

Development Review Team (DRT) Comment Form for Planning Commission

Date: October 15, 2025

DRT Member: Fire Marshal Geronimo Griego

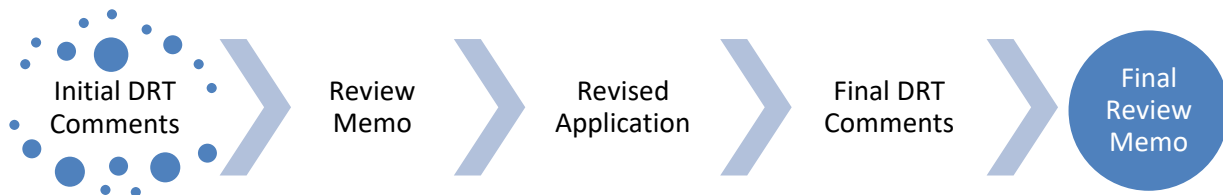
Dept/Div: Fire Marshal's Office

Case No.: 2025-11006 Vistas de la Sierra Phase 3 Subdivision

Case Planner: Senior Planner Claudia Kath

DRT Review Schedule – 9-12+ weeks*

Initial DRT Comments are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

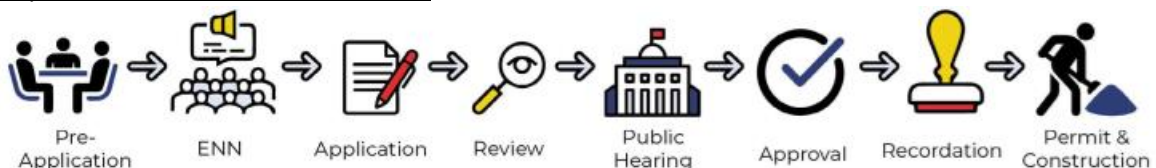


Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
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- c. *Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *At the time of development* -

Development Review Process Flow Chart



*See the 2024 Development Review Schedule for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response**:
1. Shall comply with the most currently adopted International Fire Code.	<i>Prior Building Permit Approval</i>	
2. Shall comply with Chapter 33 "Fire Safety During Construction and Demolition"	<i>At time of development</i>	

Technical Corrections:	Must be completed by:	Applicant Response**:

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following provisions or other requirements will apply to future phases of development of this project:

1. Shall verify all weather road and active fire hydrants are in place prior to combustible materials arriving on site.

Explanation of Conditions or Corrections (if needed):

1. Verify that the correct currently adopted International Fire Code (IFC) is referenced prior to building permit approval.

Development Review Team (DRT) Comment Form

Date: September 15, 2025

DRT Member: Taylor Jurgens

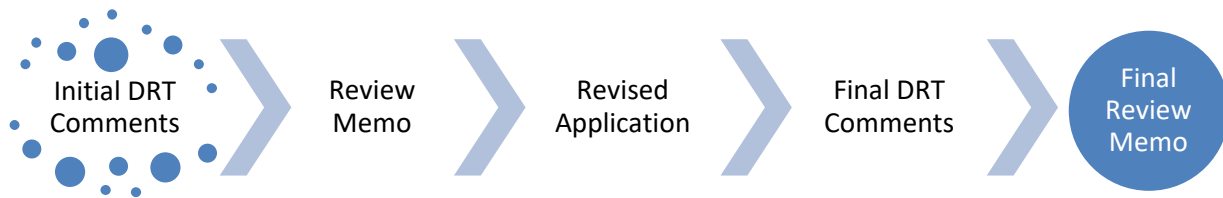
Dept/Div: Public Utilities/Water Division/Engineering

Case No.: Case 2025-11006: Vista de la Sierra Phase 3 Preliminary Subdivision Plat

Case Planner: Claudia Kath

DRT Review Schedule – 9-12+ weeks*

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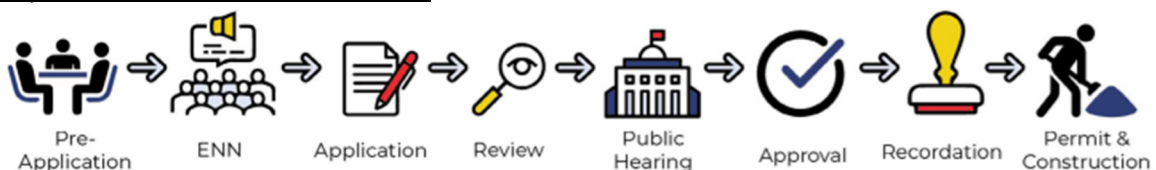


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Development Review Process Flow Chart



*See the 2024 Development Review Schedule for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
1. An approved Water Plan will be required for all new public water infrastructure and fire services.	Prior to Public Hearing for Final Subdivision Plat	
2. An approved Agreement to Construct and Dedicate (ACD) will be required with the Water Division for all new public water infrastructure and fire services.	Prior to Building Permit Approval	
3. Each lot shall be served by separate City water service at the time of development.	At the time of development	
4. Any re-grading of site over existing water mains shall maintain a minimum of 4 feet of cover and not exceed 5 feet of cover. Depth of existing main to be adjusted to maintain a minimum of 4 feet of cover and not exceed 5 feet of cover if site grading impacts these depth requirements.	At the time of development	
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant Response:
1. Specific technical comments on the water plans will be provided by the Water Division to the design engineer.	Prior to Water Plan Approval	
2. Additional looping of public water mains may be required. The need for looping will be evaluated during public water plan review.	Prior to Water Plan Approval	
3.		
4.		

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

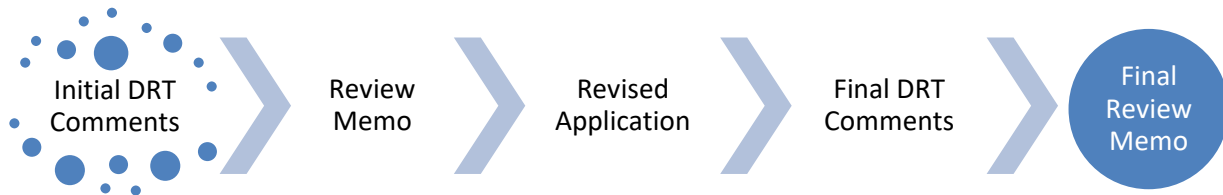
Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Development Review Team (DRT) Comment Form

DRT Review Schedule – 9-12+ weeks*

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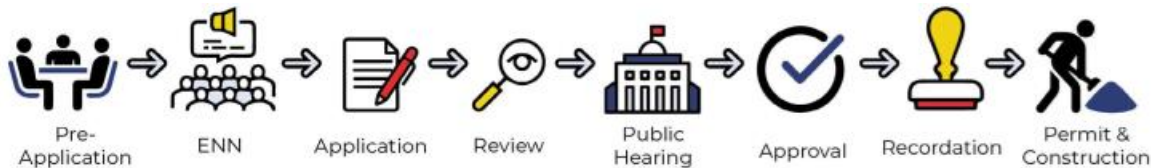


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- d. *During Construction* – these conditions/technical corrections can be addressed during construction.

Development Review Process Flow Chart



*See the *2024 Development Review Schedule* for details

DRT Review Timeline:

Application Received	DRT Application Intake	DRT Initial Review Comments Due

Date: **09/15/2025**

DRT Member: **Sergio Sandoval**

Dept/Div: **Public Utilities/ Wastewater Management Division**

Case No.: **2025-11006 - Vistas de la Sierra Subdivision Phase 3**

Case Planner: **Claudia Kath**

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case # 2025-11006 - Vistas de la Sierra Subdivision Phase 3

Conditions of Approval:	Must be completed by:	Applicant response**:
1. SDR35 shall be the only material used for public sewers main.	Construction	
2. We will accept the 0.5 percent slope detailed on Utility Plan C6.06- 0.6% is highly recommended	Construction	
3. Avoid the placement of sewer lines above water lines during crossings- none found.	Construction	
4. Make sure to contact Luke Gray, Engineer Associate 505-955-4626 before construction for inspection scheduling. He will be responsible for the issuing of letter of approval and system city acceptance.	Construction	
5. No need to change drawing, but the Wastewater Division will not accept any service connections branching from a sewer manhole	Construction	
6. Pipe Material shall be SDR26	CONSTRUCTION	

Technical Corrections:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

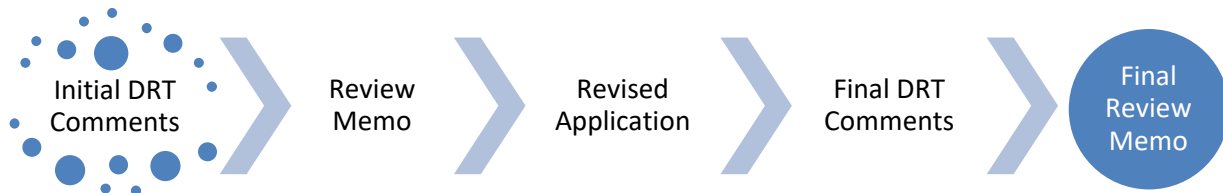
Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Development Review Team (DRT) Comment Form

DRT Review Schedule – 9-12+ weeks*

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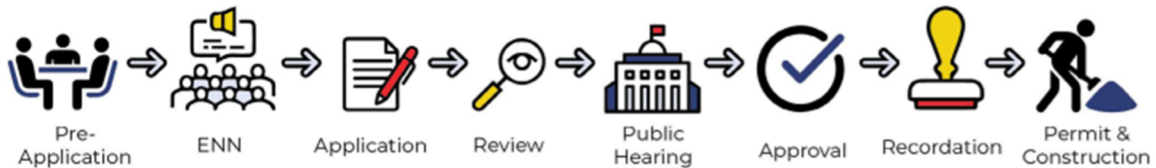


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- d. *During Construction* – these conditions/technical corrections can be addressed during construction.

Development Review Process Flow Chart



*See the *2024 Development Review Schedule* for details

DRT Review Timeline:

Application Received	DRT Application Intake	DRT Initial Review Comments Due

Date: October 30, 2025

DRT Member: Alan G. Hook

Dept/Div: Public Utilities/Water Division-Water Resources

Case No.: 2025-11006, Vistas de La Sierra Subdivision, Phase III, 3800 Governor Miles Rd.

Case Planner: Claudia Kath

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case # 2025-11006

Conditions of Approval:	Must be completed by:	Applicant response**:
1. An approved Development Water Budget including a 9.8% contingency per SFCC 14-8.13(E)(1) will be required. A technical evaluation request for a Development Water Budget needs to be submitted to the City Water Division for review.	Prior to Public Hearing for Final Subdivision Plat	
2. An Agreement to Construct and Dedicate (ACD) from the Water Division will be required for all new public water infrastructure or fire services. An approved Development Water Budget, including Landscape Irrigation Budget and a 9.8% Contingency is required prior to processing the ACD.	Prior to Public Hearing for Final Subdivision Plat	
3. Each lot shall be served by separate water service at the time of development.	Prior to Building Permit Approval.	

Technical Corrections:	Must be completed by:	Applicant response**:
1. Phase III of the Vistas de La Sierra Subdivision has added six single-family residential lots, increasing the previously approved Subdivision from 393 lots to 399 lots. An amended Development Water Budget will be required	Prior to Public Hearing for Final Subdivision Plat	
2. Water rights will be required for the additional six single-family residential lots	60 days after final plat approval of the Planning Commission	
3. Phase III of the Vistas de La Sierra Subdivision has added two SF Homes Program Home to the previously approved SF Homes Program Agreement.	Prior to Public Hearing for Final Subdivision Plat	
4. The City of Santa Fe SF Homes Program Agreement will need to be amended and the additional SFHP Homes added to the Fee Waivers for a total of 81 SFHP Homes.	Prior to Public Hearing for Final Subdivision Plat	

Development Review Team (DRT) Comment Form

Date: September 16, 2025
 DRT Member: Leroy Pacheco, PE and Phil Gallegos, PE (Wilson&Company)
 Dept/Div: Public Works Department – Traffic Engineering
 Case No.: Vista De La Sierra Subdivision Phase 3 (#2025-010486-PAR)

Case Planner: Land Use Department - Claudia Kath, Senior Planner

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
See attached comments response letter dated 9/16/25 from Wilson & Company. Comments in red are summarized below:	Prior to approval of final subdivision plat.	
1. General Comments #2 - Acceleration/deceleration lane warrants and lengths at various intersections are unresolved and developer shall provide further analysis at locations identified.	Prior to approval of final subdivision plat.	
2. General Comment #4 and Report Comment #2 – This development’s impacts to the signalized intersections on Richards Avenue at both Governor Miles Road, and Becker Road will require build year signal timing plans for each intersection analyzed and stamped by developers engineer to accommodate impacts identified (if any) by this Traffic Impact Study. In addition, the recommended traffic signal modifications at Richards/Beckner by that signal’s EOR (Mr. Mike Gomez, PE), are to be implemented as part of this development. This includes: <u>modifications to signal heads</u> at Richards/Beckner; <u>relocation of advanced northbound signal flasher</u> further south on Richards Avenue; and a <u>photometric analysis</u> to determine if installation of additional lighting for the southbound approach as described in the EOR’s close out memo dated 6/27/25 is necessary. If so, the developer shall provide this additional lighting.	Prior to approval of final subdivision plat.	
3. Report Comment #4 – developer shall provide an all way stop sign warrant analysis at the intersection of Governor Miles and Dancing Ground and shall construct any identified improvements resulting from that analysis.	Prior to approval of final subdivision plat.	
4. Summary Comment: Whispering Wing Road changes from dead end to connecting street to this subdivision as part of this development plan, and developer shall provide a more robust analysis of onstreet signage and striping per MUTCD in correspondence with signage for other connecting streets in this subdivision.	Prior to approval of final subdivision plat.	

Technical Corrections:	Must be completed by:	Applicant Response:
1. General Comment #5 and Report Comment #3 – Data and methodology for warrants at intersections 2 and 3 are incomplete.	Prior to approval of final subdivision plat.	

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

- Any and all future proposed improvements of this site that affect public infrastructure will be subject to **City Code Chapter 23**

Memorandum

To: Leroy Pacheco, PE City of Santa Fe

From: Philip A. Gallegos, PE, Senior Transportation Engineer

CC: Robert Luna, PE, PTOE, Traffic Engineering Practice Lead, SW Region

Date: 9/16/2025

Re: Vista de la Sierra Subdivision Comment Response

Wilson and Company has reviewed the updated Traffic Impact Study dated August 22, 2025 and the corresponding Response Letter from Tierra West, LLC dated August 21, 2025 for the Vista de la Sierra Subdivision TIA. Below are Wilson's responses to the developer's updated TIS and traffic engineering response letter.

General Comments:

Wilson's Comment: 1. The report states for the 2026 Implementation Year Recommendations that:

All design and construction associated with the Vista de la Sierra Subdivision shall maintain adequate traffic site distances at existing and proposed intersections / driveways. No landscaping elements or walls or other structures should be permitted that constrain site distances at an intersection / driveway that significantly compromises safety. This should be conveyed in any site plans as implementation of the project progresses. The report should state how this is going to be monitored and achieved to ensure that this will not occur as development progresses.

Developer's Response: Agreed. Recommendation 1 has been modified to require a note be added to the site plans and the development's O&M instructions to restrict landscaping that constrains site distances at the driveways.

Wilson's Response: This comment has been satisfactorily addressed.

Wilson's Comment: 2. Acceleration/Deceleration lanes recommended in the report should show that they meet the required length plus storage as per the COSF TIA Guidelines as follows.

"Note 1: Storage length should be provided from operational analyses. Storage length should be calculated as the 95th percentile queue length rounded up to the nearest 25 feet with a 50-foot minimum length. When operational analyses are not applicable (e.g., for turning movements that are uncontrolled), utilize the deceleration distance for the auxiliary lane length. Using guidance from the latest AASHTO Green Book for "Deceleration Lanes," accept a moderate amount of deceleration within the through lanes and utilize the taper length as part of the deceleration within the through lanes. Deceleration rates greater than 6.5ft/s² may be used where practical. A minimum bay length of 50 feet shall be provided.

Note 2: Deceleration length should be calculated based on the distance required to brake from the posted speed of the roadway to a stop and includes the appropriate taper based upon the posted speed. Utilize recommended deceleration distances for “Lane Change and Deceleration Distance” provided by the latest AASHTO Green Book”

This was not clear in the report as the recommendations seem based on existing auxiliary lanes lengths. This should be discussed in more detail.

Developer’s Response: Agreed. A deceleration lane warrant analysis has been conducted for all intersections in the study area. A narrative of the basis and results of the analysis are summarized in the executive summary on pages ii-iii. A more detailed description of the analysis is provided in the “Access Design Section” on page 30.

Wilson’s Response:

- Intersection 1 Governor Miles/Richards Ave.

The developer shall add left turn lanes at all approaches as they are warranted and ensure that they meet required storage and taper lengths. The EBL will have 85 feet added to the existing EBL due to limited capacity. This shall be explained as to what the limited capacity constraints for this lane are that limit the extension to 85 feet.

Right turn lanes are warranted for the EB and WB approaches for both the No Build and Build Condition however the developer states that updated signal timing restores the condition to the No Build Condition.

This needs to be explained further since Right Turn Lanes are warranted in the NO Build Condition as well.

- Intersection 2 Governor Miles/Entrada Sonata

TIA shows that existing left-turn lane has adequate storage capacity, and no mitigation is required.

- Intersection 3 Governor Miles and Rising Sun

TIA recommends installing a 150 ft left turn lane when the intersection is converted to a four-leg intersection when the Senior Center opens and states that the developer is not responsible for this lane since the Senior Center isn’t operating. This may not be a Senior Center. The developer shall confirm what this development is and when it is going to be built and operational. If this development is planned and funded, then the developer shall convert this intersection into a 4 leg intersection and construct the required 150 foot left turn lane.

- Intersection 5 Governor Miles and Dancing Ground

A left turn lane is warranted however the report states that it is an existing condition and should not be the responsibility of the developer. The developer shall show if the developer makes this condition worse from the existing condition. As per Appendix A-164 there is no existing left turn lane but recommends 150 feet of storage. The developer shall construct this left turn lane.

- **Intersection 8 Beckner and Richards**

A 225-ft of additional storage capacity is required for the northbound left lane (NBL) to prevent traffic from spilling over into the NB thru lane. The report states that this is an existing problem especially during the PM peak period. However, no additional storage capacity is recommended because the NBL cannot be extended without shortening of the SBL turn lane for the Desert Sage subdivision. The report also states that the recommended signal re-timing at Intersection 8 reduces queue lengths to less than NO BUILD conditions. By how much? This needs to be shown. The developer shall analyze the intersection with Zinnia Lane and make recommendations for the Zinnia Lane Intersection and whether this needs to be limited access intersection. Based on this additional analysis the required 225ft additional storage capacity for the NBL shall be constructed by the developer.

Wilson's Comment: 3. A complete speed study on Beckner Road from Richards Avenue west to Walking Rain Road shall be required to see if the currently posted 45 mph speed limit is adequate for the facility and if a speed reduction is warranted.

Developer's Response: Agreed. A speed study was conducted as requested and the results are discussed on pages 32- 33. Given the results of the speed study, the posted (advisory) speed of 25 mph at the roundabout, and that walking and biking is encouraged in this area, we recommend that Beckner Road between Rail Runner Road and Richards Road should be posted at 30 mph instead of the current posted speed limit of 45 mph.

Wilson's Response: A Speed Study was conducted as per request and recommends that the speed limit on Beckner should be reduced to 30 mph from 45 mph between Rail Runner Road and Richards Ave. Posted advisory speeds of 25 mph are recommended at the roundabout. City of Santa Fe Public Works staff will address this recommendation to the governing body per the terms of the City of Santa Fe's Uniform Traffic Code.

Wilson's Comment: 4. 2036 Horizon Year Recommendations

The report states that signal timing should be adjusted (cycle length and phase splits) as necessary to accommodate future volumes at the intersections of Governor Miles Rd./Richards Ave (Intersection No. 1) and Beckner Rd./Richards Ave (Intersection No.8). This assumes that the City of Santa Fe has installed and implemented a permitted / protected left turn phase for the eastbound approach (eastbound to northbound left turn) and a southbound right turn overlap phase to allow permitted / overlap operation for southbound right turns at the intersection of Beckner Rd./Richards Ave.

The city has recently installed a traffic signal at Richards/Beckner. The signal warrant analysis and close out memo recommendations post installation made

to the city by the engineer of record (Mike Gomez, PE) shall be reviewed and implemented by this development as a condition of approval. Generally the comments include the following:

- The Signal warrant analyses for both signalized intersections at Richards Avenue @Governor Miles; and at Richards Avenue@ Beckner Road will need to be updated.
 - As part of these analyses the signal at Richards Beckner will at a minimum need to be modified to change the existing 3-section indications to 5 as described in Mike Gomez's close out memo dated June 24, 2025.
 - Relocate the existing advanced signal flasher south of Governor Miles Road further to the south of Mission Bend and Zinnia Lane to notify northbound motorists in advance of the two signals.
 - Install additional lighting for the southbound approach north of the intersection for the westbound Beckner turn lane.
- **Developer's Response:** Mike Gomez (Santa Fe Engineering) completed a full warrant analysis of this intersection a year or two ago and signal is warranted. There is quite a bit of new development activity in the area, and it should not be imposed on this developer alone to comply with the close out recommendations of the signal warrant analysis by Santa Fe Engineering. It is suggested that this be a publicly funded effort. The traffic signal according to the Santa Fe Engineering's warrant analysis was warranted apart from the traffic generated by Vista de la Sierra Subdivision. The signal warrant analysis should still be valid as the volumes are increasing over time.
 - **Wilson's Response: Signal Warrant Analysis Update –** The developer's engineer states that the at Richard's/Beckner intersection is warranted in accordance with the recommendation by the EOR for that signal (Mr. Mike Gomez, PE) to revalidate the signal after its acceptance and operation.

It should be noted that all public infrastructure in the Las Soleras development area has been funded entirely by private development, including the signal at Beckner Richards which was funded with private development impact fees. This development's impact to this intersection contributes significantly to these impacts, and the recently noted signal modifications above shall be implemented by the Vista de la Sierra developer.

- **Wilson's Comment:** 5. With respect to the access proposed at Rising Sun Dr. and Entrada Sonata as full access stop controlled intersections, the recommended geometry shall be supported by a stop control warrant analysis and other required engineering analysis supporting the proposed geometry, that demonstrates that LOS are satisfied by recommended geometry. A complete geometric plan, including pedestrian/bicyclist MUTCD signage and striping plan shall be provided for review to demonstrate all modes of traffic are considered.).

- **Developer's Response:** Agreed. A 2-way Stop and All-Way stop control warrant analysis has been conducted. Results and discussion of the results are presented in the report on pages 28 thru 30. The requirement for a geometric plan showing all the MUTCD signage and stripping are included in the recommendations on page. This plan is to be included in the design plan set for the off-site improvements.
- **Wilson's Response:** A 2-way stop and all-way stop Warrant Analysis were conducted and met warrants for a 2-Way Stop for intersections 2 and 3 and the requirement for a geometric plan showing all the MUTCD signage and striping are included in the recommendations on page. This plan is to be included in the design plan set for the off-site improvements by the developer, which has not yet been reviewed by Wilson.
- The appendix shall include the raw data shown and provide an explanation of the methodology used for in the 2 Way and All Way Stop Analysis. The analysis shall be shown in detail and calculations and/or software output shall be included. Both crash data and sight distance data will need to be evaluated.
- **Comment:** 6. A better site plan is needed for the intersections in the area with an aerial showing the numbered intersections.
- **Response:** Agreed. An update to the site plan has been provided in the report.
- **Wilson's Response:** This comment has been satisfactorily addressed.
- **Comment:** 7. The proposed westbound left turn deceleration lane on Governor Miles Rd. at the new access that aligns with Rising Sun Dr. needs to be verified that it meets the required length plus storage as per the COSF TIA Guidelines as in number 2 above.
- **Response:** The calculated 95th Percentile queue length on Governor Miles Rd. for the westbound left turn movement at the intersection is 0.2 vehicles based on 2036 volumes. Therefore, the storage length is negligible.
- **Wilson's Response:** This comment has been satisfactorily addressed.
- **Comment:** 8. In the Safety Analysis Section rear end collisions were a factor at the Governor Miles/Richards intersection but as noted the intersection was not signalized during the crash study period. Crash history may have changed since the intersection has been signalized. Updated crash analyses need to be completed at both signalized intersections on Richards Avenue as part of the signal warrant analyses. The crash mitigation needs to be clarified and demonstrate how the proposed changes might affect the crash data.
- **Response:** The comment needs to be corrected. First of all, the Governor Miles Rd. / Richards Ave. intersection was signalized during the crash study period. If the comment was intended to refer to the Beckner Rd. /

Richards Ave. intersection, it was not signalized during the crash study period, but has only been signalized for about five or six months. It is not advisable to conduct a crash study for only five or six months. Also, the most current crash data available from the AASHTO Safetyware Website is to December 31, 2023. More recent crash data is not yet available.

- **Wilson's Response:** This comment has been satisfactorily addressed.

- **Comment:** 9. The report states that it assumes that sidewalks will be constructed by the development. This should state that sidewalks shall be constructed along the southside of Governor Miles and shown on site plans.

- **Response:** Complied.

- **Wilson's Response:** This comment has been satisfactorily addressed.

- **Report Comments:**
 - **Comment:** 1. Beckner Rd. and La Puerta Intersection (Intersection No. 7)
 - The report suggests that implementation of an eastbound left turn arrow and a southbound right turn arrow at the signal so that the eastbound left turn movement will operate in a permitted/protected mode and the southbound right turn movement will operate in a permitted plus overlap mode along with signal timing will achieve satisfactory results in the horizon year. The report states that the City of Santa Fe is already working on plans to add the eastbound left turn arrow and the southbound right turn arrow in the near future. Note General Comment #4 above, and that these improvements will be made a condition of approval for this development.

 - **Response:** Beckner Rd. and La Puerta Intersection (Intersection No. 7) is an unsignalized intersection, and the TIS does not recommend or suggest anything at this intersection.

 - **Wilson's Response:** This comment has been satisfactorily addressed.

 - **Comment:** 2. Richards and Beckner Intersection (Intersection No. 8)
 - The recommended mitigation is to add a new eastbound left turn arrow at the existing traffic signal to allow for permitted / protected operation of the eastbound left turn movement (eastbound to northbound) and a new southbound right turn arrow to allow for permitted plus overlap operation of the southbound right turn movement (southbound to westbound). Note General Comment #4 above, and that all geometric improvements determined by the updated signal warrant analyses will be made a condition of approval for this development.

Response: See response to General Comment #4 above.

See Wilson's Response to General Comment #4 above.

Comment: 3. Governor Miles and Entrada Sonata(La Pequena), (Intersection No. 2)
The report states that the analysis of the intersection of Governor Miles Rd. / Entrada Sonata demonstrates that the delays will be marginally acceptable for the horizon year (2036) conditions analyzed in this report. The southbound approach is projected to operate at LOS "E" by the year 2036. However, there is a refuge zone (approximately 17' wide) in the center of the intersection which should allow small to moderate sized vehicles to execute staged left turns. In consideration of this, the LOS "E" would be LOS "C". Therefore, no recommendation is made for this intersection. How was this analyzed? This needs to be confirmed as a legitimate traffic movement or explained in detail and shown in a depiction. This intersection came up as a major concern for pedestrian crossings during the ENN. An intersection stop sign warrant analysis supporting the proposed intersection geometry will need to be performed by the developer.

Response: An intersection all-way stop warrant analysis has been provided for the intersections along Governor Miles Rd. (see Pages 28 and 20 of the FINAL Traffic Impact Study.

See Wilson's Response to General Comment #5 above

Comment: 4. Governor Miles and Dancing Ground Intersection
The AM Peak Hour shows LOS E for the Northbound Through for the No Build 2036 Horizon Year but LOS D in the 2036 Build Year. Why and how did it improve with no mitigation. This intersection came up as a major concern for pedestrian crossings during the ENN. An intersection stop sign warrant analysis supporting the proposed intersection geometry will need to be performed by the developer. A complete geometric plan, including pedestrian/bicyclist MUTCD signage and striping plan shall be provided for review to demonstrate all modes of traffic are considered.

Response: The 2036 BUILD Conditions analysis considered the northbound left turn movements to be staged for the BUILD Condition, but not the NO BUILD Condition. The analysis has been modified so that both the NO BUILD and BUILD Conditions assume a staged left turn movement for the northbound left turn. The tables in the Traffic Impact Study have been corrected so that both the 2036 AM NO BUILD and the 2036 AM BUILD analyses assume a staged left turn movement from the side street (Dancing Ground). There is a 16-foot-wide buffer zone in the center of the intersection in which a left turning vehicle can be staged.

Wilson's Response: This intersection shall be reanalyzed to determine if an All Way stop is warranted. The staged left turn movement in build and horizon years may increase complexities at this intersection, and it cannot be assumed to be an acceptable movement without providing robust engineering design and supporting documentation.

In Summary:

Comment: The two signalized intersections on Richards Ave. will require updated signal warrant analyses, and optimization of signal cycle lengths and phase splits to accommodate future horizon year volumes at acceptable levels-of-service. Note

General Comment #4 above, and that these improvements will be made a condition of approval for this development.

Acceleration/Deceleration lanes recommended in the report need to show that they meet the required length plus storage as per the COSF TIA Guidelines and show that they will operate effectively and summarize mitigation efforts. This was not clear in the report as the recommendations seem based on existing auxiliary lanes lengths. This should be discussed in more detail.

A complete speed study shall be required to see if the currently posted 45 mph speed limit is adequate for the facility and if a speed reduction is warranted.

Response: To reiterate, this development should not be held responsible to revisit the signal warrant analysis that was conducted just a couple of years ago by Santa Fe Engineering. The Traffic Impact Study demonstrates that the calculated 95th Percentile queue lengths for the left turn movements off Governors Miles Rd. does not ever exceed .05 vehicles. Therefore, the queuing component of the deceleration lane is irrelevant.

A speed study was added to the Final Traffic Impact Study to comply with the City's request.

Additionally, parking along Whispering Wing Rd. east of Dancing Ground Rd. is already restricted and, in consideration of the width of paving (approximately 23 feet), parking on both sides of the street should not be permitted. Also, there is no apparent reason to post the speed limit on Whispering Wing Rd. east of Dancing Ground Rd. as anything other than the normally legal speed limit established in the City of Santa Fe Traffic Code for local residential streets.

Wilson's Response:

This development impacts the intersection of Richard's Avenue and Beckner Road. Continued development shall be required to measure additional impacts to that existing signalized intersection.

The speed study recommendations will proceed per the City of Santa Fe Uniform Traffic Code requirements per General Comment #3 above.

This development significantly changes the nature of Whispering Wing Road from a dead-end street to an interconnected local roadway. This change requires this developer to reexamine the nature of this new connection and determine any signage upgrades, or changes recommended in light of the nature of similar roadways in this development and per the criteria of the MUTCD.

Sincerely,

Philip A Gallegos, PE
Senior Transportation Engineer

Development Review Team (DRT) Comment Form

Date: 9/15/2025

DRT Member: Leah Yngve

Dept/Div: PW/MPO

Case No.: **Case #: 2025-11006 - Vistas de la Sierra Subdivision Phase 3**

Case Planner: Claudia Kath

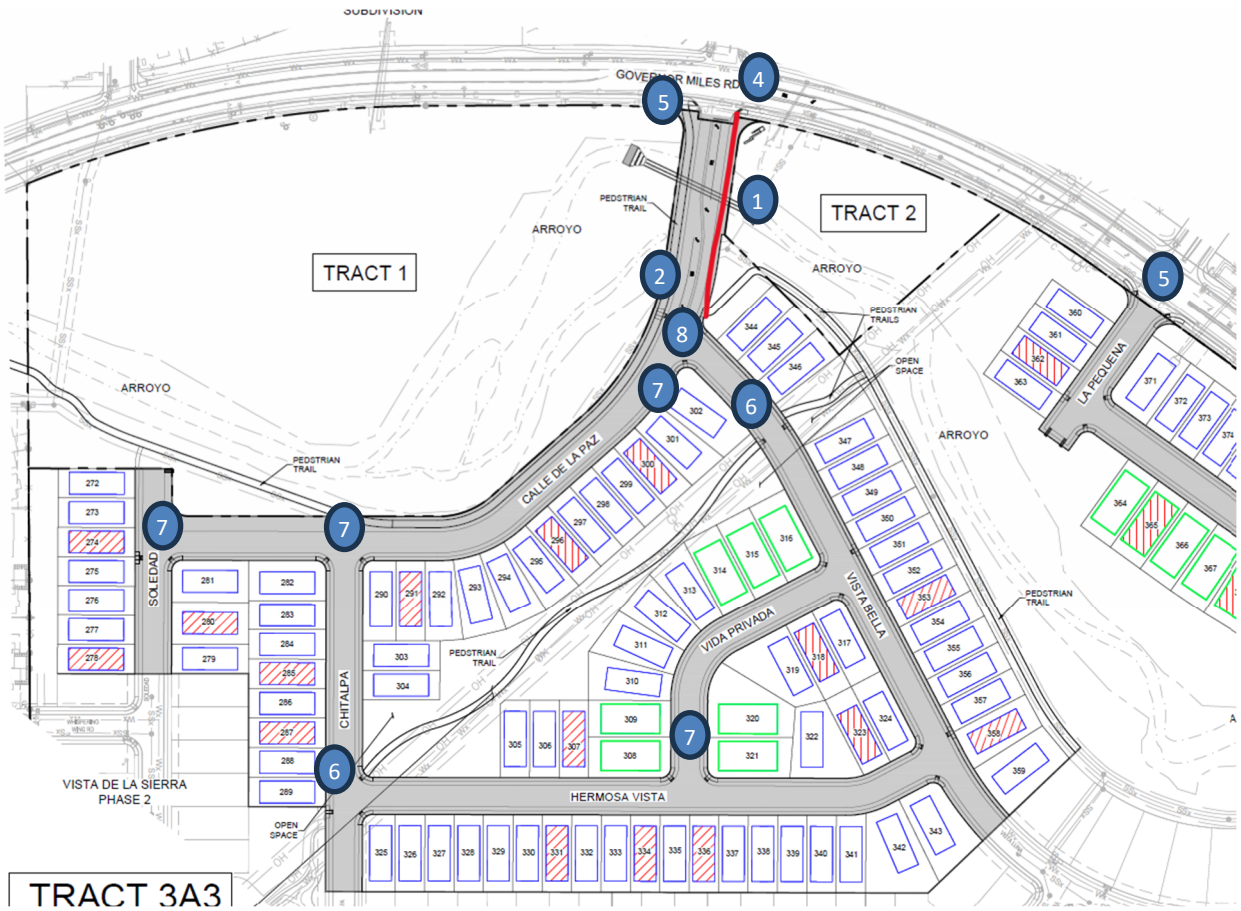
Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
1.		
2.		

Technical Corrections:	Must be completed by:	Applicant Response:
1. Plans must demonstrate a sidewalk on both sides of Calle de la Paz towards Governor Miles per Table 14-9.2-1	Prior to Public Hearing for Final Subdivision Plat	
2. The intersection of Vista Bella and Calle de la Paz was not evaluated for a left turn lane in the TIA; remove left turn lane	Prior to Public Hearing for Final Subdivision Plat	
3. Sidewalk widths on Governor Miles & Richards are unmarked; they must be a 5ft minimum	Prior to Public Hearing for Final Subdivision Plat	
4. Effective curb radii at Governor Miles and internally should be the smallest permitted radii (25ft). Effective radii includes the bike lane on Governor Miles.	Prior to Public Hearing for Final Subdivision Plat	
5. Crosswalks at Governor Miles should include continental stripes	Prior to Public Hearing for Final Subdivision Plat	
6. Mid-block trail crossings should be striped	Prior to Public Hearing for Final Subdivision Plat	
7. Orient curb ramps to the direction of the pedestrian crossing	Prior to Public Hearing for Final Subdivision Plat	
8. Consider traffic circles at internal intersections	Prior to Public Hearing for Final Subdivision Plat	

Explanation of Conditions or Corrections (if needed):



1. Calle la Paz needs a sidewalk on both sides on the street per Table 14-9.2-1

Table 14-9.2-1: Design Criteria for Street Types

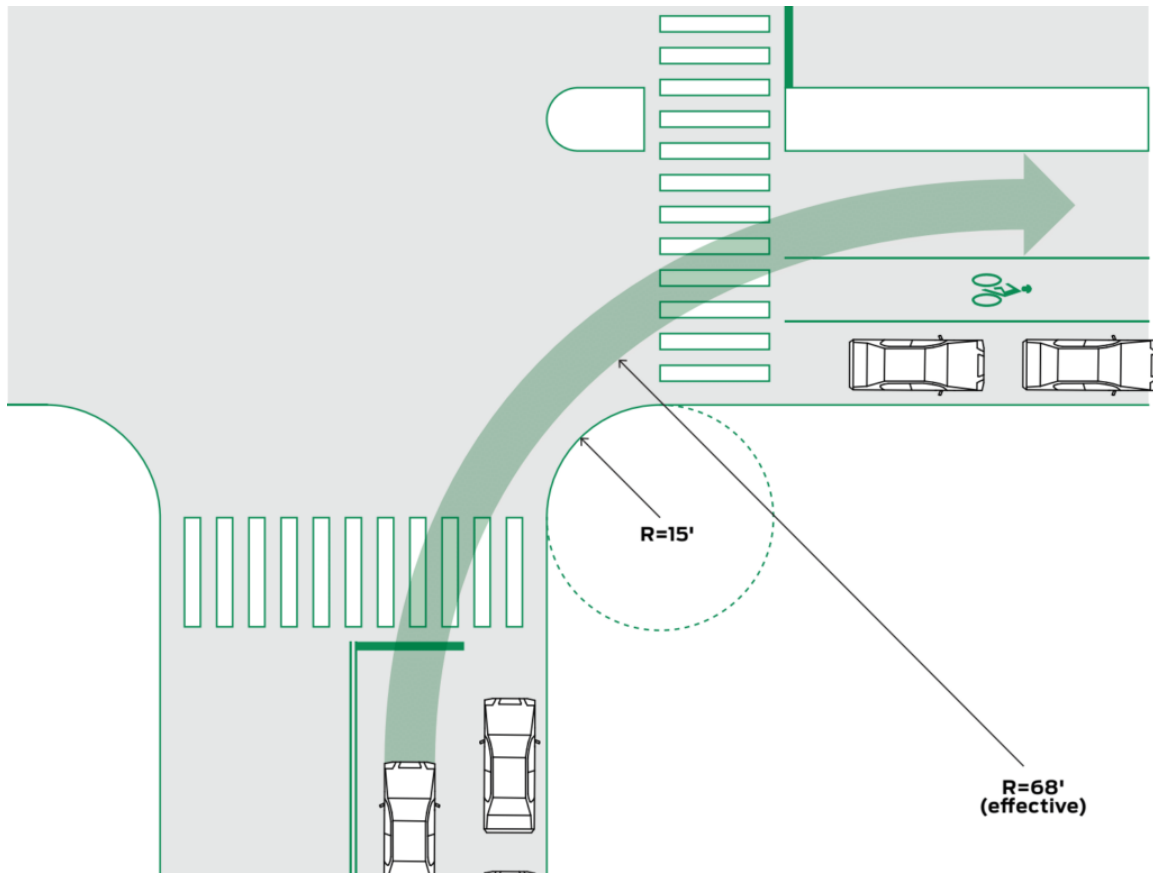
(Ord. No. 2013-16 § 59)

TABLE 14-9.2-1: Design Criteria for Street Types
 See also Chapter 12 Fire Prevention and Protection — International Fire Code Appendix D Fire Apparatus Access Roads (as amended) for mandatory standards for roadway width, steepness, dead end/turnarounds, number of access points and fire lane signage

Criteria	Major Arterial (6-Lane)	Major Arterial (4-Lane)	Secondary Arterial	Collector	Collector Mixed-Use	Subcollector		Lane	Lot Access Driveway Note 1
						No Parking	With Parking		
Average Daily Traffic	Up to 60,000	Up to 40,000	5,000-15,000	1,000—5,000	1,000—5,000	300—1,000	300—1,000	0—300	Minimum
Dwelling Unit Access						30—100	30—100	0—30	(0—8)
Minimum Right-of-way Width	120	98	70	52	50	42	50 or 56	38 or 42	NA
Slope/Grading Easement (conditional upon staff review)	0—30	0—30	0—30	0—30	0—30	0—30	0—30	0—30	NR
Number of Auto Lanes	6—7 Note 2	4—5 Note 2	2—3 Note 2	2	2	2	2	2	1
Width of Driving Lanes	11	11	11	10	10	9	10	9	10
Median/Turn Lane Width	18	18	14	NR	NR	NR	NR	NR	NR
Minimum Bikeway Width	5	5	5	4	NR	NR	NR	NR	NR
On-Street Parking Width	NA	NA	NA	NA	6 Note 3	NA	6 Note 4	NA	NA
Curb & Gutter	2	2	2	2	2	2	2	2	NR
Minimum Sidewalk Setback	5	5	5	5	NR	5	5	0 or 5 Note 1	NR
Minimum Sidewalk Width	6	6	5	5	7	5	5	5	NR

Notes:
 NA - Not Applicable
 NR - Not Required
 1. Refer to Subsection 14-9.2(C)(8) for additional standards for lanes and *lot* access driveways. *Lot* access driveway standard applicable to access from *street* to not more than eight single family *lots*.
 2. Includes Median/Turn Lane
 3. Parking required on both sides of *street*, except no parking on that side of a *street* adjoining the plaza.
 4. Parking may be on one side or both sides of the *street*; parking lane should not be continuous.(.)
 All measurements in feet, unless otherwise noted.

2. The development TIA did not include the intersection of Vista Bella and Calle de la Paz. Because the left turn lane was not determined to be warranted, it should not be included. Left turn lanes increase the width of the street and the speed that vehicles are able to travel. Neither effect is desired in a neighborhood context.
3. Governor Miles and Richards sidewalks should be 5ft; see table above.
4. Reduce curb radii to the minimum per the code: 25 ft. The right turn out of Governor Miles can include the bike lane in the radius, effectively allowing a tighter built curb radii. (Per NACTO guidance: <https://nacto.org/publication/urban-street-design-guide/intersection-design-elements/corner-radii/>)



5. Intersections with striped crosswalks clearly designate pedestrian space and increase awareness that pedestrians may be present.
6. The multi-use trail crosses mid-block and diagonally across an intersection. Striping the crossings will alert drivers that people could be crossing there.
7. Where pedestrian crossings are one direction, orient the curb ramps to the direction of the crossing.
8. Traffic circles are circular raised center islands at the intersection of two neighborhood streets and are installed for traffic calming.

<https://toolkits.ite.org/uiig/treatments/63%20Residential%20Traffic%20Circle.pdf>

Date: 9/16/25

DRT Member: Joshua Vanslambrouck, Charlie Gonzales

Dept/Div: Land Use/ Technical Review

Case No.: 2025-11006

Case Planner: Claudia Kath

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case #

Conditions of Approval:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant response**:
1. Show the required setback from the arroyo according to city code 14-8.2(D)(4)(e)(i)	Prior to Public Hearing for Final Subdivision Plat	
2. Drop inlets in roadways must be city standard, not nmdot, please provide city detail and update callouts to use city detail	Prior to Public Hearing for Final Subdivision Plat	
3. Provide details on the flow path to detention pond 8 in the drain plan	Prior to recordation	
4. Label high points in roadways with (HP) in drain plan	Prior to recordation	
5. Label Low Points in roadways with LP	Prior to recordation	
6. Provide details for the outlet of runoff from Cielo Hermosa cul-de-sac in drain plan, is this intended to drain to pond 15?	Prior to Public Hearing for Final Subdivision Plat	
7. Provide detail drawing of storm drain junction box on the corner of Chitalpa and Calle de la Paz in drain plan	Prior to recordation	
8. Are storm sewer manholes type C or type E? Clarify on the drain plan	Prior to recordation	
9. Name/number manholes	Prior to recordation	
10. Storm sewer manholes not allowed in the curb and gutter, valley gutter, or fillets	Prior to recordation	

11. Add sediment measuring posts at bottom of pond elevation	Prior to recordation	
12. Provide rock size or further details for rip-rap at end of MBC, Detail A, sheet C1.16	Prior to recordation	
13. Provide the width/thickness of the toe down for all concrete rip rap	Prior to recordation	
14. Call out inverts of all drainage structures in plan view	Prior to Public Hearing for Final Subdivision Plat	
15. City prefers sidewalk curb ramps on point of curve, not on the nose	Prior to recordation	
16. Are driveways to be cut? Or installed	Prior to recordation	
17. Install type 3 barricade at dead ends	Prior to Public Hearing for Final Subdivision Plat	
18. Asphalt in city streets must be 4 inches thick, please provide a first lift of 2.5 inches sp-iii, and a second lift of 1.5 inches sp-iv	Prior to Public Hearing for Final Subdivision Plat	
19. Provide a tree survey to show all significant trees and substantial grass coverage to be removed and too remain, city code 14-8.2(F)(1)(a)(ix)	Prior to Public Hearing for Final Subdivision Plat	
20. all <u>significant trees</u> , and other trees and vegetation, areas with substantial grass coverage and <u>drainageways</u> that are to remain undisturbed shall be fenced off prior to the use of any heavy machinery on-site and shall remain fenced during the entire construction process. Fencing material may include snow fencing, plastic mesh or other similar fencing material. To protect the root zone of all <u>significant trees</u> , and other trees and vegetation, fencing shall be placed five (5) feet to the outside of their dripline; city code 14-8.2(D)(7)(c), please provide a plan to protect the natural arroyo vegetation	Prior to Public Hearing for Final Subdivision Plat	
21. Please correct retaining wall Detail on the slope analysis sheet to reflect what the current design is (Gravity wall with large stone blocks)	Prior to Public Hearing for Final Subdivision Plat	
22. Clarify with the Design Engineer if the Updated arroyo retaining wall can be installed without grout between the	Prior to Public Hearing for Final Subdivision Plat	

<p>boulders, concrete is not allowed according to city code “Arroyo and watercourse banks shall not be armored with concrete, gabion baskets, sheet piling, rip-rap or similar hardened material unless no reasonable alternative exists to protect public <u>infrastructure</u> or pre-existing <u>structures</u>”</p>		
<p>23. Please call out the retaining wall detail on the grading plans</p>	<p>Prior to Public Hearing for Final Subdivision Plat</p>	
<p>24. Please Address comments in attached red lined plans</p>	<p>Prior to recordation</p>	
<p>25. Please add pond 9 back to the design, required by city staff to ensure proposed flows are less than existing conditions by a larger margin than what has been proposed</p>	<p>Prior to Public Hearing for Final Subdivision Plat</p>	
<p>26. Please provide peak flow analysis for 3 more storm events, of different durations and frequencies less than that of the 100-year, 24-hour event,(for example, 5-year, 24 hour; 100-year, 1-hour; 2-year, 2-hour; etc.) required by city code 14-8.2(D)(4)(b)(i) “except as otherwise required by this Section <u>14-8.2</u>, the stormwater runoff peak flow rate discharged from a site shall not exceed pre- <u>development</u> conditions for any frequency storm event up to the one percent chance, twenty-four-hour storm event at each discharge point”</p> <p>And city code 14-8.2(D)(4)(c)(i) “stormwater <u>detention</u> basins and overflow <u>structures</u> shall be sized and designed to adequately accommodate flows from <u>one percent chance</u>, twenty-four-hour storm events; provided, however, that such basins shall also be equipped with outflow <u>structures</u> that limit flow-through from lesser magnitude storms to runoff rates equal to or less than pre- <u>development</u> runoff rates;</p>	<p>Prior to Public Hearing for Final Subdivision Plat</p>	
<p>27. put in a note to restrict use of residential streets in Nava Ade for construction traffic</p>	<p>Prior to recordation</p>	

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

TOPOGRAPHIC SURVEY
PREPARED FOR
COLUMBUS CAPITAL, LLC
SHOWING
EXISTING SITE CONDITIONS ON
LOT 3A/3800 GOVERNOR MILES RD.
WITHIN SECTION 8, T16N, R9E, NMPM CITY
& COUNTY OF SANTA FE, N.M.

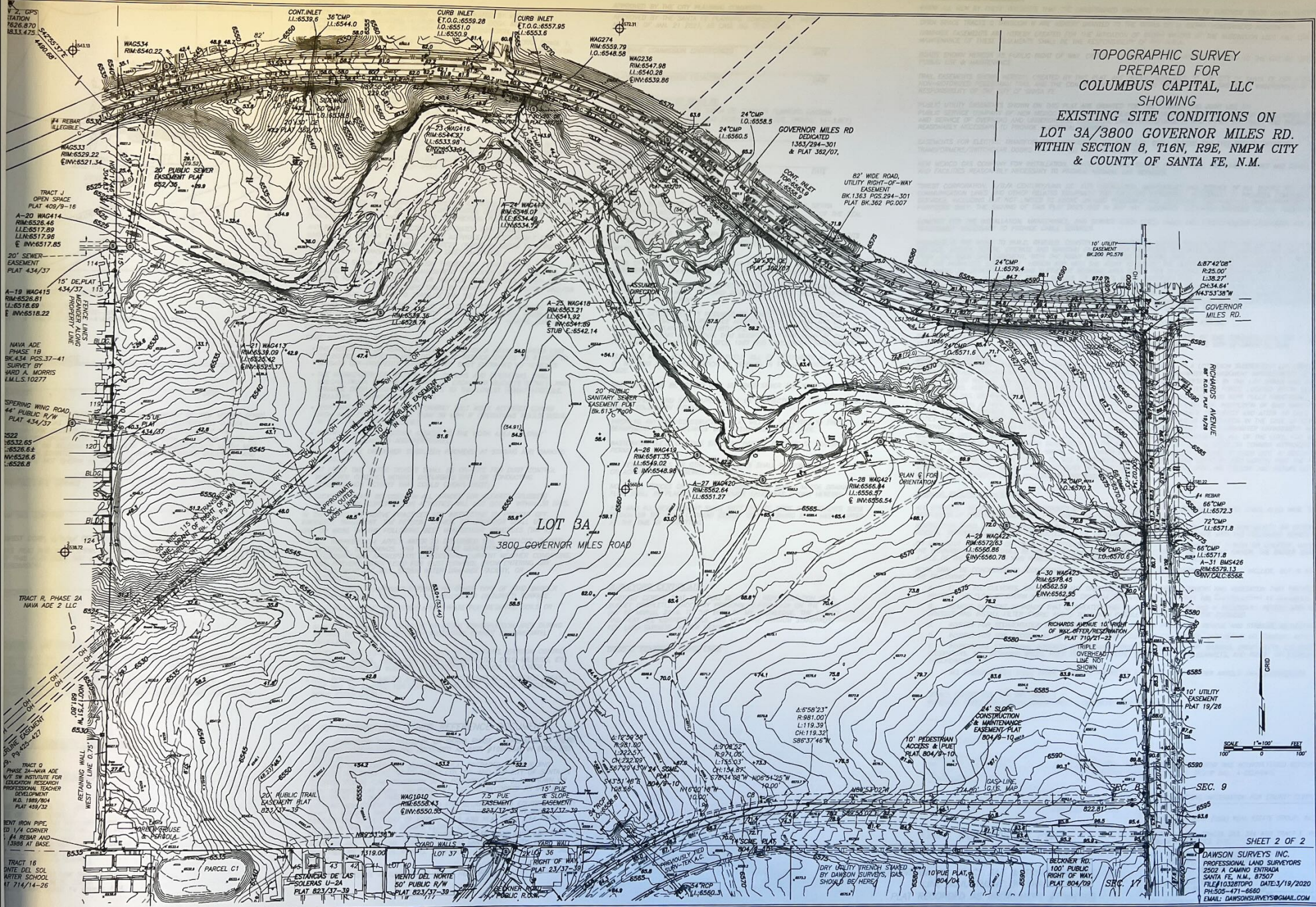
PROVIDE
STAMP.

DHI Engineering, LLC.
SAN ANTONIO, TX 78247
2101 4th Street
100% REG. NO. 11-19351



VISTAS DE LA SIERRA SUBDIVISION - PHASE 3
SANTA FE, NM
TOPOGRAPHIC SURVEY
SHEET 2 OF 2

DESIGNED BY:
DRAWN BY:
DATE: 1/28/2025
JOB NO.: 05004
SHEET NO.



SHEET 2 OF 2

DAWSON SURVEYS INC.
PROFESSIONAL LAND SURVEYORS
2502 A CAMINO ENTRADA
SANTA FE, N.M. 87507
PH:505-471-6660
DATE: 3/19/2020
EMAIL: DAWSONSURVEYS@GMAIL.COM

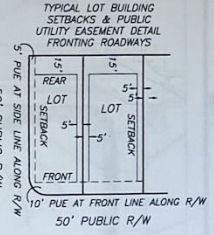
FINAL SUBDIVISION PLAT
VISTAS DE LA SIERRA PHASE 3
 SHOWING
 A SUBDIVISION OF TRACT 3A3 & TRACT 3A4 AND LOT
 LINE ADJUSTMENT OF TRACT I
 PLAT RECORDED AS INSTRUMENT #1964666
 IN BOOK 882 PAGES 2-5
 INTO 128 RESIDENTIAL LOTS

CURVE	RADIUS	DELTA	ARC	CHORD	BEARING
C56	25.00	90.0000°	39.27	35.36	N09°0'56"W
C57	25.00	90.0000°	39.27	35.36	S90°59'04"E
C58	1079.00'	0.3111°	6.79	9.49	S54°2'13"E
C60	1079.00'	2.3705°	49.31	49.30	S55°5'40"E
C61	1079.00'	2.3731°	49.44	49.44	S58°34'58"E
C62	1079.00'	2.3721°	49.39	49.39	S61°12'24"E
C63	1079.00'	2.0607°	39.59	39.58	S63°14'08"E
C64	25.00	90.0000°	39.27	35.36	S60°59'04"E
C65	14.50	90.0000°	22.78	20.51	S90°0'54"E
C66	1029.00'	0.4328°	13.01	13.01	S54°29'40"E
C67	1029.00'	2.2779°	44.15	44.14	S56°03'09"E
C68	1029.00'	2.2820°	44.28	44.28	S59°0'54"E
C69	1029.00'	2.2717°	44.08	44.08	S61°00'32"E
C70	1029.00'	2.2714°	44.17	44.17	S63°27'57"E
C71	1029.00'	2.2721°	44.28	44.28	S65°55'42"E
C72	1029.00'	2.2708°	44.04	44.04	S68°23'14"E

C73	1029.00'	2.2708°	44.04	44.04	S70°50'22"E
C74	1029.00'	2.3619°	46.79	46.79	S73°32'06"E
C75	1029.00'	2.4530°	49.54	49.54	S76°03'00"E
C76	1029.00'	2.3636°	46.87	46.87	S78°44'03"E
C77	1029.00'	2.2800°	44.30	44.30	S81°16'17"E
C78	1029.00'	2.2823°	44.42	44.41	S83°44'43"E
C79	1029.00'	2.2908°	44.63	44.63	S86°13'18"E
C80	1029.00'	0.1222°	3.70	3.70	S87°34'02"E
C81	206.00'	3.0318°	18.17	18.17	N85°09'33"W
C82	206.00'	10.2042°	37.18	37.13	N77°26'39"W
C83	206.00'	10.2115°	37.23	37.18	N67°05'45"W
C84	206.00'	10.2650°	37.55	37.50	N64°14'40"W
C85	206.00'	10.2740°	37.25	37.20	N46°17'42"W
C86	206.00'	10.2139°	37.25	37.20	N35°56'03"W
C87	206.00'	10.2173°	37.22	37.17	N25°44'17"W
C88	206.00'	10.2150°	37.35	37.30	N15°22'11"W
C89	206.00'	9.2878°	35.86	35.81	N05°01'29"W
C90	45.00'	47.4338°	37.49	36.41	N23°40'37"E
C91	45.00'	28.1548°	22.20	21.92	N61°49'35"E

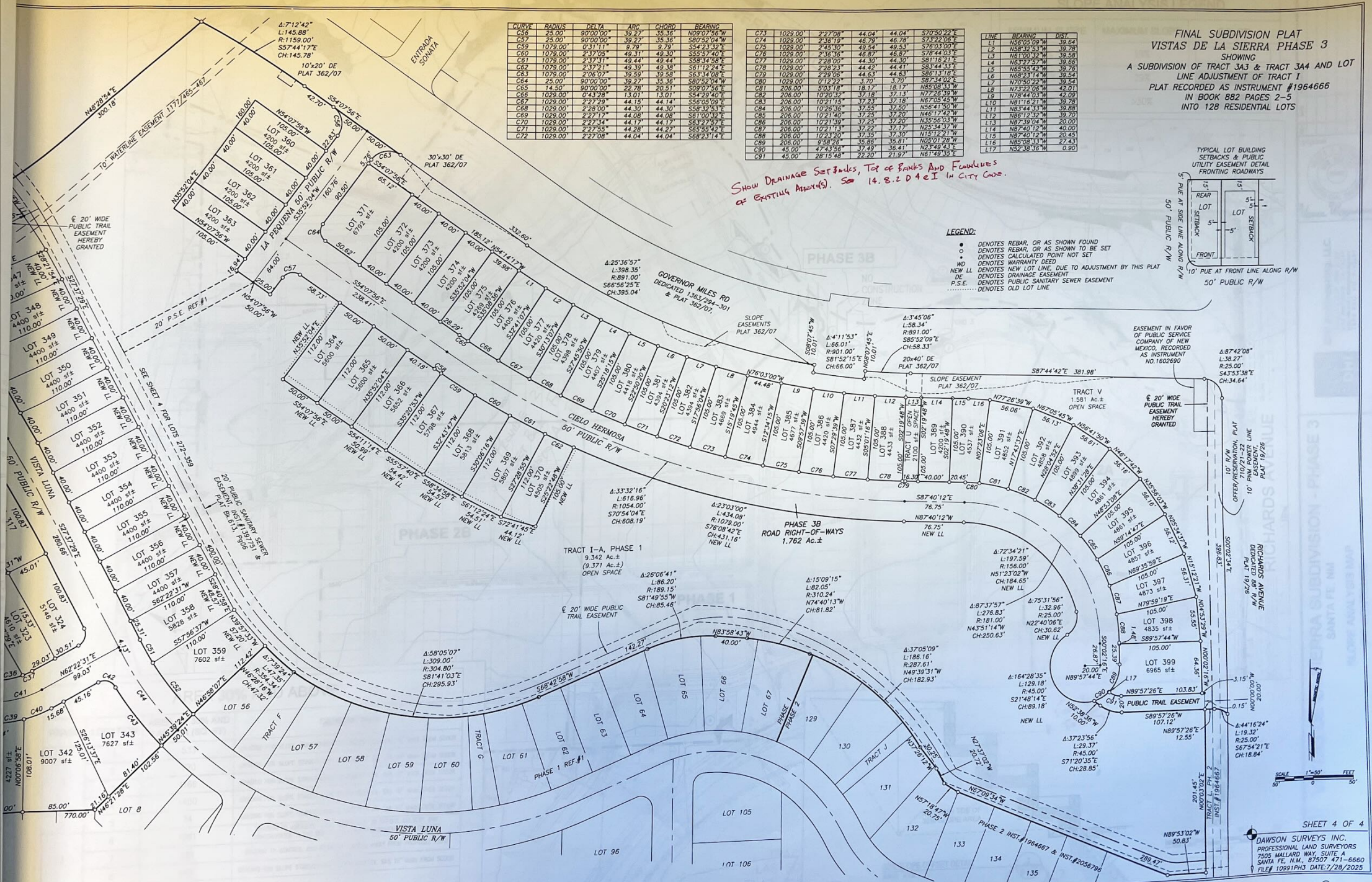
LINE	BEARING	DIST.
L1	N56°05'09"W	39.64
L2	N58°12'53"W	39.78
L3	N61°00'32"W	39.58
L4	N63°27'57"W	39.66
L5	N65°50'42"W	39.76
L6	N68°23'14"W	39.54
L7	N70°50'22"W	39.54
L8	N73°22'06"W	43.01
L9	N74°44'03"W	42.09
L10	N81°16'21"W	39.78
L11	N83°22'06"W	43.01
L12	N86°23'32"W	39.70
L13	N87°39'04"W	20.00
L14	N89°00'12"W	43.00
L15	N87°40'12"W	20.45
L16	N85°08'33"W	27.43
L17	N82°38'16"W	0.90

SHOW DRAINAGE SETBACKS, TOP OF BANKS AND FENCE LINES OF EXISTING AREAS. SEE 14.8.2 D & E I IN CITY CODE.



LEGEND:

- DENOTES REBAR, OR AS SHOWN FOUND
- DENOTES REBAR, OR AS SHOWN TO BE SET
- DENOTES CALCULATED POINT NOT SET
- WD DENOTES WARRANTY DEED
- NEW LL DENOTES NEW LOT LINE DUE TO ADJUSTMENT BY THIS PLAT
- DE DENOTES DRAINAGE EASEMENT
- P.S.E. DENOTES PUBLIC SANITARY SEWER EASEMENT
- DENOTES OLD LOT LINE



VANCE L. WEYMAN
 NEW MEXICO
 29901
 PROFESSIONAL ENGINEER
 Vance Weyman

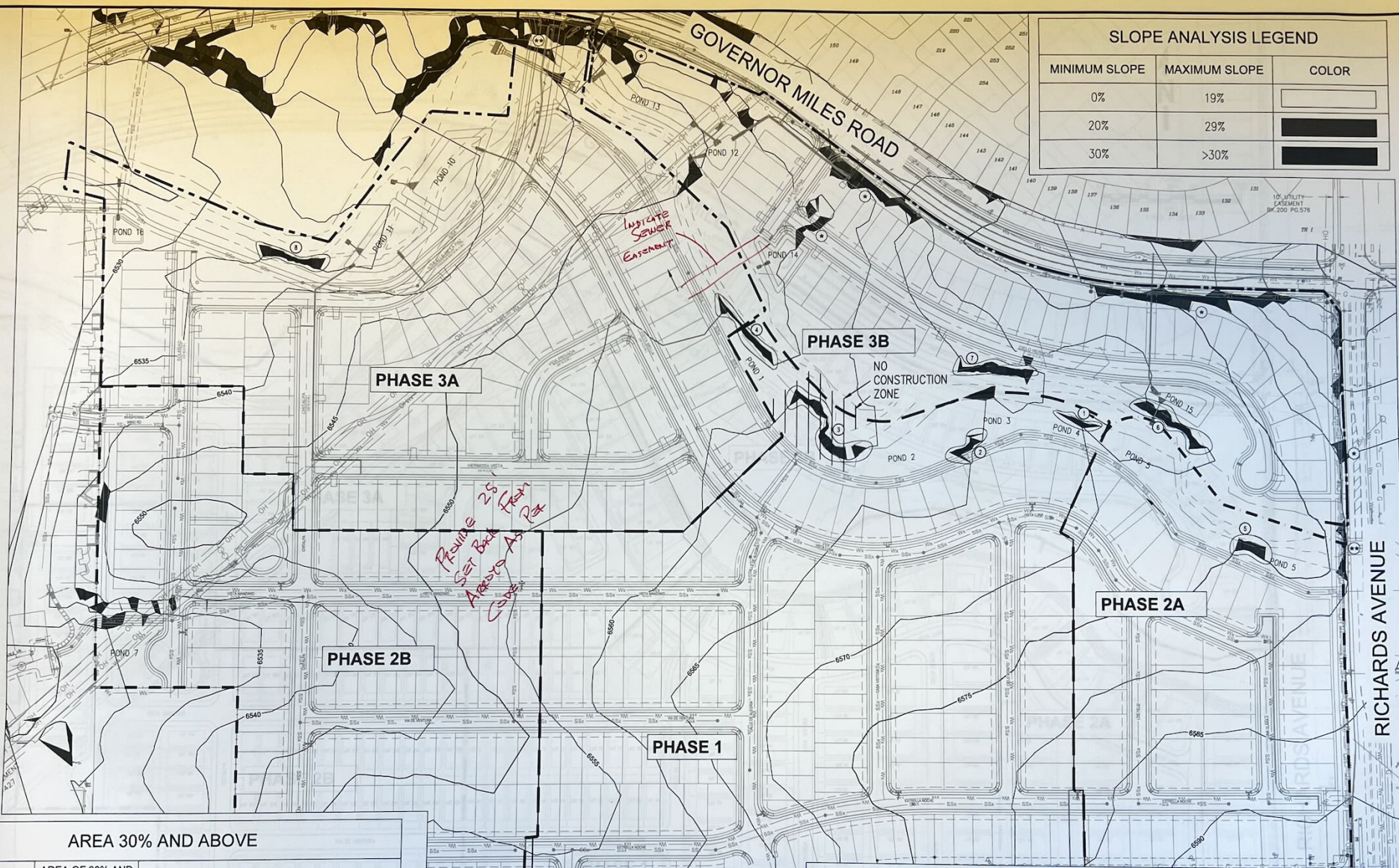
DHI Engineering, LLC.
 5419 N. LOOP 1604 EAST
 SAN ANTONIO, TX 78247
 TYPE REG. NO. F-18561



VISTAS DE LA SIERRA SUBDIVISION - PHASE 3
 SANTA FE, NM
 SLOPE ANALYSIS MAP
 5 FT CONTOURS

DESIGNED BY:
 DRAWN BY: JWS
 DATE: 5/12/2025
 JOB NO.: 05004
 SHEET NO.

SLOPE ANALYSIS LEGEND		
MINIMUM SLOPE	MAXIMUM SLOPE	COLOR
0%	19%	
20%	29%	
30%	>30%	

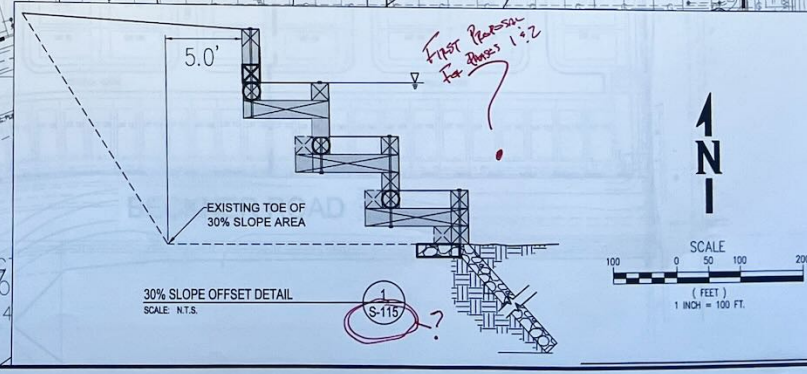


AREA 30% AND ABOVE

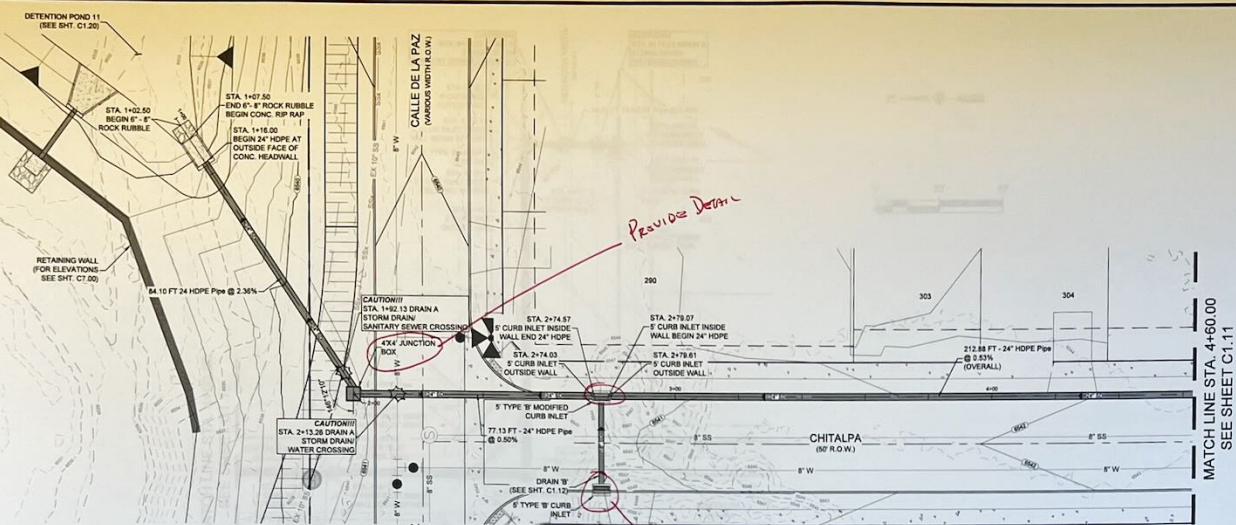
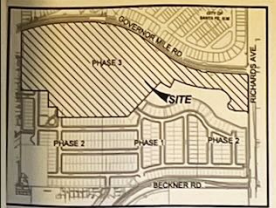
NO.	LOCATION	AREA OF 30% AND LARGER (SQ. FT)	DESCRIPTION
1	533	GRADING FOR SLOPE STABILIZATION TO PROTECT EX. SAS 10" MAIN FROM SCOUR	
2	720	GRADING FOR SLOPE STABILIZATION TO PROTECT EX. SAS 10" MAIN FROM SCOUR	
3	1166	GRADING FOR SLOPE STABILIZATION TO PROTECT EX. SAS 10" MAIN FROM SCOUR	
4	1400	GRADING FOR SLOPE STABILIZATION TO PROTECT EX. SAS 10" MAIN FROM SCOUR	
5	34	GRADING FOR SLOPE STABILIZATION TO PROTECT EX. SAS 10" MAIN FROM SCOUR	
6	1261	GRADING TO ADDRESS LOCAL SCOUR DUE TO OFFSITE CULVERT AND CONCENTRATED FLOWS DUE TO (MAN MADE) PREVIOUS DEVELOPMENTS	
7	981	GRADING TO CONTROL AND REDIRECT ARROYO AWAY FROM PROPOSED ROAD	
8	945	GRADING FOR SLOPE STABILIZATION TO PROTECT EX. SAS 10" MAIN FROM SCOUR	
TOTAL	7040		

SLOPES CREATED BY RESULT OF INFRASTRUCTURE CONSTRUCTION

SLOPES/ ARROYO THREATEN GOVERNOR MILES ROAD AND REQUIRE CORRECTIVE ACTIONS PER ANNEXATION AGREEMENT.

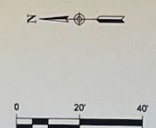


TRACT
 LAS SO
 PLAT 714



DRAIN A
STA. 1+00.00 - 4+60.00

LEGEND (DRAIN)	
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROJECT LIMITS
	PROPOSED WATER
	PROPOSED SEWER
	PROPOSED MANHOLE
	FIRE HYDRANTS
	PROPOSED WYE & LATERAL
	SINGLE WATER SERVICE
	DUEL WATER SERVICE
	STREET LIGHTS
	DRIVEWAY
	GAS, ELECTRIC, TELEPHONE & CABLE
	TELEVISION EASEMENT
	TOP OF WALL
	BOTTOM OF WALL



TRENCH EXCAVATION PROTECTION

CONTRACTOR AND CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR STRUCTURAL, GEOTECHNICAL, SAFETY EQUIPMENT CONSULTANT, IF ANY, SHALL REVIEW THESE PLANS AND ANY AVAILABLE GEOTECHNICAL INFORMATION AND THE ANTICIPATED INSTALLATION SITE WITHIN THE PROJECT'S WORK AREA IN ORDER TO IMPLEMENT THE CONTRACTOR'S TRENCH EXCAVATION SAFETY PROTECTION PROGRAMS AND PROCEDURES FOR THE PROJECT DESCRIBED IN THE CONTRACT DOCUMENTS. THE CONTRACTOR'S IMPLEMENTATION OF THESE SYSTEMS, PROGRAMS AND PROCEDURES SHALL PROVIDE FOR ADEQUATE TRENCH EXCAVATION SAFETY PROTECTION THAT COMPLY WITH AS A MINIMUM, OSHA STANDARDS FOR TRENCH EXCAVATIONS SPECIFICALLY. CONTRACTOR AND CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR SAFETY CONSULTANT SHALL IMPLEMENT A TRENCH SAFETY PROGRAM IN ACCORDANCE WITH OSHA STANDARDS GOVERNING THE PRESENCE AND ACTIVITIES OF INDIVIDUALS WORKING IN AND AROUND TRENCH EXCAVATION.

CAUTION!!!

CONTRACTOR SHALL BE REQUIRED TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES INCLUDING BUT NOT LIMITED TO: WATER, SEWER, TELEPHONE AND FIBER OPTIC LINES, SITE LIGHTING, ELECTRIC, SECONDARY ELECTRIC, PRIMARY ELECTRICAL, CABLE, LANDSCAPE IRRIGATION FACILITIES, AND GAS LINES. ANY UTILITY CONFLICTS THAT ARISE SHOULD BE COMMUNICATED TO THE ENGINEER IMMEDIATELY AND PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CONTACT 1-800-368-5858 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION. ANY DAMAGE TO EXISTING UTILITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND THE REPAIR SHALL BE AT CONTRACTOR'S EXPENSE WHETHER THE UTILITY IS SHOWN ON THESE PLANS OR NOT.

- DRAINAGE NOTES**
- CONTRACTOR SHALL COORDINATE A TRAFFIC CONTROL PLAN FOR ALL WORK WITHIN THE ROAD ADDITIONAL WORKING SIGNS MAY BE RECOMMENDED BY THE ENGINEER ONCE THE ROADWAY IS CONSTRUCTED.
 - THE CONTRACTOR WILL BE RESPONSIBLE FOR DETERMINING EXACT LOCATION OF ALL UTILITIES AND DRAINAGE STRUCTURES WHETHER SHOWN ON THE PLANS OR NOT. THE CONTRACTOR SHALL UNCOVER EXISTING UTILITIES PRIOR TO CONSTRUCTION TO VERIFY SIZE, SPACE, AND LOCATION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF ANY DAMAGE TO EXISTING UTILITIES. WHETHER SHOWN ON THE PLANS OR NOT, SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AT HIS EXPENSE.
 - ALL CONCRETE FOR MANHOLE DRAINAGE STRUCTURES SHALL MEET MINIMUM SPECIFICATIONS. ALL OTHER CONCRETE SHALL BE CLASS "C" 3000 PSI CYLINDER STRENGTH IN 28 DAYS.
 - REFERENCE DRAINAGE DETAILS FOR PIPE TRENCH DETAIL, BOX CULVERT, MANHOLE, MANHOLE CONSTRUCTION DETAILS AND BOX CULVERT BEDDING AND EXCAVATION LIMITS.
 - CONTRACTOR SHALL BUILT ALL CURB INLETS AND JUNCTION BOXES TO PROVIDE FOR POSITIVE DRAINAGE.
 - CONTRACTOR SHALL MATCH TOP OF CHANNEL TO NATURAL GROUNDING AND MAINTAIN A MINIMUM CHANNEL DEPTH AS SHOWN IN THE PROFILE.

DATE: 5/14/2025

NAME: L. WEYMAN
NEW MEXICO
29007
PROFESSIONAL ENGINEER

Vance Weyman

DHI Engineering, LLC.
5410 N. LOOP 1504 EAST
DALLAS, TEXAS 75244
TBP REG. NO. F-19581

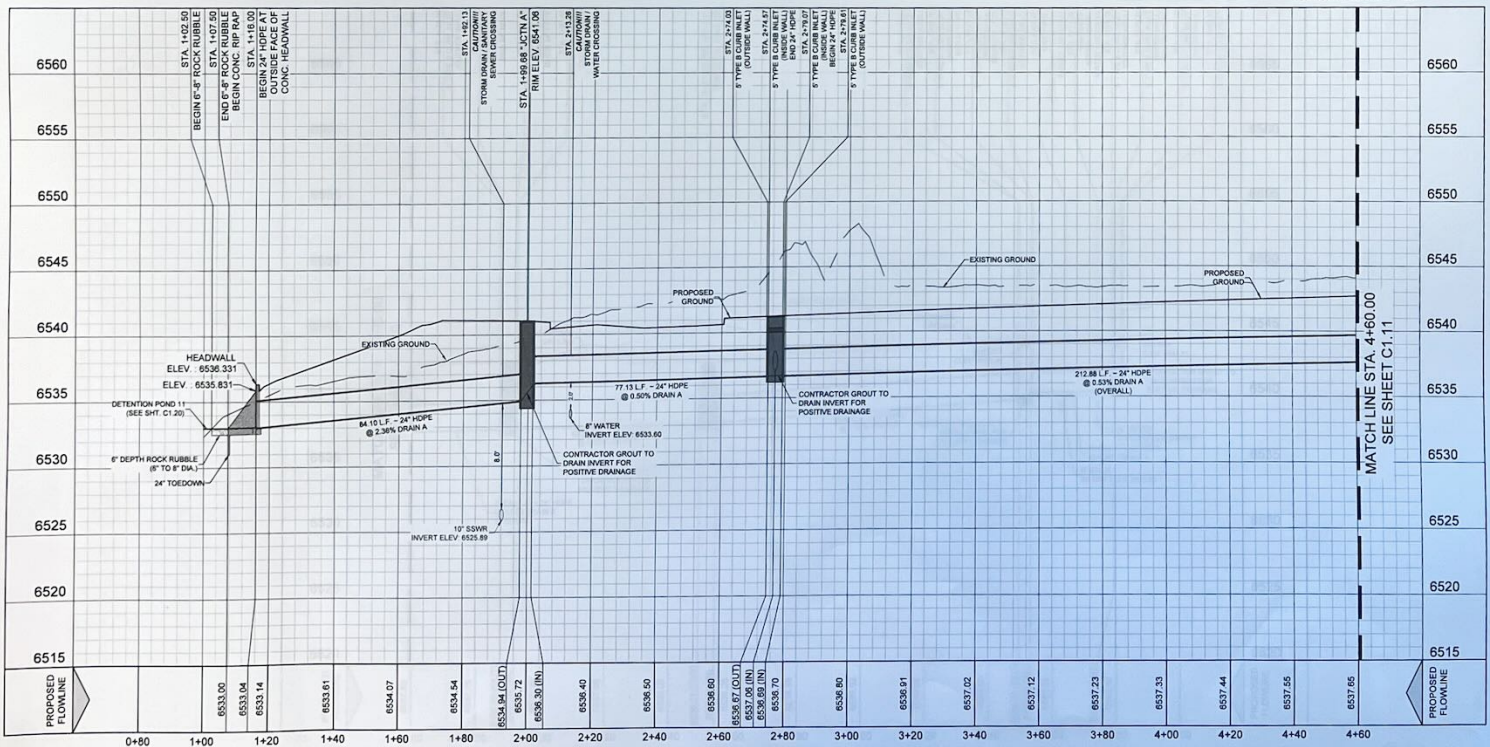
DHI

VISTAS DE LA SIERRA SUBDIVISION - PHASE 3
SANTA FE, NM

DRAIN A - STA. 1+00.00 TO 4+60.00

DESIGNED BY: JWS
DRAWN BY: JWS
DATE: 5/14/2025
JOB NO.: 05004
SHEET NO.:

C1.10



VERTICAL SCALE: 1" = 5'
HORIZONTAL SCALE: 1" = 20'

CASE PERMIT # 5-2003-2942

VISTA DE LA SIERRA SUBDIVISION

DATE: 5/14/2025

NAME: L. WEYMAN
NEW MEXICO
29007
PROFESSIONAL ENGINEER

Vance Weyman

DHI Engineering, LLC.
5410 N. LOOP 1504 EAST
DALLAS, TEXAS 75244
TBP REG. NO. F-19581

DHI

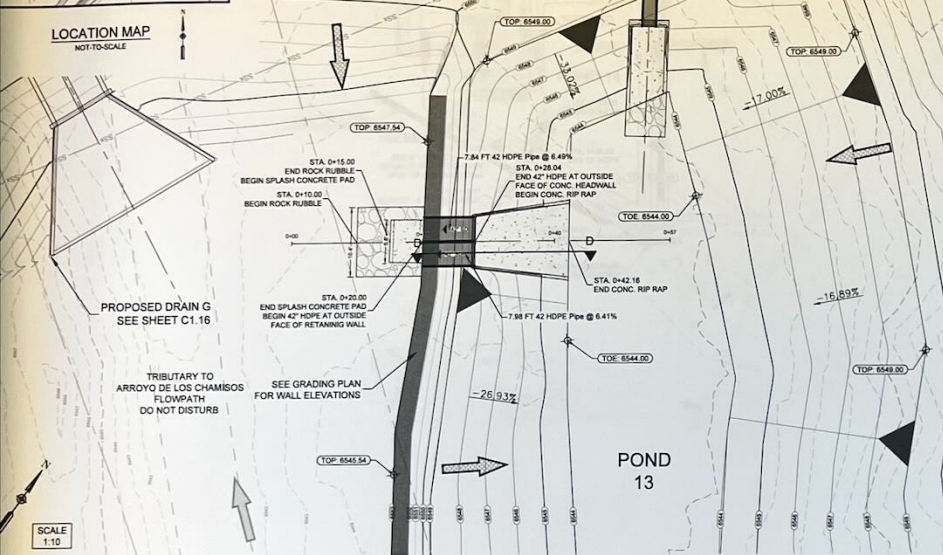
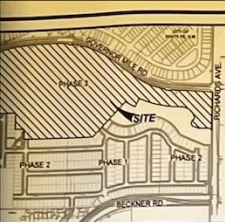
VISTAS DE LA SIERRA SUBDIVISION - PHASE 3
SANTA FE, NM

DRAIN A - STA. 1+00.00 TO 4+60.00

DESIGNED BY: JWS
DRAWN BY: JWS
DATE: 5/14/2025
JOB NO.: 05004
SHEET NO.:

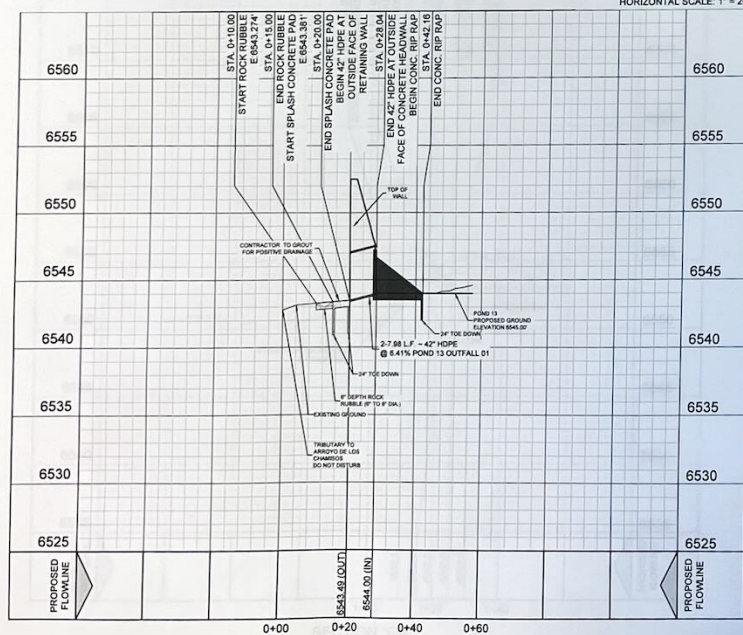
C1.10

DATE: MAY 14, 2025, 10:30AM



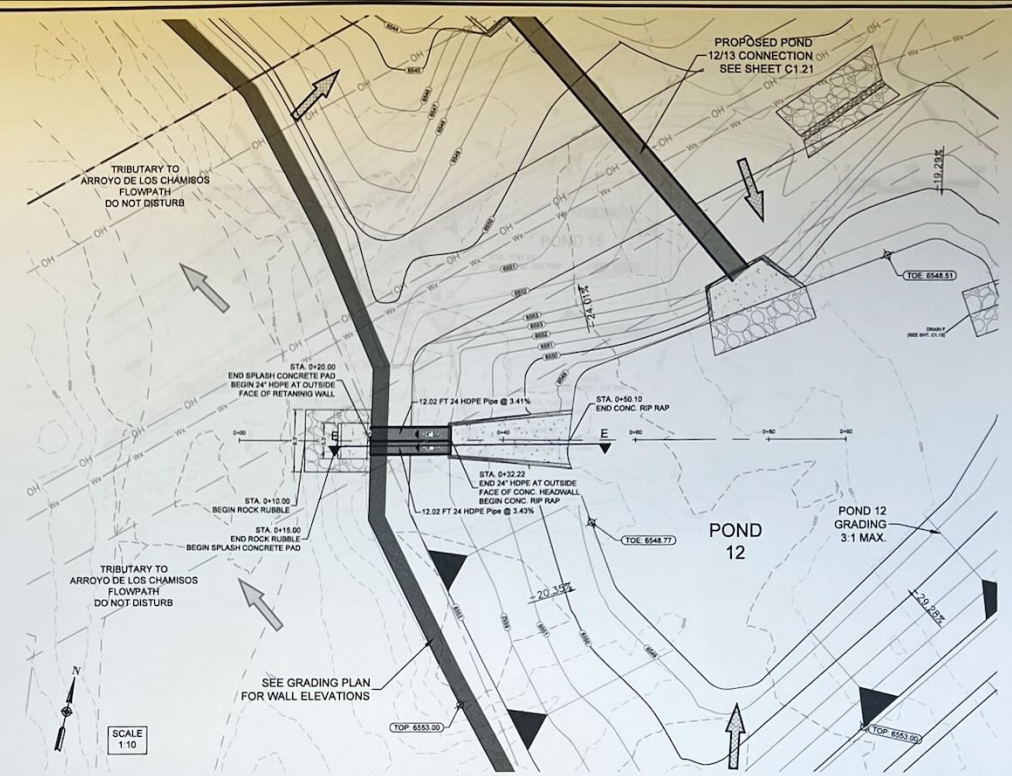
POND 13 OUTFALL 01
STA. 1+00.00 - 4+60.00

VERTICAL SCALE: 1" = 5'
HORIZONTAL SCALE: 1" = 20'



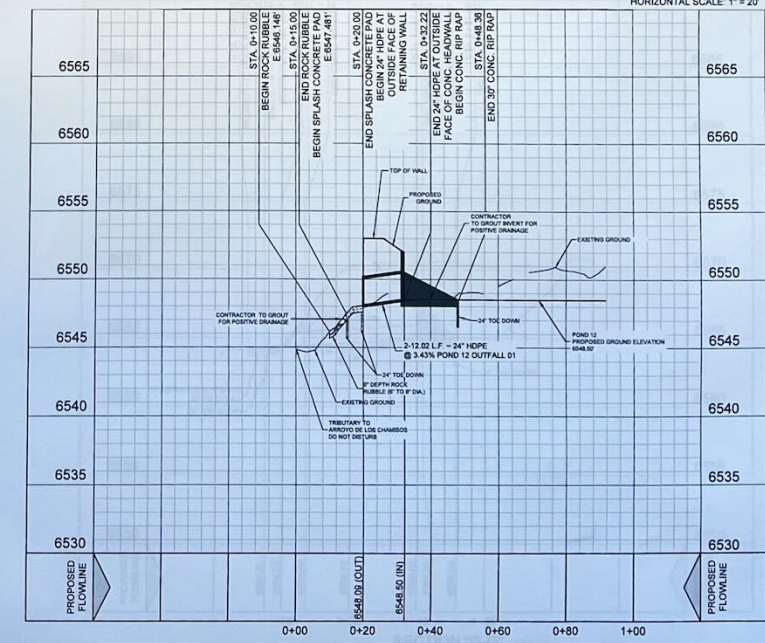
SECTION "D-D"

Show Pond Inverts on Plan View



POND 12 OUTFALL 01
STA. 1+00.00 - 4+60.00

VERTICAL SCALE: 1" = 5'
HORIZONTAL SCALE: 1" = 20'



SECTION "E-E"

REV DATE DESCRIPTION
5/14/2025

VANCE L WEYMIND
NEW MEXICO
29907
PROFESSIONAL ENGINEER
Vance Weymound

DHI Engineering, LLC.
5419 N. LOOP 1634 EAST
SAN ANTONIO, TX 78247
TX REG. NO. F-15881
TYPE REG. NO. F-15881

DHI

VISTAS DE LA SIERRA SUBDIVISION - PHASE 3
SANTA FE, NM

POND '12 & 13' SECTIONS

DESIGNED BY: AS
DRAWN BY: AS
DATE: 5/14/2025
JOB NO.: 05004
SHEET NO.: C1.22

DATE: MAY 14, 2025, 11:55am User: G:\adrian\proj\05004\Drawings\01\05004-C1.22.dwg Plot: 12 x 18 - 50% Scale

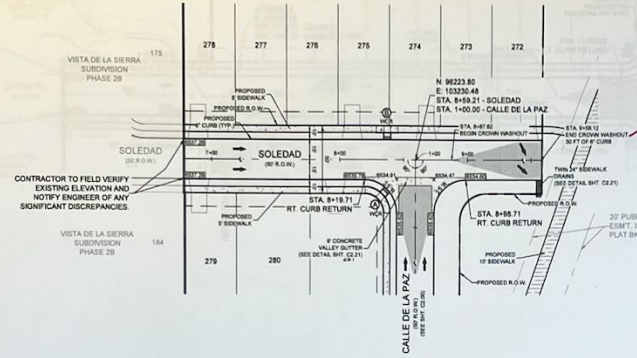
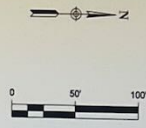
DATE	REV	DESCRIPTION
5/12/2025		

LEGEND (STREET)	
	PROJECT LIMITS
	EXISTING CONTOUR
	PROPOSED CONTOUR
	WHEELCHAIR RAMP TYPE
	POINT OF CURVATURE
	POINT OF TANGENCY
	DRAINAGE FLOW ARROW
	TOP OF CURB ELEVATION
	PAVEMENT ELEVATION
	CROWN WASHOUT
	DRIVEWAY
	SIDEWALK (DEVELOPER RESPONSIBILITY)
	SIDEWALK (HOMEBUILDER RESPONSIBILITY)

- STREET NOTES:**
- A SANTA FE COUNTY PERMIT MUST BE OBTAINED BEFORE WORKING IN SANTA FE COUNTY. THE CONTRACTOR SHALL COORDINATE A TRAFFIC CONTROL PLAN FOR ALL WORK UPON THE ROAD. ADDITIONAL WARNING SIGNS MAY BE RECOMMENDED BY THE ENGINEER ONCE THE ROADWAYS ARE CONSTRUCTED.
 - CONTRACTOR SHALL MATCH EXISTING PAVEMENT AT 16" IN IF EXISTING PAVEMENT ELEVATION DIFFERS SIGNIFICANTLY. CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO CONTINUING WORK.
 - SIDEWALKS SHALL BE CONSTRUCTED 8 FT FROM THE BACK OF CURB FOR ALL LOCATIONS WHERE THE SIDEWALK IS BIDDING. REFER TO STREET DETAIL SHEET FOR SIDEWALK AND RAMP DETAILS.
 - NO PERMANENT STRUCTURES HIGHER THAN 3 FT. AND LOWER THAN 8 FT. FROM THE PAVEMENT INCLUDING STRUCTURES, WALLS, FENCES AND SIGNPOSTS SHALL BE CONSTRUCTED OR ALLOWED WITHIN THE CLEAR VISION TRIANGLE. CONTRACTOR SHALL GRADE AREAS WITHIN CLEAR VISION TRIANGLES SUCH THAT THE ELEVATION WITHIN THE CLEAR VISION TRIANGLE IS NOT LOWER THAN 1 FT ABOVE THE ADJACENT TOP OF PAVEMENT.
 - DRIVEWAYS BIDDING ON THIS PLAN ARE FOR THE SELLER'S PURPOSE OF INDICATION. A POTENTIAL CONFLICT WITH CURB RAMP DRAINAGE INFRASTRUCTURE OR OTHER CONFLICT DRIVEWAY LOCATIONS IS SUBJECT TO CHANGE BASED ON HOME SELECTION AND FINAL LOT DESIGN.
 - CHANGES IN THE SIDEWALK LOCATION FOR A MINIMUM LINEAR DISTANCE OF TWO (2) FEET OR MORE ARE FORWARDED TO BE APPROVED BY THE FIELD INSPECTOR WITHOUT AMENDING THE STREET PLAN OR UTILITY LAYOUT.
 - ALL RAMP AND ANY PROPOSED SIDEWALKS THAT DO NOT FRONT PROPOSED RESIDENTIAL LOTS ARE REQUIRED TO BE CONSTRUCTED WITH STREET CONSTRUCTION.
 - FOR CROWN WASHOUT TRANSITIONS SEE DETAIL ON SHEET C2.21



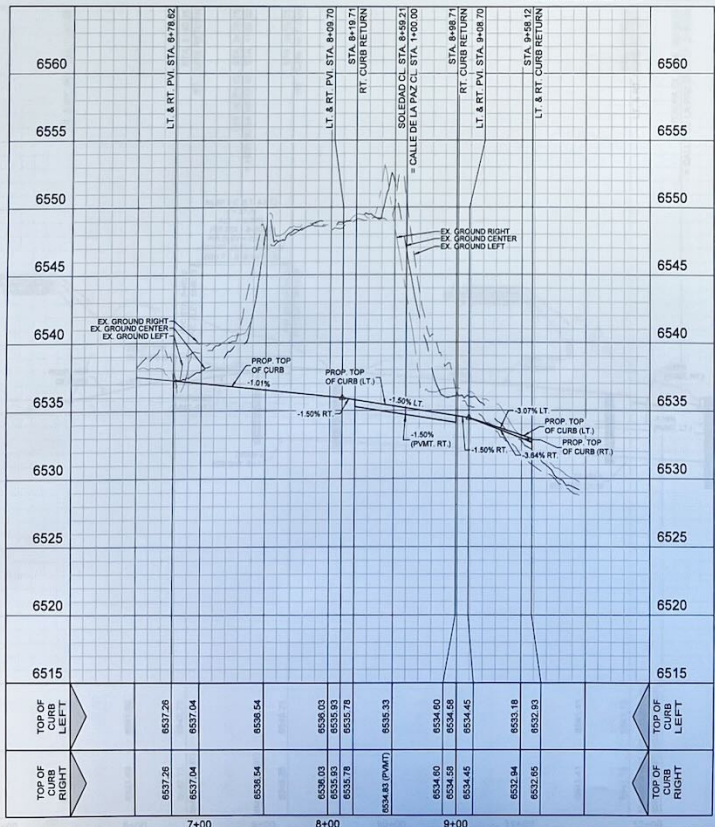
LOCATION MAP
NOT-TO-SCALE

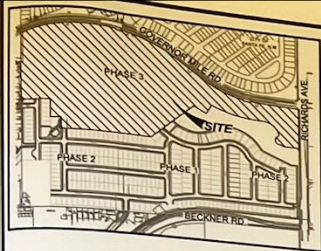


SOLEDAD
STA. 6+78.62 TO 9+58.12

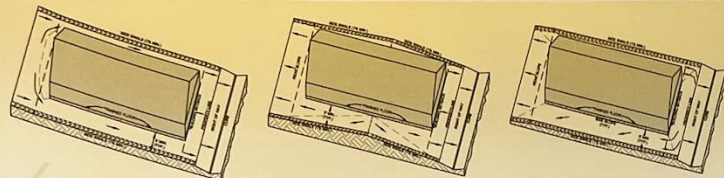
TRANSITION?
City Reviewers Advise
Ramps at Point of Curves 1
Remove Turn on Trac (Ramp)
Will Passways Be Increased?

VERTICAL SCALE: 1" = 5'
HORIZONTAL SCALE: 1" = 50'





LOCATION MAP
NOT-TO-SCALE



TYPE 'A' LOT GRADING

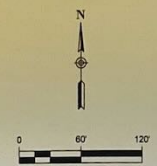
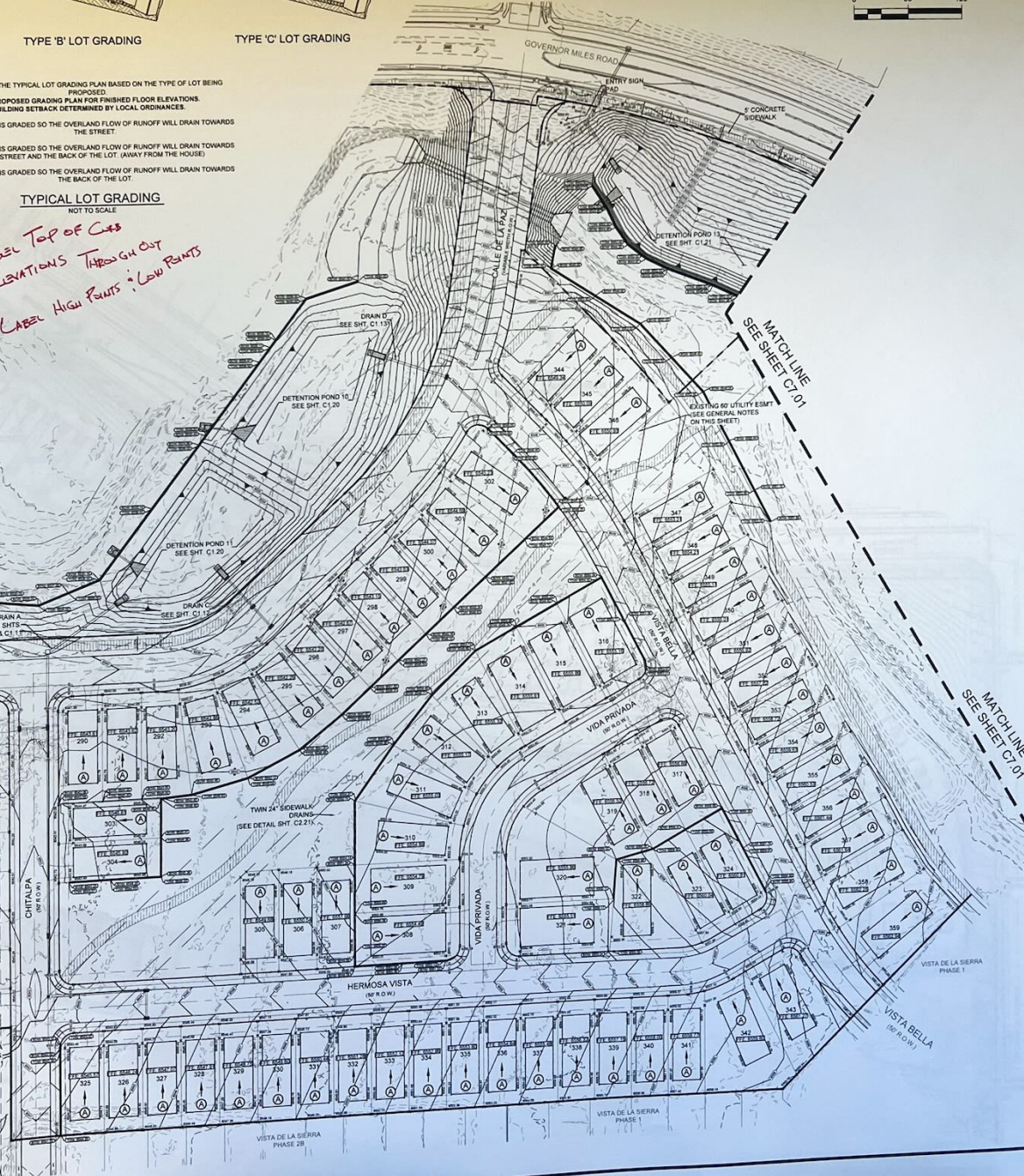
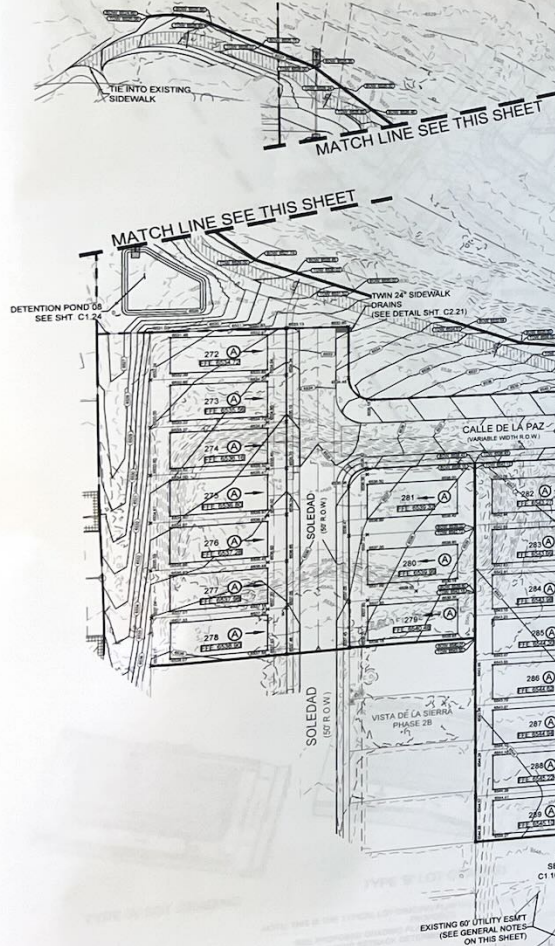
TYPE 'B' LOT GRADING

TYPE 'C' LOT GRADING

- NOTE: THIS IS THE TYPICAL LOT GRADING PLAN BASED ON THE TYPE OF LOT BEING PROPOSED.
 *SEE PROPOSED GRADING PLAN FOR FINISHED FLOOR ELEVATIONS.
 *BUILDING SETBACK DETERMINED BY LOCAL ORDINANCES.
- TYPE A LOTS IS GRADED SO THE OVERLAND FLOW OF RUNOFF WILL DRAIN TOWARDS THE STREET.
 - TYPE B LOTS IS GRADED SO THE OVERLAND FLOW OF RUNOFF WILL DRAIN TOWARDS THE STREET AND THE BACK OF THE LOT (AWAY FROM THE HOUSE).
 - TYPE C LOTS IS GRADED SO THE OVERLAND FLOW OF RUNOFF WILL DRAIN TOWARDS THE BACK OF THE LOT.

TYPICAL LOT GRADING
NOT-TO-SCALE

*Label Top of Curb
Elevations Through Out
Label High Points + Low Points*



LEGEND (GRADING)	
	PROJECT LIMITS
	OFFSITE EASEMENT
	EXISTING CONTOUR
	PROPOSED CONTOUR
	FLOW ARROW
	TYPICAL LOT GRADING
	PROPOSED ELEVATION
	GRADE BREAK
	TOP OF WALL ELEV.
	BOTTOM OF WALL ELEV.
	FINISHED FLOOR ELEV.
	SIDEWALK (DEVELOPER RESPONSIBILITY)
	SIDEWALK (HOMEBUILDER RESPONSIBILITY)

- GENERAL GRADING NOTES:**
1. ALL CONSTRUCTION PROCEDURES AND MATERIALS WITHIN THE SCOPE OF THE PROJECT THAT IS NOT SPECIFICALLY COVERED IN THE PLANS OF GEOTECHNICAL REPORT SHALL BE CONFORMED TO ALL APPLICABLE CITY, COUNTY AND MDCOT STANDARD REQUIREMENTS FOR PUBLIC WORKS CONSTRUCTION.
 2. SITE PREPARATION AND GRADING SHALL BE COMPLETED IN ACCORDANCE WITH THE PROJECT'S GEOTECHNICAL REPORT AND SPECIFICATIONS.
 3. ALL SELECT FILL MATERIALS PROVIDED SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER PRIOR TO PLACING AND COMPACTING.
 4. ALL ELEVATIONS AND PROPOSED CONTOURS SHOWN ON THIS PLAN REFLECTS FINISHED GRADES. THE THICKNESS OF PAVEMENT, BASE, GRADE, AND TOP SOIL MUST BE SUBTRACTED TO OBTAIN SUBGRADE ELEVATIONS.
 5. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY QUESTIONS THAT HAVE REGARDING THE INTENT, PLACEMENT OR LIMITS OF DIMENSIONS OR GRADIES NECESSARY FOR CONSTRUCTION OF THIS PROJECT.
 6. THE CONTRACTOR SHALL VERIFY THE SUITABILITY OF ALL EXISTING AND PROPOSED SITE CONDITIONS INCLUDING GRADES AND DIMENSIONS FOR THE EXISTING STATE OF CONSTRUCTION. IF THERE ARE ANY DISCREPANCIES, CONTACT THE ENGINEER.
 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ALL PERMITS, TEST, APPROVALS AND ACCEPTANCES REQUIRED TO COMPLETE CONSTRUCTION OF THIS PROJECT.
 8. THE CONTRACTOR SHALL REMOVE TOP SOIL, GRASS, ROOTS, DEBRIS, ETC. AND DISPOSE OF THOSE ITEMS THAT ARE NOT SUITABLE FOR LIMBARKING AND TOPSOIL. CLEAN STRIPPING AND TOPSOIL MAY BE STOCKPILED ON SITE FOR REUSE IN A LOCATION SPECIFIED BY THE OWNER.
 9. THE CONTRACTOR IS RESPONSIBLE FOR REVEGETATE ALL DISTURBED AREAS IN ACCORDANCE WITH PROJECT SPECIFICATION AND WREDEVELOPP REQUIREMENTS. REFER TO THE LANDSCAPE ARCHITECT'S PLAN IF APPLICABLE.
 10. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO KEEP ORANGE AND RED FROM NEARING DRIVE ADJACENT PROPERTY, CURBS OR DRAINAGE WAY. CONTRACTOR SHALL IMMEDIATELY REMOVE ALL DEBRIS THAT WARMS OFFSITE OR INTO EXISTING STORM DRAIN SYSTEMS. REFER.
 11. THE CONTRACTOR SHALL OBTAIN GRADES SHOWN HEREIN WITHIN A ONE TENTH OF A FOOT.
 12. IN PROPOSED PAVED AREAS STREET DESIGN PLANS SHALL CONTROL. ALL EARTHEN SLOPES SHALL BE A MAXIMUM OF 3:1 AND A MINIMUM OF 1:5 UNLESS OTHERWISE STATED.
 13. THE CONTRACTOR SHALL PROVIDE A SMOOTH TRANSITION BETWEEN EXISTING AND PROPOSED SITE CONDITIONS.
 14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING ANY DAMAGE DONE TO EXISTING TREES, BUILDINGS, UTILITIES, FENCES, PAVEMENT, CURBS, OR DRIVEWAYS BACK TO EXISTING CONDITIONS OR BETTER, (NO SEPARATE PAY ITEMS).
 15. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION IN WORKING NEAR UTILITIES. EDUCATION. CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND ASSURE HIMSELF THAT ALL UTILITIES HAVE BEEN ACCURATELY LOCATED AND IDENTIFIED. ENGINEER SHALL BE NOTIFIED IF ANY UTILITY CONFLICTS ARE IDENTIFIED.
 16. UTILITIES SHOWN ON THE PLANS ARE FROM INFORMATION SOURCES AVAILABLE AT THE TIME OF DESIGN BUT MAY NOT REPRESENT ALL EXISTING UTILITIES ON SITE. THE CONTRACTOR WILL BE RESPONSIBLE FOR DETERMINING EXACT LOCATIONS OF ALL UTILITIES. WEATHER SHOWN ON THE PLANS OR NOT. THE CONTRACTOR SHALL UNCOVER EXISTING UTILITIES BEFORE CONSTRUCTION TO CONFIRM SIZE, GRADE AND LOCATION AND WILL CONTACT THE ENGINEER OF ANY DEVIATIONS FROM THE PLANS PRIOR TO BEGINNING CONSTRUCTION. ANY DAMAGE TO EXISTING UTILITIES WHETHER SHOWN ON THE PLANS OR NOT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND THEIR OWN EXPENSE.
 17. POSITIVE DRAINAGE SHALL BE MAINTAINED THROUGHOUT THE SCOPE OF THE PROJECT. DRAINAGE SHALL BE DIRECTED AWAY FROM ALL BUILDING FOUNDATIONS. CONTRACTOR SHALL TAKE PRECAUTIONS NOT TO ALLOW ANY PONDING OF WATER. CONTRACTOR SHALL TAKE PRECAUTIONS TO PROTECT EXISTING UTILITIES.
 18. FILL PLACED ON HILL SIDES OR STEEP SLOPED AREAS, THE CONTRACTOR SHALL REFER TO THE PROJECT'S SPECIFICATIONS AND GEOTECHNICAL REPORT FOR SPECIAL INSTRUCTION REGARDING BENCHING.
 19. NO WORK SHALL BE PERFORMED IN A PUBLIC RIGHT-OF-WAY WITHOUT A PERMIT.

TRENCH EXCAVATION PROTECTION
 CONTRACTOR AND GEOTECHNICAL SAFETY EQUIPMENT. INFORMATION AND FOR STRUCTURAL DESIGN, GEOTECHNICAL SAFETY EQUIPMENT. INFORMATION AND SHALL REVEAL THESE PLANS AND ANY AVAILABLE GEOTECHNICAL INFORMATION IN ORDER TO THE ANTICIPATED INSTALLATION SITE WITHIN THE PROJECT'S WORK AREA IN ORDER TO IMPLEMENT CONTRACTOR'S TRENCH EXCAVATION SAFETY PROTECTION SYSTEM. PROGRAMS AND PROCEDURES FOR THE PROJECT DESCRIBED IN THE CONTRACT DOCUMENTS. THE CONTRACTORS IMPLEMENTATION OF THESE SYSTEMS, PROGRAMS AND PROCEDURES SHALL PROVIDE FOR ADEQUATE TRENCH EXCAVATION SAFETY AND PROTECTIVE MEASURES TO PROTECT EXISTING UTILITIES. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE CITY, COUNTY AND STATE REQUIREMENTS SPECIFICALLY GOVERNING THE PRESENCE AND RETAINED EMPLOYEE OR SAFETY CONSULTANT. THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND EXISTING UTILITIES SHALL BE THE CONTRACTOR'S EXPENSE WHETHER THE UTILITY IS SHOWN ON THESE PLANS OR NOT.

CAUTION!!!
 CONTRACTOR SHALL BE REQUIRED TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES INCLUDING BUT NOT LIMITED TO: WATER, SEWER, TELEPHONE AND FIBER OPTIC LINES, SITE LIGHTING, ELECTRIC, SECONDARY ELECTRIC, PRIMARY ELECTRICAL, GAS LINES, ANY UTILITIES, DUCTS, LANDSCAPE IRRIGATION FACILITIES, AND GAS LINES. ANY UTILITIES THAT ARE SHOWN ON THESE PLANS, THE CONTRACTOR SHALL CONTACT 1-800-GO-TERRA AND PRIOR TO CONSTRUCTION. THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND EXISTING UTILITIES SHALL BE THE CONTRACTOR'S EXPENSE WHETHER THE UTILITY IS SHOWN ON THESE PLANS OR NOT.

VISTAS DE LA SIERRA SUBDIVISION
 SANTA FE, NM
 OVERALL GRADING PLAN
 C7.00

DESIGNED BY: JWS
 DRAWN BY: JWS
 DATE: 6/11/2025
 JOB NO: 05004
 SHEET NO: C7.00

VANCE W. WETLAND PROFESSIONAL ENGINEER 20097
 Vance Weyland

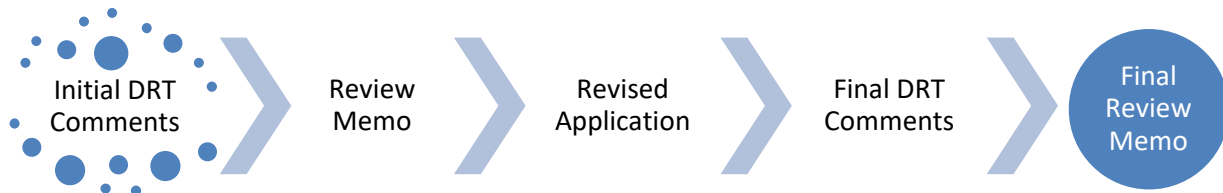
DHI Engineering, LLC
 5419 N. LOOP, SUITE 100, EAST TULSA, OKLAHOMA 74116
 TEL: 918-438-1111 FAX: 918-438-1112
 WWW.DHIENGINEERING.COM
 LICENSE NO. 11-1387

DHI

Development Review Team (DRT) Comment Form

DRT Review Schedule – 9-12+ weeks*

Initial DRT Comments are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

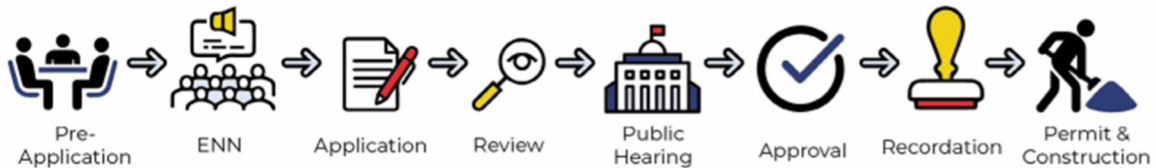


Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior to Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *During Construction* – these conditions/technical corrections can be addressed during construction.

Development Review Process Flow Chart



*See the *2024 Development Review Schedule* for details

DRT Review Timeline:

Application Received	DRT Application Intake	DRT Initial Review Comments Due

Date: September 10, 2025

DRT Member: Lawrence Rivera

Dept/Div: Land Use Technical Review, Landscape, Irrigation, and Outdoor Lighting

Case No.: Case #: 2025-11006 - Vistas de la Sierra Subdivision Phase 3

Case Planner: Claudia Kath, Senior Planner, cmkath@santafenm.gov, Office: 505-955-6661

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case #

Conditions of Approval:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant response**:
1. Provide a significant tree survey, list species, size, and quantities. Any trees to be removed shall be mark on the plan with a red X.	Prior to Recordation	
2. Provide required street trees and mulch within the subdivision, on the landscape and irrigation sheets. Add a note indicating that the street trees fronting residences shall be installed with each lot building permit and that irrigation shall be provided as part of the construction of each residential lot. Include in street tree count.	Prior to Recordation	
3. Desert Willow (<i>Chilopsis linearis</i>) shall not be used as a street tree within the parkway (between curb and sidewalk) due to its low branching and general growth habits. Provide a replacement tree for all Desert Willow trees used as a street tree in parkways.	Prior to Recordation	
4. The required open space evergreen count is short 28 shrubs, revise and resubmit.	Prior to Recordation	
5. Identify each type of mulch within the landscape using a different hatch mark.	Prior to Recordation	

6. Review stone colors, Staff has concerns with the effect of reflective heat on the plant material.	Prior to Recordation	
7. Staff respectfully request areas of mass plantings be mulched with non-stone mulches and a reduction of gravel/cobble stone over weed fabric mulching by fifty percent.	Prior to Recordation	
8. Review the street trees along Richards Avenue for compliance with code 14-8.4(G)(3)(f) street trees located under utility lines shall be a species that maintains a minimum of five (5) feet of clearance from overhead utility lines at maturity.	Prior to Recordation	
9. Tree counts for Common Hackberry (Celtis Occidentalis) are excessive due to the poor growth caused by sunscald and wind damage. Staff respectfully request a reduction in the use of this plant and one or two alternate tree varieties be chosen from the city approved plant list.	Prior to Recordation	
10. Landscape irrigation sheets shall utilize a scale not greater than one-inch equals 20 feet (Engineers 20 scale).	Prior to Recordation	
11. Provide native seeding calculations within the Water Budget.	Prior to Recordation	
12. Staff have concerns with the rotor sprinklers that throw 50-feet with trees and shrubs within 10-15 feet in front of the sprinklers. Rotors cannot spray into tree canopies.	Prior to Recordation	
13. Landscape irrigation meters are not shown on the Water Distribution Plans C5.0 & C5.01.	Prior to Recordation	
14. Provide Retaining wall, perimeter walls, yard walls, and fence sheets including details.	Prior to Recordation	
15. Stormwater Ponds: A water level measuring device with zero set at finish grade located at the center of each pond is required. Retention ponds deeper than three feet require a security fence and maintenance gate.	Prior to Recordation	

Fence shall be five in height. The following shall also apply 14-8.4(J) Screening and Buffering, 14-8.5 Walls and Fences.		
<p>16. Provide an outdoor lighting plan with photometric data per 14-8.9(C) COSF code. Include:</p> <p>(b) a description of the luminaires, including lamps, poles or other supports, and shielding devices, that may be provided as catalogue cuts from the manufacturer; and</p> <p>(c) Photometric data, such as that furnished by the manufacturer, showing the angle of light emission.</p>	Prior to Recordation	

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

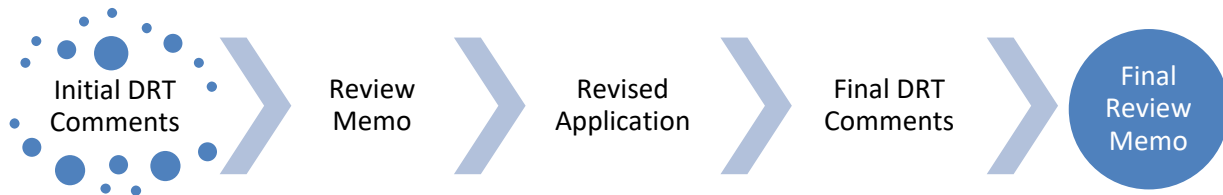
Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Development Review Team (DRT) Comment Form

DRT Review Schedule – 9-12+ weeks*

Initial DRT Comments are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

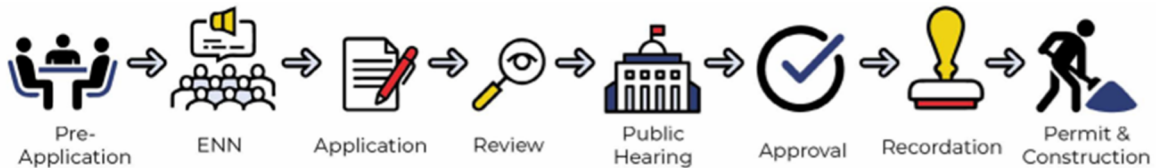


Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior to Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *During Construction* – these conditions/technical corrections can be addressed during construction.

Development Review Process Flow Chart



*See the *2024 Development Review Schedule* for details

DRT Review Timeline:

Application Received	DRT Application Intake	DRT Initial Review Comments Due

Date: 9/16/2025

DRT Member: Teddy Padilla

Dept/Div: Land Use / Technical Review

Case No.: 2025-11006

Case Planner: Claudia Kath

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case #

Conditions of Approval:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant response**:
1. A concrete curb Ramp for pedestrian crossing must be added at the southwest corner at the intersection of Richards Ave and Governor Miles. Curb Ramp shall be designed accordingly to get pedestrians across Governor Miles Rd. Which will include striping and mid row crossing if necessary.	Permitting	
2. Extend/Add the sidewalk going north on Calle De La Paz from the Vista Bella intersection.		
3.		
4.		

**The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

KATH, CLAUDIA M.

From: DURAN, PAUL A.
Sent: Friday, September 12, 2025 3:15 PM
To: KATH, CLAUDIA M.; PADILLA, TEDDY L.; YNGVE, LEAH X.; JURGENS, TAYLOR R.; ISAACSON, ZOE R.; VALORA SANDOVAL, SERGIO A.; HOLLAND, TOWNSEND S.; HOOK, ALAN G.
Subject: RE: Case #: 2025-11006 - Vistas de la Sierra Subdivision Phase 3

Hi Claudia,

I have reviewed the ARC action letter, and they are cleared from any further archaeological review. Please let me know if you will need any further information from me.

Thank you and hope you all have a wonderful weekend,

Paul

From: KATH, CLAUDIA M. <cmkath@santafenm.gov>
Sent: Friday, September 12, 2025 3:11 PM
To: PADILLA, TEDDY L. <tlpadilla@santafenm.gov>; DURAN, PAUL A. <paduran@santafenm.gov>; YNGVE, LEAH X. <lxngve@santafenm.gov>; JURGENS, TAYLOR R. <trjurgens@santafenm.gov>; ISAACSON, ZOE R. <zrisaacson@santafenm.gov>; VALORA SANDOVAL, SERGIO A. <savalorsandoval@santafenm.gov>; HOLLAND, TOWNSEND S. <tsholland@santafenm.gov>; HOOK, ALAN G. <aghook@santafenm.gov>
Subject: FW: Case #: 2025-11006 - Vistas de la Sierra Subdivision Phase 3

Hi DRT –

Just a friendly reminder to please review the Vista de la Sierra Preliminary Subdivision Plat be the email below. Please send me your comments at your earliest convenience. Thanks so much!

~ Claudia

From: KATH, CLAUDIA M.
Sent: Tuesday, August 26, 2025 9:30 AM
To: GRIEGO, GERONIMO G. <gggriego@santafenm.gov>; YNGVE, LEAH X. <lxngve@santafenm.gov>; ISAACSON, ZOE R. <zrisaacson@santafenm.gov>; VALORA SANDOVAL, SERGIO A. <savalorsandoval@santafenm.gov>; HOLLAND, TOWNSEND S. <tsholland@santafenm.gov>; JURGENS, TAYLOR R. <trjurgens@santafenm.gov>; VALDEZ, BENJAMIN P. <bpvaldez@santafenm.gov>; HOOK, ALAN G. <aghook@santafenm.gov>; BEINGESSNER, DEE <dabeingessner@santafenm.gov>; RIVERA, WILLIAM L. <wlriviera1@santafenm.gov>; PADILLA, TEDDY L. <tlpadilla@santafenm.gov>; DURAN, PAUL A. <paduran@santafenm.gov>; Leroy Pacheco <engineer@leroypacheco.com>; NELSON, JOHANNA C. <jcnelson@santafenm.gov>; WILLIAMSON, WALKER R. <wrwilliamson@santafenm.gov>
Subject: Case #: 2025-11006 - Vistas de la Sierra Subdivision Phase 3

Date sent: 8/26/2025

Dearest DRT –