

May 02, 2024
Planning Commission
Cases #2023-(7662), (7663) & (7079).
27794 W I-25 Frontage Road
Variances & Development Plan
Bungalows on Cerrillos (Apartments)

EXHIBIT B

Development Review Team (DRT)

1. City's Traffic consultant Wilson & Company, Inc., Engineers & Architects (City traffic Engineer)
2. New Mexico Department of Transportation
3. Metropolitan Planning Organization (MPO), Leah Yngve
4. Technical Review Division (Landscaping), Lawrence Rivera
5. Water Resources Division, Alan G. Hook, Water Resources Coordinator
6. Water Division, John Del Mar, P.E.
7. Wastewater Division, Stan Holland, PE
8. Fire Department, Geronimo Griego, Fire Marshal
9. City Subdivision Engineer, Dee Beingessner, PE

Development Review Team

Comment Form

Date: August 10, 2023

City Agents: Philip A. Gallegos, PE Traffic Engineer - Wilson & Company

Leroy N. Pacheco, PE

Dept/Div: Public Works Department

Case #2023-7079 - 27794 West I-25 Frontage Road, Bungalows on Cerrillos

Case Mgr: Dan Esquibel, AICP Senior Planner



Review of the developer's TIA and associated development plans by this division/department has determined that this application will meet applicable standards if the following are met:

Conditions of Approval:

Must be completed by:

<ol style="list-style-type: none">1. Note that primary access for the development is on NMDOT Frontage Road (see attached TIA approval letter from NMDOT with conditions to be met prior to issuance of building permit).2. Note that driveway on Cerrillos Road is for emergency access only. Plan Set to include a roadway signage and striping plan to be reviewed/approved by NMDOT and City Traffic Engineer for respective jurisdictions.	Prior to filing the Development Plan
--	--------------------------------------

Technical Corrections*:

Must be completed by:

<ol style="list-style-type: none">1. None	N/A
---	-----

*Must made prior to recording and/or permit issuance

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [None]

August 10, 2023

Leroy Pacheco, PE
Land Use Department Case Manager
City of Santa Fe
500 Market Street, Suite 200
Santa Fe, NM 87501

Re: Cavan Multi Family Development TIA Review

Dear Mr. Pacheco,

On behalf of the City of Santa Fe, Wilson & Company has completed review of Cavan Multi Family Development TIA for compliance with the New Mexico Department of Transportation State Access Management Manual and Chapter 14 Land Development Code. A traffic engineering review was performed for conformance with existing applicable codes and traffic engineering standards (e.g. MUTCD, NMDOT - State Access Management Manual, NMDOT Standard Specifications, City of Santa Fe Public Works standards, and Land Development Code - Chapter 14, etc.). This review does not replace the developer's Engineer-of-Record's professional responsibility for modeling, traffic data collection, methodology, and data analysis.

Technical Corrections*:	
	Must be completed prior to Planning Commission

* Must be completed prior to recording and/or permit issuance.

Thank you,

Philip A Gallegos

Digitally signed by Philip A Gallegos
DN: C=US,
E=philip.gallegos@wilsonco.com,
O=Wilson and Company, CN=Philip A
Gallegos
Date: 2023.08.10 16:06:12-06'00'

Sincerely,
Philip A. Gallegos, PE
Senior Transportation Engineer
505 348 4126



New Mexico DEPARTMENT OF
TRANSPORTATION
MOBILITY FOR EVERYONE

July 20, 2023

Mr. Carl Vermillion, P.E.
Bohannon Huston, Inc.
7500 Jefferson St NE
Albuquerque, NM 87109

Re: Cavan Multi-Family Development in Santa Fe, on I-25 West Frontage Road

The District Five Traffic Section has reviewed the subject Traffic Impact Analysis (TIA) by Carl Vermillion, P.E. of Bohannon Huston Inc., dated July 20, 2023. The District concurs with the report's "Conclusions and Recommendations" in Section VIII in regards to the improvements to the signalized intersection, proposed Roundabout and Frontage Road realignment. The TIA has meet New Mexico Department of Transportation (NMDOT) requirements and is approved.

This development will require an access permit submitted to NMDOT along with Roadway Improvement Plans for the project. The access control line will need to be modified and approved by the NMDOT Access Control Committee. Right of Way will need to be dedicated to NMDOT by the property owner for the realignment of the Frontage Road. Your design Engineer should contact the District Five Traffic Section to discuss the access permit application. If you have any questions feel free to contact me at 505-500-2360.



District Five Assistant District Engineer - Engineering

**Michelle Lujan
Grisham**
Governor

Ricky Serna
Cabinet Secretary

Commissioners

Jennifer Sandoval
Commissioner, Vice-Chairman
District 1

Gary Tonjes
Commissioner
District 2

Hilma E. Chynoweth
Commissioner
District 3

Walter G. Adams
Commissioner, Chairman
District 4

Thomas C. Taylor
Commissioner
District 5

Charles Lundstrom
Commissioner, Secretary
District 6



New Mexico DEPARTMENT OF
TRANSPORTATION
MOBILITY FOR EVERYONE

February 27, 2024

Carl Vermillion, PE, PTOE
Bohannon Huston
7500 Jefferson Street NE
Albuquerque, NM 87109

SUBJECT: Cavan Multi-Family Development Traffic Generation Update

Dear Applicant:

The traffic section has reviewed your letter dated 2/12/2024 regarding the change in dwelling units for the development. We are in agreement that this change has minimal impact to the original Traffic Impact Analysis and would not create any additional improvements that are recommended in the original study.

If you have any question regarding this matter, please do not hesitate to contact me at (505) 500-2360.

Thank you,

Javier Martinez, P.E.
Assistant District
Engineer – Engineering
Support

**Michelle Lujan
Grisham**
Governor

Ricky Serna
Cabinet Secretary

Commissioners

Chandelle Sisneros
Commissioner
District 1

Gary Tonjes
Commissioner
District 2

Hilma E. Chynoweth
Commissioner
District 3

Walter G. Adams
Commissioner, Chairman
District 4

Thomas C. Taylor
Commissioner
District 5

Charles Lundstrom
Commissioner, Secretary
District 6

Development Review Team (DRT) Comment Form

Date: 3/28/24

DRT Member: Leah Yngve

Dept/Div: Land Use/MPO

Case No.: **Case #2023-7079, 27794 West I-25 Frontage Road, Bungalows on Cerrillos
Development Plan.**

Case Planner: Dan Esquibel

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
1. Extend the north/south sidewalk parallel to Cerrillos to the intersection with Herrera Drive.	Prior to Public Hearing	
2. Illustrate on the development plan a new sidewalk on the south side of the NM I-25 Frontage Road to conform to city street design standards	Prior to Public Hearing	
3. Illustrate on the development plan a 10ft sidepath along the west side of the NM I-25 Frontage Road to the property line, as detailed in the Santa Fe Bicycle Master Plan	Prior to Public Hearing	
4. Illustrate on the development plan pedestrian crossings to each leg of the roundabout	Prior to Public Hearing	
5. Improve pedestrian crossing safety by illustrating on the development plan high visibility crosswalk markings on all legs of the Cerrillos/Beckner/Frontage intersection and adding a pedestrian refuge island to the southern crossing of Cerrillos.	Prior to Public Hearing	
6. Redesign the right turn slip lanes onto and off of the frontage road to limit speeds to 15mph or less.	Prior to Public Hearing	
7. Evaluate, select, and design additional pedestrian safety countermeasures at the Cerrillos intersection such as a pedestrian signal, leading pedestrian intervals, etc.	Prior to Public Hearing	
8. Add two bus stops (northbound/southbound) to the development plan per Santa Fe Trails request and in accordance with Santa Fe Trails design and location criteria. Coordinate with Santa Fe Trails on design & location.	Prior to Recordation	
9. Ensure the trail crossing at the Cerrillos emergency access is appropriately designed with ramps, etc.; demonstrate this on the development plan	Prior to Public Hearing	

1. Extend the north/south sidewalk parallel to Cerrillos to the intersection with Herrera Drive.

Terminating the asphalt trail at the property boundary creates an environment without safe pedestrian or bicycle access to the amenities to the north of the development. The trail should be continued to the north to connect into the Herrera Drive sidewalk so that residents can access the bus stops and businesses north of the development. Without this extension, there will be no safe or accessible access to transit.

2. Illustrate on the development plan a new sidewalk on the south side of the NM I-25 Frontage Road to conform to city street design standards

City code calls for 5 foot sidewalks with a 5 foot landscape buffer on collectors (see below). The north side of the road has a trail, which satisfies this requirement, but the south side needs a sidewalk as well tying into the Cerrillos/Beckner intersection.

Table 14-9.2-1: Design Criteria for Street Types

(Ord. No. 2013-16 § 59)

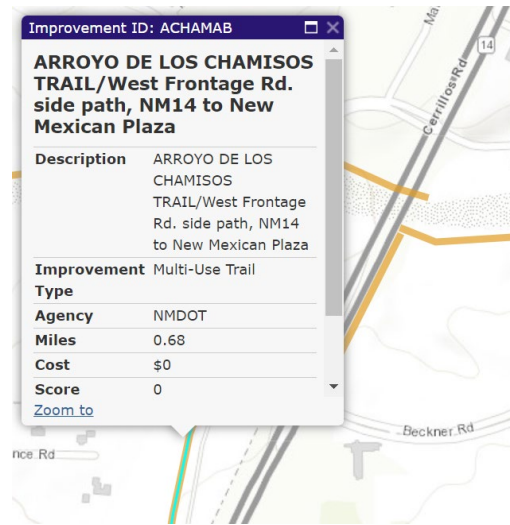
Criteria	Major Arterial (6-Lane)	Major Arterial (4-Lane)	Secondary Arterial	Collector	Collector Mixed-Use	Subcollector		Lane	Lot Access Driveway Note 1
						No Parking	With Parking		
Average Daily Traffic	Up to 60,000	Up to 40,000	5,000-15,000	1,000—5,000	1,000—5,000	300—1,000	300—1,000	0—300	Minimum
Dwelling Unit Access						30—100	30—100	0—30	(0—8)
Minimum Right-of-way Width	120	98	70	52	50	42	50 or 56	38 or 42	NA
Slope/Grading Easement (conditional upon staff review)	0—30	0—30	0—30	0—30	0—30	0—30	0—30	0—30	NR
Number of Auto Lanes	6—7 Note 2	4—5 Note 2	2—3 Note 2	2	2	2	2	2	1
Width of Driving Lanes	11	11	11	10	10	9	10	9	10
Median/Turn Lane Width	18	18	14	NR	NR	NR	NR	NR	NR
Minimum Bikeway Width	5	5	5	4	NR	NR	NR	NR	NR
On-Street Parking Width	NA	NA	NA	NA	6 Note 3	NA	6 Note 4	NA	NA
Curb & Gutter	2	2	2	2	2	2	2	2	NR
Minimum Sidewalk Setback	5	5	5	5	NR	5	5	0 or 5 Note 1	NR
Minimum Sidewalk Width	6	6	5	5	7	5	5	5	NR

Notes:
 NA - Not Applicable
 NR - Not Required
 1. Refer to Subsection 14-9.2(C)(8) for additional standards for lanes and lot access driveways. Lot access driveway standard applicable to access from street to not more than eight single family lots.
 2. Includes Median/Turn Lane
 3. Parking required on both sides of street, except no parking on that side of a street adjoining the plaza.
 4. Parking may be on one side or both sides of the street; parking lane should not be continuous.()

All measurements in feet, unless otherwise noted.

3. Build a 10ft sidepath along the west side of the NM I-25 Frontage Road to property line, as detailed in the Santa Fe Bicycle Master Plan

The Santa Fe Bicycle Master Plan calls for a sidepath multiuse trail on the western side of the frontage road. The proposed asphalt trail should be extended through the property and along the frontage road with a proposed ending at Mutt Nelson instead of full completion of the trail at the New Mexican Plaza.



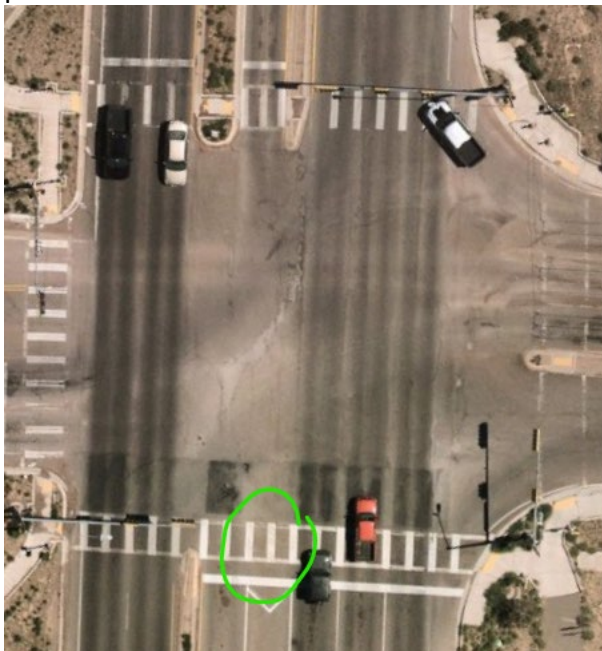
4. Add pedestrian crossings to each leg of the roundabout

With the trail and sidewalk extensions, each leg of the roundabout should have high visibility bicycle and pedestrian crossings with refuge islands.

5. Improve pedestrian crossing safety by illustrating on the development plan high visibility crosswalk markings on all legs of the Cerrillos/Beckner/Frontage intersection, including slip lanes, and adding a pedestrian refuge island to the southern crossing of Cerrillos.

With residential density on the west side of Cerrillos and many destinations on the east side of Cerrillos, ensuring the Cerrillos crossing is safe for pedestrians is critical. High visibility continental crosswalk striping should be added and/or rehabilitated on each side of the Cerrillos and Beckner/Frontage crosswalk.

On the southern crossing on Cerrillos road, there is not a pedestrian refuge island as seen on the northern crossing. A minimum 6ft wide pedestrian island should be demonstrated on the plans.



6. Redesign the right turn slip lanes onto and off of the frontage road to limit speeds to 15mph or less.

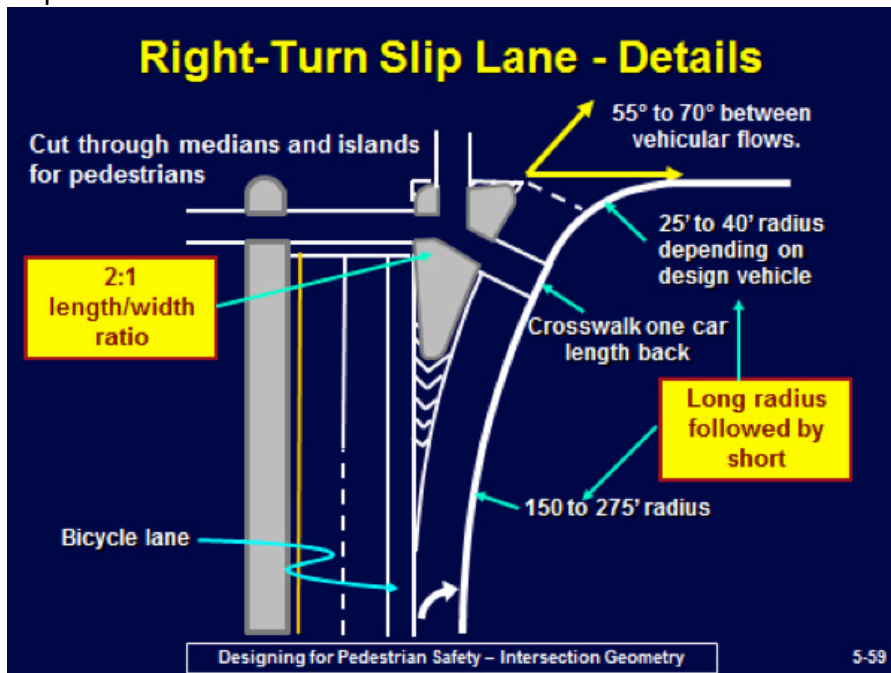
The existing channelized right turn lanes are designed to facilitate high speed right turns. In the existing context without significant bicycle or pedestrian traffic this may be appropriate, however, the addition of a residential community that necessitates residents to use this intersection for all modes of transportation creates a significant safety concern.

NACTO guidelines recommend a turning speed of 15mph or less for pedestrian safety.

<https://nacto.org/publication/urban-street-design-guide/intersection-design-elements/corner-radii/>

The northern turning radius is approximately 140 ft and the southern turning radius is approximately 120 ft. These far exceed the minimum turning radii in the City Code (25ft) and the minimum needed to accommodate large vehicles (35 ft) (14-9.2 (c)(6)(f)). The large turning radii are dangerous for pedestrians because they allow for high speed turning which makes it more difficult for vehicles to stop if there is a pedestrian. Pedestrian likelihood of fatality and severe injury increases exponentially as vehicle speed increases. Additionally, there are not traffic control devices for this crossing relying on drivers to see the pedestrian, and the channelization increases the pedestrian crossing distance.

The right turn channels should be redesigned for safety as shown in FHWA guidance to improve visibility and reduce speeds. This should include narrowing the existing lane to the minimum requirement.



http://www.pedbikesafe.org/pedsafe/countermeasures_detail.cfm?CM_NUM=24

If designs other than the above are considered, the turning speed must still be limited to 15mph.

Curb radius resources:

https://safety.fhwa.dot.gov/older_users/fhwasa15088/ch2.cfm#ss17

<https://safety.fhwa.dot.gov/saferjourney1/library/countermeasures/09.htm>

Form Updated: September 2023

7. Evaluate, select, design, and implement additional pedestrian safety countermeasures at the Cerrillos intersection.

Urban SDK speed data at Cerrillos and Beckner/Frontage road indicate that 85th percentile vehicle speeds are around 40mph at this intersection. Because the higher speeds are more dangerous for vulnerable road users such as pedestrians and bicyclists, additional safety countermeasures should be selected and implemented in coordination with the NMDOT to improve the safety of this intersection. These countermeasures could include:

- A pedestrian signal or rectangular rapid flashing beacon to cross the right turn slip lanes to/from the frontage road
- Leading pedestrian intervals
- Two-stage pedestrian crossings

8. Add two bus stops (northbound/southbound) to the development plan per Santa Fe Trails request and in accordance with Santa Fe Trails design and location criteria. Coordinate with Santa Fe Trails on design & location.

The TIA States on page 9 "The Santa Fe Trails bus system services parts of Santa Fe. Bus route 22 travels north and south on NM 14 in the project area, but the nearest stop is at Herrera and NM 14. As development occurs in this area of Santa Fe, coordination with the Santa Fe Trails bus system should occur to discuss the possibility of adding additional bus stops along this route."

Convenient and safe access to transit stops should be an essential requirement for this project. Rather than put the burden of providing bus stops on the City of Santa Fe, the developer should incorporate the design of two bus stops into their plans. The developer should coordinate with Santa Fe Trails directly on location and design criteria. If Santa Fe Trails is not ready to expand service at the time of development, they have committed to storing the completed shelters until service can be expanded.

9. Ensure the trail crossing at the Cerrillos emergency access is appropriately designed with ramps, etc.

The plan set is not clear on how the emergency access road from Cerrillos will cross the trail. The crossing should include ramps if they will be built at different grades and any other crossing requirements.

10. Add a pedestrian exit to the northern property boundary to enable access to the arroyo and open space.

The Santa Fe Pedestrian Master Plan includes connectivity to recreation spaces as an overarching goal. The Arroyo Chamiso north of the property can provide an opportunity for walking and recreation to the development's residents. Increasing access to this asset via a pedestrian path and gate on the north side of the property will help meet this goal.

Technical Corrections:

The plan set indicates that bicycle racks must have 1 foot clearance on all sides. The clearance is actually determined by how the rack is installed relative to nearby walls and sidewalks. See the below specifications to determine bike rack placement.

The screenshot to the right shows the placement of the bike rack (24) too close to the adjacent walls and pedestrian walkway.

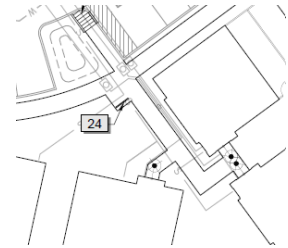


Exhibit D - Bicycle Rack Standards and Dimensions (Subsection 14-8.6(E))

Racks:

- Inverted U type bicycle racks are the required bicycle parking rack.
- Each rack must be securely anchored and accommodate a bicycle frame where one wheel can be locked to the rack with a high security, U-shaped shackle lock if both wheels are left on the bicycle.
- A space of two (2) feet by six (6) feet (12 square feet) must be provided for each required bicycle parking space so that a bicycle six (6) feet long can be securely held with two points supported so that the bicycle cannot be pushed, or fall in a way that would damage the bicycle frame, wheel, or components.
- All racks must provide two points of contact with the frame at least 6" apart horizontally.
- If a bicycle corral is sought within a public street right-of-way, all design elements shall be developed in coordination with and approved by the city of Santa Fe public works department and parking division.

Distance to other racks:

- Racks placed parallel to each other (side by side) must be at least thirty-six (36) inches apart, this includes rack units sold as multiple units attached together.
- Racks aligned end to end must be at least ninety-six (96) inches apart.

Distance from wall:

- Racks placed perpendicular to a wall must be at least forty-eight (48) inches from the wall to the nearest vertical component of the rack.
- Racks parallel to a wall must be at least thirty-six (36) inches from the wall.

Distance from curb:

- Racks placed perpendicular to a curb must be at least forty-eight (48) inches from the curb to the nearest vertical component of the rack.
- Racks placed parallel to a curb must be at least twenty-four (24) inches from the curb to the rack.

Distance from pedestrian aisle:

- Rack units perpendicular to a pedestrian aisle must be at least forty-eight (48) inches from the rack to the edge of the aisle, and the pedestrian aisle should be at least sixty (60) inches wide.

Parking and maneuvering areas:

- Each required bicycle parking space must be accessible without needing to move another bicycle.
- There must be an aisle of at least five (5) feet wide behind all required bicycle parking to allow for maneuvering of the bicycle. Where bicycle parking is next to a sidewalk, the maneuvering area may extend into the sidewalk.
- The area devoted to bicycle parking must be hard surfaced.

Development Review Team

Comment Form

Date: August 4, 2023

Staff person: Lawrence Rivera

Dept. /Div.: Land Use/Terrain Management- Landscape/Irrigation Review

Case: #2023-7079. **27794 West I-25 Frontage Road, Bungalows on Cerrillos Development Plan.**



Case Manager: Dan Esquibel, daesquibel@santafenm.gov, 505 955-6857

Review by this division/department a site visit with applicant's representatives has determined that this application will meet applicable standards if the following are met:

Conditions of Approval:

Must be completed by:

Conditions of Approval:	Must be completed by:

Technical Corrections*:

Must be completed by:

1. significant tree survey, list species and quantities. Existing plant material shall be inspected prior to any demolition. Trees to remain shall be identified as existing and tree species / variety on the landscape plan.	Prior to recordation
2. City Staff respectfully request the replacement of Pinus edulis / Pinon Pine, with another evergreen tree variety from the City approved plant list. Due to the black scale and bark beetle infestations plaguing Pinon in many areas of Santa Fe.	Prior to recordation
3. Reduce shrub quantities by 30% for water conservation needs.	Prior to recordation
4. Water conservation and stormwater management shall guide landscape and site planning, design, installation, and management. Landscape planning shall begin early in the development process in conjunction with the requirements of Section 14-8.2 (Terrain and Stormwater Management). Landscape design shall apply the principals of xeriscaping and achieve the highest industry standards for irrigation efficiency. Alternate sources of irrigation water shall be developed, including harvesting water from roof and site runoff. Gray water use is recommended where appropriate. <u>Potable water shall be used only as a back-up or temporary irrigation water source to the greatest extent possible.</u> The purpose of these strategies is to develop drought tolerant landscapes and to reduce the demand on the potable water system. Identify the primary and secondary water sources.	Prior to recordation

5. Required open space shrub count is incorrect on sheet L-0.00.	Prior to recordation
6. Tree and shrub calculations for retention areas as required by 14-8.4(F). Provide retention pond square footage and lineal footage for street trees. Provide calculations for street trees and spacing averages.	Prior to recordation
7. 14-8.4(F)(2)(e) stormwater detention ponds and retention ponds shall be planted with appropriate trees, shrubs and grasses, with a minimum of one tree and three shrubs per five hundred (500) square feet of required ponding area. Plants located in the bottom third of the detention pond or retention pond must be adaptable to periods of submersion and may require replacement during periodic maintenance to remove silt:	Prior to recordation
8. A water level measuring device with zero set at finish grade located at the center of each pond is required.	Prior to recordation
9. Retention ponds deeper than three feet require a security fence and maintenance gate. Fence shall be five in height. The following shall also apply 14-8.4(J) Screening and Buffering, 14-8.5 Walls and Fences.	Prior to recordation
10. Storm water storage ponds are not to exceed 1 foot in depth without a verification that storm water will drain within 24 hrs. A percolation test may be required for each pond.	Prior to recordation
11. 14-8.4(I)(3) Interior Parking Lot Landscape Requirements (Ord. No. 2014-31 § 34) (a) The purpose of interior planting requirements in parking <i>lots</i> is to provide visual relief from large expanses of cars and pavement, provide shade to reduce heat and <i>glare</i> , help direct traffic flow and reduce and control stormwater runoff. (b) When forty or more <i>off-street parking</i> spaces are provided, interior <i>lot landscaping</i> shall be provided. (d) A minimum of ten (10) square feet of permeable <i>landscaped</i> area shall be provided per parking space. A minimum of one deciduous tree shall be planted per ninety (90) square feet of <i>landscaped</i> area. (e) Traffic islands shall have a minimum dimension of six (6) feet and contain a minimum of ninety (90) square feet of permeable surface, exclusive of curbing, and shall be distributed throughout the <i>lot</i> . As a component of a stormwater management plan, traffic islands may be combined to facilitate <i>water harvesting</i> and these combined islands shall be distributed within each subarea. Provide parking lot tree and shrub calculations.	Prior to recordation
12. Provide landscape and irrigation to the Cerrillos Road Corridor per 14-5.5 - HIGHWAY CORRIDOR PROTECTION DISTRICTS.	Prior to recordation
13. 14-8.4(F)(2)(a) all required deciduous trees shall be two (2) inch caliper minimum; 14-8.4 (F)(2)(c) All required evergreen trees shall be a minimum of six (6) feet in height.	Prior to recordation
14. City staff respectfully requests two (2) <i>Gymnocladus dioica</i> , Kentucky Coffeetree (<i>Espresso</i> or <i>Prairie Titan</i> varieties), or another tree chosen from the City approved plant list with trunk protection and irrigation within the northeast park. r shade structure.	Prior to recordation
15. City Staff respectfully requests all gravel and cobble to be screened and washed.	Prior to recordation

16. 14-8.4(E)(1) The landscaping plan shall include water harvesting for landscape irrigation purposes as a minimum requirement. Water harvesting that is a primary component of stormwater management may qualify for open space reduction as provided in Section 14-7.5(D)(6). Provide strategic curb cuts at islands and planter stripes for water harvesting.	Prior to recordation
17. 14-8.4(E)(4)(g) planting beds shall be swaled, sloped or recessed below grade prevent fugitive water.	Prior to recordation
18. Turf grass is not permitted within medians and planting strips per City Resolution 2010-66.	Prior to recordation
19. No stop & waste valves or automatic drain valves (King Drains) permitted per COSF Irrigation Design Standards.	Prior to recordation
20. Irrigation water lines shall not cross city water mains.	Prior to recordation
21. Provide irrigation to all revegetation native seed areas. (Per COSF Code 14-8.2 D(5)(c) all trees and shrubs shall be mulched and irrigated until established. Grass seed should either be hydroseeded or covered with bio degradable material or synthetic soil erosion control blankets or matting and irrigated until established. Irrigation shall be pursuant to the irrigation requirements in Section 14-8.4 (Landscape and Site Design).	Prior to recordation
22. 14-8.4(E)(4)(h) irrigation systems shall be zoned by levels of water use. For the most efficient water use, plant with similar water use requirements shall be grouped together. Trees and shrubs shall have separate zones. Drought tolerant and very low water-use tree shall have a separate zone. Native shrubs and very low water-use shrubs shall be irrigated on a separate zone. Separate zones are required for permanent and temporary irrigation lines.	Prior to recordation
23. 5.5 Emitters: Pressure compensating emitters are required for systems with lateral distances of more than 150 feet and rolling terrain per COSF Irrigation Design Standards.	Prior to recordation
24. Photometrics do not meet Code requirements. See Article 14-8.9 and revise for compliance. Building #2 has 4 points of illumination of 11+ and one point at 7. All illumination points at the property lines shall be 0.0. Outdoor light plan shows pool house. Provide an up to date lighting plan.	Prior to recordation
Engineer Estimates for landscape and irrigation financial guarantees shall be shown as installed unit cost breakdowns. Breakdown to include quantity, unit cost, and total cost. Mulch materials shall be in square foot units, square foot costs, and total cost. Plant material unit costs shall be separated into caliper sized deciduous trees, foot tall sized evergreens and gallon sized shrubs.	Prior to recordation
STAFF RESERVES THE RIGHT TO REQUIRE ADDITIONAL SUBMITTALS UPON RECEIVING REVISIONS	

*Must made prior to recording and/or permit issuance

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]

Explanation of Conditions or Corrections (if needed):

Development Review Team

Comment Form

Date: August 1, 2023

Staff person: Alan G. Hook, Water Resources Coordinator

Dept/Div: Public Utilities/ Water Division

Case: **Case #2023-7079. 27794 West I-25 Frontage Road, Bungalows on Cerrillos Development Plan**

Case Mgr: Dan Esquibel



Review by this division/department has determined that this application will meet applicable standards if the following are met:

Conditions of Approval:	Must be completed by:
1 An approved Development Water Budget including a 9.8% contingency per SFCC 14-8.13(E)(1) will be required. A technical evaluation request for a Development Water Budget needs to be submitted to the City Water Division for review.	Prior to Planning Commission packet submittal.
2 An Agreement to Construct and Dedicate (ACD) from the Water Division will be required for all new public water infrastructure or fire services. An approved Development Water Budget, including Landscape Irrigation Budget and a 9.8% Contingency is required prior to processing the ACD.	Prior to Recordation of Development Plan or Plat.
3 Each lot shall be served by separate water service at the time of development.	Prior to construction permit approval.
4 An agreement for metered service (AMS) will be required to install new services, including water offset fees.	Prior to construction permit approval.

Technical Corrections*:	Must be completed by:
1	
2	
3	

4	
---	--

*Must be made prior to recording and/or permit issuance

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]

Explanation of Conditions or Corrections (if needed):

Development Review Team (DRT) Comment Form

Date: January 3, 2024

DRT Member: Taylor Jurgens

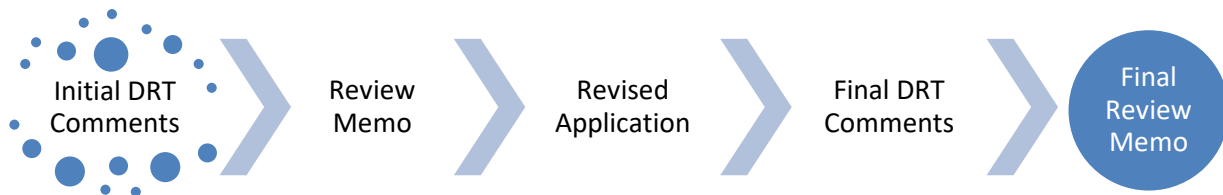
Dept/Div: Public Utilities/Water Division

Case No.: Case 2023-7079, 7662, & 7663: Bungalows on Cerrillos Development Plan, Setback Variance, and Slope Variance

Case Planner: Dan Esquibel

DRT Review Schedule – 9-12+ weeks*

Initial DRT Comments are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.



Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *At the time of development* -

Development Review Process Flow Chart



*See the 2024 Development Review Schedule for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
1. An approved Water Plan will be required for all new public water infrastructure and fire services. Water Plan shall be submitted to the City Water Division for review.	Prior to Public Hearing Prior to Recordation	
2. An accepted application for an Agreement to Construct and Dedicate (ACD) shall be submitted to the Water Division for all new public water infrastructure and fire services.	Prior to Recordation	
3. An approved ACD will be required with the Water Division.	Prior to building permit approval	
4. Any re-grading of site over existing water mains shall maintain a minimum of 4 feet of cover and not exceed 5 feet of cover. Depth of existing main to be adjusted to maintain a minimum of 4 feet of cover and not exceed 5 feet of cover if site grading impacts these depth requirements.	At the time of development	
5. Proposed development layout based on the requested setback variance has a conflict with the existing water main and other existing water infrastructure. This conflict will need to be resolved by either relocating the existing water infrastructure or redesigning this portion of the development to avoid the conflict. Final changes needed to resolve this conflict must be reflected on the approved water plan.	Prior to Public Hearing Prior to Recordation	
6.		

Technical Corrections:	Must be completed by:	Applicant Response:
1. Technical comments on the new public water infrastructure will be provided to the engineer via the water plan review.	Prior to Water Plan approval Prior to Recordation	
2.		
3.		
4.		

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Development Review Team (DRT) Comment Form

Date: 3/26/2024

DRT Member: Stan Holland, P.E.

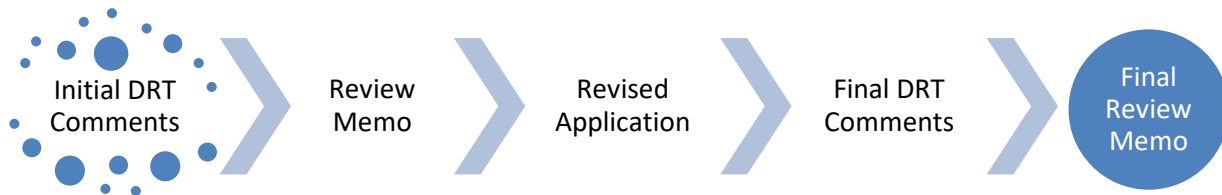
Dept/Div: Utilities/Wastewater Division

Case No.: 2023-7079-7622-7663

Case Planner: Daniel Esquibel

DRT Review Schedule – 9-12+ weeks*

Initial DRT Comments are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

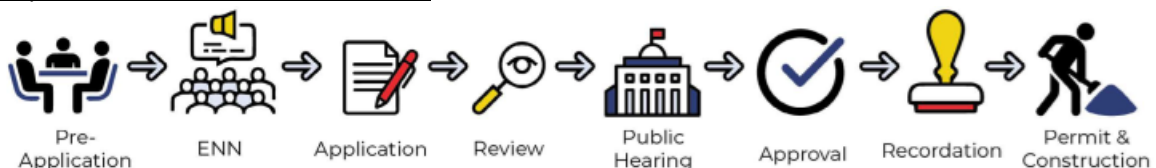


Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- Prior to Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- During Construction* – these conditions/technical corrections can be addressed during construction.

Development Review Process Flow Chart



*See the *2024 Development Review Schedule* for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case #

Conditions of Approval:	Must be completed by:	Applicant response**:
1. Access to sewer manhole 1 is required. The sewer discharge of the entire project discharges to the City's public sewer system through manhole 1. In the event of a stoppage in this section of the sewer system and the City needs to respond, access would be required to manhole 1.	Prior to Recordation	
2. Show the location and the separation distance between the existing 24-inch sewer lines and the storm water discharge pipe in all profiles of the storm water system	Prior to Recordation	
3. The existing 20-foot-wide sewer access easement for the 24-inch public sewer line runs along a large portion of the project property line on its north side. The sewer easement in this section shall be graded and a 14-foot-wide gravel road is to be provided in this section.	Prior to Recordation	
4. All 8 inch sewer pipe shall be SDR 26 PVC	Prior to Recordation	
5. All 4 and 6 inch sewer pipe shall be SCH 40 PVC	Prior to Recordation	
6. The existing City's 24-inch sewer line running along the north side of the project has a depth of 15 feet in areas. Because of the sewer line depth and the proximity to the retaining walls, the Wastewater Division requires a detailed analysis of this area showing the depth and widths of the proposed walls and footers with respect to the required excavation required for a 15-foot-deep excavation. The same analysis is also required for the area for the proposed 16-foot-deep manhole 1 and connection to the existing public sewer line.	Prior to Recordation	
7. There are several locations on the plan set that show a block retaining wall constructed on or next to a sanitary sewer easement boundary. Please place a note on each utility plan sheet that states; the City's Wastewater Management shall not be responsible for damages to any decorative or retaining wall this is constructed on or proximate to any sanitary sewer easement boundary.	Prior to Recordation	
8. Identify the type and the detail location sheet for all block retaining walls to be used on each of the retaining walls in each utility plan sheet	Prior to Recordation	

--	--	--

Technical Corrections:	Must be completed by:	Applicant Response**:
1.		
2.		
3.		
4.		

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Development Review Team (DRT) Comment Form for Planning Commission

Date: 12/18/2023
 DRT Member: Geronimo Griego
 Dept/Div: Fire Prevention Bureau
 Case No.: 2023-7663 Bungalows on Cerrillos variance (Slopes)
 Case Planner: Daniel Esquibel

DRT Review Schedule – 9-12+ weeks*

Initial DRT Comments are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.



Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *At the time of development* -

Development Review Process Flow Chart



*See the 2024 Development Review Schedule for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response**:
1. Shall verify roadway alignment slopes not to exceed 10%	Prior to public hearing	
2. Shall verify roundabout can support fire apparatus access with all-weather road		
3. Shall verify turning radii are a minimum of 28'-0"		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant Response**:
<ol style="list-style-type: none"> 1. Shall provide code analysis of project for proper occupancy classification designation as per IFC 2015. 2. Shall provide engineered (civil engineered) all weather road as per IFC 2015, 104.7.2 Technical Assistance, 503.2.3 Surface. 3. Fire apparatus access shall have an unobstructed width of not less than 20 feet exclusive of shoulders and an unobstructed vertical clearance of not less than 13 feet 6 inches (IFC 2015 Section 503.2.1) 4. Fire Department shall have 150 feet distance to any portion of the building on any new construction. (IFC 2015 section 503.1.1) 5. 503.4 Obstruction of Fire Apparatus Access Roads. Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths and clearances established in Section 503.2.1 and 503.2.2 shall be maintained at all times. 6. 507.5 Fire hydrant systems. Fire hydrant systems shall comply with Sections 507.5.1 through 507.5.6. 507.5.1 Where required. Where a portion of the facility or building hereafter constructed or moved into or within the jurisdiction is more than 400 feet (122 m) from a hydrant on a fire apparatus access 	Time of permit if applicable.	

<p>road, as measured by an approved route around the exterior of the facility or building.</p> <ol style="list-style-type: none"> 7. Shall have water supply that meets fire flow requirements as per IFC (Appendix B) 8. Shall comply with Section D102.1 Access and Loading (75,000 lbs). 9. Shall comply with IFC 2015 Section D103.5 Fire apparatus access road gates. 10. Shall meet the 150 feet driveway requirements must be met as per IFC, or an emergency turn-around that meets the IFC requirements shall be provided. (as per Appendix D Table D103.4) 11. Shall comply with IFC 2015 Section D103.2 Grade. Fire apparatus access roads shall not exceed 10 percent in grade. 12. Shall comply with D105 Aerial Fire Apparatus Access Roads. (width and proximity to building) 13. Shall comply with Section D106 Multiple-Family Residential Developments. 14. D107.1 One- or two-family dwelling residential developments. Developments of one- or two-family dwellings where the number of dwelling units exceeds 30 shall be provided with two separate and approved fire apparatus access roads. 15. D107.2 Remoteness. Where two fire apparatus access roads are required, they shall be placed a distance apart equal to not less than one-half the length of the maximum overall diagonal dimension of the property or area to be served measured in a straight line between accesses. 16. Shall meet the IFC code requirements 2015 edition or the most current edition the governing body has adopted at the time of permitting. 		
1.		
2.		
3.		
4.		

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

- 1.
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Development Review Team Comment Form

Date: 8/2/23
 Staff person: Dee Beingessner
 Dept/Div: Land Use/Terrain Management
 Case: **Case #2023-7079 Bungalows on Cerrillos Development Plan**
 Case Mgr: Dan Esquibel



Review by this division/department has determined that this application will meet applicable standards if the following are met:

Conditions of Approval :	Must be completed by:
1 Drainage report discussion in proposed conditions section must include the total amount of impervious surface to be installed for the development, pond volume required, and pond volume provided for the site. Provide a small table here with pond information for quick reference of each pond's volume. Also include this information in the future conditions section.	Prior to recording Development Plan

Technical Corrections*:	Must be completed by:
1 Dust Control Note Shall be placed on the Development Plan or construction plans (see attached)	Prior to recordation
2 Gunnison's Prairie dog note shall be placed on the Development Plan or construction plans (see attached)	
3 Drainage Facilities Maintenance note shall be placed on the Plat or Development Plan (see attached)	
4 Couldn't find label for Pond 3 with perimeter fence. Include label. Pond 2 does not appear to be deep, why is there a perimeter fence?	
5 Need pond details to be shown. Include inlet and outlet details. Include details for erosion control in pond. Include pond measuring device in pond (example included below)	
6 Grading plan states that all slopes are 3:1 maximum unless otherwise noted but many areas appear greater than 3:1 and are not noted.	
7 Show floodplain limits on grading plans. Not all line types have a label on the legend. There are numerous dashed lines of various thickness with no legend.	

8 Label concrete alley gutter locations. Provide flared apron into concrete alley gutters to direct flow at the start of the alley gutter.	
9 Erosion control note references topsoil disturbance permit. This will be a grading permit.	
10 A variance to construction on 30% slopes will be required and will likely not be supported by staff per code. 30% slope disturbances are only allowed for roadways and utilities.	
11 Need offsite construction plans for roadway, paving and lighting.	
12 No recycled basecourse may be used on City streets, state untreated basecourse on road cross sections for off-site plans. Rollover curbs are not to be used on City streets. Pavement on City Street must be 2" of SPIII under 2" of SPIV	
13 North arrow is incorrect on Sheet GR-5 and GR-6, please check all pages for north arrow.	
13 This Development shall comply with the most current requirements of the American with Disabilities Act (ADA). Place a note on the Plan to reflect this condition	
14 Other comments will be provided on these plans at the time of the building permit and changes may be required during permitting process	

*Must made prior to recording and/or permit issuance

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]

Explanation of Conditions or Corrections (if needed): (see following pages for notes required)

DRAINAGE FACILITIES MAINTENANCE NOTE

ALL DRAINAGE FACILITIES SHALL BE MAINTAINED BY ALL PROPERTY OWNERS. THE PROPERTY OWNERS SHALL ALSO HAVE THE RESPONSIBILITY FOR MAINTENANCE OF THE PUBLIC DRAINAGE EASEMENT.

INSPECTIONS SHALL BE CONDUCTED BY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF NEW MEXICO OR OTHER QUALIFIED PERSON. INSPECTIONS SHALL BE CONDUCTED ON THE DRAINAGE FACILITIES IN ACCORDANCE WITH THE FOLLOWING SCHEDULE:

ON OR ABOUT MARCH 15, ON OR ABOUT SEPTEMBER 15, AND AFTER EACH STORM EVENT OF 1-INCH OR GREATER. MAINTENANCE OF THE DRAINAGE FACILITIES SHALL BE CONDUCTED AND DOCUMENTED BY THE ENGINEER OR QUALIFIED PERSON AND THE OWNER. THE OWNER SHALL MAINTAIN A FILE OF THE INSPECTIONS AND REMEDIAL ACTION CONDUCTED ON THE DRAINAGE FACILITIES.

WORK SHALL BE CONDUCTED IN ACCORDANCE WITH THE ENGINEER'S RECOMMENDATIONS, AND SHALL INCLUDE, BUT IS NOT LIMITED TO THE FOLLOWING:

A. FLUSH ALL CULVERTS, DROP INLETS AND DRAINAGE PIPES TO REMOVE SEDIMENT AND VEGETATION THAT PREVENTS OR HINDERS THE FLOW OF STORM WATER IN THE DRAINAGE STRUCTURE(S).

B. REMOVE SEDIMENT IN PONDS THAT IS GREATER THAN 6-INCHES IN DEPTH, AND IF NECESSARY, RESEED WITH NATIVE GRASSES AND INSTALL FILTER FABRIC AND 3 TO 4 INCH COBBLE AS DIRECTED BY THE ENGINEER.

C. INSPECT FOR SEDIMENTATION IN ALL SWALES, DITCHES, DRAINAGE PONDS AND REMOVE AND STABILIZE AS NECESSARY.

D. INSPECT FOR SOIL EROSION AT ALL DRAINAGE PONDS, CUT AND FILL SLOPES, AND REPAIR OR STABILIZE ACCORDINGLY.

E. INSPECT THE STRUCTURAL INTEGRITY OF DRAINAGE PONDS, SLOPES, RIP-RAP, GABIONS, DROP INLETS, CULVERTS, ENERGY DISSIPATORS, AND RETAINING WALLS, ROCK PLATING, EROSION CONTROL MATS OR BLANKETS, AND REPAIR OR STABILIZE ACCORDINGLY.

GUNNISON'S PRAIRIE DOG NOTE

THE PROJECT SHALL COMPLY WITH THE PROVISIONS OF THE GUNNISON'S PRAIRIE DOG ORDINANCE (ARTICLE 14-8.12).

DUST CONTROL NOTE

All on-site soil disturbing construction activities shall be addressed and provide measures to mitigate or control dust from being transported offsite and polluting neighboring properties.

Any person, owner, contractor or operator who conducts earthmoving and/or dust generating activities is responsible for implementing Best Management Practices (BMPs) in order to mitigate off-property transport of fugitive dust emissions.

A plan, or storm water prevention plan (SWPPP) when applicable, listing the Best Management Practices (BMPs), shall be provided to the City Engineer, or their designee for review and approval. The approved BMPs shall be applied to the graded and/or disturbed soil in order to stabilize the site.

The initial BMP shall address how the Contractor will minimize the amount of disturbed soil, and how the Contractor will stabilize the disturbed surface area exposed to wind or vehicle traffic during construction.

Some BMPs shall include:

- The reduction of vehicle speeds: establish a maximum speed limit or install traffic calming devices to reduce speeds to a rate to mitigate off-property transport of dust entrained by vehicles.
- The minimization of drop height: Drivers and operators shall unload truck beds and loader or excavator buckets slowly, and minimize drop height of materials to the lowest height possible, including screening operations.
- High winds restriction: temporarily halt work activities during high wind events greater than 30 mph if operations would result in off-property transport.
- Restrict access: restrict access to the work area to only authorized vehicles and personnel.

In the event the above practices are ineffective to prevent off property transport, the owner or operator shall use at one or more of the following Best Management Practices (BMPs):

- Wet suppression: apply water to disturbed soil surfaces, backfill materials, screenings, and other dust generating operations as necessary and appropriate considering current weather conditions, and prevent water used for dust control from entering any public right-of-way, storm water drainage facility, or watercourse.
- Wind barrier: construct a fence or other type of wind barrier to prevent wind erosion of the graded or disturbed surface.
- Vegetation: plant vegetation appropriate for retaining soils or creating a wind break.
- Surface roughening: stabilize an active construction area during periods of inactivity or when vegetation cannot be immediately established.
- Cover: install cover materials such as tackifiers, erosion control blankets, gravel, vegetation (when appropriate), cold-millings, etc. during periods of inactivity and properly anchor the cover.
- Soil retention: stabilize disturbed or exposed soil surface areas that will be inactive for more than 30 days or while vegetation is being established.

ENGINEER'S STORMWATER INFRASTRUCTURE CERTIFICATION

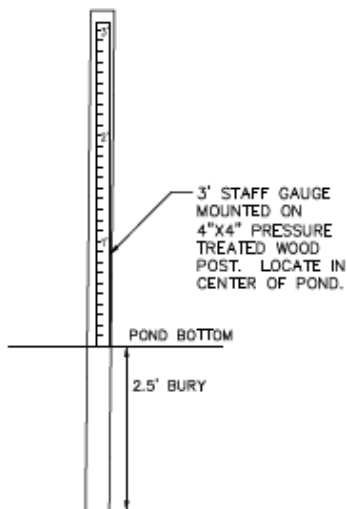
I, the undersigned professional engineer in the State of New Mexico, do hereby certify that the record information shown hereon is based on actual field measurements and visual inspections performed by myself or under my direct supervision. I further certify that the record condition as of _____ is in substantial compliance with the approved grading and drainage plan prepared by _____, dated _____.

Signature

NMPE # _____

Date

Example for pond measurement post



POST & STAFF GAUGE
NTS