hand corner of your online application. Learn more on our Application Portal FAQs (<https://bicyclefriendly.secure-platform.com/a/page/FAQs>).
- The application will refer to your type of jurisdiction as 'community' throughout the application, for a lack of a better term. So when the application asks about bicycle amenities, services and other resources in your community, **please only list what is provided within your jurisdiction's boundaries**. However, if there is a significant bicycle amenity or activity close by that benefits your community, you can tell us about it in the bonus point question at the end of the appropriate 'E' section.
- The application is designed **for communities of all sizes**. The conditions that make your community unique -- size, type, location, climate, demographics -- are important when determining how to best encourage and support bicycling, and will be taken into consideration when we review your application.
- **It is not necessary to be able to check every box on this application to earn a BFC designation**. We've provided a comprehensive menu of all the ways a community can be bicycle-friendly, and some options are more valuable than others, or more relevant to some communities than others, but we don't expect any community to do everything on the list.
- Unless a question specifically asks about plans for the future, **only check boxes for things that are already being done**. So if several improvements for bicyclists are still in the planning stage, you can either wait a year or two before you apply to increase your chances to receive an award, or you can apply now to benchmark and watch your community move up in award levels in the future (which can be a powerful way to show the impact of investments).
- If your community is doing something that isn't listed in the checkboxes, or that goes above and beyond any of the check box options, please tell us about it! Check "other" on the appropriate question, or **use the bonus point questions at the end of each 'E' section and the 'Final Overview' section at the end of the application to give us more details**. This not only helps us to better understand your community, it also helps improve the program by identifying new trends and best practices.
- If you would like to share any documents such as a community bike map or a file with photos with the reviewers, **please include a link or upload the file** either under the relevant question (if possible) or in the designated space at the end of the Final Overview section. Applicants can compile several photos into a single zip file to save space on their application.
- Don't be shy to **tell us about your community's weaknesses**. This gives us a more accurate snapshot of your community, and displays that you are critically evaluating the community's internal efforts, which is an important component of the final 'E', Evaluation & Planning.
- **A note about the impact of the COVID-19 Pandemic:** We recognize that many communities had to cancel or severely alter their in-person classes and events due to the COVID-19 pandemic over the last several years. On questions that ask about in-person activities that happen "at least annually" you can check the box for activities that typically happened annually before the pandemic, and that have resumed or are planned to be resumed when it is safe to do so.
- If you need help with the online application, visit our Application Portal FAQs page here (<https://bicyclefriendly.secure-platform.com/a/page/FAQs>) or contact us at [bfa@bikeleague.org](mailto:bfa@bikeleague.org).

#### Page: Application Intro

##### Community Name:

Santa Fe

##### Has the community applied to the Bicycle Friendly Community program before?

Returning Application > Renewing at Current Award Level

##### What year was the community's most recent BFC application?

2017

##### What was the result of the community's most recent BFC application?

Silver

*If awarded, the following links will appear on your BFA Award Profile on the League's Connect Locally Map (<http://bikeleague.org/bfa/search/map?bfaq=>) and used to promote your community through League social media channels.*

##### Community Website:

<http://www.santafenm.gov> (<http://www.santafenm.gov>)

**Community's Twitter URL:**

**Community's Facebook URL:**

<https://www.facebook.com/SantaFeGov/> (<https://www.facebook.com/SantaFeGov/>)

**Community's Instagram URL:**

**Community's Flickr or other public photo sharing URL:**

**Page: Contact Information**

**Applicant First Name**

Romella

**Applicant Last Name**

Glorioso-Moss

**Job Title**

Capital Projects Manager

**Department**

Public Works

**Employer**

City of Santa Fe, New Mexico

**Street Address (No PO Box, please)**

737 Agua Fria Street

**City**

Santa Fe

**State**

New Mexico

**Zip**

87501

**Phone #**

5059556623

**Applicant Email Address**

rsglorioso-moss@santafenm.gov

**Did you work with any other local government agencies, departments, or city staff on this application?**

Yes

**Please list up to 10 additional government agency contacts.**

**Agency Contacts**

Erick Aune  
Metropolitan Planning Organization  
Officer  
ejaune@santafenm.gov

Leah Yngve  
Santa Fe MPO  
MPO Transportation Planner  
lyngve@santafenm.gov

Daniel Alvarado  
City of Santa Fe  
Planner Senior  
djalvarado@santafenm.gov

**Did you work with any local advocacy organizations or citizen volunteers on this application?**

Yes

**Please list up to 10 additional local advocacy contacts.**

**Advocacy Contacts**

Tim Rogers  
Santa Fe Conservation Trust  
Trails Program Manager  
tim@sfct.org

Jennifer Wellington  
Bike Santa Fe  
Member

Paloma  
Chainbreaker Collective  
paloma@chainbreaker.org

**Are there other local bicycle, active transportation, or transportation equity advocacy groups in your community not already identified?**

No

*(For internal use only.)*

**Name:**

Alan Webber

**Title:**

Mayor

**Email:**

mayor@santafenm.gov

**Street Address**

200 Lincoln Avenue

<b>City</b> Santa Fe
<b>State</b> New Mexico
<b>Zip</b> 87504
<b>Page: Community Profile pt. 1</b>
<i>Please note: The application will refer to your type of jurisdiction as 'community' throughout the application, which should not include any bicycle amenities, services and other resources outside your boundaries.</i>
<b>A1. Name of Community:</b> Santa Fe
<b>A2. Name of County/Borough/Parish:</b> Santa Fe
<b>A3. State:</b> New Mexico
<b>A4. Link to map of community boundaries:</b> <a href="https://www.google.com/maps/place/Santa+Fe,+NM/">https://www.google.com/maps/place/Santa+Fe,+NM/</a> ( <a href="https://www.google.com/maps/place/Santa+Fe,+NM/">https://www.google.com/maps/place/Santa+Fe,+NM/</a> )
<b>A5. Type of Jurisdiction</b> Town/City/Municipality
<b>A6. Size of community</b> 52.5
<b>A7. Total Population:</b> 87505
<b>A8. Population Density:</b> 1675 For definitions, see FHWA Bikeway Selection Guide (page 20) ( <a href="https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf">https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf</a> ).
<b>A9a. Rural</b> 1-24%
<b>A9b. Rural Town</b> 0%
<b>A9c. Suburban</b> 25-49%
<b>A9d. Urban</b> 25-49%
<b>A9e. Urban Core</b> 1-24%
<b>A10. What is the street network density of the community?</b> 5.1-10.0
<b>A11. What is the average intersection density?</b>

**A12. Do any of the following significant physical barriers to cycling exist in your community?**

Major highways or divided arterials with no or limited crossings, Topographical challenges (e.g. very hilly community), Geographical challenges (e.g. large or sprawling community), Extreme cold/heavy snow, Other seasonal or weather challenges, Bridges that are inaccessible or unsafe for cyclists, Railroad corridors, Other

**A12a. If other, please describe.**

Numerous arroyos & suburban street design limit connectivity

**Page: Community Profile pt. 2**

Find the following information for your community at: <https://data.census.gov/> (<https://data.census.gov/>)

**Click here for detailed instructions to find answers to each question below:**

**A13. Census Profile link:**

[https://data.census.gov/profile/Santa\\_Fe\\_city,\\_New\\_Mexico?g=160XX00US3570500](https://data.census.gov/profile/Santa_Fe_city,_New_Mexico?g=160XX00US3570500) ([https://data.census.gov/profile/Santa\\_Fe\\_city,\\_New\\_Mexico?g=160XX00US3570500](https://data.census.gov/profile/Santa_Fe_city,_New_Mexico?g=160XX00US3570500))

*\*\*NOTE: this question has been updated in February 2023 to better reflect the updated format presented at <https://data.census.gov/> (<https://data.census.gov/>).*

**A14a. Median Age in community:**

42.5

**A14b. Older Population (Percent of Population that is age 65 and older):**

16045

**A15. Percent of the Population that Speaks a Language Other Than English at Home:**

35.5

**A16. Percent of population that is Foreign Born:**

11.2

**A17. Median Household Income:**

60517

**A18. Poverty Rate:**

12.7

**Enter numbers only - answer must be in ##.## format.**

**A19a. Total Bicycle Commuters:**

1

**A19b. Male Bicycle Commuters:**

**A19c. Female Bicycle Commuters:**

**A20. Percent of Household with No Vehicles Available:**

4.2

***\*Note:** The League recognizes that not every disability is visible to others, and that not every person with a permanent or temporary mobility or accessibility need identifies as 'disabled'. Whether from a cognitive, sensory, or physical disability, or from age, temporary illness, or injury, there are people in every community who face a range of mobility challenges for whom a bike or cycle may open a world of possibilities to increase accessibility.*

*For examples, see "Disabled People Ride Bikes (and Trikes, and Tandems and Recumbents)!" (<https://rootedinrights.org/video/disabled-bikers/>) a short film produced by Rooted in Rights (<https://rootedinrights.org/>).*

*We invite BFC applicants to consider what visible and invisible disabilities and other mobility needs may exist in your community, and if you aren't already, to use the BFC application as a starting place to engage directly with people with disabilities and other mobility needs in your community to ensure that your bicycle network, classes, events, and rides are inclusive and accessible to all.*

**A21a. Total percent of "Disabled Population":**

12.6

**A21b. Percent of population with a hearing difficulty:**

4.2

**A21c. Percent of population with a vision difficulty:**

2.8

**A21d. Percent of population with a cognitive difficulty:**

5.2

**A21e. Percent of population with an ambulatory difficulty:**

6.4

*Enter numbers only - answers must be in ##.## format.*

**A22a. White:**

50.1

**A22b. Black or African American:**

0.9

**A22c. American Indian and Alaska Native:**

2

**A22d. Asian:**

1.7

**A22e. Native Hawaiian and Other Pacific Islander:**

0

**A22f. Some other race:**

18.2

**A22g. Two or more races:**

27

**A22h. Hispanic or Latino (of any race):**

50.2

**A23. If you have any other socioeconomic or demographic data from your community that you feel is significant for the BFC review team to better understand your community, please use this space to describe.**

**Page: Engineering**

**B1. Does your community currently have any of the following policies in place? Check all that apply.**

Local Complete Streets resolution , State, County, or other regional Complete Streets ordinance or resolution that only affects some roads within community

**B1a2. What year was the local Complete Streets resolution passed?**

2022

**B1b2. Please provide a link to the local Complete Streets resolution.**

[https://santafenm.gov/media/ordinances\\_resolutions/2022-46\\_web.pdf](https://santafenm.gov/media/ordinances_resolutions/2022-46_web.pdf) ([https://santafenm.gov/media/ordinances\\_resolutions/2022-46\\_web.pdf](https://santafenm.gov/media/ordinances_resolutions/2022-46_web.pdf))

**B1c2. Since the passing of the local resolution, what percentage of the implemented road projects (where bicycle facilities were considered) have included bicycle facilities?**

More than 75%

**B1b3. Please provide a link to the state, county, or regional Complete Streets ordinance or resolution.**

[https://www.santafecountynm.gov/documents/ordinances/Resolution\\_2022-036.pdf](https://www.santafecountynm.gov/documents/ordinances/Resolution_2022-036.pdf) ([https://www.santafecountynm.gov/documents/ordinances/Resolution\\_2022-036.pdf](https://www.santafecountynm.gov/documents/ordinances/Resolution_2022-036.pdf))

**B2. Does your community have bicycle facility selection criteria that increases separation and protection of bicyclists based on levels of motor vehicle speed and volume?**

Yes

**B2a. Please describe.**

Santa Fe Metropolitan Bicycle Master Plan Appendix A Santa Fe Metropolitan Bicycle Design Toolkit

**B3. Has your community adopted a design manual or guidelines that establish minimum standards for the design of safe, comfortable, and accessible bicycle facilities?**

Yes

**B3a. Please provide a link to your community's bicycle facility design manual or guidelines.**

[https://library.municode.com/nm/santa\\_fe/codes/code\\_of\\_ordinances?nodeId=CH14LADE\\_ART14-9INDEIMDEST\\_14-9.2STIMDEST](https://library.municode.com/nm/santa_fe/codes/code_of_ordinances?nodeId=CH14LADE_ART14-9INDEIMDEST_14-9.2STIMDEST)  
([https://library.municode.com/nm/santa\\_fe/codes/code\\_of\\_ordinances?nodeId=CH14LADE\\_ART14-9INDEIMDEST\\_14-9.2STIMDEST](https://library.municode.com/nm/santa_fe/codes/code_of_ordinances?nodeId=CH14LADE_ART14-9INDEIMDEST_14-9.2STIMDEST))

**B3b. Do/es the manual or guidelines incorporate, reference, or follow any of the following standards? Select all that apply**

Bike facility design standards that incorporate Universal Design principles by addressing the needs of all users and mobility types in public rights-of-way, FHWA Bikeway Selection Guide, FHWA Small Town and Rural Multimodal Network Guide, NACTO Urban Bikeway Design Guide, NACTO Urban Street Design Guide, AASHTO Guide for the Development of Bicycle Facilities, 4th Edition

**B4. Does your community currently have any of the following land use or development policies in place that promote shorter distances between homes and destinations?**

Mixed-use zoning or incentives, Planned Unit Development zoning, Transit Oriented Development ordinance or program, Form-based/design-based codes, Connectivity policy or standards, Affordable Housing policy or plan, Infill development incentives, Urban Growth Boundary or similar

**B5. Does your community currently have any of the following policies or ordinances in place related to motor vehicle parking or traffic?**

Paid public car parking , Shared-parking allowances

**B6. Does your community have any of the following additional policies or standards that support the development of bicycle infrastructure?**

None of the above

**B7. Does your community have any of the following other policies, standards, or other barriers that limit the development of bicycle infrastructure?**

State-level policies that limit or complicate the development of bike infrastructure

**B7b. Please describe the state-level policy or requirement.**

State highway access management manual is used as default design standard for local roads. State paving practices supersede local resolutions against partial paving of shoulders.

**B8. What policies or programs are in place to ensure that high quality bike parking is available throughout the community?**

Bike parking ordinance for existing buildings specifying amount and location , Bike parking ordinance for all new developments specifying amount and location , Incentives, requirements, or other program(s) to encourage multi-family residential buildings and properties to provide secure bike parking and other end-of-trip facilities for residents and guests, Incentives, requirements, or other program(s) to encourage local schools to provide secure bike parking and other end-of-trip facilities for students and employees, Incentives, requirements, or other program(s) to encourage local retail such as grocery stores to provide secure bike parking and other end-of-trip facilities for customers and employees, Ordinance that allows on-street bike parking/bicycle corrals , Ordinance that allows bike parking to substitute for car parking

**B9. Does your community have bicycle parking design standards that establish minimum requirements for the quality, security, and design of bike parking?**

Yes

**B9a. Please provide a link to your community's bicycle parking design standards.**

[https://library.municode.com/nm/santa\\_fe/codes/code\\_of\\_ordinances?nodeId=APXCHAPTER\\_14\\_LAND\\_DEVELOPMENT\\_CH14AP\\_EXHIBIT\\_DBIRASTDI](https://library.municode.com/nm/santa_fe/codes/code_of_ordinances?nodeId=APXCHAPTER_14_LAND_DEVELOPMENT_CH14AP_EXHIBIT_DBIRASTDI)  
([https://library.municode.com/nm/santa\\_fe/codes/code\\_of\\_ordinances?nodeId=APXCHAPTER\\_14\\_LAND\\_DEVELOPMENT\\_CH14AP\\_EXHIBIT\\_DBIRASTDI](https://library.municode.com/nm/santa_fe/codes/code_of_ordinances?nodeId=APXCHAPTER_14_LAND_DEVELOPMENT_CH14AP_EXHIBIT_DBIRASTDI))

**B9b. Do your community's bicycle parking design standards meet any of the following?**

Other

**B9b2. If other, please describe.**

None

**B10. What percentage of public and private bike racks conform with APBP guidelines?**

26-50%

**B11. What, if any, end-of-trip facilities are available to the general public in your community?**

Public bicycle repair or fix-it stations, Public air pumps, Public uncovered bike racks, Public bicycle lockers or similar individual secure bike parking spaces, On-street bike corrals

**B12. Has your community taken any of the following steps to increase the accessibility and equitable distribution of end-of-trip facilities, including bike parking?**

None of the above

*The Bicycle Friendly Community program has updated our questions around on- and off-street bicycle facilities with the goal of encouraging communities to focus on building cohesive, connected bicycle networks, as opposed to piecemeal bike facilities. This update reflects the Safe System Approach ([https://safety.fhwa.dot.gov/zerodeaths/docs/FHWA\\_SafeSystem\\_Brochure\\_V9\\_508\\_200717.pdf](https://safety.fhwa.dot.gov/zerodeaths/docs/FHWA_SafeSystem_Brochure_V9_508_200717.pdf)) and emphasizes slow design speed and the importance of building safe, low-stress bicycle networks that are comfortable, equitable, and accessible to all cyclists.*

*The following section has been developed using national guidance and standards such as FHWA’s Bikeway Design Guide ([https://safety.fhwa.dot.gov/ped\\_bike/tools\\_solve/docs/fhwasa18077.pdf](https://safety.fhwa.dot.gov/ped_bike/tools_solve/docs/fhwasa18077.pdf)) and Small Town and Rural Design Guide ([https://www.fhwa.dot.gov/environment/bicycle\\_pedestrian/publications/small\\_towns/fhwahep17024\\_lg.pdf](https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/small_towns/fhwahep17024_lg.pdf)), as well as NACTO’s Designing for All Ages and Abilities contextual guide ([https://nacto.org/wp-content/uploads/2017/12/NACTO\\_Designing-for-All-Ages-Abilities.pdf](https://nacto.org/wp-content/uploads/2017/12/NACTO_Designing-for-All-Ages-Abilities.pdf)) and Urban Bikeway Design Guide (<https://nacto.org/publication/urban-bikeway-design-guide/>).*

*As guidance and best practices continue to advance around building safe, accessible, and equitable facilities for cycling, we will continue to follow data-driven standards and encourage every Bicycle Friendly Community to do the same.*

*We also strive to encourage and support every BFC applicant community to better inventory and document their current and planned bikeway facilities to help facilitate future growth in and investments of the network. For more information, see the League’s 2022 report, Benchmarking Bike Networks (<https://bikeleague.org/sites/default/files/Benchmarking-Bike-Networks-Report-final.pdf>).*

**B13. Please provide a map to show your community's current bicycle network, including all current on- and off- street bike facilities.**

I would like to link to an online map

**B13a. Bicycle Network Map URL:**

<https://santafempo.org/resources/bikeways-map/> (<https://santafempo.org/resources/bikeways-map/>)

**B13a1. Secondary Bicycle Network Map URL:**

**B13b. Optional comment field to provide more information about link(s):**

**B14. Bicycle Network Worksheet**

No File Uploaded

The following answers for B15a-e should be copied from the "Summary" tab of your completed BFC Bicycle Network Worksheet (see question B14 above).

**B15a. Total current Roadway Network:**

755

**B15b. Percentage of Roadway Network that is high-speed:**

**B15c. Percentage of Roadway Network that is low-speed:**

**B15d. Percentage of Roadway Network where speed limit is unknown:**

The following answers for B16a-h should be copied from the "Summary" tab of your completed BFC Bicycle Network Worksheet (see question B14 above). The worksheet also includes explanations for how these numbers are calculated (for example, what counts as "low-stress").

**B16a. Total current mileage of on-street bike facilities:**

**B16b. Total current mileage of off-street bike facilities:**

**B16c. Total current mileage of Bicycle Network (ALL on-street and off-street facilities):**

**B16d. Total current mileage of other markings and features:**

**B16e. Ratio of Total Current Bicycle Network to Roadway Network:**





**B27c. How are residents and visitors encouraged to combine cycling and public transportation?**

Cyclists can practice mounting their bike on a bus bike rack at community events, Information on bike racks/storage provided on transit schedules

*Exclude any private bike sharing systems that are limited to employees of a certain business or students of a certain university.*

**B28. Does your community currently have a community-wide bike sharing program that is open to the general public?**

No

Bike share programs are becoming popular and can contribute to making it possible for more people to choose to bike more often. In many communities bike share programs are large investments and it is important that they are sustainable in order to provide a dependable system for people in the community. While bike share programs are important and encouraged, having one is not essential to receiving a Bicycle Friendly Community award, particularly for smaller communities or non-urban communities.

**B29. Which of the following permanent recreational or educational bicycling facilities are available within your community boundaries?**

BMX track, Mountain bike park, Pump tracks, Bicycle-accessible skate park, Snow/Fat tire bike trails, Other

**B29a. If other, please describe.**

Extensive natural surface networks within City limits.

**B30. Please list all communities or jurisdictions that directly border your community, and their current BFC status.**

Santa Fe County, Bureau of Land Management, Agua Fria Traditional Village, US Forest Service, no BFC status

**B31. How, if at all, is your community coordinating with neighboring communities and other local jurisdictions (neighboring cities or towns, and/or the surrounding county or MPO/RPO) to ensure that people traveling by bike between jurisdictions will find a connected, cohesive network?**

City of Santa Fe through Bicycling and Pedestrian Advisory Committee has a County Representative who liaise and coordinates with Santa Fe County's Open Space and Trails Program. The Santa Fe MPO provides a forum for regional transportation decision making.

**B32. Has your community partnered with any neighboring jurisdictions or other local communities in support of legislative policies at the state, county, or regional level?**

No

**B33. Describe any other policies, amenities, infrastructure improvements or maintenance programs that your community provides or requires that create a comfortable and attractive bicycling environment for bicyclists.**

The City of Santa Fe (CoSF) has recently approved the E-Bike Ordinance, which is an amendment to the Uniform Traffic Ordinance which defines e-bikes and its legal operations. CoSF is also the first municipality in New Mexico to pass such an Ordinance.

**B34. If this is a renewing application, please summarize the biggest changes to your community's bicycle-related Engineering efforts or investments since your last BFC application.**

CoSF has recently put considerable investments into redesigning streets with bike lanes, buffered bike lanes, and sidewalks and additional miles of paved multi-use paths.

**Page: Education**

**C1. Do any public or private elementary schools offer regular bicycle education to students?**

Yes

**C1a. What percentage of your public and private elementary schools offer bicycle education?**

1-25%

**C1b. What kinds of bike education/curriculum is offered in elementary schools? Check all that apply.**

Learn to ride, ABC Quick Check, Bike handling skills, Traffic safety/rules of the road

**C1c. Are there any on-bike learning opportunities for students at these elementary schools?**

Yes

**C1c1. What type of on-bike cycling education is offered?**

Optional on-bike education

**C1c2. Approximately how many elementary school students receive on-bike education annually?**

200

**C1c3. Are bicycles provided to elementary school students by the school district, municipality, non-profit or other entity to allow every student the opportunity to participate in on-bike instruction?**

Yes, a limited number of bicycles are available for students in need

**C1c3a. If yes, does the fleet include adaptive bikes for elementary school students with physical and/or cognitive disabilities?**

No

**C1d. What actions, if any, have been taken to ensure this bike education is open, equitable, and accessible to all elementary school students?**

Majority of teachers are bilingual (English/Spanish). All of the focus schools for Safe Route to Schools program are designated 100% free and reduced lunch.

**C2. Do any public or private middle schools offer regular bicycle education to students?**

Yes

**C2a. What percentage of your public and private middle schools offer bicycle education?**

1-25%

**C2b. What kinds of bike education/curriculum is offered in middle schools? Check all that apply.**

Learn to ride, ABC Quick Check, Bike handling skills, Traffic safety/rules of the road, Bicycle maintenance

**C2c. Are there any on-bike learning opportunities for students at these middle schools?**

Yes

**C2c1. What type of on-bike cycling education is offered?**

Optional on-bike education

**C2c2. Approximately how many middle school students receive on-bike education annually?**

120

**C2c3. Are bicycles provided to middle school students by the school district, municipality, non-profit or other entity to allow every student the opportunity to participate in on-bike instruction?**

Yes, a limited number of bicycles are available for students in need

**C2c3a. Does the fleet include adaptive bikes for middle school students with physical and/or cognitive disabilities?**

No

**C2d. What actions, if any, have been taken to ensure this bike education is open, equitable, and accessible to all middle school students?**

Majority of teachers are bilingual (English/Spanish). All of the focus schools for Safe Route to Schools program are designated 100% free and reduced lunch.

**C3. Do any public or private high schools offer regular bicycle education to students?**

No

Offering bicycle education to students is extremely important to receiving a Bicycle Friendly Community award. If your community does not currently offer bicycle education opportunities to at least some students other portions of your application will need to be exceptional in order to receive an award. In order to receive higher award levels it is expected that bicycle education is available to some students at all education levels.

**C4. Outside of schools, how are children and youth taught safe cycling skills? Check all that apply.**

Bike clinics or rodeos, Youth development bike clubs or teams (including road, cross racing, BMX, mountain biking, or other recreational youth cycling clubs or teams that include educational component), Summer camps, Bicycle-related after school programming

**C4b. Please estimate how many total children and youth are reached annually in your community through these out-of-school efforts?**

75

**C5. Are bicycle safety or riding skills-related classes or hands-on instruction offered to adults in your community?**

Yes

**C5a. What type of classes are available for adults? Check all that apply.**

Classes that include on-bike instruction, Information sessions/workshops, Online live/virtual classes

**C5b. What topics are covered in these classes? Check all that apply.**

Introduction to bicycling/Learn to ride/Bike handling basics, Safe riding skills/habits, Bicycle maintenance, Sharing the road, trail, or path with vehicles or pedestrians, Bike commuting basics

**C5c. Who teaches these classes? Check all that apply.**

League Cycling Instructor, Local bike shop employee, Local bicycle advocate

**C5d. On average, how often are these classes offered?**

Annually or more frequently

**C5e. Are bicycles provided to adults by the community, non-profit, or other entity to allow every resident to participate in on-bike instruction?**

No

**C5f. Please estimate how many total adults are reached annually in your community through these classes?**

20

**C5g. How has the community made adult bike safety classes and other similar learning opportunities more inclusive and accessible for all members of the community?**

None of the above

**C6. Which of the following communications methods are used to share bicycle information with adults in your community, at least annually?**

Educational group rides, Bike-specific website or social media accounts for community, Community maps (print or digital), Handouts or brochures, Table or booth at community events

**C6b. If checked, please provide up to 3 links to your community's bike-specific website and/or social media account(s):**

<https://www.bikesantafe.org/> (<https://www.bikesantafe.org/>)

**C6b. optional second link:**

<https://santafempo.org/> (<https://santafempo.org/>)

**C6b. optional third link:**

<https://www.chainbreaker.org/> (<https://www.chainbreaker.org/>)

**C7. Which of the following information is shared using the methods checked above? Check all that apply.**

Safe riding skills/habits, Traffic Skills/Rules of the Road/Sharing the road with vehicles, Sharing trail or path or pedestrians (e.g. path or trail etiquette) , Route planning, Commuting tips and resources, Traffic laws/ rules of the road, Family biking/riding with children, Multi-modal/combining bikes and transit

**C8. In what ways are motorists in your community educated on sharing the road safely with bicyclists, at least annually?**

Public service announcements, Bicycle-related traffic signs installed (e.g. Share the Road, Bicycles May Use Full Lane, etc.)

**C9. Which of the following groups of professional drivers in your community routinely receive Bicycle Friendly Driver training or similar?**

None of the above

**C10. Are any of the following educational materials provided to community residents and/or businesses?**

None of the above

**C11. How many League Cycling Instructors (LCIs) are active (have taught a class in the last year) in your community?**

0

**C12. When was the last time your community hosted a LCI seminar to bring on new instructors?**

In the last 5 years

**C13. Are there any professional development opportunities, requirements, or incentives, for city agency staff to pursue bicycling safety education?**

None of the above

**C14. Do any of the above educational classes, resources, or programs for adults specifically focus on reaching any of the following historically-underrepresented groups?**

Seniors

**C14c. Please provide an example or describe how educational efforts have focused on reaching seniors.**

Programming through AARP.

**C15. Is any demographic or socioeconomic data collected about the participants/students and/or instructors/providers of any of the above learning opportunities offered in the community?**

No

**C16. What, if any, efforts has your community made to increase the diversity and representation of LCIs or other credentialed bike safety instructors in your community in the last 5 years?**

None

**C17. Do any of your community's LCIs or other bike safety instructors have any of the following additional trainings or certifications?**

None of the above

**C18. How else is the community working to make bike education more equitable, accessible, and inclusive to all members of the community?**

None of the above

**C19. Describe any other education efforts in your community that promote safe cycling.**

Santa Fe Fat Tires Society hosts a weekly children's bike ride in La Tierra Trails. These efforts include bicycle skills training that applies to all facets of cycling.

**C20. If this is a renewing application, please summarize the most significant changes to your community's bicycle Education efforts or investments since your last BFC application.**

Santa Fe's Safe Routes to School program has provided bike education in schools since 2021.

**Page: Encouragement**

**D1. Which of the following community-wide bicycle encouragement programs or policies exist in your community?**

None of the above

**D2. What other groups actively promote bicycling in the community? Check all that apply.**

Tourism Board, Other

**D2a. If other, please describe.**

**D3. Does your community actively promote the League of American Bicyclists' Bicycle Friendly Business (BFB) or Bicycle Friendly University (BFU) programs in your community?**

No

The Bicycle Friendly Business (<https://bikeleague.org/business>) and Bicycle Friendly University (<https://bikeleague.org/university>) programs can be great tools to increase ridership in your community. Learn more and find resources to help you promote these programs at [bikeleague.org/bfa/toolkit](https://bikeleague.org/bfa/toolkit) (<https://bikeleague.org/bfa/toolkit>).

**D4. What up-to-date mapping and route-finding information is available for your community? Check all that apply.**

Web-based route finding service, Printed/digital bicycle network map, Printed/digital mountain bike trails map, Printed/digital greenways and trails map

**D4a. Provide URL for web-based route finding service:**

<https://www.google.com/maps/@35.6614144,-105.9946496,13z?entry=ttu> (<https://www.google.com/maps/@35.6614144,-105.9946496,13z?entry=ttu>)

**D5. How is bicycling typically promoted in your community at least annually or regularly throughout the year?**

Celebrate National Bike Month (and/or a state-specific Bike Month), Bike to Work Day(s), Bike To School Day(s), Affinity or identity-based group rides or cycling events, Family-friendly group rides or events (e.g. riding with young children), Bike commuter events, Non-commuting related (i.e. errand-running) challenges and programs, Charity rides, Local business program that provides discounts for customers arriving by bicycle, Trail construction or maintenance days, Non-competitive, no-drop group rides, Triathlons and bicycle races, Publish a guide or calendar of community bicycle events, Bike valet parking at events

**D5a. Provide a link to your community's Bike Month or Bike to Work Day website:**

**D6. How does the municipality sponsor or actively support bicycle events in the community? Check all that apply.**

Organize event(s), Contribute in-kind funding (i.e. police presence, closing roads, etc.), Assist in promoting event(s)

**D7. Do any of the above events or encouragement efforts specifically focus on reaching any of the following historically-underrepresented groups?**

Women , Seniors

**D7a. Please provide an example or describe how encouragement efforts have focused on reaching women.**

**D7c. Please provide an example or describe how encouragement efforts have focused on reaching seniors.**

**D8. How else is the community working to make bike encouragement efforts more equitable, accessible, and inclusive to all members of the community?**

None of the above

**D9. Are there any organized social or recreational cycling clubs or groups for adults in your community?**

Yes

**D9a. Please provide details for each cycling club, group, or team that is active in your community.**

Social or Recreational Clubs or Groups

**D9b. Is there anything else you would like to share about the social or recreational cycling clubs or groups that are active in your community?**

**D10. Does your community have any of the following youth programs centered on encouraging cycling for children and youth?**

**D11. What public or private programs are in place to provide youth and/or adult bicyclists with necessary equipment and accessories?**

None of the above

**D12. What programs or services does your community use to reduce the risk of bicycle theft, beyond providing secure bike parking?**

**D13. What is the ratio of for-profit specialty bicycle retailers (shops dedicated primarily to selling bikes and bike-related equipment) to population within your community's boundaries?**

1 shop for every 1 -15,000 residents

**D14. Are there any bike co-ops or non-profit community bike shops within the community's boundaries?**

Yes

**D14a. Please provide the name and contact information for each co-op/non-profit community bike shop in your community.**

**D14b. Does the local government provide any of the following types of support for the bike co-op/non-profit community bike shop(s).**

Free bicycle safety accessories for distribution, e.g. helmets or lights, Provision of abandoned or impounded bicycles for resale

**D15. Has the community taken any steps to reduce vehicle miles traveled or to encourage biking among its own employees conducting work-related trips?**

**D16. Describe any other events, programs or policies your community has to encourage and promote bicycling.**

Chainbreaker Collective non-profit: affordable transportation for low-income population: <https://chainbreaker.org/brc/>  
<https://chainbreaker.org/help-santa-feans-ride-bus-pass-rebate/>  
<https://chainbreaker.org/brc-university/>

Bike Santa Fe- Advocacy and bicycling safety education  
[bikesantafe@gmail.org](mailto:bikesantafe@gmail.org)  
[bikesantafe.org](http://bikesantafe.org)  
<https://www.facebook.com/bikesantafe/>

Annual Community Health Fair/ bicycle and safety equipment giveaway: <http://www.stvin.org/>

Trails Alliance / trail maintenance volunteers and Community Cruises/ publicize, coordinate and lead themed promotional rides year-round:  
<https://www.sfct.org/trails>

Department of Tourism: [https://santa-fe.org/visiting Santa Fe/About Santa fe/Getting to and Around Santa Fe/ Bikeways and Trails/index.html](https://santa-fe.org/visiting-Santa-Fe/About-Santa-Fe/Getting-to-and-Around-Santa-Fe/Bikeways-and-Trails/index.html)

Bike to Work celebration/Bike and Brew 2017 event: [outsidesantafe.com/events/](http://outsidesantafe.com/events/)  
<https://santafefattiresociety.org/>  
<https://www.biketoworksantafe.com/>

Seniors on Bikes: [sfsob.cosine-santafe.com](http://sfsob.cosine-santafe.com)

Pedal Queens: non profit women's cycling club in Santa Fe <https://www.facebook.com/Pedal-Queens-171124046276109/>

Cranksgiving fundraiser / food gathering by bicycle [https://www.facebook.com/pg/CranksgivingSantaFe/about/?ref=page\\_internal](https://www.facebook.com/pg/CranksgivingSantaFe/about/?ref=page_internal)

Loops Slow-ride group <https://www.facebook.com/LoopsSantaFe/>

Custom bike racks non-profit: [www.artrackssantafe.org](http://www.artrackssantafe.org)

Bike Valet volunteer coordination for City events and Annual Bike Santa Fe bike swap: <http://bikesantafe.org/events.html>

Santa Fe Century bike donation program for kids who read collaboration with Santa Fe Public Library: <http://www.santafecentury.com/>

National Bike to School Day in May (Bike Month): Four schools participated in 2017 as listed at <http://www.walkbiketoschool.org/registration/pastevents.php?sid=NM&yid=2017&cityid=SANTA%20FE&etid=bttd> and described at <http://www.sfct.org/trails/sfct-trail-events-in-2017/bike-to-school-celebration-may-16>

NM Walk and Roll to School Day in October: Primarily a Bike-to-School event for Cesar Chavez Elementary <http://www.walkbiketoschool.org/registration/pastevents.php?sid=NM&yid=2016&cityid=SANTA%20FE&etid=wtsd>

City of Santa Fe Bicycle and Trails Advisory Committee: BTAC is a 8 member committee, currently chaired by City Councilor, Joseph Maestas. Members are recommended by the Mayor and approved by City Council. Membership is based upon a balance of city council district residencies and recently with the option to include one resident of the county. There is no compensation for serving on this committee. BTAC was established in 2003 to bring direct community involvement and encouragement to the city's plans and policies related to bicycle and pedestrian trails, and to advise the governing body on such matters. Monthly meetings are open to the public.

Santa Fe Metropolitan Planning Organization:  
City of Santa Fe Bike to Work Week 2017: <http://santafempo.org/bicycle-master-plan/bike-to-work-week/>

The MPO is actively working with City GIS staff to develop an Interactive GIS map solely for the transportation network including bicycle facilities. You can view the beginning here. They will soon have the ADA sidewalk inventory to fold into this along with more detail around existing and proposed projects.  
[http://santafe.geocortex.com/Html5Viewer/Index.html?](http://santafe.geocortex.com/Html5Viewer/Index.html?configBase=http://santafe.geocortex.com/Geocortex/Essentials/REST/sites/Santa_Fe_MPO_Projects/viewers/Viewer_for_Santa_Fe/virtualdirectory/Re)  
[configBase=http://santafe.geocortex.com/Geocortex/Essentials/REST/sites/Santa\\_Fe\\_MPO\\_Projects/viewers/Viewer\\_for\\_Santa\\_Fe/virtualdirectory/Re](http://santafe.geocortex.com/Geocortex/Essentials/REST/sites/Santa_Fe_MPO_Projects/viewers/Viewer_for_Santa_Fe/virtualdirectory/Re)

**D17. If this is a renewing application, please summarize the most significant changes to your community's bicycle Encouragement efforts or investments since your last BFC application**

**Page: Evaluation & Planning**

**E1. Is there a paid bike program manager position or similar primary point of contact for bicycling issues at your local government?**

No primary bike program manager, but bike responsibilities are in multiple employees' job descriptions

**E2. Is there a Safe Routes to School Coordinator position?**

Yes, there is one or more part-time, paid position(s) (can include a FT employee with SRTS as partial responsibility)

**E3. How many paid government employees (including the Bicycle Program Manager and the Safe Routes to Schools Coordinator), expressed in full-time equivalents (FTE), work on bicycle issues in your community?**

Please download the FTE worksheet by clicking here ([https://bit.ly/BFC\\_E3\\_FALL23](https://bit.ly/BFC_E3_FALL23)) to find guidance on what roles to include, how to calculate the estimates for each role, and how to tally your total FTE estimate. When you are finished, please save your worksheet with the naming convention "BFC\_Fall 2023\_E3 FTE Worksheet\_ YOUR COMMUNITY NAME.xlsx" and upload the worksheet below. If you have any questions, please contact [bfa@bikeleague.org](mailto:bfa@bikeleague.org).

**E3a. Please upload your completed FTE worksheet here:**

No File Uploaded

**E4. Does your local government provide or cover the cost of any of the following professional development opportunities for employees who have bicycle-related responsibilities?**

None of the above

**E5. Does your community have an officially-recognized Bicycle Advisory Committee?**

Yes

**E5a. How often does the committee meet?**

Monthly or more frequently

**E5b. First Name:**

Michael

**E5b. Last Name:**

Garcia

**E5b. Email:**

mjgarcia@santafenm.gov vc vuj

**E5c. Which of the following groups are represented on the Bicycle Advisory Committee?**

Local bike advocacy organization or citizen advocate(s), Engineering Department, County/MPO/RPO transportation officials, Other

**E5c1. If other groups are represented, please describe.**

Santa Fe Conservation Trust

**E5d. Please describe what, if any, efforts the community has made to ensure that your Bicycle Advisory Committee is inclusive and representative of the full range of cyclists in your community?**

**E6. What tools or systems are in place to inform, consult, involve, collaborate, and/or empower citizens in your community to be engaged in the bicycle planning process?**

None of the above

**E7. How does your community actively seek public input and engage local residents throughout the ongoing planning process for bicycle infrastructure improvements?**

Project- or plan-specific community advisory or citizen oversight task group(s) or committee(s) appointed by local government, Public Meetings , Tabling at other community events/destinations, Pop-up bike infrastructure or demonstration projects with a feedback collection mechanism

**E8. How have these engagement opportunities been made more accessible and inclusive to increase the diversity and representation of opinions and perspectives heard?**

Public meetings are offered both virtually and in-person, Sign language interpreters and/or captioning is/are made available upon request

**E9. Does your community have a comprehensive bicycle master plan or similar section in another document?**

Yes

**E9a. What year was the plan originally adopted?**

2012

**E9a1. Has the plan been updated or revised since it was first adopted?**

No, the originally adopted plan is the most recent version

**E9b. Provide a link to the current plan.**

<http://santafempo.org/documents/bicyclemasterplan> (<http://santafempo.org/documents/bicyclemasterplan>)

**E9c. Is there a dedicated budget for implementation of the plan?**

No

**E9d. Does your plan include a specific, measurable goal to increase bicycle facilities?**

Yes

**E9d1. Please list the goals and/or what page(s) and paragraph(s) of the plan linked above the goal(s) can be found on.**

**E9e. Since the adoption or most recent update of the plan, approximately what percentage of the plan has been implemented?**

Unknown

**E9f. If there is a website to show your community's progress on implementation of the bike plan, please provide the link here:**

**E9g. What steps has your community taken to specifically engage with women, People of Color, people with disabilities, non-English speakers, low-income residents, and/or any other marginalized communities to invite their input and participation on the development and implementation of the bike plan?**

Santa Fe is a very diverse community. The entire Land Use Department and Public Works Departments reach out to minority, non-English speaking, and or low income populations. This is built into the systems in all facets of development in Santa Fe, NM.

**E10. Does your community pass a budget on a regular basis? (e.g. annual capital budget, operating budget, etc.)**

Yes

**E10a. If yes, please provide a brief description and link(s) to the most recent budget(s) passed.**

The City's 2023-24 Budget can be found at: [https://santafem.gov/FY24\\_Adopted\\_Budget\\_Book.pdf](https://santafem.gov/FY24_Adopted_Budget_Book.pdf)

**E10b. Please upload the most recent transportation budget for your community.**

No File Uploaded

**E10c. On average, over the last 5 years, what percentage of your community's total annual transportation budget was invested in bicycle projects?**

**E10d. Optional: Please tell us how you calculated this answer or what you included in the calculation?**

**E11. Is bicycle-related funding consistently specifically allocated to historically underinvested or underrepresented areas of your community?**

No

**E12. Has your community conducted any kind of connectivity analysis for the bicycle network?**

**E13. Does your community have a performance measurement program for biking or active transportation infrastructure?**

**E14. How does your community collect information on bicycle usage? Check all that apply.**

Temporary or mobile automated /electronic bicycle counters

*Additional files may be uploaded at the end of the application.*

**E14a. Utilitarian ridership data collected locally (e.g. bicycle trips for commuting, running errands, transportation, etc.)**

No File Uploaded

**E14b. Recreational ridership data collected locally (e.g. rides solely for exercise or fun.)**

No File Uploaded

**E14c. Demographic ridership data collected locally (e.g. rider age, race, gender, etc.)**

No File Uploaded

**E14d. School ridership data collected locally (e.g. rides by or with K-12 or younger children – either riding on their own or being carried in a child seat, trailer, etc.)**

No File Uploaded

**E14e. Other ridership data (e.g. any other bicycle ridership data collected locally that doesn't fall under the above categories.)**

No File Uploaded

**E15. What is your best estimate for your community's current bicycle mode share?**

**E16. Does your community establish target goals for bicycle use? (e.g. a certain level of bicycle mode share)**

Yes

**E16a. Please list or describe these goals.**

See 2019 Bicycle Master Plan

**E17. Does your community collect and track bicyclist crash data?**

Yes

**E17a. On average over the past five calendar years, how many bicyclists have been in a crash involving a motor vehicle annually?**

31

**E18. Does your community collect and track bicyclist "near misses"?**

No

**E19. On average over the past five calendar years, how many bicyclists have died due to a crash involving a motor vehicle annually?**

0

**E20. Has your community conducted any kind of analysis to identify High Injury Networks or to prioritize infrastructure investments based on bicycle crash data?**

No

**E21. Has the community set a goal or established a policy or plan to eliminate traffic fatalities within the next 20 years or less?**

Yes

**E21a. If yes, please state or summarize the goal, including timeframe.**

Vision Zero

**E21b. What is the primary method or mechanism the community has implemented to achieve this goal?**

**E21c. Please provide a link to any associated plan, policy, or program.**

**E22. Are there any local or statewide traffic ordinances or laws in place designed to improve bicyclists' safety in your community?**

It is illegal to park or drive in a bike lane (intersections excepted), Ban on cell phone use while driving , Ban on texting while driving, Safe passing distance law, It is illegal to harass a cyclist

**E23. Do any of the following local ordinances or enforcement practices exist in your community that place restrictions on cyclists or criminalize cycling?**

Local law requires bicyclists to use bike lanes when provided, Local law requires that bicyclists are required to ride as far to the right of the road as practicable without exceptions , Restrictions on sidewalk riding inside the Central Business District

**E24. Please describe any efforts in place to evaluate how equitably and effectively any of the laws, ordinances, or enforcement practices describe above are currently applied in the community.**

**E25. Have there been any efforts in the last 5 years to repeal or amend any local laws, ordinances, or enforcement practices that place(d) restrictions on cyclists or criminalize(d) any aspect of cycling or walking in your community?**

No

**E26. What kind of data is currently collected around traffic law enforcement stops, citations and/or arrests in your community?**

**E27. How is the data described above shared or made available to increase transparency and accountability around traffic law enforcement stops, citations, and arrests?**

**E28. What, if any, policies or practices does your community have in place to measure and eliminate racial bias in traffic law enforcement, including in-person and automated enforcement practices?**

**E29. Besides the Bicycle Friendly Community program, what other national programs does your community participate in to improve bicycling?**

None of the above

**E30. Describe any other efforts by your community to evaluate and/or plan for bicycle improvements that have not already been covered.**

Socioeconomic study prepared by the Santa Fe Metropolitan Planning Organization as a basis for planning in the 20 year horizon.

**E31. If this is a renewing application, please summarize the most significant changes to your community's bicycle Evaluation & Planning efforts or investments since your last BFC application.**

**Page: Equity & Accessibility**

The League defines (<https://bikeleague.org/content/equity-initiative>) "**Equity**" as the just and fair inclusion into a society in which everyone can participate and prosper. The goals of equity must be to create conditions that allow all to reach their full potential, by erasing disparities in race, income, ability, geography, age, gender and sexual orientation.

The principle of equity acknowledges that there are historically underserved and underrepresented populations, and that fairness regarding these unbalanced conditions is needed to assist equality in the provision of effective opportunities to all groups.

"**Accessibility**" refers to improving and increasing access and mobility options for everyone, including, and in particular, for people with disabilities. The League recognizes that not every disability is visible to others, and that not every person with a permanent or temporary mobility or accessibility need identifies as 'disabled'. Whether from a cognitive, sensory, or physical disability, or from age, temporary illness, or injury, there are people in every community who face a range of mobility challenges for whom a bike or cycle may open a world of possibilities to increase accessibility (<https://rootedinrights.org/video/disabled-bikers/>).

We firmly believe that *Equity & Accessibility* are the essential lenses through which all other BFC work must be viewed in order to achieve a Bicycle Friendly America for everyone.

As such, you will find questions and answer options related to improving equity & accessibility throughout the BFC application in each previous "E" section, in addition to this new dedicated "Equity & Accessibility" section.

These Equity & Accessibility updates have been developed thanks to the input and participation of hundreds of communities and local advocates with a wide range of lived experiences and areas of expertise, to whom we are very grateful.

The League presents this new section with the recognition that our work to listen, learn, and engage on the topics of Equity & Accessibility in the BFC program is not yet finished, and never will be. Equity is an ongoing practice and the League will continue to engage with partners, local advocates, and communities in the BFC program to incorporate new ideas, reflect new challenges, and continue to grow and learn. If you have questions, comments, or feedback about the updated BFC application, please email us at [bfa@bikeleague.org](mailto:bfa@bikeleague.org).

Thank you for continuing to learn and engage in this work along with us.

**F1. Does your local government have an internal Diversity, Equity, and Inclusion (DEI) or similar initiative, department, or position?**

No

**F2. Does your community have an officially-recognized DEI or Transportation Equity Committee or similar equity-related advisory committee or working group focused on equity or mobility justice issues?**

No

**F3. Does your local government have a dedicated Disability Services Department, Office of Disability Rights, or similar department(s) or role(s) dedicated to supporting and advocating for residents with disabilities?**

No

If your local governmental agency has 50 or more employees (full-time, part-time, or seasonal), it is required by federal law to have a designated ADA coordinator.

While your community may not have a Disability Services or Disability Rights office or department, the ADA coordinator may be someone to connect with during the bike planning process.

**If your agency is that large or larger, who is the ADA coordinator?**

**F4. What types of partnerships has the community established to reach new audiences and increase representation and inclusion in your education, encouragement, outreach and/or engagement processes?**

**F4a. For EACH category checked in F4, please list the partner group(s) and briefly describe or summarize the activity or partnership involving that group.**

**F5. Does your community collect or track any other bicycle-related data with socioeconomic and/or demographic details that hasn't already been mentioned elsewhere on this application?**

No

**F6. Has your community established any specific and measurable equity-related goals or performance measures that relate to bicycling?**

No

**F7. Has your community conducted any equity-centered analysis (e.g., social vulnerability assessment, equity matrix, index, or similar effort) as part of a community Bicycle Master Plan, Vision Zero Action Plan, Safe Routes to School Plan, ADA Transition Plan, or other similar planning effort or document?**

Yes

**F7a. If yes, please provide a brief description of assessment or efforts and any relevant links.**

Vision Zero Action Plan

**F7b. If yes, please summarize the ways this assessment has influenced the decision-making process for the implementation of the related plan or effort?**

**F8. Does your community regularly incorporate any of the following socioeconomic or demographic variables into any planning or decision-making processes for the development or prioritization of bike infrastructure projects?**

None of the above

**F8b. For any variable selected above in F8, please describe how the variable(s) have informed decision-making or prioritization around bicycle investments and project implementation. Please include any relevant links, if applicable, that could help us to better understand your methodology, and inspire other BFC applicants to learn from your community's process.**

**F9. Has your community adopted any of the following types of equity-related action plans?**

None of the above

**F10. Has the community developed any anti-displacement programs or strategies that relate to transportation investments?**

None of the above

The following fields (F11 & F12) are optional but can earn additional points toward a community's Equity & Accessibility score. Communities are encouraged to use these spaces to describe any efforts that have not already been covered on the application, and/or to provide additional details, links, or information about a corresponding checkbox elsewhere on the application.

**F11. Are there any other innovative strategies, programs, facilities, or initiatives in place to specifically support cyclists with any vision, hearing, mobility, and/or cognitive needs in your community, that have not already been covered on your BFC application?**

**F12. Are there any other innovative strategies programs, facilities or initiatives in place to make cycling more equitable and inclusive for any historically underrepresented or marginalized groups or individuals in your community, including women, people of color, non-English speakers, people with disabilities, non-drivers, young children, and/or seniors?**

**F13. If this is a renewing application, please summarize the most significant improvements to your community's bicycle-related Equity and Accessibility efforts or investments since your last BFC application.**

**Page: Final Overview**

**G1. What are the top three reasons your community has made bicycling a priority?**

Improved quality of life, Improving public health, Support Smart Growth or other growth management goals

**G2. Briefly describe the most positive outcome of your community's support for bicycling.**

Recreational and economic growth for the city and surrounding areas. The city has a reasonably complete bike travel and recreational network.

**G3. How is the local government specifically educating the community about how the benefits of biking align with the broader goals of the community?**

**G4. What are your community's greatest achievements in the last 12 months in the work to become more bicycle-friendly?**

**G5. What could be done differently in order to make bicycling safer, more enjoyable and/or more convenient in your community?**

Apply bike lane requirements in City Development Code (Chapter 14, see File 5 attached) to all public projects, not just private. Raise bike lanes or shoulders to a higher level of priority in retrofit efforts vs. other discretionary uses of the street cross section such as continuous turn lanes, on-street parking, over-sized curb extensions, and oversized built medians at crossings. Better enforcement of parking and blocking bicycle lanes.

**G6. What are the greatest challenges or barriers your community faces in taking action toward the efforts described in your response to G5?**

**G7. What specific bicycle-related improvements are planned in the next 12 months that directly affect your community?**

The City and partners have designed nearly five more miles of multi-use trails in the city that are not yet built. The following projects have been programmed for construction and are anticipated for completion before the end of 2017:  
Acequia Trail: Underpass of St. Francis Dr. to connect to Railyard Park  
Acequia Trail: Two bridges over acequia (irrigation ditch) to tie in Kathryn and Onate dead-ends  
Galisteo St.: Stripe bike lanes between Harkle St. and Hospital Dr.  
St. Vincent Hospital Trail (construction by hospital): Complete second half of 0.7-mile loop trail  
Municipal Recreation Center (MRC) Trail: Within the greater FLAP-funded "Camino Real Retracement Trail," this City trail will connect the River Trail on the City's South Side to the MRC sports complex through two grade-separated crossings in a distance of 1.8 miles (one underpass already exists under the Relief Route).  
In 2018, the City will complete 0.4 miles of the Rail Trail from Pen Rd. to Alta Vista St. and improve its connection to Monterrey Dr.  
The City also anticipates extension of the River Trail by one more mile, from Frenchy's Field to Siler Rd., in 2018, as part of a Santa Fe County project in an area that is now within City limits.

**G8. Optional: What other communities do you look to as peers or comparable role models for your community?**

**G9. We often get requests for example BFC applications from aspiring communities. Are you willing to share your application?**

Yes

**G10. How did you hear about the Bicycle Friendly Community program?**

Bicycle and Trails Advisory Committee Members

**Page: Supplementary Materials**

Optional: If you would like to share any supplemental materials to support your application, please upload your files here.  
The League wants to showcase the places, organizations, and individuals working to build a Bicycle Friendly America for everyone. By sharing photos of your bicycling programs, community outreach efforts, or people biking in your community, you'll help us highlight your community's commitment to making bicycling better.  
By submitting photos here, you are granting the League of American Bicyclists the right to use your images to promote bicycling.

**File 1**

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**File 2**

City of Santa Fe employee Bike Share Program  
Download File (<https://bicyclefriendly.secure-platform.com/file/37227/eyJ0eXAiOiJKV1QiLCJhbGciOiJIUzI1NiJ9.eyJtZWVpYUlkIjozNzIyNywiYWxsbn3dOb3RTaWduZWRVcmwiOiJGYWxzZSIImInbm9yZfJJEkw3To?Hi%20Shelley.docx>)

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**File 4**

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**File 5**

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