



APPELLANT INFORMATION

**EXHIBIT A**

Name	Old Santa Fe Association		
Address:	PO Box 1055	Suite/Unit #	
	Street Address		
	Santa Fe	New Mexico	87504
	City	State	ZIP Code
Phone	505-603-6940	Email Address	director@oldsantafe.org
Additional Appellant Names			
Correspondence Directed to	<input type="checkbox"/> Owner	<input type="checkbox"/> Applicant	<input checked="" type="checkbox"/> Both

APPLICANT/AGENT INFORMATION (IF DIFFERENT FROM OWNER)

Company Name:	JenkinsGavin, Inc.		
Name (First, Last):	Jennifer Jenkins		
Address:	130 Grant Avenue	Suite 101	
	Street Address	Suite/Unit #	
	Santa Fe	New Mexico	87501
	City	State	ZIP Code
Phone:	505-820-7444	E-mail Address:	jennifer@jenkinsgavin.com

AGENT AUTHORIZATION (IF APPLICABLE)

I am/We are the owner(s) and record title holder(s) of the property located at:

I/we authorize \_\_\_\_\_ to act as my/our agent to execute this application.

Signed: \_\_\_\_\_ Date: \_\_\_\_\_

Signed: \_\_\_\_\_ Date: \_\_\_\_\_

SUBJECT OF APPEAL

Project Name:	Titan Land Development	
Applicant or Owner Name:	Gilliam Land Trust	
Location of Subject Site:	1000,1101, 1103 Cerrillos Road, Santa Fe, NM	
Case Number:	2025-11380	Permit Number (if applicable):
Final Action Appealed:	<input type="checkbox"/> Issuance of Building Permit	<input type="checkbox"/> Other Final Determination of LUD Director
Final Action of Board or Commission:	<input checked="" type="checkbox"/> Planning Commission	<input type="checkbox"/> BCD-DRC
	<input type="checkbox"/> Board of Adjustment	<input type="checkbox"/> Historic District Review Board

Two originals of this form must be filed. The Land Use Department Director or their designee will enter the date and time of receipt and initial both original copies. See Section 14-3.17(D) SFCC 2001 for the procedure.

LAND USE STAFF ONLY

Time Filed:	2:55 pm	Fee Paid: \$	200	(Receipt Attached)
Date Filed:	3/6/2026	Staff Signature:	<i>[Signature]</i>	



BASIS OF STANDING (SECTION 14-3.17 SFCC)

Basis for Appeal: x The facts were incorrectly determined Ordinance/laws were violated and/or misrepresented

Description of the final action appealed from, and date on which final action was taken:

Decision of the Planning Commission, date of final action , February 5, 2026

Check here if you have attached a copy of the final action that is being appealed.

A. DESCRIPTION OF HARM Describe the harm that would result to you from the action appealed (attach additional pages if necessary):

See attached

B. EXPLAIN THE BASIS FOR APPEAL Please detail the basis for Appeal here (be specific):

See attached

SIGNATURE & VERIFICATION

I hereby certify that the documents submitted for review and consideration by the City of Santa Fe have been prepared to meet the minimum standards outlined in the Land Development Code, Chapter 14 SFCC 2001. Failure to meet these standards may result in the rejection or postponement of my application. I also certify that I have met with the City's Current Planning staff to verify that the attached proposal is in compliance with the City's zoning requirements

Appellant Signature: Edward Archuleta

Date: 3/13/26

Agent Signature:

Date:

I/We Edward Archuleta, being first duly sworn, depose, and say: I/We have read the foregoing appeal petition and know the contents thereof and that the same are true to my/ our own knowledge.

Appellant/s Signature Edward Archuleta

Print Name Edward Archuleta

Signature

Print Name

Subscribed and shown to before me this 3 day of March, 2026.

NOTARY PUBLIC (Signature)

My commission expires: 04/01/2026

STATE OF NEW MEXICO NOTARY PUBLIC MELISSA A. SMITH COMMISSION # 1095327 COMMISSION EXPIRES 04/01/2026

## Old Santa Fe Association Appeal of the Marriot Hotel

### A. DESCRIPTION OF HARM Describe the harm that would result to you from the action appealed (attach additional pages if necessary).

The Old Santa Fe Association, as a recognized preservation organization with members directly affected by this decision, has standing to appeal under Section 14-2.1.B.5.IV of Chapter 14, Land Development Code. As stated, “Appeals of final actions may be filed by the following persons:

3. Persons or organizations duly organized at the time the decision appealed from was rendered alleging injury to their economic, environmental or aesthetic interests.

The Cerrillos/St. Francis intersection is extremely dangerous, with curving lanes, no painted stripes for vehicles turning left on either Cerrillos or St. Francis, and trains running through it. A 2023 New Mexico Department of Transportation report of the most hazardous intersections in Santa Fe determined that this intersection has the 3rd most accidents in town. The proposed hotel would add numerous cars to this already congested and badly designed intersection.

The chart below was provided by the New Mexico Department of Transportation and can be found in Marriott’s application submittals.

*Table 1 - Crash Data Summary: Cerrillos Rd / St Francis Dr*

<i>Cerrillos Rd. / St Francis Dr. 5 - Year (2019-2023)</i>		
<b>Total Crashes</b>		<b>186</b>
<b>Pedestrian Involved</b>		<b>1</b>
<b>By Year</b>	2023	51
	2022	41
	2021	25
	2020	26
	2019	43
<b>By Type of Crash</b>	Front-to-Rear	52
	Not Available	43
	Sideswipe	21
	Front-to-Side	12
	Front-to-Front	2
	Other Vehicle - From Opposite Direction	0%
	Other Vehicle - From Same Direction/Both Going Straight	3%
	Other Vehicle - From Same Direction/Rear End Collisions	11%

**B. EXPLAIN THE BASIS FOR APPEAL Please detail the basis for Appeal here (be specific):**

In the applicant's Traffic Impact Analysis their engineer, Tierra West, pointed out the proposed hotel's location could cause problems. On the 9<sup>th</sup> page of the traffic analysis (marked page iv) it states:

*During turning movement counts at Cerrillos Rd. / St. Francis St., it was observed that eastbound queuing on Cerrillos Rd. backed up to the proposed main driveway of the Marriott (also serving the School for the Deaf) coinciding with the passage of the Rail Runner through the signalized intersection. As traffic volumes continue to grow over time, this issue is expected to worsen. During these periods, westbound traffic on Cerrillos Rd. may be unable to turn left into the main driveway, potentially resulting in complaints to the City and/or NMDOT. (Italics appellants)*

The full Traffic Impact Analysis report can be found at:  
<https://santafenm.portal.civicclerk.com/event/1295/files/agenda/2092> in the applicant's submittal packet.

April 6, 2026

City of Santa Fe – Planning and Land Use Department  
Attn: Land Use Director / Appeal Intake  
200 Lincoln Avenue  
Santa Fe, NM 87501

Re: Case No. 2025-11380 – AC Marriott Hotel Development Plan

Old Santa Fe Association – Amended Appeal (Incorporation of Member Supporting Materials)

To Whom It May Concern:

Old Santa Fe Association (“OSFA”) submits this letter in support of its AMENDED Verified Appeal Petition regarding the Planning Commission final action in Case No. 2025-11380 (AC Marriott Hotel Development Plan).

OSFA is submitting an amended appeal as permitted by the City’s appeal procedures, which allow an appellant to amend an appeal until the time for appeal has expired.

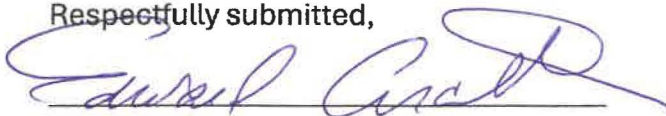
As part of this amendment, OSFA is incorporating and attaching additional supporting materials prepared by OSFA members Aurora Martinez and Sean Noonan, to be included in the appeal record and considered in support of the issues raised in OSFA’s appeal. The incorporated member materials include:

- 1) “Appeal – Case No. 2025-11380 AC Marriott Hotel” (Memorandum); and
- 2) all attachments/exhibits submitted with that memorandum (including Exhibits A, A-1, B, B-1, and any additional exhibits submitted therewith).

These materials are submitted as supporting documentation and additional grounds within OSFA’s amended appeal record concerning the same final action.

Please include this letter and the attached supporting materials in the official appeal file for Case No. 2025-11380.

Respectfully submitted,



Edward Archuleta  
Director, Old Santa Fe Association  
505-603-6940  
director@oldsantafe.org

*Amendment of Appeal  
Received 2:40 PM 4-6-2026*

*(Signature) for Heather Curboy, AICP*

I hereby certify that the documents submitted for review and consideration by the City of Santa Fe have been prepared to meet the minimum standards outlined in the Land Development Code, Chapter 14 SFCC 2001.

Appellant Signature: Edward Archuleta Date: April 6, 2026

Agent Signature: \_\_\_\_\_ Date: \_\_\_\_\_

I/We, Edward Archuleta being first duly sworn, depose, and say: I/We have read the foregoing appeal petition and know the contents thereof and that the same are true to my/ our own knowledge.

Appellant/s Signature

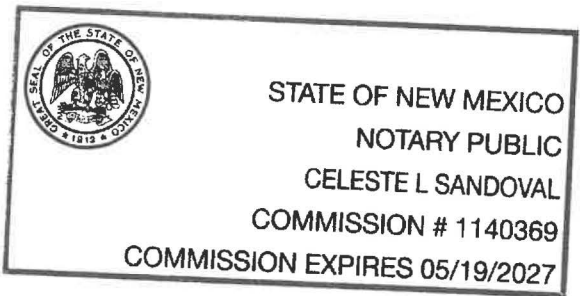
Edward Archuleta  
Edward Archuleta

Print Name Print Name

Subscribed and shown to before me this day of 6<sup>th</sup> day of April, 2026 20.

NOTARY PUBLIC C. Sandoval

My commission expires: 5/19/2027



## **APPEAL OF PLANNING COMMISSION APPROVAL**

Case No. 2025-11380 AC Marriott Hotel

Appellants: Aurora Martinez & Sean Noonan

Date: April 3, 2026

### **Executive Summary**

This appeal asks the Governing Body to apply the City's adopted rules as written. The Planning Commission approved a major hotel on a State corridor while the hearing packet lacked: (1) a fully reproducible, City-grade Level-2 corridor package; (2) the written State access permit the highway requires; and (3) core infrastructure documents (water, drainage, architecture) that Chapter 14 says must be in the record at the time of decision. Corridor reconciliation and runnable models are still missing; the TIA continues to contain internal conflicts on driveway movements; and key mitigation details remain off the stamped plan set. The approval should be denied or at minimum stayed and remanded until the record contains the required proofs (Ch14; TIA 2024; NMAC 18.31.6).

These record deficiencies are most consequential for corridor safety and access on a rail-adjacent, coordinated State highway corridor, where accurate corridor demand and a written State access decision are required to evaluate driveway safety, queues, and congestion.

### **I. Introduction & Purpose**

Appellants respectfully seek review of the Planning Commission's approval of the AC Marriott Development Plan. Under Chapter 14, development-plan findings must be supported by substantial evidence in the record at the time of decision; conditions or later "technical corrections" cannot substitute for that foundation (Ch14). The Commission's Findings of Fact & Conclusions of Law state approval "subject to the Conditions and the technical corrections set forth in the Staff Report and attachments," with a public hearing on February 5, 2026, and final action on March 5, 2026, confirming that key items remained open when the required findings were made.

Because this project sits on a rail-adjacent, coordinated State highway corridor, the adequacy of the record at decision is most critical for traffic safety, access, and public interest.

## II. Jurisdiction & Standard of Review

On appeal, the Governing Body determines whether the Commission's SFCC §14-3.8(D)(1) findings were supported by substantial evidence in the record at the time of decision. If foundational materials were deferred or missing, the approval cannot stand (Ch14).

## III. Core Deficiencies- What Was Missing at the Time of the Vote

Under SFCC Chapter 14, development-plan approval requires complete submittals and findings supported by substantial evidence in the record at the time of decision; submittal requirements are specified in SFCC §14-3.8(C)(1) and are not satisfied by later "technical corrections" or conditions and while the Commission may impose conditions, conditions/"technical corrections" do not substitute for missing evidence. The Commission's Findings approved the plan "subject to Conditions and technical corrections," confirming that key items remained open when findings were made.

### A. Core Deficiencies Missing From the Record At the Vote

- **No reproducible City-grade Level 2 corridor package.**

The City's 2024 TIA Guidelines require Level 2 ( $\geq 100$  peak-hour trips) and require the developer before initiating TIA work to provide a written methods/assumptions summary for City Traffic Engineer approval (study area, agency involvement, parameter values, data collection methods, horizon years, and forecasting methodology), and Level-2 includes defined analysis components (e.g., data collection requirements, queuing analysis, access management/auxiliary turn lanes, multimodal review). The hearing packet includes the TIA report, but the record does not include a City-approved methods/assumptions summary as a standalone record item, and the City/NMDOT review record required Driveway "B" assumptions be reevaluated across the report and appendices, showing core access assumptions were still being corrected at decision.

SFCC Chapter 14 / General Plan / TIA Guidelines: Chapter 14 submittals require documentation showing "proposed modifications to the infrastructure serving the site, including ... traffic control measures" and require a Traffic Impact Analysis (if required) as part of the development plan record. The Staff Report expressly ties "public interest" review to General Plan implementation through Chapter 14 and states approval is recommended subject to conditions and technical corrections, confirming the City is applying its adopted transportation standards through the Chapter 14 review process.

- **No written NMDOT access permit for NM-14 / US-84/285**

State law requires an NMDOT access permit to construct or modify access to a state highway and clarifies that abutting owners have a right to reasonable access to the system, not to a particular means of access/turning movement. No written NMDOT access permit appears anywhere in the Feb. 5 hearing packet exhibits (Staff Report attachments and Applicant Materials). This omission is material because the DRT traffic record documents NMDOT direction that Driveway “B” must be right-in/right-out only and requires the applicant’s analysis be reevaluated accordingly across the report and appendices yet the movement authority is not memorialized by permit in the record.

SFCC Chapter 14 / General Plan / TIA Guidelines: The City’s 2024 TIA Guidelines expressly recognize that where state roads are impacted, the NMDOT State Access Management Manual remains governing and coordination with NMDOT is essential. Chapter 14 requires the development plan record to include documentation showing the proposed infrastructure and traffic control measures will function as approved; without the controlling State permit, final driveway movements on NM-14 are not fixed “as analyzed” in the decision record.

- **No final Approved Water Plan and no executed Agreement to Construct & Dedicate (ACD) were in the decision record**

The applicant’s own Development Plan Report lists as water documentation only a “Utility Service Application” and a “Preliminary Water Budget,” not an executed ACD or an “Approved Water Plan.” The Utility Service Application itself references the attached “Preliminary Water Budget,” and while it lists ACD/AMS as selectable pathways, it does not include an executed ACD agreement.

The “Preliminary Alternative Water Budget” is explicitly labeled preliminary, includes an offset credit line marked “TBD,” and states it is awaiting IPRA-sourced historical usage data, confirming it was not final water-approval documentation at the time it was submitted.

Chapter 14’s stated purposes include implementing the City’s General Plan and ensuring that development regulations and approvals are in accordance with that plan. In this case, the City’s Findings document cites SFCC §14-3.8(C)(1) as requiring the applicant to submit “plans and other documentation” demonstrating conformance (submittal requirements). Yet the plan sets provided contain utilities and irrigation water-budget calculations, but no executed ACD document and no City “Approved Water Plan Drawing Number” or similar approval identifier of the type the City typically uses for ACD acceptance.

- **No staff-accepted hydrology reruns are in the record at approval**

City Technical Corrections flag errors and missing support in the stormwater analysis (for example, “V100 yr volume calcs appear to have an error”), ask whether hydrographs were generated to prove pond sufficiency, and require reruns and post-detention discharge summaries “Prior to Recordation,” indicating that stormwater adequacy had not yet been verified when the Commission voted.

Chapter 14 lists Terrain Management Plans (as required by Section 14-8.2) among the development-plan submittal items and requires applicants to submit plans and documentation showing compliance with applicable code provisions. In this case, the City’s own record shows that the Terrain Management analysis still needed corrected calculations and supporting proof prior to recordation, indicating that the terrain/stormwater compliance documentation demonstrating adequacy was not complete in the record at the time of approval.

- **Architectural points: worksheet is in the packet; measured façade substantiation for the massing criterion is not.**

The record includes the architectural points worksheet/letter, but it assigns 0 points for the code-defined Building Form/Massing criterion that turns on measured façade geometry (projection depth and extent), and staff likewise states the wall-plane projections/setbacks do not fully meet the dimensional requirement. The record does not include measured façade calculations demonstrating compliance with the dimensional massing standard at the time of decision.

SFCC Chapter 14 / General Plan: Chapter 14 requires “documentation of compliance with development standards” and, for a development plan/final development plan, “sufficient detail to clearly show how each applicable development standard is to be met.” The Staff Report frames review and findings as implementing the General Plan through Chapter 14; where a standard is dimensional, the record must include the measurements/calculations or explicitly acknowledge noncompliance.

**Result-**The Commission approved the Development Plan “**subject to conditions and technical corrections,**” and the hearing record itself assigns multiple key compliance items to be completed after the hearing (e.g., “Prior to Recordation” stormwater reruns/adequacy proof), confirming that several proofs were deferred beyond the vote.

## B. What was present in the record

- Turning-movement count sheets for all study intersections for 7–9 AM and 4–6 PM on August 19, 2025, are included in Appendix A-72 through A-75 of the Final TIA.
- Bicycle/pedestrian count sheets for the same date/peaks are included in Appendix A-76 through A-79.
- Controller timing databases for St. Francis/Cerrillos (A-80–A-92) and Cerrillos/Cordova (A-93–A-104) are in the TIA record.
- The Final TIA includes a 5-year crash history (2019–2023): St. Francis/Cerrillos reports 186 crashes (rate  $\approx$  1.61 crashes/MEV, rear-end  $\approx$  11%), and Cerrillos/Cordova reports 64 crashes (rate  $\approx$  1.84 crashes/MEV, rear-end  $\approx$  8%); the consultant recommends no safety mitigation.
- Rail Runner effect: peak-hour video showed eastbound queues at St. Francis/Cerrillos blocking the median opening to the main driveway (A/NMSD) four times per peak hour, with average clearance 30–36 seconds; the TIA recommends added storage/tapers at Driveway “A”.
- Storage/auxiliary lane recommendations at project driveways: EB LT = 136', WB LT = 70', and EB RT = 120' (per NMDOT CN S100681) are stated in the TIA's access design recommendations.
- Multimodal: The TIA's PLTS/BLTS shows PLTS 4 along the eastbound frontage and recommends reconstructing the sidewalk (6' width + 5' setback).
- Utility Service Application is in the hearing packet and references the attached “Preliminary Water Budget”; it does not include an executed ACD agreement.
- Preliminary Alternative Water Budget is in the hearing packet and is explicitly labeled preliminary; it includes an offset credit line marked “TBD” and notes it is awaiting IPRA-sourced historical usage data.
- DRT / Water Division comment sheet is in the packet and includes the City's stated prerequisite that an approved Water Plan is required prior to public hearing (and related staged requirements for water approvals).
- Technical Corrections are in the packet and identify errors and missing support in the stormwater analysis (including the V100-year volume calculation issue and required reruns/post-detention discharge summaries “Prior to Recordation”).

### C. What is still missing for Level-2 compliance

- No reproducible Level-2 corridor package: the record does not include the RAW 24-hour ATR/link datasets with vehicle classification and station metadata (dates/locations), same-day/season reconciliation worksheets to SFMPO/NMDOT MS2, or runnable model files (Synchro/SimTraffic: .INP/.SYN/.SDF with final timing sheets) needed for independent reruns and corridor reconciliation. These are the sorts of data collection and analysis elements contemplated by the City's adopted TIA Guidelines for Level-2, which expressly call out data collection requirements, queuing analysis, access management, and a Level-2 outline sufficient for third-party verification.
- Movement-by-movement corridor queue/storage checks across the coordinated system with rail pre-emption are not provided at the reproducible level needed to verify that the recommended storage lengths function during pre-emption and peak coordination; Level-2 requires corridor-level queuing and access management analysis.
- Driveway "B" contradiction remains unresolved:
  - The response matrix in the Final TIA's front matter says Driveway "B is now evaluated as Right-In only...", reflecting the City/State comment exchange.
  - The body of the Final TIA (access design and Synchro summaries) models and recommends Right-Out only at Driveway "B."
  - There is no NMDOT access permit in the record memorializing the final, permitted movements at the state facility required under NMAC 18.31.6.
- School-area multimodal data/mitigation gap (NMSD): Although the TIA identifies PLTS 4 and recommends frontage sidewalk reconstruction, there are no school bell-time pedestrian counts for NMSD, and no APS/refuge upgrades proposed in the record falling short of the City's corridor safety/multimodal emphasis for Cerrillos Road.

### D. Why conditions cannot cure the missing proofs

- Chapter 14 requires that submittals be in the record when the Commission makes findings (e.g., §14-3.8(C)(1)). The Commission may add conditions to accomplish proper development, but conditions do not substitute for the evidentiary foundation that must exist at the time of approval. The FOF & COL language "subject to Conditions and technical corrections" confirms items were missing at decision.

- The TIA Guidelines (2024) are adopted City policy pending codification and govern the level, content, and reproducibility of analyses; Level-2 requires the corridor-grade analysis and data necessary for independent reruns (data collection requirements, queuing, access management, Level-2 outline). Absent the reproducible package, the City and public cannot verify LOS/queue results or reconcile corridor operations.
- NMAC 18.31.6 requires a written NMDOT access permit for state highways and clarifies that an abutting owner has reasonable access to the system, not to specific movements; local approval cannot supersede the State's access control. Without the permit in the record, the modeled driveway operations are not legally secured.
- The Cerrillos corridor is governed by adopted safety and multimodal priorities (City/SFMPO/NMDOT), and NMDOT's active corridor project (CN S100681) emphasizes ADA/PROWAG compliance, access management, and safety upgrades further underscoring the need for documented mitigation and state coordination at the time of approval.

#### IV. Comparative Evidence: MS2 Monitoring vs. Applicant TIA Counts; Preliminary Water Budget

##### A. State NMDOT/Santa Fe MPO Monitoring Comparison (MS2 July 2025) vs Applicant One-Day TMC Snapshot (Aug. 19, 2025)

This section is provided to compare the corridor monitoring evidence available through NMDOT/SFMPO's MS2 traffic count database (publicly available) with the applicant's one-day turning-movement count snapshot used in the TIA record.

What the applicant did (record inputs): The TIA ~~relies on~~ turning-movement counts collected on one weekday (08/19/2025) ~~during AM and PM peak windows~~, rather than multi-day corridor monitoring ~~and normalization~~.

What the State/SFMPO ~~monitoring shows~~ (MS2): MS2 export for Location ID 12097 on Cerrillos Rd (2-way) with QC Status Accepted covers 07/08/2025–07/09/2025 and reports hourly corridor volumes. The highest hourly total in this export is 2,663 vph (12:00 - 13:00).

Comparison to the TIA corridor peak input ( $\approx 1,852$  vph): MS2 monitoring ~~values are~~ higher by approximately 805 vph ( $\approx 43.4\%$ ) using the MS2 value ( $\approx 2,657$  vph). Using the MS2 export peak hour (2,663 vph) yields a similar difference of 811 vph ( $\approx 43.8\%$ ).

SFMPO/NMDOT monitoring context: The Santa Fe Metropolitan Planning Organization states that NMDOT operates permanent count stations within the SFMPO planning area and that traffic count data may be viewed via the MS2 Traffic Count Database System (TCDS).

Summary table:

Metric	Applicant/TIA (record)	NMDOT/SFMPO monitoring (MS2)	Difference
Corridor peak hour (vph)	≈ 1,852 (Exhibit A)	≈ 2,657(Exhibit B MS2)	+805 (≈43.4%)
MS2 export peak hour (vph) (07/08/2025–07/09/2025)	≈ 1,852 (Exhibit A)	2,663 (12:00 - 13:00)	+811 (≈43.8%)

Reference (MS2 usage): <https://santafemppo.org/resources-2/traffic-counts/> ; NMDOT MS2 program overview: <https://www.dot.nm.gov/planning-research-multimodal-and-safety/planning-division/data-management-bureau/>

This corridor involves two State highway facilities (NM-14/Cerrillos Road and US-84/285/St. Francis Drive) operating as a coordinated signal system, together with an active highway-rail grade crossing that can induce queueing and spillback into access points during preemption and peak periods. NMDOT’s access management rule (18.31.6 NMAC) is expressly intended to preserve public health, safety, and welfare and to maintain smooth traffic flow on the State highway system, and it requires that access modifications be controlled through NMDOT’s permitting process; abutting owners have reasonable access to the system but not to any particular turning movement. NMDOT’s Traffic Monitoring Program maintains monitoring-grade traffic datasets (including CCS-supported monitoring) in MS2 as the official repository for accepted traffic monitoring data used by agencies and the public, including within the Santa Fe MPO area. FHWA’s Traffic Monitoring Guide provides the national framework for State monitoring programs that support planning, operations, and safety decisions.

Given (i) the material discrepancy between one-day turning-movement snapshots and MS2 monitoring products and (ii) the record’s own NMDOT-directed reevaluation of Driveway “B” movements, NMDOT should base its access determination on MS2 corridor volumes and applicable State corridor study practices and memorialize any movement restrictions

or median controls in a dated written permit or coordination memo placed in the City project file and also because this is a rail-adjacent, coordinated State-highway corridor where NMDOT must protect safety and smooth flow under 18.31.6 NMAC, MS2 monitoring products and a dated written permit/memo are the appropriate basis to fix final movements and median controls in the City record.

Under-representing traffic volumes can understate required mitigation (missed turn lanes, missed signal warrants, underestimated queues). Traffic data is a basis for the calculations of background growth, future volumes (no-build vs build), intersection capacity, LOS, Volume to capacity, average delay, queue length, storage requirements, signal timing, cycle design, crash rates, turn lane length, and number of lanes.

### B. Preliminary Water Budget Analysis

This section summarizes the discrepancy identified in Exhibit B\_Preliminary Water Budget Analysis between the applicant’s reported total water demand offset and a comparable-hotel scaled estimate.

Summary Table:

Item	AFY	Gallons per year	Notes	Percent difference
Applicant total water demand offset (incl. landscape + 9.8% contingency)	12.84 AFY	4,183,705 gallons/year	Reported by applicant	-
Comparable-hotel scaled total (incl. landscape + 9.8% contingency)	14.20 AFY	4,625,929 gallons/year	Scaled estimate for comparable hotel	-
Underestimate (Comparable – Applicant)	1.36 AFY	442,224 gallons/year	Shortfall between estimates	<b>9.58%</b>

Underestimating the total demand by **1.36 AFY (~442,224 gallons/year; ~9.58%)** increases the likelihood that actual use will exceed the approved development water budget and could trigger post-approval corrections or enforcement.

## **V. Conclusion**

This appeal asks the Governing Body to apply the City's adopted rules as written. Under Chapter 14, development-plan findings must be supported by substantial evidence in the record at the time of decision; conditions or later "technical corrections" cannot substitute for that foundation. The Commission's Findings approved the plan "subject to the Conditions and the technical corrections set forth in the Staff Report and attachments," confirming that key items remained open when the required findings were made.

As shown in this appeal, the hearing packet lacked: (1) a fully reproducible, City-grade Level-2 corridor package; (2) the written State NMDOT access permit the highway requires; and (3) core infrastructure documents (water, drainage, architecture) that Chapter 14 requires to be in the record at the time of decision. Corridor reconciliation and runnable models remain missing; the TIA continues to contain internal conflicts on driveway movements; and key mitigation details remain off the stamped plan set.

These record deficiencies are most consequential for corridor safety and access on a rail-adjacent, coordinated State highway corridor, where accurate corridor demand and a written State access decision are required to evaluate driveway safety, queues, and congestion. Accordingly, the record demonstrates that the approval was issued before the decision file contained the evidentiary foundation required to support the findings—particularly on the corridor safety and access issues central to this project.

## **V. Remedy Sought**

Because the record lacked the required Level-2 corridor proof, the written State NMDOT access authorization, and other Chapter 14 infrastructure submittals at the time of the Planning Commission vote, the Governing Body should deny the approval or, at minimum, stay and remand until the record contains the required proofs (Ch14; TIA 2024; NMAC 18.31.6).

To address the corridor safety and access issues central to this project, the minimum required traffic-critical proofs are:

1. A full, reproducible Level-2 corridor package (signed scoping; multiperiod coverage; RAW 24-hr ATR/link files with classification; same-day/season reconciliation to SFMPO/NMDOT MS2; runnable Synchro/SimTraffic files and timing sheets; movement-by-movement queue/storage checks with rail preemption).
2. A written NMDOT access permit memorializing final movements and median controls for NM-14/US-84/US-285 access points.

3. Final water approvals and executed ACD, accepted hydrology reruns with staff verification, and architectural points & façade calculations in the record before any new approval.

## VI. Appellants Backgrounds

Aurora Martinez has nearly two decades of public-sector experience in program implementation, research, and data analysis, including leadership at NMDOT overseeing statewide traffic monitoring under federal standards, implementing MPO traffic data collected by MRCOG, and FHWA Traffic Monitoring Certified. Subsequent federal program work at NM DFA focused on data evaluation, research, and compliance.

Sean Noonan holds an MBA in Financial Management (UNM Anderson) and FHWA Traffic Monitoring Certified. Served as a NMDOT's Traffic Monitoring Program Manager, leading improvements to compliance processes, modernization of collection equipment, and processing end of year annualization of traffic data. Additional federal program work at NM DFA focused on data analysis, compliance, and grant management.

### MS2 Traffic Monitoring Data Systems

Aurora and Sean led the modernization of NMDOT's Traffic Database from a legacy system to MS2 Traffic Monitoring Data System in 2017 with a go-live in 2018. As part of the implementation, we met with SFMPO to help them and authorize the inclusion of NMDOT's traffic data in the SFMPO area into the MS2 TMDS system they had resulted in expanding the availability of data they had in their database.

### Notes on sources

- **Chapter 14 / Municode** (development-plan submittals & findings; infrastructure prerequisites).
- **FOF & COL practice** (approvals "subject to conditions/technical corrections").
- **TIA Guidelines (Aug. 29, 2024)** (Level-2 scope, data collection, queuing, access management, reproducibility; adopted pending codification).
- **NMAC 18.31.6** (permit requirement; "reasonable access" not to a specific turning movement).
- **TIA record** (counts; controller databases; crash summaries; rail-preemption queues; auxiliary lane/storage recommendations; PLTS/BLTS).
- **Cerrillos corridor policy context** (SFMPO corridor assessment; NMDOT corridor project).

## EXHIBIT A – Traffic Monitoring Reconciliation (MS2 vs. TIA Peak-Hour Inputs)

Case No. 2025-11380 – AC Marriott Hotel Development Plan

*Purpose: Provide a concise, record-based comparison between the Applicant’s TIA peak-hour corridor inputs and NMDOT/Santa Fe MPO monitoring products available via MS2 for the Cerrillos Road corridor segment used in the record.*

### A. Data provenance (MS2 export)

- Source: NMDOT Data Management Bureau Traffic Monitoring Program (MS2 export)
- Location ID: 12097 | Facility: CERRILLOS RD | Direction: 2-WAY | Count window: 07/08/2025–07/09/2025 | Interval: 15 minutes | QC Status: Accepted

### B. Key findings (material difference)

- 1) Record comparison: TIA peak-hour corridor input  $\approx$  1,852 vph vs MS2 monitoring  $\approx$  2,657 vph ( $\approx$ 43.4% higher).
- 2) MS2 export peak hour (Location ID 12097): 2,663 vph (12:00 - 13:00) which is +811 vph ( $\approx$ 43.8%) compared to  $\approx$ 1,852 vph.
- 3) A discrepancy on the order of  $\sim$ 43% is operationally significant on a coordinated arterial corridor and can materially change queue/storage outcomes used to evaluate access configuration and mitigation.

### C. Summary comparison table

Metric	Applicant/TIA (record)	MS2 monitoring	Difference
Peak-hour corridor volume (vph) – as cited in record comparison	$\approx$ 1,852	$\approx$ 2,657	+805 ( $\approx$ 43.4%)
MS2 export peak hour (vph) (Location ID 12097; QC Accepted)	$\approx$ 1,852	2,663 (12:00 - 13:00)	+811 ( $\approx$ 43.8%)

### D. Attachments

- Exhibit A-1: MS2 Export Excerpt (Location ID 12097)

Location Info	
Location ID	12097
Type	I-SECTION
Functional Class	3
Located On	CERRILLOS RD
Direction	2-WAY
Community	-
MPO_ID	
HPMS ID	
Agency	NMDOT - Data Management Bureau Traffic Monitoring Program

Count Data Info	
Start Date	7/8/2025
End Date	7/9/2025
Start Time	1:00 PM
End Time	1:00 PM
Direction	
Notes	
Count Source	
File Name	
Weather	
Study	
Owner	jeremy.sanchez
QC Status	Accepted

Interval: 15 mins					
Time	15 Min				Hourly Count
	1st	2nd	3rd	4th	
00:00 - 01:00	67	44	28	27	166
01:00 - 02:00	22	12	20	12	66
02:00 - 03:00	13	16	4	12	45
03:00 - 04:00	4	11	18	20	53
04:00 - 05:00	10	16	35	35	96
05:00 - 06:00	51	54	69	127	301
06:00 - 07:00	112	127	165	223	627
07:00 - 08:00	258	321	425	509	1513
08:00 - 09:00	477	442	485	450	1854
09:00 - 10:00	459	457	461	527	1904
10:00 - 11:00	501	553	514	512	2080
11:00 - 12:00	569	552	591	629	2341
12:00 - 13:00	678	645	688	652	2663
13:00 - 14:00	688	612	653	647	2600
14:00 - 15:00	581	626	628	617	2452
15:00 - 16:00	618	697	675	620	2610
16:00 - 17:00	662	682	641	672	2657
17:00 - 18:00	666	573	571	562	2372
18:00 - 19:00	504	482	419	372	1777
19:00 - 20:00	386	374	350	319	1429
20:00 - 21:00	367	323	309	273	1272
21:00 - 22:00	273	225	253	221	972
22:00 - 23:00	211	153	145	133	642
23:00 - 24:00	117	72	81	45	315
TOTAL					32807

**EXHIBIT B – Preliminary Water Budget Reconciliation (Hotel Comparables vs. Applicant Total)**

Case No. 2025-11380 – AC Marriott Hotel Development Plan

*Purpose: Provide a concise comparison of the Applicant’s total water demand offset to a comparable-hotel scaled estimate using the Applicant’s stated conversion (1 acre-foot = 325,851 gallons).*

**A. Inputs (from Exhibit B\_Preliminary Water Budget Analysis)**

- Applicant total water demand offset (incl. landscape + 9.8% contingency): 12.839 AFY
- Comparable-hotel scaled total (incl. landscape + 9.8% contingency): 14.196 AFY
- Difference (underestimate): 1.357 AFY ( $\approx$  442,224 gallons/year)

**B. Why material**

*An underestimate of this magnitude increases the risk that actual use will exceed the approved development water budget, triggering noncompliance and post-approval corrective actions.*

**C. Summary table**

Metric	Applicant	Comparable scaled
Total water demand offset (AFY)	12.839	14.196
Underestimate (AFY)		1.357
Underestimate (gallons/year)		442,224

**D. Attachment**

- Exhibit B-1: Exhibit B\_Preliminary Water Budget Analysis.xlsx

**Average AFY/RM Calculation Scaled to 150 Rooms**

	21-22	22-23	23-24	24-25	Subtotal
El Rey Hotel AFY/Room	0.082075	0.096181069	0.097619164	0.081015	0.356890184
Spring Hill Suites			0.061755686		0.061755686
Subtotal					0.41864587
<b>Average AFY/RM</b>					<b>0.083729174</b>

**Steps to scale**

Find gallons per room	1. GPY/number of hotel rooms
	2. Muiltiply by 150 rooms for scaled GPY
Scaled AFY Calucation	Scaled GPY / 325851
Scaled AFY/RM Calucation	Scaled AFY/150 rooms

	Titan Calculations		Calculations based on Reported AFY		Calculations based on Scaled Average	
	GPY	AFY Acutal Used	GPY	AFY (Reported)	GPY	Average AFY/RM
Domestic Water Use/Rm	24,598.21	0.0755	26,068.08	0.08	27,283.24	<b>0.083729174</b>
Subtotal Domestic Use for 150 Room	3,689,731.05	11.32	3,910,212.00	12.00	4,092,485.26	12.56
Landscape 3-year average	121,094.00	0.37	121,094.00	0.37	121,094.00	0.37
Water Budget Subtotal	3,810,825.05	11.69	4,031,306.00	12.37	4,213,579.26	12.93
Contingency 9.8%	373,460.85	1.15	395,067.99	1.21	412,930.77	1.27
<b>Total Water Demand Offset</b>	<b>4,184,285.90</b>	<b>12.84</b>	<b>4,426,373.99</b>	<b>13.58</b>	<b>4,626,510.03</b>	<b>14.20</b>
<b>Titan underestimated water budget:</b>			<b>242,088.08</b>		<b>442,224.13</b>	

**Underestimating water demand risks non-compliance and future penalties if actual use exceeds the budget.**

Legend:

1 acre-foot equals 325,851

gallons.

AFY = GPY ÷ 325,851

GPY = AFY × 325,851

City of Santa Fe, NM

200 Lincoln  
Santa Fe, NM 87507-4808

03/06/2026 02:55PM Benjamin  
002876-0014

EG INVOICE

Archuleta, Edward (Old Santa Fe  
Association)

2026-012028--APPL

2026 INV-00111815

Appeal Fee - Plan - All

Appeals \$200.00

-----  
\$200.00

Subtotal \$200.00

Total \$200.00

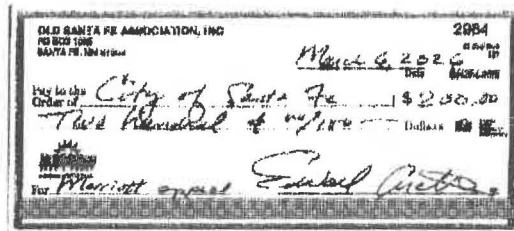
Tenders

CHECK \$200.00

Check Number 2964

-----  
Change due \$0.00

Paid by: Archuleta, Edward (Old Santa Fe  
Association)



Thank you for your payment

CUSTOMER COPY

**City of Santa Fe  
Planning Commission  
Findings of Fact and Conclusions of Law**

**Case # 2025-11380**

**1000, 1101 & 1103 Cerrillos Road Development Plan**

**Owner's/Applicant's Name - Titan Land Development**

**Agent's Name - Jenkins/Gavin**

THIS MATTER came before the Planning Commission (Commission) for public hearing on February 5, 2026 (Hearing) upon the development plan application (Application) of Jenkins/Gavin Planning as Agent for Titan Land Development (Applicant).

The Application pertains to the property located at 1000, 1101 & 1103 Cerrillos Road totaling approximately 3.40 +/- acres (Property). The Applicant requests approval of a development plan for a 79,491-square-foot, four-story, 150-room "Antonio Catalan" Marriott hotel (Project). The property is zoned C-2 (General Commercial) and is within the Suburban Archaeological Review District and the Cerrillos Road Highway Corridor (Zone 1).

After conducting a public hearing and having heard from staff and all interested persons, the Commission hereby FINDS, as follows:

**FINDINGS OF FACT**

General

1. The Applicant requested approval of a development plan for a four story, 150-room, AC Marriott hotel totaling 79,491 square feet.
2. At the hearing, the Commission received reports from the City's Land Use Staff, testimony and evidence from the Applicant, and testimony offered by any interested members of the public.
3. SFCC 1987 Section 14-3.1 sets out certain procedures to be followed on the Application, including, without limitation: (a) a pre-application conference [SFCC 1987 § 14-3.1(E)]; (b) an Early Neighborhood Notification (ENN) meeting [SFCC 1987 § 14-3.1(F)(2)(a)(iv)]; and (c) compliance with notice and public hearing requirements [SFCC 1987 § 14-3.1(H)-(I)].
4. The Applicant attended a pre-application conference on August 14, 2025, with City Land Use Department Staff (City Staff).
5. The Applicant conducted an ENN meeting for this project. The Applicant gave notice of the ENN meetings to neighbors and neighborhood associations within 300 feet of the subject property and posted posters on the subject property.
6. The Applicant held the virtual ENN meeting on September 30, 2025. The ENN meeting was attended by members of the Applicant team, City Staff, and approximately 31 members of the public.
7. City staff reviewed the Application, as well as the related materials and information submitted by the Applicant for conformity with applicable SFCC requirements. Staff also provided the Planning Commission with a written report of its findings (Staff Report), which evaluates the factors relevant to the Application.

8. Staff recommended that the Commission approve the development plan, subject to certain conditions (the Conditions) and the technical corrections set forth in the Staff Report and Attachments.
9. Pursuant to SFCC 1987 Section 14-2.3(C)(1), the Commission has the authority to review and approve development plans.

#### Development Plan

1. Under SFCC 1987 Section 14-3.8(B)(3)(a), a new development with a gross floor area of thirty thousand (30,000) square feet or more requires approval of a development plan.
2. SFCC 1987 Section 14-3.8 establishes certain procedures for development plan approval including, without limitation, a public hearing by the Commission and a decision based on the criteria set out in SFCC 1987 Section 14-3.8(D).
3. SFCC 1987 Section 14-3.8(C)(1) requires the Applicant to submit plans and other documentation that demonstrates conformance with applicable provisions of the SFCC (Submittal Requirements).
4. SFCC 1987 Section 14-3.8(D)(1) sets out approval criteria and requires the Commission to make complete findings of fact sufficient to show that these criteria have been met before approving a development plan.
5. Pursuant to SFCC 1987 Section 14-3.8(D)(2), the Commission “may specify conditions of approval that are necessary to accomplish the proper development of the area and to implement the policies of the general plan.”
6. The Commission finds that the Conditions and technical corrections set forth in the Staff Report and attachments are necessary to accomplish the proper development of the area and to implement the policies of the general plan.

### **CONCLUSIONS OF LAW**

Under the circumstances and given the evidence and testimony submitted during the hearing, the Commission CONCLUDES as follows:

#### General

1. Pursuant to SFCC 1987 Section 14-3.1, all procedural requirements regarding the pre-application conference, ENN meeting, and notice of public hearing have been met.

#### Development Plan

1. Pursuant to SFCC 1987 Section 14-2.3(C)(1) and Section 14-3.8(B)(4), the Commission has authority to review and approve this final development plan.
2. Pursuant to SFCC 1987 Section 14-3.1, all procedural requirements regarding the pre-application conference, ENN meeting, and notice of public hearing have been met.
3. Pursuant to SFCC 1987 Section 14-3.8(C)(1), the Applicant met the applicable Submittal Requirements.
4. Pursuant to SFCC 1987 Section 14-3.8(D)(1)(a), the Commission finds that it has the authority to review the Development Plan under SFCC 1987 Sections 14-2.3(C)(1), 14-3.8(B)(4), and Table 14-2.1-1.

5. Pursuant to SFCC 1987 Section 14-3.8(D)(1)(b), the Commission finds that the Development Plan will not adversely affect the public interest. The Governing Body has implemented the General Plan and ordinances to establish minimum standards for health, safety and welfare affecting land uses and developments as a means to protect the public interest. This project serves the public interest through the provision of much-needed housing.
6. Pursuant to SFCC 1987 Section 14-3.8(D)(1)(c), the Commission finds that the hotel use and any associated buildings are compatible with and adaptable to buildings, structures, and uses of the abutting property and other properties in the vicinity of the premises under consideration.
7. The Commission approves the Development Plan subject to the conditions and technical corrections recommended by Staff because all applicable code requirements and criteria for preliminary development plan approval have been met.

**WHEREFORE, IT IS ORDERED ON THE 5th DAY OF MARCH, 2026 BY THE PLANNING COMMISSION OF THE CITY OF SANTA FE:**

Considering the foregoing findings and conclusions, the Commission approves the development plan for the Property, as requested in the Application for Case # 2025-11380, subject to the Conditions and the technical corrections attached herewith. The development plan shall expire three years after issuance of this final action unless actual development of the site or off-site improvements has begun and is continued pursuant to Subsection 14-3.19(B)(6). If the development plan approval expires, approval of any corresponding preliminary development plan expires simultaneously pursuant to SFCC 1987 Section 14-3.19(B)(4).

\_\_\_\_\_  
Janet Clow, Chair

\_\_\_\_\_  
Date

FILED:

\_\_\_\_\_  
Geraldyn F. Cardenas  
City Clerk

\_\_\_\_\_  
Date

APPROVED AS TO FORM:

\_\_\_\_\_  
Frank Ruybalid  
Assistant City Attorney

\_\_\_\_\_  
Date



City of Santa Fe

# MINUTES

REGULAR MEETING OF THE  
PLANNING COMMISSION  
February 5, 2026, AT 6:00 PM

## A. ROLL CALL

**EXHIBIT C**

### Commissioners Present:

- Chair Janet Clow
- Vice Chair Peter Smith
- Commissioner Thomas (TJ) Rieland
- Commissioner Sasha McGhee
- Commissioner Gurushabad (Sheb) Mirando
- Commissioner Kelly McReynolds
- Commissioner Scott Barber

## B. PLEDGE OF ALLEGIANCE

## C. APPROVAL OF AGENDA

**MOTION:** 1. Commissioner Mirando moved, seconded by Vice Chair Smith, to approve the agenda with the amendment to hear Staff Communications ahead of New Business. The amendment included switching the order of New Business to Case # 2025-11380, 1000, 1101, and 1103 Cerrillos Road Development Plan, being heard first and Case #2025-11386. 214 Camino de los Marquez, heard second.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:**

- Vice Chair Peter Smith Yes
- Commissioner TJ Rieland Yes
- Commissioner Sasha McGhee Yes
- Commissioner Sheb Mirando Yes
- Commissioner Kelly McReynolds Yes
- Commissioner Scott Barber Yes

**Against:** None

**Abstain:** None



# MINUTES

## D. APPROVAL OF MINUTES

1. December 18, 2025

**MOTION:** Vice Chair Smith moved, seconded by Commissioner Rieland, to approve the minutes with changes to show Chair Clow did not vote on matters from Planning Commission meeting on December 18, 2025 .

**VOTE:** The motion was approved on the following Roll Call vote:

**For:**  
Vice Chair Peter Smith Yes  
Commissioner TJ Rieland Yes  
Commissioner Sheb Mirando Yes  
Commissioner Piper Kapin Yes  
Commissioner Kelly McReynolds Yes  
Commissioner Scott Barber Yes

**Against:** None

**Abstain:** Commissioner Sasha McGhee

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## E. STAFF COMMUNICATIONS

1. Affordable Housing Presentation – Faviola Chavez

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City of Santa Fe

# MINUTES

REGULAR MEETING OF THE  
PLANNING COMMISSION  
February 5, 2026, AT 6:00 PM

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## F. APPROVAL OF FINDINGS/CONCLUSIONS

1. **Case #2026-11807. 7261 Airport Road Development Plan Time Extension.** Orion-West, LLC, Agent, MBR 7261 Airport Road LLC, Applicant (Buyer), Request Development Plan approval to construct 261,000 square feet of new construction for a 221-unit multi-family apartment complex. The project is a 7.96-acre lot zoned C-2 (General Commercial) within the Airport Road Overlay and River and Trails Archaeological Review District. (Claudia Kath, Case Manager, cmkath@santafenm.gov).

**MOTION:** Commissioner Mirando moved, seconded by Commissioner McGhee, to approve Findings for Case #2026-11807 subject to conditions of approval and technical corrections.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:**  
Vice Chair Peter Smith Yes  
Commissioner TJ Rieland Yes  
Commissioner Sasha McGhee Yes  
Commissioner Sheb Mirando Yes  
Commissioner Kelly McReynolds Yes  
Commissioner Scott Barber Yes

**Against:** None

**Abstain:** None



City of Santa Fe

# MINUTES

REGULAR MEETING OF THE  
PLANNING COMMISSION  
February 5, 2026, AT 6:00 PM

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## G. NEW BUSINESS

2. **Case #2025-11380. 1000, 1101, and 1103 Cerrillos Road Development Plan.**  
 JenkinsGavin, Inc., Agent, for Titan Land Development, on Behalf of Gilliam Land Trust, owner and applicant (“Applicant”), requests approval of a Development Plan, for a 79,491 square foot, four-story, 150-room AC Marriott hotel. The subject properties comprise 3.40 +/- acres and are located within the Suburban Archaeological Review District and Cerrillos Road Highway Corridor (Zone 1) and zoned C-2 (General Commercial) (Claudia Kath, Case Manager, cmkath@santafenm.gov)

**MOTION:** Commissioner Mirando moved, seconded by Commissioner Smith, to approve Case #2025-11380, subject to conditions of approval, technical corrections, the applicant will work with staff to architecturally soften the 4<sup>th</sup> story corner deck, and the property owner have good faith to mitigate/ help the Gunnison Prairie Dogs.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Vice Chair Peter Smith No  
 Commissioner TJ Rieland No  
 Commissioner Sasha McGhee Yes  
 Commissioner Sheb Mirando Yes  
 Commissioner Scott Barber No  
 Commissioner Kelly McReynolds Yes  
 Chair Clow (tie) Yes

**Against:** None

**Abstain:** None



City of Santa Fe

# MINUTES

REGULAR MEETING OF THE  
PLANNING COMMISSION  
February 5, 2026, AT 6:00 PM

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3. **Case #2025-11386. 214 Camino de los Marquez.**

JenkinsGavin, Agent, for Santa Fe Opera, Owner and Applicant (“Applicant”), requests approval of a Development Plan for up to 83 multifamily residential units. The property is located with the Suburban Archaeological and River and Trails Archaeological Review Districts and consists of +/-2.5-acre, zoned R-29 (Residential).(Nathan Lindquist, Case Manager, njlindquist@santafenm.gov).

**MOTION:** Commissioner Barber moved, seconded by Commissioner Rieland, to approve Case #2025-11386, subject to conditions of approval and technical corrections.

**VOTE:** The motion was approved on the following Roll Call vote:

**For:** Commissioner Peter Smith Yes  
Commissioner TJ Rieland Yes  
Commissioner Sasha McGhee Recused  
Commissioner Sheb Mirando Yes  
Commissioner Scott Barber Yes  
Commissioner Kelly McReynolds  
Chair Clow Recused

**Against:** None

**Abstain:** None

## H. MATTERS FROM THE COMMISSION

### I. ADJOURNMENT

10:00 P.M.

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Liaison

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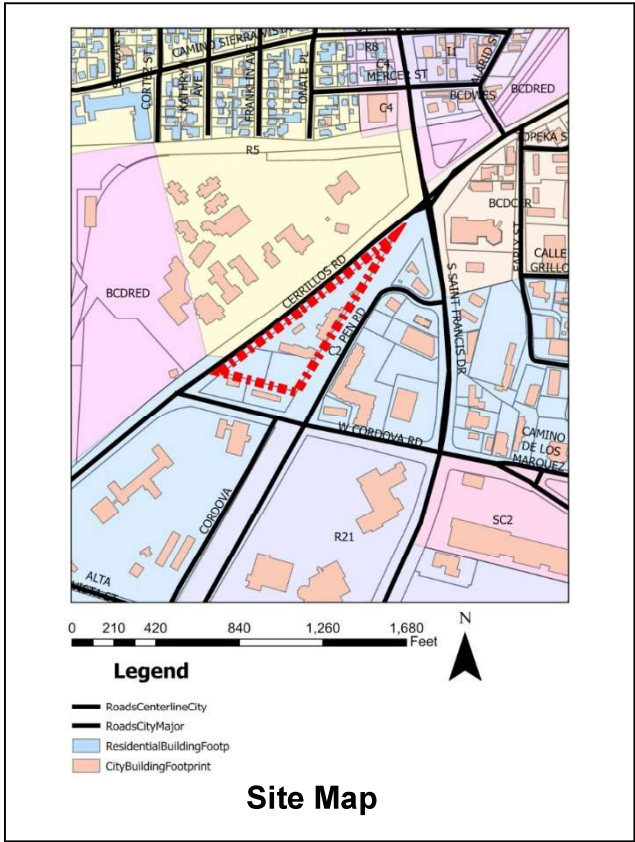
Chair

# EXHIBIT D



## Land Use Department Planning Commission Staff Report

<b>Case No:</b>	2025-11380
<b>Hearing Date:</b>	February 5, 2026
<b>Agent:</b>	JenkinsGavin, Inc.
<b>Applicant:</b>	Titan Development
<b>Request:</b>	Development Plan
<b>Location:</b>	1000, 1101, 1103 Cerrillos Road
<b>Case Mgr.:</b>	Claudia Kath
<b>Zoning:</b>	C-2 (General Commercial)
<b>Overlay:</b>	Zone 1 of the Cerrillos Road Highway Corridor Protection District and Suburban Archaeological District
<b>Pre-app Mtg:</b>	August 14, 2025
<b>ENN Mtg:</b>	September 30, 2025
<b>Proposal:</b>	The Applicant requests approval of a Development Plan to construct a 79,491sq.ft. AC Marriott hotel with 150 rooms on a 3.40-acre parcel.



**Case #2025-11380. 1000, 1101, and 1103 Cerrillos Road Development Plan.** JenkinsGavin, Inc., Agent, for Titan Land Development, on Behalf of Gilliam Land Trust, owner and applicant (“Applicant”), requests approval of a Development Plan, for a 79,491 square foot, four-story, 150-room AC Marriott hotel. The subject properties comprise 3.40 +/- acres and are located within the Suburban Archaeological Review District and Cerrillos Road Highway Corridor (Zone 1) and zoned C-2 (General Commercial) (Claudia Kath, Case Manager, cmkath@santafenm.gov).

**I. RECOMMENDATION:**

Staff recommends **APPROVAL** of the Development Plan for Case #2025-11380, subject to conditions of approval and technical corrections noted in Attachment A of this report.

*One motion will be required for this case:*

Approve or deny the Development Plan (Case#: 2025-11380), subject to the conditions of approval and technical corrections recommended by staff in Attachment A.

## II. EXECUTIVE SUMMARY:

Titan Development is requesting approval of a Development Plan to construct a 79,491 sq.ft. (Gross Floor Area) AC Marriott hotel on an approximately 3.40-acre parcel. The proposed use is a 150-room AC Marriott brand hotel. The project requires a Development Plan to allow for a full review of the proposed structure and its infrastructure. The site currently has commercial structures, which will be demolished at the time of development.



Figure 1-AC Marriott Development Plan

The proposed building will be four stories tall, with a maximum height of 45 feet and no below-grade construction. It will feature a meeting room, lounge, coffee bar, market, fitness room, and a rooftop terrace with an additional guest lounge. A total of 150 parking spaces are required (one per hotel room), and the plan provides 151 spaces. Eight accessible spaces, including two van-accessible spaces—are both required and provided. All parking spaces will meet the City standard parking stall dimensions of 9 feet in width and 18 feet in length.

## III. BACKGROUND

The 3.40-acre subject parcel is located at the southwestern corner of the intersection at Cerrillos Road and St. Francis Drive. The site is bordered by Cerrillos Road to the northwest, the AT&SF railroad R.O.W. to the southeast, and two existing commercial businesses to the south.

## IV. PROJECT ANALYSIS: DEVELOPMENT PLAN

### a. Project Zoning

The proposed hotel parcel is zoned C-2 (General Commercial). The proposed Development Plan complies with the C-2 zoning standards and the Cerrillos Road Overlay District. Required dimensional standards and proposed development are captured on Table 1.

Table 1. Dimensional Standards for C-2 District

Element	Requirement	Proposed
Height	45 feet	45 feet
Setbacks	Street: 15 feet Side: 0 feet Rear: 10 feet	Street: 67 feet Side: 51.5 feet Rear: 10.6 feet
Open Space	20%	27.3% of total lot area
Lot Coverage	60%	13.4%



Figure 2 – Project Zoning Map

**b. Access and Traffic**

A Traffic Impact Analysis (TIA) prepared by the Applicant is included in Attachment C of this report. The site currently contains nine curb cuts along Cerrillos Road; these will be consolidated into two access driveways serving the hotel. The Development Review Team (DRT) traffic engineer has noted that Driveway “B” (the access point closest to Burrito Spot) must be restricted to right-in/right-out movements in accordance with New Mexico Department of Transportation (NMDOT) comments.

There is no sidewalk abutting the site; as a result, a volunteer path has formed along the very narrow dirt section adjacent to the fence. A 6-foot sidewalk will be constructed which will provide safer pedestrian access.

NMDOT is planning improvements to the Cerrillos Road / St. Francis Drive intersection, and the City Water Division will upgrade a waterline in Cerrillos Road. These infrastructure projects are unrelated to the AC Marriott hotel project. However, they are expected to occur concurrently with the hotel construction.

The recommended DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

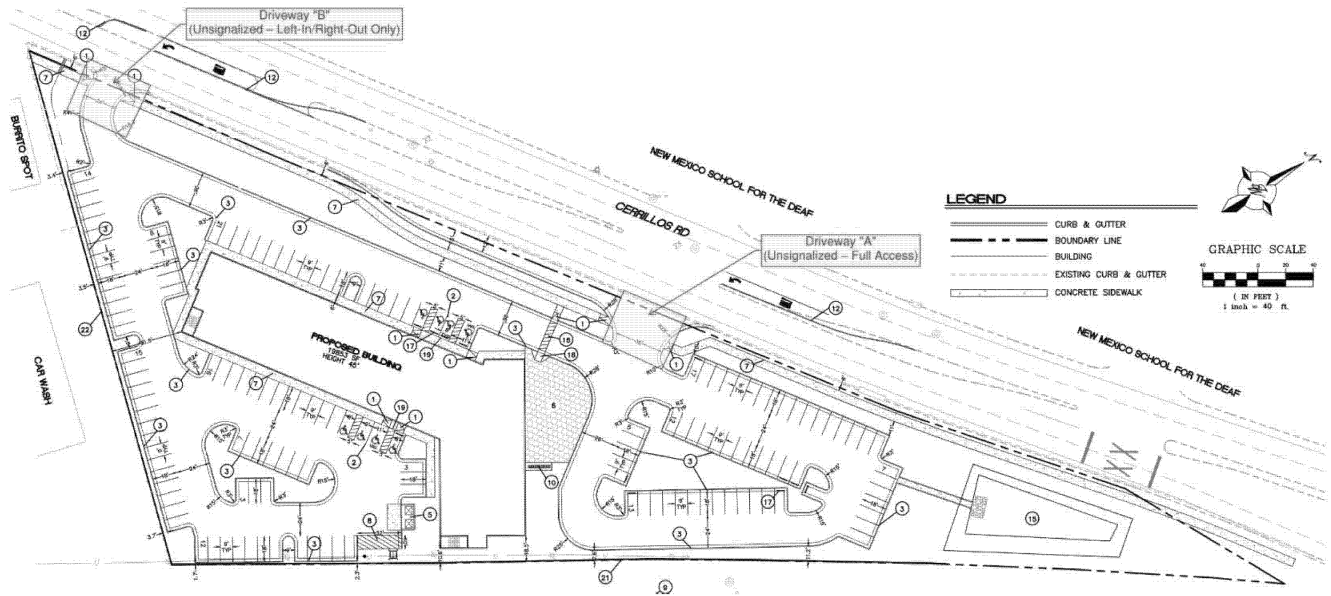


Figure 3 - Traffic Analysis Plan

**c. Grading and Terrain Management**

The subject parcel slopes gently from east to west, with no drainageways or areas exceeding a 3% slope. The improved site will generate approximately 7 cubic feet per second (CFS) of stormwater runoff—about 20% less than existing conditions. A stormwater retention pond will be constructed at the north end of the site, designed to overflow into an existing catch basin on Cerrillos Road.

The DRT has required the retention pond incorporate a meandering layout to be planted with raingarden-appropriate vegetation and trees. Along the Cerrillos Road frontage, the project will include a 5-foot-wide planter with street trees and a 6-foot-wide sidewalk.

The Terrain Management DRT Conditions of Approval and Technical Corrections are provided in Attachment A of this report.

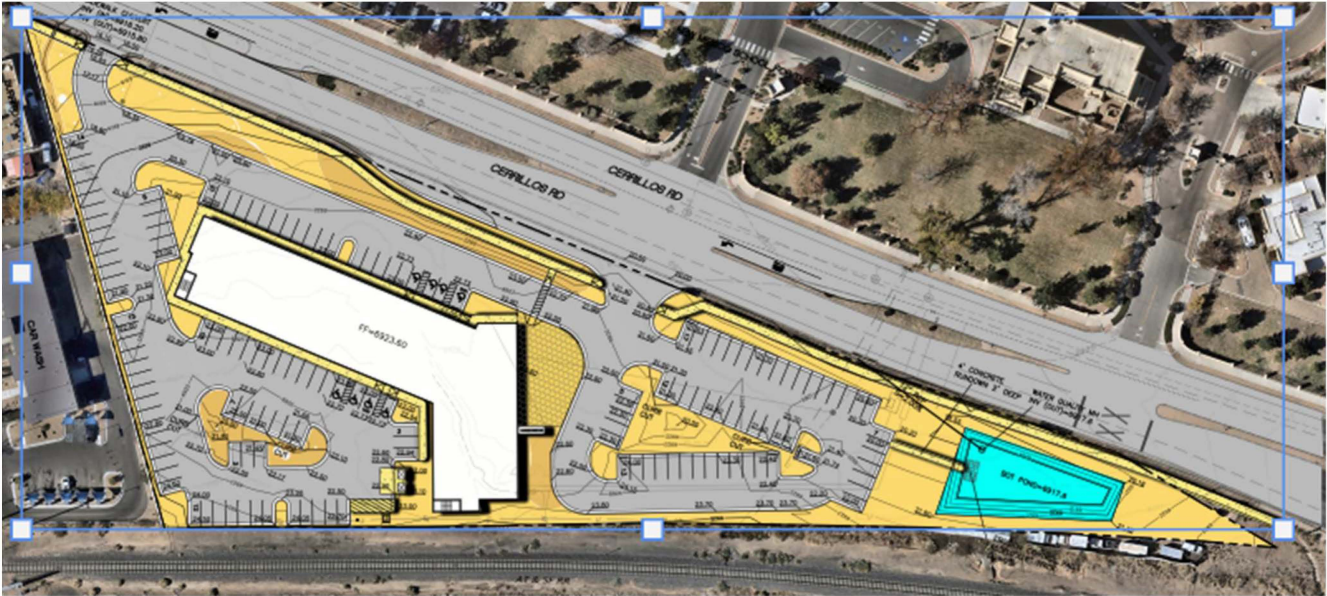


Figure 4 - Terrain Management Plan

**d. Landscaping and Lighting**

The landscape plan complies with code requirements for nonresidential development and the landscape standards per § 14-8.4 Landscape and Site Design. The landscape plan calls for new trees and shrubs throughout the site, including within parking lot medians, bordering the building, and adjacent to the drainage pond.



Figure 5 - Landscape Plan

The Lighting plan shown in the diagram below includes only overhead downcast pole-mounted fixtures within the parking area. All exterior fixtures will be required to meet the City’s Night Sky Ordinance and the requirements of § 14-8.9 Outdoor Lighting. Staff is adding a condition that the Applicant revise the Lighting plan to show all exterior fixtures and address nuisance glare. This includes ensuring all pole

fixtures and any additional outdoor fixtures include shields on that focus the illumination downward.

*§ 14-8.9(D)(3) General Standards (3) All outdoor luminaires shall be designed, installed, located and maintained such that nuisance glare onto adjacent properties or streets shall be minimized to the greatest extent practicable. Disabling glare onto adjacent properties or streets is not allowed.*

The DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

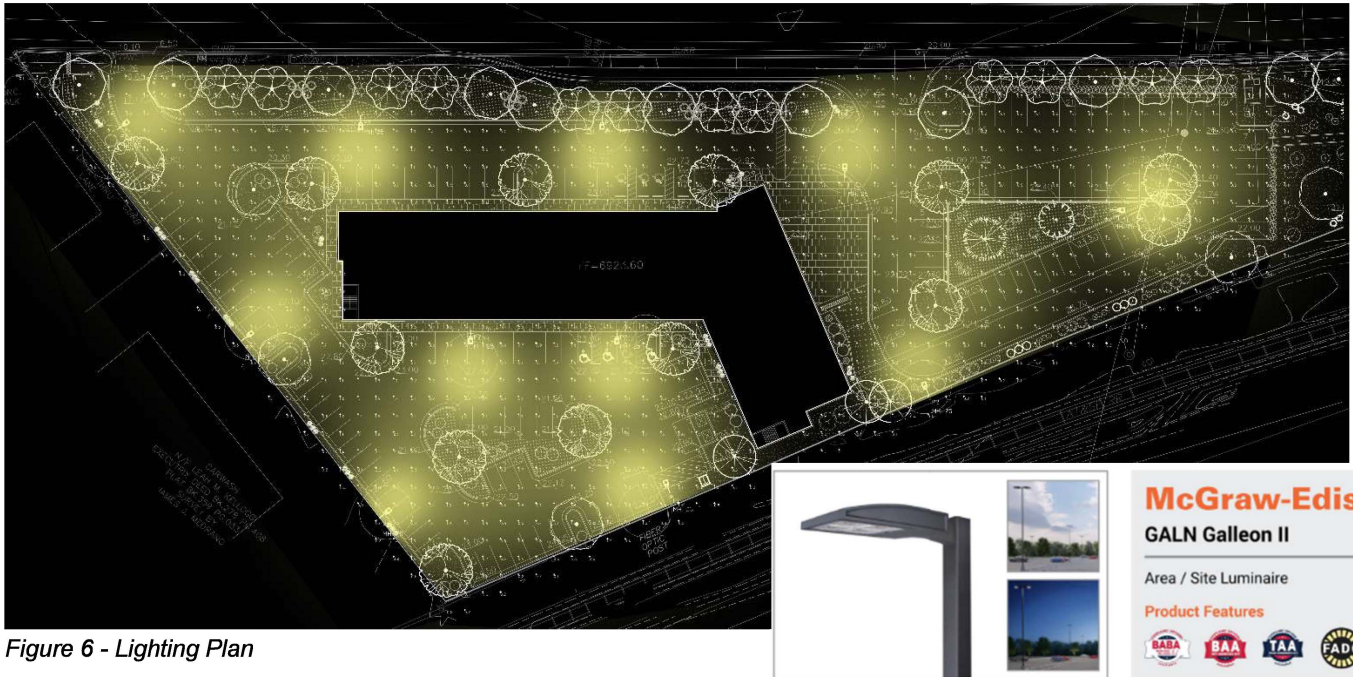


Figure 6 - Lighting Plan

### e. Architectural Design

According to the applicant's renderings, the architectural design follows the "Santa Fe Style," featuring exterior stucco walls in a two-tone earth-color palette. The proposed building is four stories tall and includes more than 20,000 square feet of gross floor area.



01 NORTHWEST PERSPECTIVE VIEW

Figure 7 - Perspective View

Publicly visible façades incorporate step-backs and wall-plane projections; however, these projections and setbacks do not fully meet the requirement that they have a depth of at least three percent of the façade length while also extending across at least 20 percent of that length. The changes in color along the facade help to provide visual interest in order to break up the building’s mass.

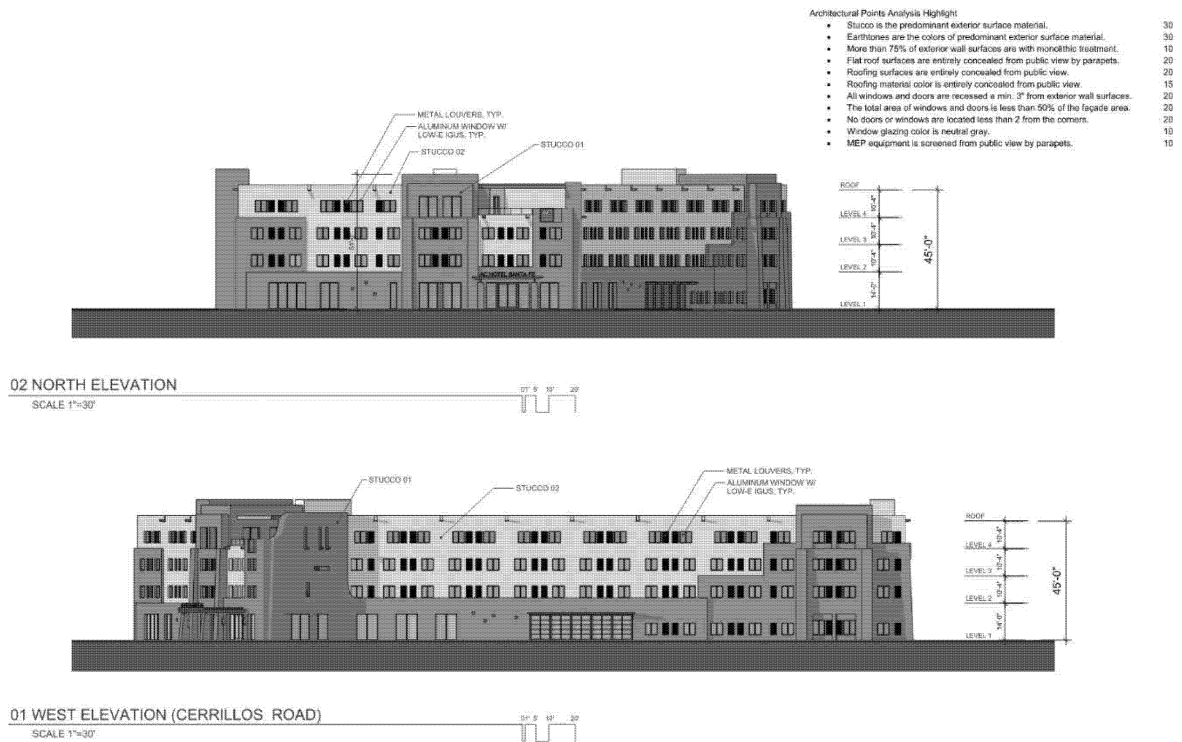


Figure 8 - Building Elevations – North and West

The building meets the required point total in the Architectural Design Review section of the code

applicable to this zoning district. Its exterior color scheme reflects traditional Santa Fe earth tones, achieved through colored masonry. The structure includes a flat roof with concealed roof surfaces and screened mechanical equipment.

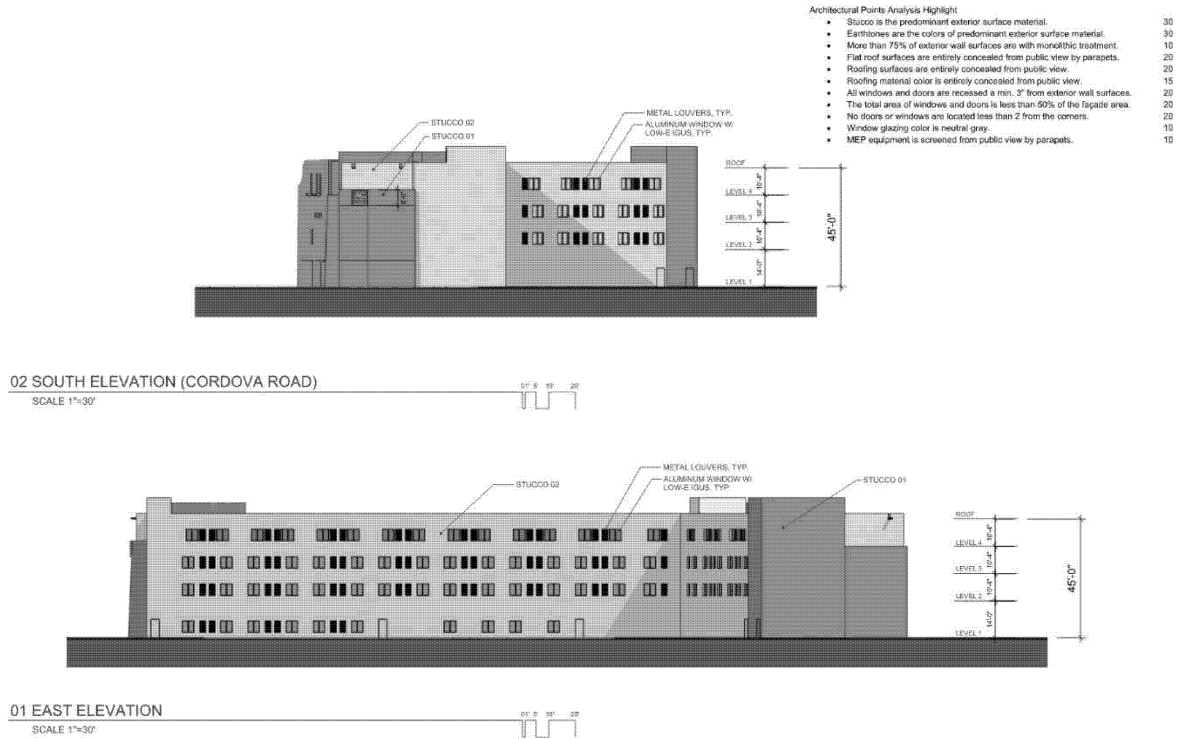


Figure 9 - Building Elevations - South and East

The DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

**f. Fire Prevention and Emergency Access**

Two 26-foot aerial apparatus access drives are provided along the west and north sides of the building, each with the required 28-foot turning radius as required by the IFC. The building will have an automatic fire-suppression system that meets all IFC standards. Two existing fire hydrants are located on the west side of Cerrillos Road directly across from the site. A 6-inch fire line will connect to the water main and to the building through a backflow preventer and hot box.

The DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

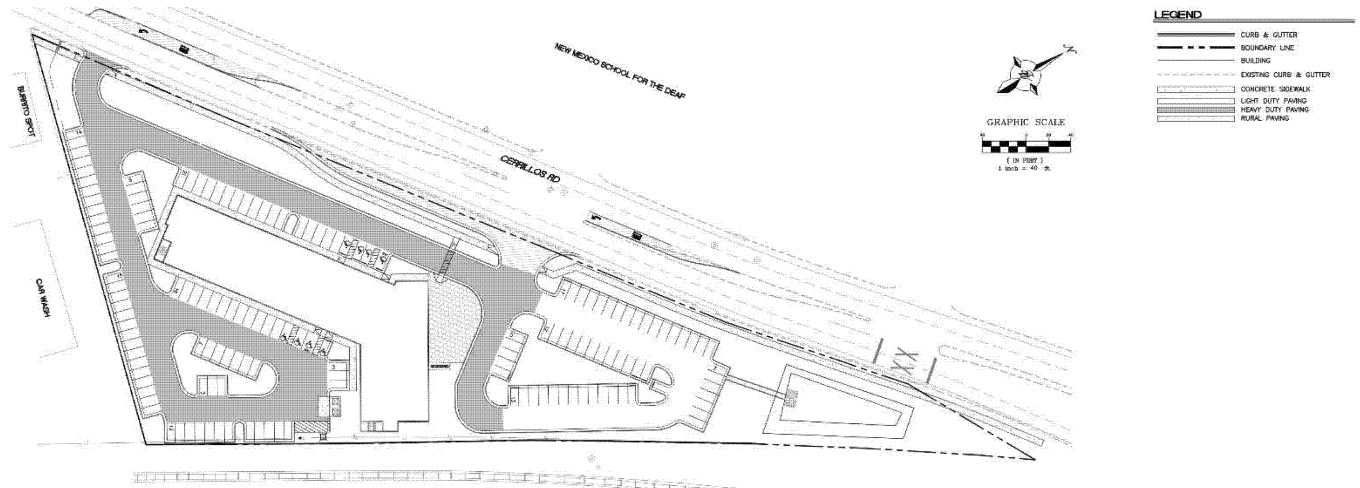


Figure 10 - Fire One Plan

**g. Water**

The Project will be served by a new 4-inch service line connected to the existing 6-inch water main along Cerrillos Road. A 4-inch meter and vault already exist on-site.

The proposed development requires the existing structures on the Property to be demolished and entirely new construction added, the Applicant asserts that the development water budget should be reduced by the average annual consumption during the twenty-four months of highest water use within the past ten years.

The Applicant submitted a water budget projecting a maximum annual water demand of 12.82 acre-feet per year (AFY). The Applicant is requesting approval of an alternative development water budget pursuant to SFCC §14-8.13(B)(2)(b), proposing to reduce annual water consumption based on the 24 months during which the highest water use was active within the preceding 10 years, to discount their annual water budget. The applicant has not provided the data for evaluation as to the amount of offset. Furthermore, the Code specifically adds the provision that, for this provision to apply, the previous offset must be higher than the proposed water consumption amount. The Applicant shall use like for like data i.e. Hotels within Santa Fe (not motels or other transient uses) to support their adjusted water budget. As the previous uses were warehouse and commercial uses, it is likely that the water use will be substantially lower than the hotel use.

Pursuant to SFCC §14-8.13-"Dedication of Water to Development," a building permit shall not be issued until the Applicant has dedicated water sufficient to meet the approved development water budget, plus a 9.8% contingency to account for water utility delivery requirements. While §14-8.13(B)(2)(b) allows for an alternative methodology to determining a development water budget, it should not override or waive the dedication requirements of SFCC §14-8.13.

Allowing construction to proceed prior to completion of the required water rights transfer would place a direct strain on City water resources. In the absence of a completed transfer, the City would be required to temporarily support the proposed development using City held water rights, thereby assuming the risk that the required offset may not ultimately be secured.

The States water rights transfer process is subject to protest and appeal, and such appeals may extend for multiple years or result in the transfer being denied.

During this period of uncertainty, the City would continue to serve the development with the assurance that the water demand will be fully offset, which is inconsistent with the purpose of SFCC §14-8.13.

Accordingly, the Applicant shall submit a revised water budget identifying the annual water consumption based on the 24 months during which the highest water use was active within the preceding 10-year period. The revised water budget shall utilize available historic water use data from the existing property and shall be submitted to the City Water Resources Division for review and approval.

Approval of the development water budget and completion of the required water rights transfer shall be required prior to the issuance of any construction/building permit.

Should subsequent monitoring or verification demonstrate that actual water use is less than the approval development water budget, the City may adjust water use accounting and billing consistent with the adopted administrative procedures. However, any such adjustment shall not delay or replace the requirement to complete the water rights transfer in advance of permitting.

The DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

**h. Sewer**

An existing 10-inch sewer main runs east–west along the north side of the site. A new 8-inch sewer service line will connect to this main via a new manhole.

The DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

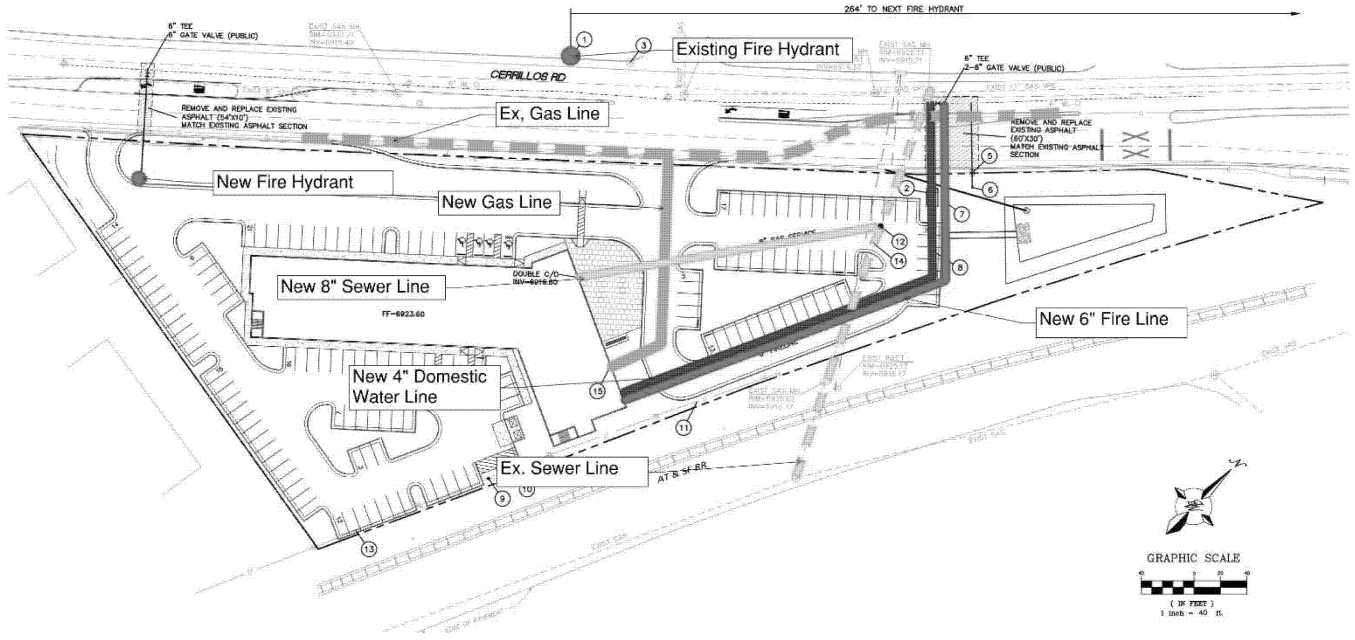


Figure 11- Utility Service Plan

**i. Signage**

The Applicant has complied with the signage requirements, per SFCC14-8.10(G)(8), in the Cerrillos Highway Corridor Protection District. Three signs are proposed, which include a freestanding Monument Sign, a porte-cochere mounted sign, and a wall sign on the hotel building. Signage will be reviewed in detail and approved at the time of building permit.

**D/F DIRECTIONAL SIGN**

1. ALL directional signs shall be illuminated at night.

2. ALL directional signs shall be illuminated at night.

3. ALL directional signs shall be illuminated at night.

4. ALL directional signs shall be illuminated at night.

5. ALL directional signs shall be illuminated at night.

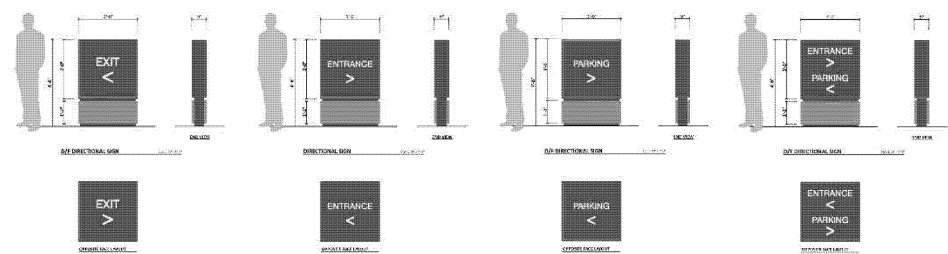
6. ALL directional signs shall be illuminated at night.

7. ALL directional signs shall be illuminated at night.

8. ALL directional signs shall be illuminated at night.

9. ALL directional signs shall be illuminated at night.

10. ALL directional signs shall be illuminated at night.



**BUILDING SIGNAGE - SMALL SIGN**

1. ALL building signage shall be illuminated at night.

2. ALL building signage shall be illuminated at night.

3. ALL building signage shall be illuminated at night.

4. ALL building signage shall be illuminated at night.

5. ALL building signage shall be illuminated at night.

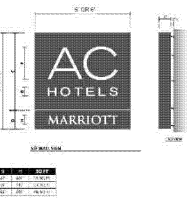
6. ALL building signage shall be illuminated at night.

7. ALL building signage shall be illuminated at night.

8. ALL building signage shall be illuminated at night.

9. ALL building signage shall be illuminated at night.

10. ALL building signage shall be illuminated at night.



**PORT COCHERE SIGNAGE - 12'**

1. ALL port cochere signage shall be illuminated at night.

2. ALL port cochere signage shall be illuminated at night.

3. ALL port cochere signage shall be illuminated at night.

4. ALL port cochere signage shall be illuminated at night.

5. ALL port cochere signage shall be illuminated at night.

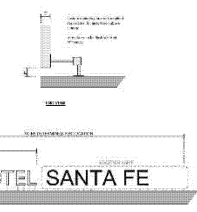
6. ALL port cochere signage shall be illuminated at night.

7. ALL port cochere signage shall be illuminated at night.

8. ALL port cochere signage shall be illuminated at night.

9. ALL port cochere signage shall be illuminated at night.

10. ALL port cochere signage shall be illuminated at night.



**MONUMENT SIGN**

1. ALL monument signs shall be illuminated at night.

2. ALL monument signs shall be illuminated at night.

3. ALL monument signs shall be illuminated at night.

4. ALL monument signs shall be illuminated at night.

5. ALL monument signs shall be illuminated at night.

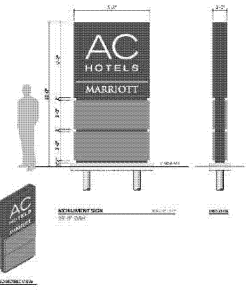
6. ALL monument signs shall be illuminated at night.

7. ALL monument signs shall be illuminated at night.

8. ALL monument signs shall be illuminated at night.

9. ALL monument signs shall be illuminated at night.

10. ALL monument signs shall be illuminated at night.



**5GSTUDIO**  
ARCHITECTURAL

**OWNER/DEVELOPER:**  
5G Studio  
500 Phoenix Plaza, Ste 300  
Phoenix, AZ 85004

**ARCHITECT:**  
5G Studio  
500 Phoenix Plaza, Ste 300  
Phoenix, AZ 85004

**CIVIL ENGINEER:**  
5G Studio  
500 Phoenix Plaza, Ste 300  
Phoenix, AZ 85004

**CONSULTANT:**  
5G Studio  
500 Phoenix Plaza, Ste 300  
Phoenix, AZ 85004

**LANDSCAPE ARCHITECT:**  
5G Studio  
500 Phoenix Plaza, Ste 300  
Phoenix, AZ 85004

**MEP:**  
5G Studio  
500 Phoenix Plaza, Ste 300  
Phoenix, AZ 85004

NO.	DATE	REVISION	BY



AC BY MARRIOTT HOTEL  
DESIGN/CONCEPT PLAN  
5000 PHOENIX PLAZA, STE 300  
PHOENIX, AZ 85004

SCALE: AS SHOWN

**A303**

Figure 12 - Proposed Site Signage

**j. Impact on Schools**

This project does not include any residential units, and thus there is no impact on schools.

**k. Archaeology**

The Project is located within the Suburban Archaeological District. As the Project is less than 10 acres in size, archaeological clearance is not required. However, the site is the former location of the 1800's Penitentiary. Staff encourages monitoring of ground disturbing activities as there is the unknown potential to encounter historic prison cultural remains in and around the site boundaries.

The DRT Conditions of Approval and Technical Corrections may be found in Attachment A of this report.

**l. Santa Fe HOMES Program**

The proposal does not include residential units, so no Santa Fe HOMES requirements are triggered.

**V. DEVELOPMENT PLAN APPROVAL CRITERIA SECTION 14-3.8(D)(1)**

SFCC Section 14-3.8 governs the authority, procedures, and restrictions for development plans. The Criteria for approval of Development Plans are detailed below:

<p><b>§14-3.8(D)(1)(a)</b> - that it is empowered to approve the plan under the section of Chapter 14 described in the application;</p>	<p>Criterion Met: (Yes/No) YES</p>
<p><i>Applicant Response: SFCC 14-2.3(C)(1) states, “Unless otherwise provided in Chapter 14, the planning commission shall review and approve or disapprove various specific plans, requests and subdivision plats.”</i></p>	
<p>Staff Response: The Santa Fe City Code (SFCC) Subsection 14-2.3(D)(1) authorizes the Planning Commission to review and approve or disapprove development plans and subdivision plans. Subsection 14-3.8(B) requires approval of a development plan for projects with a gross floor area of thirty thousand feet or more.</p> <p>The Development Plans comply with Santa Fe City Code Section 14-3.8(D)(1)(a).</p>	
<p><b>§14-3.8(D)(1)(b)</b> - that approving the development plan will not adversely affect the public interest;</p>	<p>Criterion Met: (Yes/No) YES</p>
<p><i>Applicant Response: Approving the Development Plan will not adversely affect the public interest. The site houses older commercial structures with significant outdoor storage areas. Redevelopment of the property will greatly enhance and improve this gateway to downtown. The property is centrally located within walking distance to bus service, the Rail Runner Station, and area amenities such as restaurants, the Santa Fe Railyard Park, retail services, the Santa Fe Rail Trail, and the Acequia Trail, supporting multi-modal transportation and reducing automobile dependency. Furthermore, the proposed hotel provides economic benefits through employment opportunities, tourism revenue, lodgers tax revenue, and the multiplier effect of increased visitor spending.</i></p>	
<p>Staff Response: Staff agrees with the Applicant. The term “Public Interest” is not specifically defined in Chapter 14; however, the Governing Body has implemented the General Plan as stated in Subsection Section 14-1.3 (General Purposes). The resulting ordinances establish minimum standards for health, safety and welfare affecting land uses and developments as a means to protect the public interest from within the municipality.</p> <p>Staff has reviewed the Development Plan applications in accordance with the City Code. Subject to staff recommended conditions of approval, the Development Plans comply with minimum standards of Chapter 14 SFCC and would not adversely affect the public interest.</p> <p>The Development Plans comply with Santa Fe City Code 14-3.8(D)(1)(b).</p>	

<p><b>14-3.8(D)(1)(c)</b> - that the use and any associated buildings are compatible with and adaptable to buildings, structures and uses of the abutting property and other properties in the vicinity of the premises under consideration.</p>	<p>Criterion Met: (Yes/No) YES</p>
<p><i>Applicant Response: The proposed hotel is compatible with surrounding structures and uses, which include Capital Flats, a four-story multi-family housing development, the New Mexico School for the Deaf (NMSD), and the South Capitol complex of large, multi-story state government buildings. Inspired by the Pueblo Revival architecture of the NMSD campus, the building design is adaptable to area structures. In addition, development of a quality hotel at the heart of this lively commercial and recreational area aligns with the General Plan policy supporting a diverse mix of land uses in all areas of the City, as well as with the policy encouraging infill development.</i></p>	
<p>Staff Response: The proposed hotel use is a use that is allowed in a C-2 Zoned district. The adaptability of the structure within the C-2 District is compatible with the uses adjoining the property. The architecture deviates from the traditional Santa Fe Style with setbacks that do not specifically meet the SFCC code. However, the building, through the architectural points analysis does comply with 14-8.7 – “Architectural Design Review.”</p> <p>The Development Plans comply with Santa Fe City Code Section 14-3.8(D)(1)(c).</p>	

**VI. EARLY NEIGHBORHOOD NOTIFICATION**

An Early Neighborhood Notification meeting was held on September 30, 2025. Approximately 31 neighbors were in attendance. Discussion centered around concerns regarding traffic, building design/height, environmental impacts, views, safety and the need for a new hotel.

**VII. EXPIRATION**

Per SFCC Section 14-3.19(B)(4) “Approval of a final development plan, or any development plan for which no preliminary development plan was required, shall expire three years after final action approving it unless actual development of the site or offsite improvements has begun and is continued pursuant to Subsection 14-3.19(B)(6).” Therefore, should the Commission approve the development plan, the expiration date would be three years from the adoption of the Findings of Fact and Conclusions of Law or 35 days after the date of the hearing.

**VIII. ATTACHMENTS:**

ATTACHMENT A: Conditions of Approval and Technical Corrections

- A-1: Table of Conditions of Approval
- A-2: Table of Technical Corrections
- A-3: Development Review Team Compiled Comments

ATTACHMENT B: Maps and Figures

- B-1: Maps and Figures

ATTACHMENT C: Applicant Materials

- C-1: Letter of Intent – Development Plan Report
- C-2: Certificate of Compliance as Legal Lot of Record
- C-3: Architectural Points Checklist
- C-4: Utility Service Application
- C-5: Preliminary Water Budget
- C-6: Traffic Impact Analysis
- C-7: Development Plan Drawing Set
- C-8: Infrastructure Drawing Set

ATTACHMENT D: Early Neighborhood Notification

- D-1: ENN Packet
- D-2: ENN Meeting Notes

**APPROVED BY THE PLANNING AND LAND USE DEPARTMENT:**

<b>Title</b>	<b>Name</b>	<b>Initials</b>
Department Director	Heather Lamboy	HLL
Assistant Department Director	Maggie Moore	MRM
Planning Manager	Daniel Esquibel	DAE
Planner Senior	Claudia Kath	CK

## Attachment A

<b>1. TABLE OF CONDITIONS OF APPROVAL</b>		<b>Department</b>	<b>To be completed by:</b>
1	Shall comply with the most currently adopted International Fire Code (IFC) and adopted city ordinances upon building permit approval.	Fire Marshall	Prior to Building Permit Approval
2	Shall comply with the most currently adopted fire code at the time of building permit approval to include adopted city ordinances for access and fire flow requirements.	Fire Marshall	Prior to Building Permit Approval
3	An approved Water Plan will be required for all new public water infrastructure and fire services.	Water Division	Prior to Public Hearing for Final Subdivision Plan
4	An approved Agreement to Construct and Dedicate (ACD) will be required with the Water Division for all new public water infrastructure and fire services.	Water Division	Prior to Building Permit Approval
5	A separate irrigation meter will be required if the total landscaped area is 1,000 square feet or greater. An approved backflow prevention device shall be installed beyond the meter on any new irrigation service.	Water Division	At the time of Development
6	Any re-grading of site over existing water mains shall maintain a minimum of 4 feet of cover and not exceed 5 feet of cover. Depth of existing main to be adjusted to maintain a minimum of 4 feet of cover and not exceed 5 feet of cover if site grading impacts these depth requirements.	Water Division	At the time of Development
7	The Wastewater Division requires design drawings for both the sanitary sewer and storm water systems for this project to have both plan and profile drawings. Also, these drawings shall be highlighted to have the storm and sewer systems stand out beyond the other features.	Wastewater	Prior to Building Permit Approval
8	Resubmit Utility Service Application and complete the Commercial subtotal including the landscape irrigation demand to get the Total Project Water Demand in AFY for AC Marriott Hotel	Water Resources	Prior to Building Permit Approval
9	Include the following reliable data for your Alternative Development Water Budget (DWB) :	Water Resources	Prior to Building Permit Approval

10	For the Preliminary Alternative Development Water Budget, provide 3 comparable hotels for reliable data comparison a. El Rey was traditionally a Motel, not a Hotel, and is now the El Rey Motor Court. b. Not aware of a Spring Hill Suite in Santa Fe, Provide 3 hotels in Santa Fe of comparable size (i.e. Courtyard by Marriott @ 3347 Cerrillos Rd)	Water Resources	Prior to Building Permit Approval
11	At time of construction, the Contractor shall ensure ADA compliance for construction of ADA accessible features and appurtenances, as detailed in, and in addition to, the approved construction permit documents as required. Improvements shall comply with ICC ANSI A117.1-2009 Chapters 1-5 and Chapter 7, 2015 New Mexico Accessible Parking Checklist, MUTCD, NMDOT Pedestrian Access Standards and PROWAG, NM State Statute and Administrative Code and DOJ regulations as applicable.	ADA	At the time of Construction
12	1000 Cerrillos Road is the historic location of the 1800s State Penitentiary. Staff is requesting monitoring of the ground disturbance for the development of the proposed hotel given the unknown potential to encounter historic prison burials around the site boundaries. This opportunity to monitor the ground disturbance will clear this area from future ground disturbance where areas are proposed to be disturbed.	Archaeology	At the time of Development
13	On Sheet W-1, Utility Contact information needs to be updated. Mr. Frank Leyba is retired and no longer works for the City of Santa Fe.	Archaeology	At the time of Development
14	No color, grayscale, or photos on any plan sheets – black and white linework only	Land Use	Prior to Recordation
15	Confirm an approved Water Budget prior to requesting an Alternative water Budget	Land Use	Prior to Building Permit Approval
16	Show all exterior light fixtures with cutsheets in revised photometric plan	Land Use	Prior to Building Permit Approval
17	Show shields on all light fixtures to prevent nuisance glare	Land Use	Prior to Building Permit Approval

## Attachment A

<b>2. TABLE OF TECHNICAL CORRECTIONS</b>		<b>Department</b>	<b>To be completed by:</b>
1	Retention / detention pond conveyance system from the parking lot is very ridged. Provide landscape plantings along the conveyance swale to screen and soften the aesthetics of the swale in the landscape as seen from Cerrillos Road.	Landscape	Prior to Recordation
	Provide areas of bark mulch to reduce the gravel quantity in strategic areas	Landscape	Prior to Building Permit
2	V100 yr volume calcs appear to have an error. While utilizing the equation they provided, staff has found significantly different results	Terrain Management	Prior to Recordation
3	Could they provide the equation for how the flow rates are calculated in the existing drainage and proposed drainage data tables?	Terrain Management	Prior to Recordation
4	Please provide a summary of the total developed discharge rate, post detainment	Terrain Management	Prior to Recordation
5	How was the volume of the pond determined to be sufficient, were hydrographs generated?	Terrain Management	Prior to Recordation
6	B.O.P. elevation in the volume calculations for the north pond has not been updated to 6917.10, please re-run these calculations to provide updated volumes and orifice flow rates	Terrain Management	Prior to Recordation
7	There appears to still be some curb cuts without call outs	Terrain Management	Prior to Recordation
8	Could we modify the recessed area adjacent to cerillos road to include the entire planter strip, so that the street trees can maximize the amount of rainwater harvesting to them	Terrain Management	Prior to Recordation
9	Roof flow was stated as "flow to surface" Is the roof just going to drip right on the sidewalk? Or is there a gutter, sidewalk culverts, etc?	Terrain Management	Prior to Recordation
10	Please edit the pond riser detail to show the orifice 6 inches above BOP	Terrain Management	Prior to Recordation
11	Please meander swale in basin 2 to the pond and add water harvesting feature	Terrain Management	Prior to Recordation
15	See attached letter date 11/3/2025 from Wilson & Company	Traffic	Prior to Recordation

# Development Review Team (DRT) Comment Form for Planning Commission

Date: November 12, 2025

DRT Member: Fire Marshal Geronimo Griego

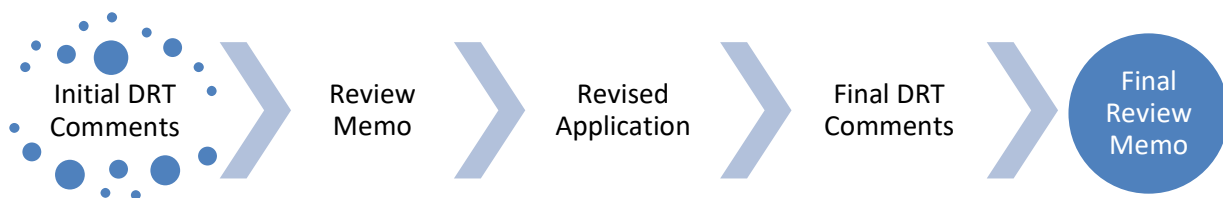
Dept/Div: Fire Marshal's Office

Case No.: 2025-11380 - AC Marriott Hotel

Planner: Claudia Kath, Senior Land-Use Planner

DRT Review Schedule – 9-12+ weeks\*

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

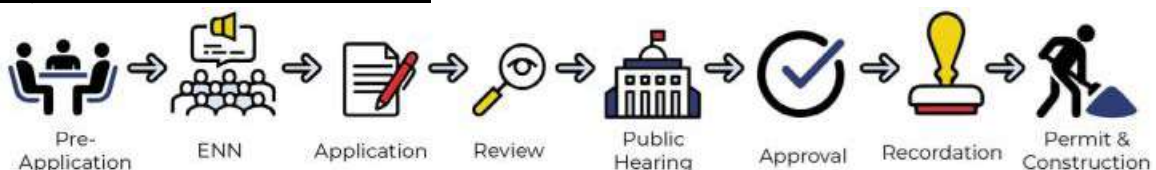


Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *At the time of development* -

Development Review Process Flow Chart



\*See the 2024 Development Review Schedule for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

<b>Conditions of Approval:</b>	<b>Must be completed by:</b>	<b>Applicant response**:</b>
1. Shall comply with the most currently adopted International Fire Code (IFC) and adopted city ordinances upon building permit approval.	<i>Prior to Building Permit Approval.</i>	

<b>Technical Corrections:</b>	<b>Must be completed by:</b>	<b>Applicant Response**:</b>

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

- 1.

Explanation of Conditions or Corrections (if needed):

1. Shall comply with the most currently adopted fire code at the time of building permit approval to include adopted city ordinances for access and fire flow requirements.

# Development Review Team (DRT) Comment Form

Date: October 29, 2025  
DRT Member: Taylor Jurgens  
Dept/Div: Public Utilities/Water Division/Engineering  
Case No.: Case 2025-11380: AC Marriott Hotel Development Plan  
Case Planner: Claudia Kath

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## DRT Review Schedule – 9-12+ weeks\*

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

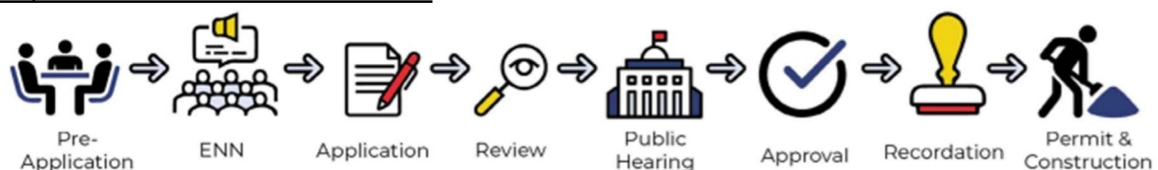


## Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- a. *Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *At the time of development* -

## Development Review Process Flow Chart



\*See the 2024 Development Review Schedule for details

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
1. An approved Water Plan will be required for all new public water infrastructure and fire services.	Prior to Public Hearing	
2. An approved Agreement to Construct and Dedicate (ACD) will be required with the Water Division for all new public water infrastructure and fire services.	Prior to Building Permit Approval	
3. A separate irrigation meter will be required if the total landscaped area is 1,000 square feet or greater. An approved backflow prevention device shall be installed beyond the meter on any new irrigation service.	At the time of development	
4. Any re-grading of site over existing water mains shall maintain a minimum of 4 feet of cover and not exceed 5 feet of cover. Depth of existing main to be adjusted to maintain a minimum of 4 feet of cover and not exceed 5 feet of cover if site grading impacts these depth requirements.	At the time of development	
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant Response:
1. Specific technical comments on the water plans will be provided by the Water Division to the design engineer.	Prior to Water Plan Approval	
2.		
3.		
4.		

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

# Development Review Team (DRT) Comment Form

Date: January 29, 2026  
DRT Member: Stan Holland, P.E.  
Dept/Div: Utilities\ Wastewater Division  
Case No.: Case #: 2025-11380 - AC Marriott Hotel Development Plan Resubmittal  
Case Planner: Claudia Kath Senior Planner

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## DRT Review Schedule – 9-12+ weeks\*

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review

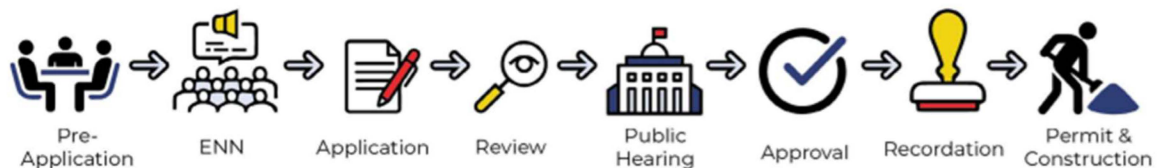


## Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

- Prior to Public Hearing* – these conditions/technical corrections must be addressed before the case may move forward to the public hearing phase of the Development Review Process.
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- Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- At the time of development* -

## Development Review Process Flow Chart



\*See the *2024 Development Review Schedule* for details

Form Updated: September 2023

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

<b>Conditions of Approval:</b>	<b>Must be completed by:</b>	<b>Applicant response**:</b>
1. The Wastewater Division requires design drawings for both the sanitary sewer and storm water systems for this project to have both plan and profile drawings. Also, these drawings shall be highlighted to have the storm and sewer systems stand out beyond the other features.	<i>Prior to Building Permit Approval</i>	
2.		
3.		
4.		
5.		
6.		

<b>Technical Corrections:</b>	<b>Must be completed by:</b>	<b>Applicant Response**:</b>
1.		
2.		
3.		
4.		

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

Date: 1/29/2026

DRT Member: Alan Hook

Dept/Div: Water Resources

Case No.: 2025-11380

Case Planner: Claudia Kath

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case #

Conditions of Approval:	Must be completed by:	Applicant response**:
1. Resubmit Utility Service Application and complete the Commercial subtotal including the landscape irrigation demand to get the Total Project Water Demand in AFY for AC Marriott Hotel	Prior to Building Permit Approval	
2. Include the following reliable data for your Alternative Development Water Budget (DWB)	Prior to Building Permit Approval	
3. For the Preliminary Alternative Development Water Budget, provide 3 comparable hotels for reliable data comparison a. El Rey was traditionally a Motel, not a Hotel, and is now the El Rey Motor Court. b. Not aware of a Spring Hill Suite in Santa Fe, c. Provide 3 hotels in Santa Fe of comparable size (i.e. Courtyard by Marriott @ 3347 Cerrillos Rd)	Prior to Building Permit Approval	
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]

Explanation of Conditions or Corrections (if needed):

# Development Review Team (DRT) Comment Form

Date:

DRT Member: Teddy Padilla

Dept/Div Land Use/ Technical Review

Case No.: 2025-11380

Case Planner: Claudia Kath

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## DRT Review Schedule – 9-12+ weeks\*

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

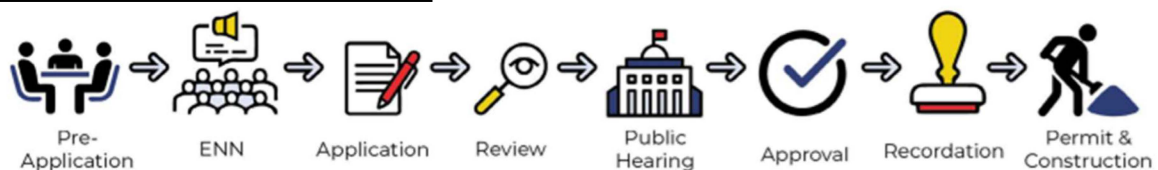


## Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

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- Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- At the time of development* -

## Development Review Process Flow Chart



\*See the *2024 Development Review Schedule* for details

Form Updated: September 2023

**Conditions of Approval and Technical Corrections Tables**

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

<b>Conditions of Approval:</b>	<b>Must be completed by:</b>	<b>Applicant response**:</b>
1. At time of construction, the Contractor shall ensure ADA compliance for construction of ADA accessible features and appurtenances, as detailed in, and in addition to, the approved construction permit documents as required. Improvements shall comply with ICC ANSI A117.1-2009 Chapters 1-5 and Chapter 7, 2015 New Mexico Accessible Parking Checklist, MUTCD, NMDOT Pedestrian Access Standards and PROWAG, NM State Statute and Administrative Code and DOJ regulations as applicable.	Recordation	
2.		
3.		
4.		
5.		
6.		

<b>Technical Corrections:</b>	<b>Must be completed by:</b>	<b>Applicant Response**:</b>
1.		
2.		
3.		
4.		

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

# Development Review Team (DRT) Comment Form

Date: 12/04/2025

DRT Member: Paul A. Duran

Dept/Div: Land Use/ Historic Preservation

Case No.: 2025-11380-AC Marriott Hotel 1000 Cerrillos Rd

Case Planner: Claudia Kath

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## DRT Review Schedule – 9-12+ weeks\*

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

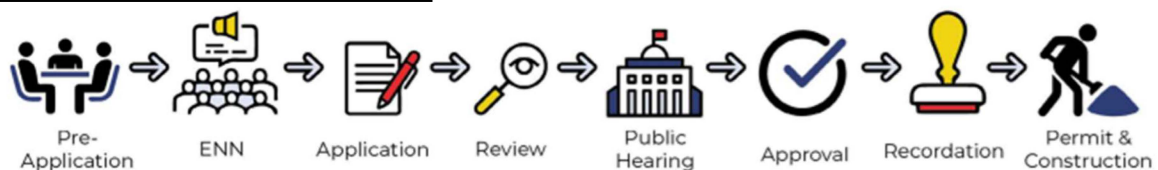


## Timing of Conditions of Approval + Technical Corrections

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

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- Prior Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- At the time of development* -

## Development Review Process Flow Chart



\*See the *2024 Development Review Schedule* for details

Form Updated: September 2023

**Conditions of Approval and Technical Corrections Tables**

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

<b>Conditions of Approval:</b>	<b>Must be completed by:</b>	<b>Applicant response**:</b>
1. 1000 Cerrillos Road is the historic location of the 1800s State Penitentiary. Staff encourages monitoring of ground disturbing activities for the development of the proposed hotel given the unknown potential to encounter historic prison burials around the site boundaries. This opportunity to monitor the ground disturbance will clear this area from future monitoring of ground disturbance where areas are proposed to be disturbed.		
2. On Sheet W-1, Utility Contact information needs to be updated. Mr. Frank Leyba is retired and no longer works for the City of Santa Fe.		
3.		
4.		
5.		
6.		

<b>Technical Corrections:</b>	<b>Must be completed by:</b>	<b>Applicant Response**:</b>
1.		
2.		
3.		
4.		

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

# Development Review Team (DRT) Comment Form

**DRT Review Schedule – 9-12+ weeks\***

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

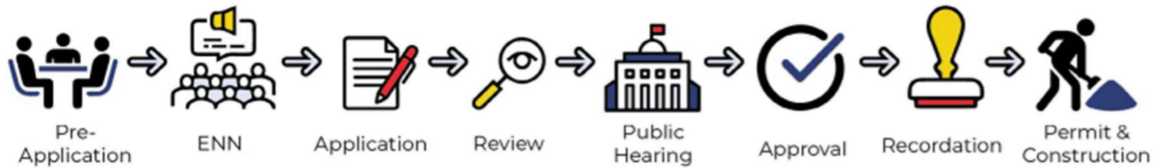


**Timing of Conditions of Approval + Technical Corrections**

While all DRT conditions of approval and technical corrections must be met by the applicant, the timing of compliance varies. In the “Must be completed by” column in the following tables, please time your conditions of approval and technical corrections to the following development review stages:

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- b. *Prior to Recordation* – these conditions/technical corrections may be resolved after the public hearing but must be addressed before the Development Plan or Subdivision plat is recorded.
- c. *Prior to Building Permit Approval* – these conditions/technical corrections can be addressed during the building permit review process, but prior to issuance of the permit.
- d. *During Construction* – these conditions/technical corrections can be addressed during construction.

**Development Review Process Flow Chart**



\*See the *2024 Development Review Schedule* for details

**DRT Review Timeline:**

Application Received	DRT Application Intake	DRT Initial Review Comments Due

Date: January 29, 2026

DRT Member: Claudia Kath

Dept/Div: Current Planning Land Use

Case No.: 2025-11380

Case Planner:

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Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Case #

<b>Conditions of Approval:</b>	<b>Must be completed by:</b>	<b>Applicant response**:</b>
1. No color, gray scale or photos on any plan sheets – black and white linework only	Prior to Recordation	
2. Confirm an approved Water Budget prior to requesting an Alternative Water Budget	Prior to Recordation	
3. Show all exterior light fixtures with cutsheets in revised photometric plan	Prior to Building Permit Approval	
4. Show shields on all lights to prevent nuisance glare	Prior to Building Permit Approval	
5.		

<b>Technical Corrections:</b>	<b>Must be completed by:</b>	<b>Applicant response**:</b>
1.		
2.		
3.		
4.		

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

# Development Review Team (DRT) Comment Form

Date: 12/9/2025

DRT Member: Dee Beingessner

Dept/Div: Terrain Management

Case No.: 2025-11380

Case Planner: Claudia Kath

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## DRT Review Schedule – 9-12+ weeks\*

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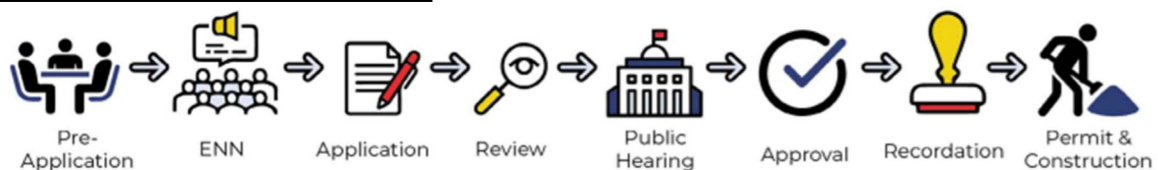


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- At the time of development* -

## Development Review Process Flow Chart



\*See the *2024 Development Review Schedule* for details

Form Updated: September 2023

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant response**:
1. V100 yr volume calcs appear to have an error. While utilizing the equation they provided, staff has found significantly different results	Prior to recordation	
2. Could they provide the equation for how the flow rates are calculated in the existing drainage and proposed drainage data tables?	Prior to recordation	
3. Please provide a summary of the total developed discharge rate, post detainment	Prior to recordation	
4. How was the volume of the pond determined to be sufficient, were hydrographs generated?	Prior to recordation	
5. B.O.P. elevation in the volume calculations for the north pond has not been updated to 6917.10, please re-run these calculations to provide updated volumes and orifice flow rates	Prior to recordation	
6. There appears to still be some curb cuts without call outs	Prior to recordation	
7. Could we modify the recessed area adjacent to cerillos road to include the entire planter strip, so that the street trees can maximize the amount of rainwater harvesting to them	Prior to recordation	
8. Roof flow was stated as "flow to surface" Is the roof just going to drip right on the sidewalk? Or is there a gutter, sidewalk culverts, etc?	Prior to recordation	
9. Please edit the pond riser detail to show the orifice 6 inches above BOP	Prior to recordation	
10. Please meander swale in basin 2 to the pond and add water harvesting feature	Prior to recordation	

\*\*The Applicant must respond to the condition of approval or technical correction, indicating they have met the requirement and providing a reference in their revised submittals. If the applicant has not met the requirement, they must indicate as much and provide a response.

# Development Review Team (DRT) Comment Form

Date: December 10, 2025

DRT Member: Lawrence Rivera

Dept/Div: Landscape, Irrigation, and Outdoor Lighting

Case No.: Case #: 2025-11380 - AC Marriott Hotel Development Plan Resubmittal

Case Planner: Claudia Kath Senior Planner, Office: 505-955-6661, Cell: 505-231-3399

## DRT Review Schedule – 9-12+ weeks\*

*Initial DRT Comments* are due to the case planner within three weeks of the *DRT Application Intake* meeting. Initial DRT review should confirm that the application is complete (i.e. Water Budget has been submitted) and/or identify additional submittals or corrections (i.e. Water Budget needs revision). The case planner will review and convey all *Initial DRT Comments* to the applicant via a *Review Memo*. The applicant must respond to all *Initial DRT Comments* and submit a revised application for Final Review. *Final DRT Comments* are due to the case planner within two weeks of receipt of the revised application. The case planner will review and convey all *Final DRT Comments* to the applicant in a *Final Review Memo*. The complete DRT Review Timeline can range from 9-12+ weeks, depending on the complexity and quality of the application and the total number of applications under review.

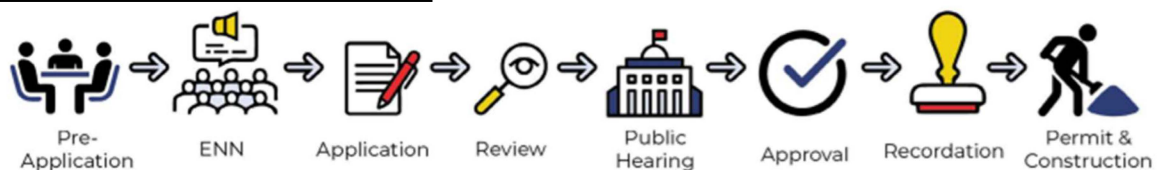


## Timing of Conditions of Approval + Technical Corrections

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## Development Review Process Flow Chart



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Form Updated: September 2023

Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response**:
1.		
2.		
3.		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant Response**:
1. Retention / detention pond conveyance system from the parking lot is very ridged. Provide landscape plantings along the conveyance swale to screen and soften the aesthetics of the swale in the landscape as seen from Cerrillos Road.	Prior to Recordation	
2.		
3.		
4.		

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The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:


1. [list any additional items]
- 2.

Explanation of Conditions or Corrections (if needed):

(see following pages for notes required)

## Development Review Team (DRT) Comment Form

Date: November 3, 2025

DRT Member: Leroy Pacheco, PE and Phil Gallegos, PE (Wilson&Company) 

Dept/Div: Public Works Department – Traffic Engineering

Case No.: Case #: 2025-11380 - AC Marriott Hotel Development Plan

Case Planner: Land Use Department - Claudia Kath, Senior Planner

### Conditions of Approval and Technical Corrections Tables

Review by this division/department has determined that this application will meet applicable standards if the following Conditions of Approval and Technical Corrections are met:

Conditions of Approval:	Must be completed by:	Applicant response:
1. None		
2.		
3.		
4.		
5.		
6.		

Technical Corrections:	Must be completed by:	Applicant Response:
1. See attached letter date 11/3/2025 from Wilson & Company	Next DRT review submittal est. 11/27/25	
2.		
3.		
4.		

The applicant should be aware that the following code provisions or other requirements will apply to future phases of development of this project:

1. Any and all future proposed improvements of this site that affect public infrastructure will be subject to **City Code Chapter 23** and additional reviews and possible corrections to meet Public Works Standards for roadways, ADA, signing, striping, lighting, etc. may be required at that time.

**City Code Chapter 23-3.18 City construction and assessment.**

*Nothing in subsections 23-3.12 and 23-3.15 SFCC 1987 prevents the city from ordering construction of curbs, gutters and sidewalks and assessing the cost thereof against the property benefited in any manner provided by the laws of the state. (Code 1953, § 24-28; Code 1973, § 30-29; SFCC 1981, § 4-13-18; Ord. #38-1987, § 8).*

# Memorandum

To: Leroy Pacheco, PE COSF

From: Philip A. Gallegos, PE, Senior Transportation Engineer

CC: Robert Luna, PE, PTOE, Traffic Engineering Practice Lead, SW Region

Date: 11/3/2025

Re: Marriott Hotel TIA Review

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Wilson and Company has reviewed the DRAFT TIA dated October 14, 2025 for the Marriott Hotel – Santa Fe Development prepared by Tierra West and have the following comments. Please let me know if you have any questions or require further information.

## GENERAL COMMENTS

- NMDOT does not approve the proposed secondary west driveway as a right-out, left-in-only access. And only supports this driveway as a right-in, right-out only configuration. As per email from Kathleen Garcia, PE, D5 Traffic Engineer dated 10/10/25. All analysis for this driveway shall be reevaluated and included in all references to driveway including in the tables and appendices.
- Appendices need to be listed in the Table of Contents
- Calculations shall be included for the proposed auxiliary lanes.
- Multimodal Review beginning on page 33 is generally thorough however a paragraph discussing the Deaf Population utilizing pedestrian facilities in the vicinity of the School for the Deaf should be included and any mitigations to improve access for this population.

## REPORT COMMENTS

- Executive Summary page iv Driveway B needs to be referenced as a Right In/Right Out and evaluated as per NMDOT direction
- Page 2 Driveway B shall be analyzed as a Right In/Right Out driveway as per NMDOT direction.
- Page 3 Driveway B shall be shown as a Right In/Right Out driveway as per NMDOT direction.
- Page 4 No. 5 Driveway B shall be analyzed as a Right In/Right Out driveway as per NMDOT direction.
- Page 5 last paragraph shall reflect the most current NMDOT design as per email from Kathleen Garcia, PE, D5 Traffic Engineer dated 10/10/25.
- Page 7 Safety Analysis the most recent 3 years were evaluated. This should be 5 years as determined by the COSF.
- Page 7 Cerrillos Rd./ST. Francis Dr. first sentence should read 117 crashes. Also since rear end collisions are the major contributing factor and paragraph discussing possible mitigation measures should be included.
- Page 8 The Crash Summary Table provided shall be included with all of the required information. The table provided is only partial. Also since rear end collisions are the major contributing factor and paragraph discussing possible mitigation measures should be included.

- Page 9 The Crash Summary Table provided shall be included with all of the required information. The table provided is only partial.
- Page 15 Need to show 2027 data first or is this a typo? Also Need to reevaluate with correct Rt. In/Rt. Out Configuration for Driveway B which isn't even shown.
- Page 16 Where are the rest of the intersections? And Driveway B needs to be evaluated as a Rt. In/Rt. Out.
- Page 17 the year 2027 needs to be shown first
- Page 18 Driveway B needs to be evaluated as a Rt. In/Rt. Out
- Table 4 2027 AM Peak Hour shows a 520-foot queue length. Why is this so high from the No Build since delay is about the same?
- Page 28 Driveway B needs to be evaluated as a Rt. In/Rt. Out.
- Table 12 Driveway B needs to be evaluated as a Rt. In/Rt. Out.
- Page 30 Driveway B needs to be evaluated as a Rt. In/Rt. Out.
- Page 30 The proposed Driveway A WB Deceleration Lane shall reflect the NMDOT design.
- Driveway B needs to be evaluated as a Rt. In/Rt. Out.
- Page 37 Potential mitigations to address rear end collisions shall be included since they are the major contributing factor.
- Page 37 Recommendations Driveway B needs to be evaluated as a Rt. In/Rt. Out.

Sincerely,

Philip A Gallegos, PE  
Senior Transportation Engineer

# RE: [EXTERNAL] [#2025080] - Request to Reconsider Extension of Westbound Left Turn Lane on Cerrillos Rd. at Cordova Rd. for Marriott Hotel Project

---

From Garcia, Kathleen, DOT <kathleen.garcia@dot.nm.gov>

To Terry Brown<tbrown@tierrawestllc.com>

CC Ron Bohannan<rrb@tierrawestllc.com>, Jacob Liberman<jliberman@tierrawestllc.com>, Jay Nelson<JNelson@tierrawestllc.com>, Vince Carrica<vcarrica@tierrawestllc.com>, Leroy Nicholas Pacheco, PE<engineer@leroypacheco.com>, Gallegos, Phil<philip.gallegos@wilsonco.com>, Martinez, Javier, NMDOT<Javier.Martinez@dot.nm.gov>

Date Friday, October 10th, 2025 at 12:34 PM

---

Good afternoon Terry,

After review, NMDOT does not approve the proposed secondary west driveway as a right-out, left-in-only access. We can only support this driveway as a right-in, right-out only configuration.

Allowing left-in movements at that location raises safety and operational concerns, especially given how close it is to the westbound left-turn lane at Cordova Road. Introducing additional left-turn movements in that area could negatively impact traffic flow and safety on Cerrillos Road.

In terms of the reconstruction project on Cerrillos Road (CN S100681), the plan to extend the westbound left-turn lane at Cordova Road to 175 feet plus transition is necessary. This design accommodates both deceleration and queuing needs and will move forward as planned.

Thank you,

Kathleen Garcia, P.E.

NMDOT District 5

Traffic Engineer

Cell: (505) 618-0635

Email: [kathleen.garcia@dot.nm.gov](mailto:kathleen.garcia@dot.nm.gov)

CAUTION: This email originated outside of our organization. Exercise caution prior to clicking on links or opening attachments.

Javier,

I am writing on behalf of the design team for the proposed Marriott Hotel project located along the south side of Cerrillos Road west of the signalized intersection of St. Francis Drive. As part of this development, we propose two unsignalized driveways:

- A full-access driveway along the south side of Cerrillos Road, approximately 850 feet west of St. Francis Drive (aligned with the existing School for the Deaf driveway).
- A secondary right-out, left-in-only driveway located approximately 425 feet west of the full-access driveway.

The need for the secondary (west) driveway was identified during the traffic data collection phase of our study. Observations revealed that eastbound queuing on Cerrillos Road at St. Francis Drive occasionally extends to the full-access driveway. While our calculated 95th percentile queue length at this intersection is approximately half that observed distance, the occasional passage of the Rail Runner train (approximately once per hour or so) is anticipated to periodically cause queues exceeding the 95th percentile during affected signal cycles. Consequently, the west driveway provides essential secondary left-turn access to the site should queuing block the east (full-access) driveway.

This driveway has been positioned as far west as feasible on the site to accommodate (1) grading constraints and (2) the construction of an adequate length of westbound left-turn lane. Please see the attached site plan (2025080-SP-SP-1 with Driveways.pdf) for details.

During yesterday's design team meeting, we reviewed the New Mexico Department of Transportation (NMDOT) plans for project CN S100681 – NM 14 (Cerrillos Road) Reconstruction. These plans indicate an intention to extend the existing westbound left-turn lane on Cerrillos Road at Cordova Road from its current length of approximately 85 feet (plus transition) to about 175 feet (plus transition).

According to the Traffic Impact Study for the Marriott Hotel site, the maximum 95th percentile queue length for the westbound left-turn movement at Cerrillos Road and Cordova Road is 65 feet, based on the horizon year (2037) build scenario volumes. The proposed extension of this left-turn lane would conflict with our west driveway location. We evaluated relocating the west driveway eastward to align with the NMDOT design but determined that site grading constraints preclude this adjustment. For reference, please see the attached NMDOT plan sheets, particularly Sheet No. 3-64.

In light of this conflict, we kindly request that NMDOT consider retaining the existing left-turn lane length at Cerrillos Road and Cordova Road. This would enable the west driveway to serve as an alternate access point for westbound left-turn traffic to the Marriott Hotel site during periods when eastbound queuing obstructs the primary east driveway. Our analyses confirm that the current lane configuration provides sufficient storage to accommodate the 95th percentile queue through the 2037 horizon year.

We appreciate your consideration of this matter and look forward to your feedback. Please let us know a convenient time for a discussion if needed.

Best Regards,

**TERRY O. BROWN, P.E.**



5571 Midway Park Pl. NE

Albuquerque, NM 87109



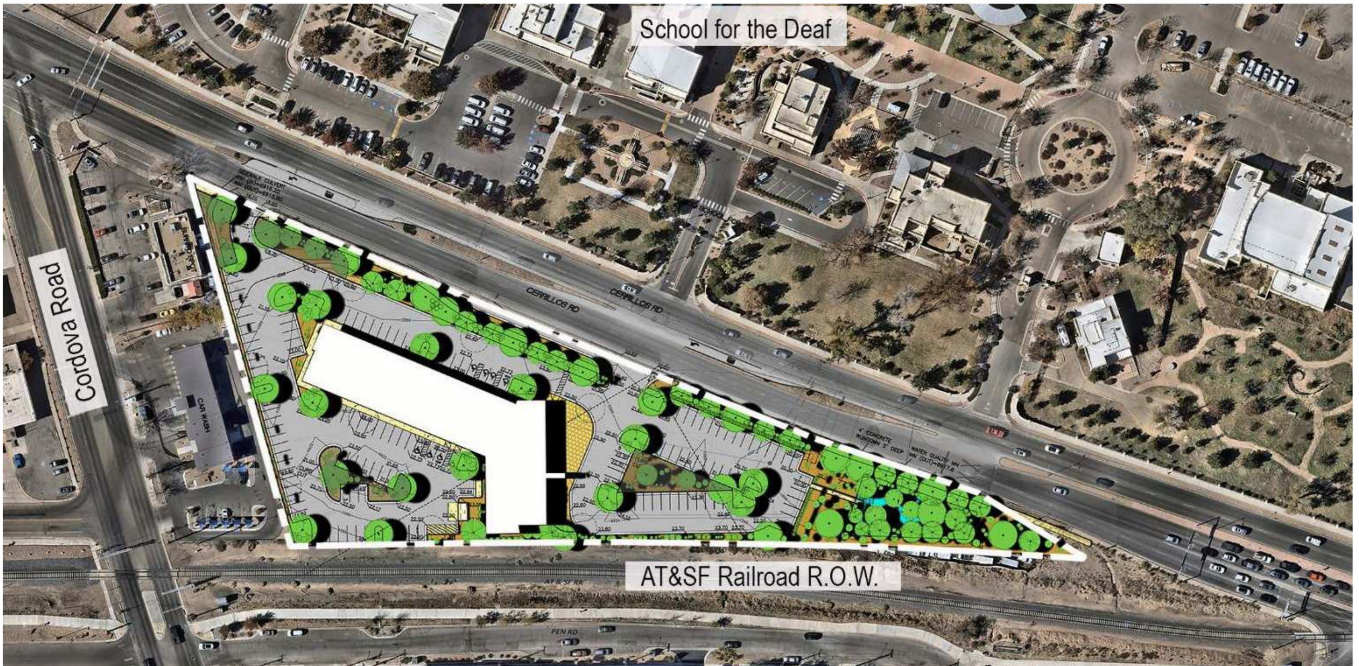


Figure 1-AC Marriott Development Plan



Figure 2 – Project Zoning Map

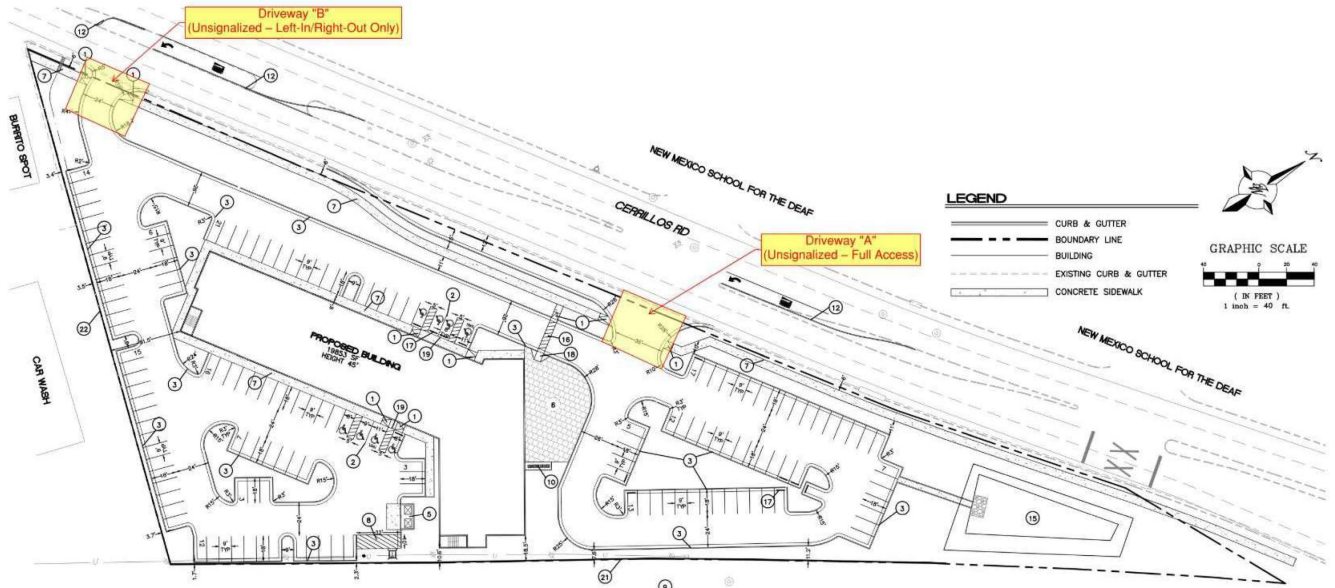


Figure 3 - Traffic Analysis Plan



Figure 4 - Terrain Management Plan



Figure 5 - Landscape Plan

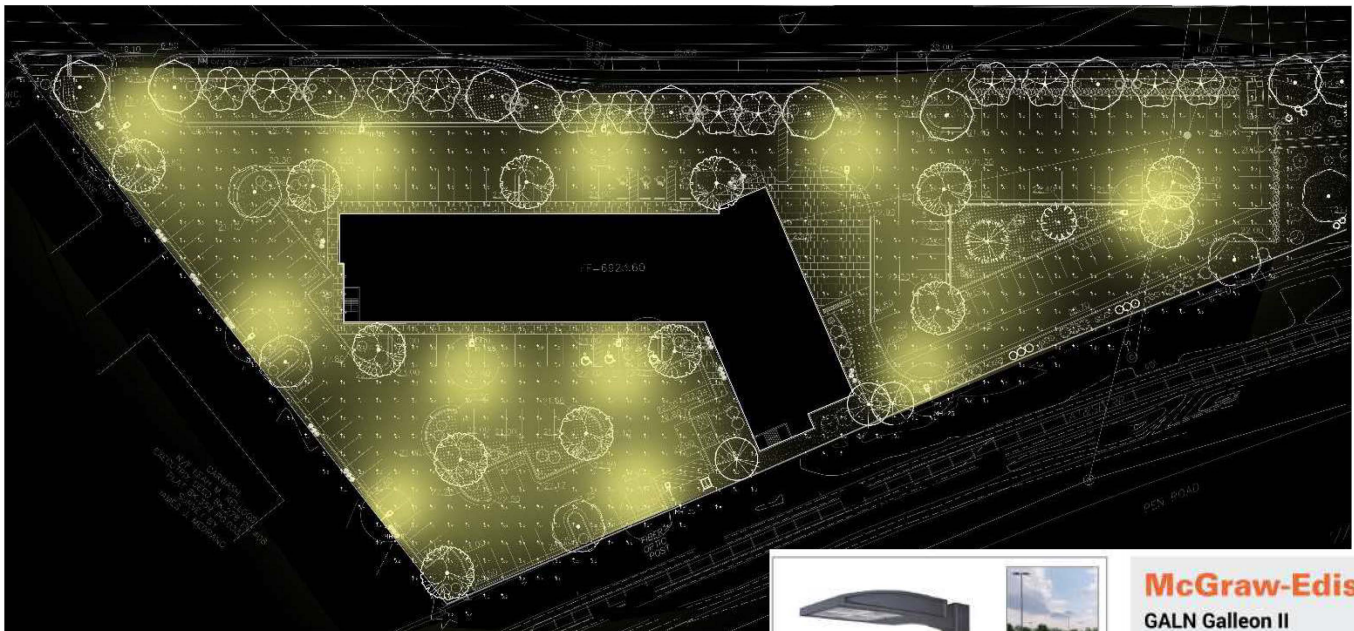


Figure 6 - Lighting

**McGraw-Edison**  
**GALN Galleon II**  
Area / Site Luminaire

**Product Features**

BABA BAA TAA PADC

Light Authority



01 NORTHWEST PERSPECTIVE VIEW

Figure 7 - Perspective View

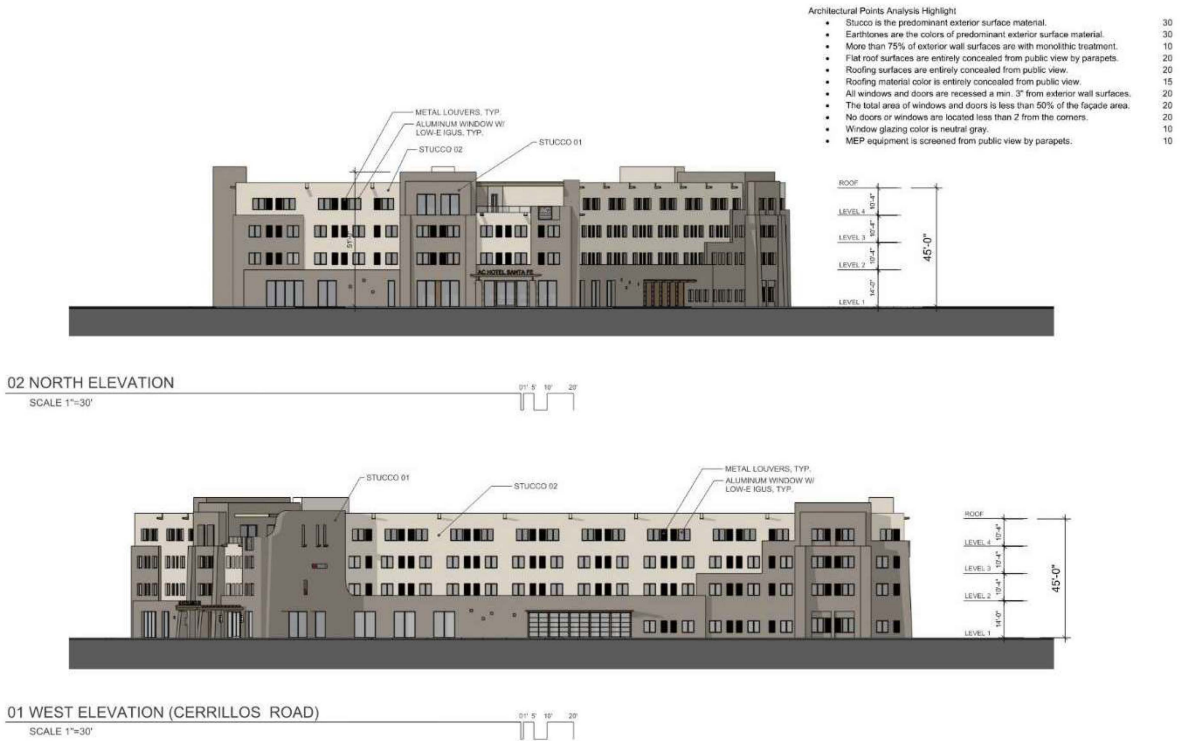


Figure 8 - Building Elevations – North and West

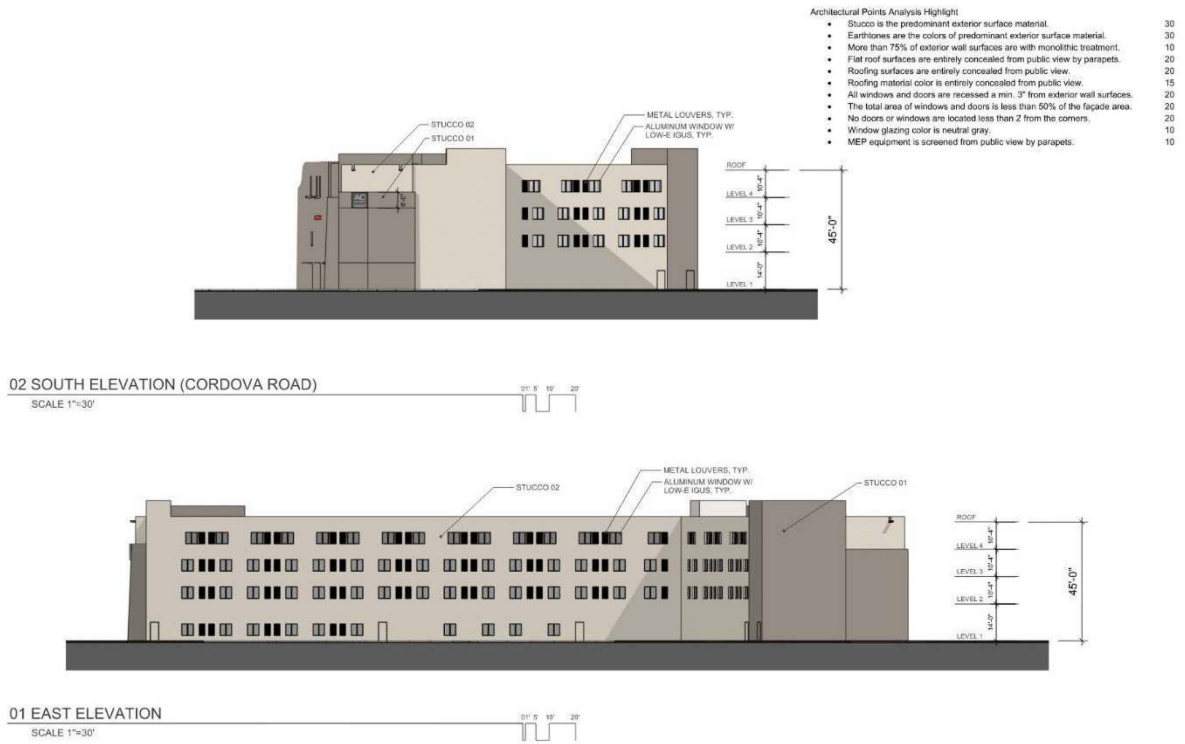


Figure 9 - Building Elevations - South and East

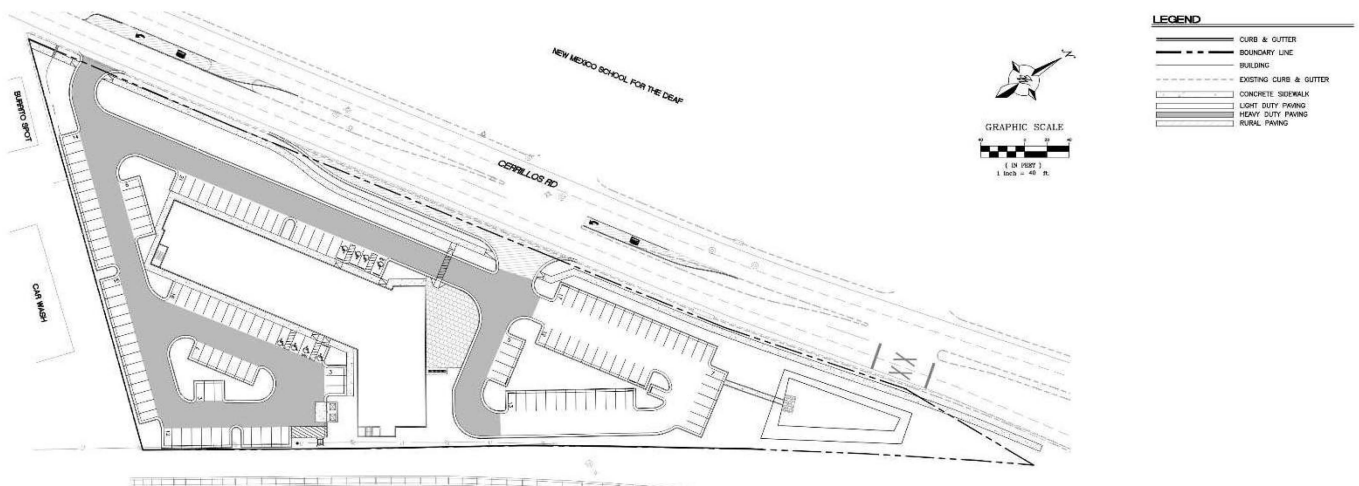


Figure 10 - Fire One Plan

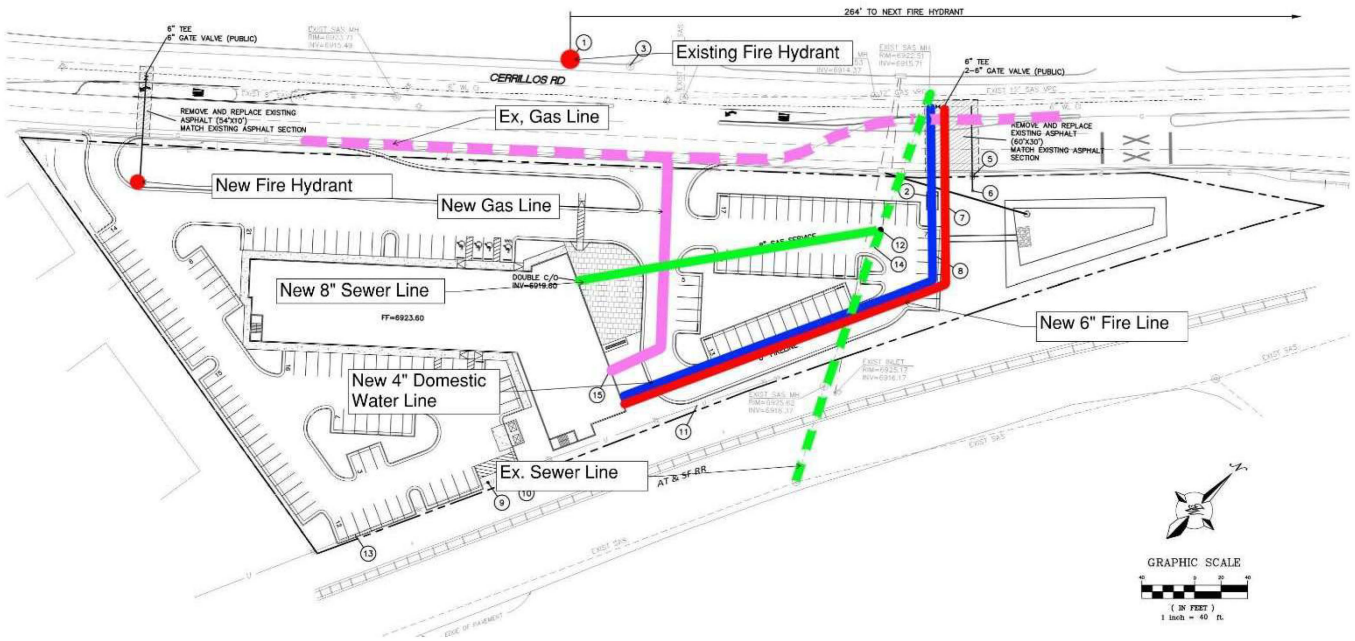


Figure 11- Utility Service Plan

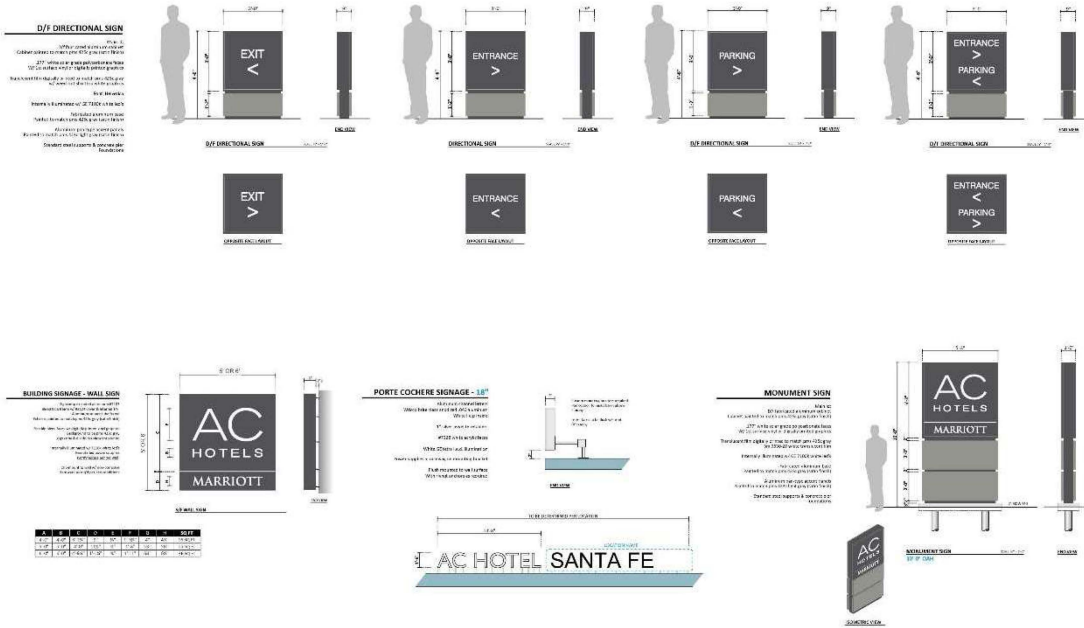


Figure 12 - Proposed Site Signage



**JENKINSGAVIN**  
LAND USE | PROJECT MANAGEMENT

**EXHIBIT E**

October 13, 2025  
Revised October 14, 2025  
Revised December 3, 2025

Claudia Kath, Senior Planner  
Current Planning Division  
City of Santa Fe  
200 Lincoln Avenue  
Santa Fe, NM 87501

**RE: Santa Fe AC Marriott Hotel  
Development Plan Application**

Dear Claudia:

This letter is respectfully submitted on behalf of Titan Development in application for Development Plan approval for a 150-room AC Marriott hotel (the “Project”), for consideration by the Planning Commission at their meeting of January 15, 2026. The 3.40-acre subject property comprises two parcels located at 1000, 1101, and 1103 Cerrillos Road, is zoned C-2 (General Commercial), and located in Zone 1 of the Cerrillos Road Highway Corridor Protection District. This request also includes a Lot Consolidation Plat Application to consolidate the existing lots into one tract.

**Project Summary**

The subject property is located southwest of the Cerrillos Road/St. Francis Dr. intersection, bordered by Cerrillos Road to the northwest, Railroad ROW/Pen Road to the southeast, and two existing commercial businesses to the south. The site is currently improved with warehouse-type commercial structures, which will be demolished at the time of development. The proposed 150-room hotel is a permissible use in the C-2 district and will comprise approximately 79,491 square feet in four stories.

The building design draws inspiration from the vernacular Santa Fe style. The layered façade treatment recalls traditional pueblo architecture, establishing a clear massing hierarchy while visually reducing the scale of the multi-story structure. Exterior materials feature an earthtone color palette, and the overall design reflects the region’s aesthetic character. Furthermore, significant landscaping improvements will significantly enhance and beautify this segment of the Cerrillos Road corridor. 205 architectural points are achieved as required in the C-2 district. Please refer to the attached Architectural Points Checklist for further information.

Compliance with the applicable dimensional standards is summarized below:

	<i>Required/Maximum</i>	<i>Provided</i>
Building Height to Roof Deck	45 feet	45 feet
Lot Coverage	60%	13.4%
Open Space	20%	25.3%
Setbacks:		
Street	15 feet	67 feet
Side	0 feet	51.5 feet
Rear	10 feet	10.6 feet

### Development Plan Approval Criteria

The Development Plan Approval Criteria in SFCC §14-3.8(D)(1) are addressed below.

*To approve a Development Plan, the Planning Commission must make the following findings:*

- (a) *that it is empowered to approve the plan under the section of Chapter 14 described in the application;*

**SFCC 14-2.3(C)(1) states, “Unless otherwise provided in Chapter 14, the planning commission shall review and approve or disapprove various specific plans, requests and subdivision plats.”**

- (b) *that approving the Development Plan will not adversely affect the public interest;*

Approving the Development Plan will not adversely affect the public interest. The site houses older commercial structures with significant outdoor storage areas. Redevelopment of the property will greatly enhance and improve this gateway to downtown. The property is centrally located within walking distance to bus service, the Rail Runner Station, and area amenities such as restaurants, the Santa Fe Railyard Park, retail services, the Santa Fe Rail Trail, and the Acequia Trail, supporting multi-modal transportation and reducing automobile dependency. Furthermore, the proposed hotel provides economic benefits through employment opportunities, tourism revenue, lodgers tax revenue, and the multiplier effect of increased visitor spending.

- (c) *that the use and any associated buildings are compatible with and adaptable to buildings, structures and uses of the abutting property and other properties in the vicinity of the premises under consideration.*

The proposed hotel is compatible with surrounding structures and uses, which include Capital Flats, a four-story multi-family housing development, the New Mexico

School for the Deaf (NMSD), and the South Capitol complex of large, multi-story state government buildings. Inspired by the Pueblo Revival architecture of the NMSD campus, the building design is adaptable to area structures. In addition, development of a quality hotel at the heart of this lively commercial and recreational area aligns with the General Plan policy supporting a diverse mix of land uses in all areas of the City, as well as with the policy encouraging infill development.

### Access & Traffic

The subject property is currently served by nine curb cuts on Cerrillos Road, which will be reduced to two access drives for the hotel. The north main driveway will be full access, and the south driveway will be limited to right-in, right-out, and left-in turning movements. A new six-foot sidewalk will be constructed along the Cerrillos frontage behind a 5-foot planter strip with street trees, greatly improving pedestrian connectivity, safety, and user experience. Access improvements include the following:

1. Construct new 6-foot sidewalk along the Cerrillos frontage
2. Construct a right-turn decel lane at main entrance
3. Lengthen existing southbound left-turn lane serving the main entrance
4. Construct a new southbound left-turn lane serving the south entrance

A revised Traffic Impact Analysis (“TIA”) was performed by Tierra West, LLC and is submitted with this application. The scope of the analysis was established through collaboration with the City Public Works Department and the New Mexico Department of Transportation (NMDOT), with peak hour traffic counts conducted at the following intersections: Cerrillos Road/St. Francis Drive; Cerrillos Road/Cordova Road, both NMSD driveways, and the site access. All intersections continue to operate at acceptable levels of service in the Build and Horizon Year conditions, per the summary chart below:

**Executive Summary Results Table**

Intersection No. / Name	Intersection Operation	Case Evaluation	Implementation Year (2027) Conditions		Horizon Year (2037) Conditions	
			AM Peak LOS - Delay (s)	PM Peak LOS - Delay (s)	AM Peak LOS - Delay (s)	PM Peak LOS - Delay (s)
1 Cerrillos Rd. / St Francis Dr.	Signalized	No Build (Exist. Geo.)	C - 28.4	D - 35.2	C - 30.8	C - 38.0
		Build (Pro. Geo.)	C - 28.6	D - 35.5	C - 31.0	D - 38.3
2 Cerrillos Rd. / Cordova Rd.	Signalized	No Build (Exist. Geo.)	B - 13.5	B - 15.7	B - 14.5	B - 16.7
		Build (Pro. Geo.)	B - 13.8	B - 16.1	B - 14.9	B - 17.1
3 Cerrillos Rd. / NM School for the Deaf Northeast Driveway	Unsignalized	No Build (Exist. Geo.)	B - 11.8	C - 15.9	B - 12.3	C - 17.0
		Build (Pro. Geo.)	B - 11.8	C - 16.1	B - 12.4	C - 17.2
4 Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway "A"	Unsignalized	No Build (Exist. Geo.)	B - 12.7	C - 16.8	B - 13.3	C - 18.7
		Build (Pro. Geo.)	C - 22.8	C - 21.8	D - 25.9	C - 24.5
5 Cerrillos Rd. / Driveway "B"	Unsignalized	No Build (Exist. Geo.)	N/A	N/A	N/A	N/A
		Build (Pro. Geo.)	B - 12.1	B - 11.7	B - 12.6	B - 12.2

NMDOT is finalizing the design for the reconstruction of Cerrillos Road between St. Francis Drive and St. Michael’s Drive. Our engineering team is coordinating closely with NMDOT staff

regarding the project access design and will continue to do so during the review process. Please refer to the Traffic Impact Analysis for further information.

### **Parking**

Per SFCC Table 14-8.6-1, one parking space is required per hotel room totaling 150 spaces and 151 spaces are provided, inclusive of eight accessible parking spaces. In accordance with SFCC Table 14-8.6-4, 6 bicycle spaces are required and 6 are provided.

### **Open Space & Landscaping**

Per SFCC §14-7.5(D)(6)(A), passive water harvesting is incorporated into the design and, therefore, 20% open space is required (0.68 acres). The Project includes 0.86 acres of open space comprising 25.3% of the site. Plantings are provided in accordance with Code requirements and include a combination of deciduous and evergreen trees, shrubs, and ornamental grasses, all served with drip irrigation. Street trees will be planted along Cerrillos Road in a 5-foot planter strip between the sidewalk and the roadway. Furthermore, in compliance with the provisions of the Cerrillos Road Highway Corridor Protection District, a 10-foot landscaped area is provided behind the sidewalk. Please refer to the attached Landscape Plans for further details.

### **Terrain Management**

The site slopes gently from east to west and there are no drainageways or 30% slopes present. The property is improved and the current impervious area is approximately 85,000 square feet. With the redevelopment, impervious area will increase to 99,316 square feet. However, under current conditions, 8.86 CFS of stormwater runoff exits the site, which will be reduced by approximately 20% to 7.0 CFS with the new development. A stormwater retention pond will be constructed at the north end of the site, which will overflow to an existing storm drain in Cerrillos Road. Furthermore, stormwater flowing to the southwest corner of the site will overflow to Cerrillos Road via a sidewalk culvert. Stormwater will be passively harvested throughout the landscaping areas per Code requirements. Please refer to the Terrain Management Plans for further details.

### **Water & Sewer**

The Project will be served by a new 4-inch service line that will connect to the existing 6-inch water main in Cerrillos Road. A 4-inch meter and vault are provided on-site. In addition, a 6-inch fire line will connect to the same water main and extend to the building through a backflow preventer and hot box.

An Alternative Water Budget is proposed in accordance with SFCC 14-8.13(B)(2)(b), which is based on water use data for El Rey Hotel and Springhill Suites. In addition, per SFCC 14-8.13(B)(3), since the hotel is replacing existing structures, the water budget will be reduced by an amount equal to the average annual consumption in the twenty-four months that the highest water use was active during the preceding ten years. An IPRA request has been submitted to the

City of Santa Fe requesting the last ten years of data for the existing uses and the water budget will be amended once the information is provided.

An existing 10-inch sewer main crosses the north side of the site from east to west. A new 8-inch sewer service line will connect to this main at a new manhole. Please refer to the attached Utility Plans for more information.

### **Fire Protection and Emergency Access**

In accordance with IFC requirements, two 26-foot aerial apparatus access drives are provided on the west and north sides of the building with the requisite 28-foot turning radii. The building will be equipped with automatic fire suppression in accordance with IFC requirements. Lastly, there are two existing fire hydrants on the west side of Cerrillos Road across from the site.

### **Archaeology**

The Project is located within the Suburban Archaeological District. As the Project is less than 10 acres in size, archaeological clearance is not required.

### **Early Neighborhood Notification**

An Early Neighborhood Notification Meeting was held on September 30, 2025. Discussion included the following topics: access/traffic, building design/height, and the need for a new hotel. Please refer to the ENN Meeting Notes for further information.

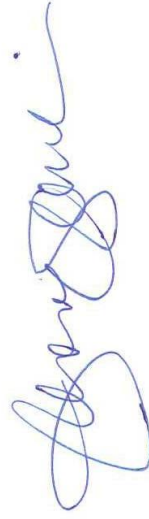
In support of this request, the following documentation is submitted herewith for your reference:

1. Development Plan Application
2. Lot Consolidation Plat Application
3. Letters of Owner and Applicant Authorization
4. Warranty Deeds
5. Architectural Points Checklist & Color Building Elevations
6. Utility Service Application
7. Preliminary Water Budget
8. Traffic Impact Analysis
9. ENN Meeting Notes
10. Development Plans

The Application Fees are calculated as follows: Development Plan (Construction Valuation \$22.5 million) = \$20,000.00; Lot Consolidation Plat = \$200.00; **Total = \$20,200.00.**

Please contact me should you have any questions or require additional information. Thank you.

Sincerely,



Jennifer Jenkins, Principal

December 1, 2025

**City of Santa Fe**  
Planning Division  
200 Lincoln Ave., 1<sup>st</sup> Floor  
Santa Fe, NM 87501

Re: **AC by Marriott Hotel Development Plan Architectural Points Analysis**

Dear Planning Division Staff,

Please see brief narratives below regarding the Architectural Points Analysis.

### **Walls**

**Predominant Exterior Surface Material:** With the exception of windows, doors, and accents, the exterior finish for all exterior walls will be stucco (30 points).

**Color of Predominant Material:** The proposed stucco colors are a lighter earthtone color and a second earthtone color with the same general hue but a darker shade. The included exterior elevations and perspective rendering show the earthtone colors that we are proposing (30 points).

**Exterior Surface Treatment:** Our design intent is that the two earthtone stucco colors be considered one color since they are of the same general hue (10 points).

### **Roofs**

**Form:** The building's parapets will conceal the flat roof entirely from public view (20 points).

**Predominant Material:** All roof surfaces are concealed entirely from public view (20 points).

**Predominant Color:** All roof surfaces are concealed entirely from public view (15 points).

### **Building Form**

**Massing:** This multi-story building will have more than 20,000 square feet of gross floor area, and it is designed with wall plane projections and/or setbacks on each publicly visible façade. However, while some of the building design could be interpreted to meet this section's criteria, each publicly visible façade's wall plane projections and setbacks do not meet the criteria of having a depth of at least three percent of the length of the façade and simultaneously extending at least 20 percent of the length of the façade (0 points).

### **Doors & Windows**

**Treatment:** More than 50 percent of doors, windows and glazed surfaces will have frames recessed a minimum of two inches (20 points).

**Area:** All wall surfaces (those which are not located under canopies having a horizontal depth of at least six feet) will have less than 50 percent openings consisting of doors, windows, glazing and other penetrations (20 points).

**Location:** All doors, windows and glazed surfaces will be located at least two feet from outside building corners (20 points).

**Glazing:** All glazing will be clear or tinted neutral gray (10 points).

**Equipment**

**Screening:** All roof and wall mounted mechanical, electrical, communications, and service equipment (including satellite dishes and vent pipes) will be screened from public view by parapets, walls, fences, dense evergreen foliage, or by other means (10 points).

Please let us know if you have any questions about the narratives above. Thank you!

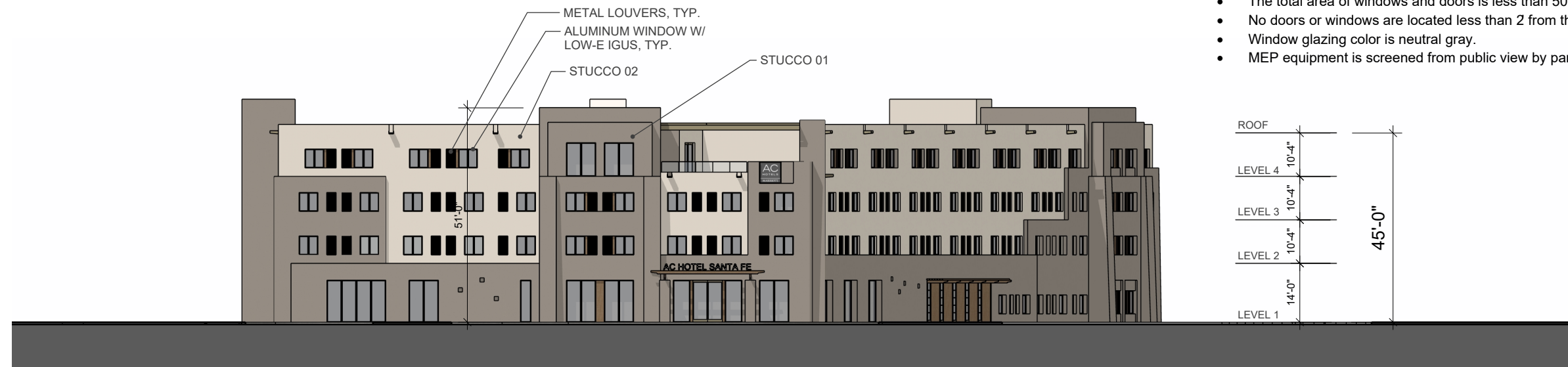
Very Sincerely and Respectfully,



Christine Robbins-Elrod, AIA, LEEP AP BD+C/ND/Homes, WELL AP  
Principal  
ChristineRElrod@5gstudio.com

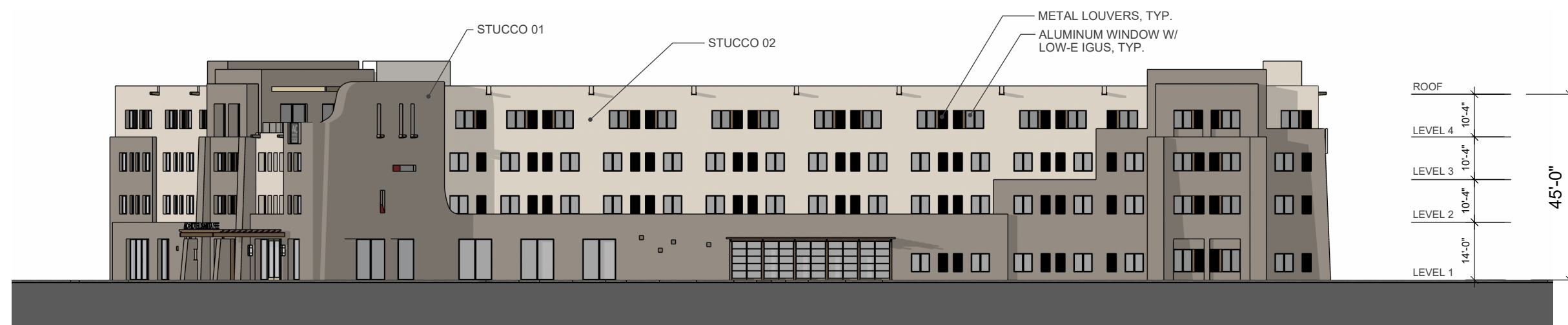
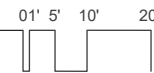
Architectural Points Analysis Highlight

- Stucco is the predominant exterior surface material. 30
- Earthtones are the colors of predominant exterior surface material. 30
- More than 75% of exterior wall surfaces are with monolithic treatment. 10
- Flat roof surfaces are entirely concealed from public view by parapets. 20
- Roofing surfaces are entirely concealed from public view. 20
- Roofing material color is entirely concealed from public view. 15
- All windows and doors are recessed a min. 3" from exterior wall surfaces. 20
- The total area of windows and doors is less than 50% of the façade area. 20
- No doors or windows are located less than 2' from the corners. 20
- Window glazing color is neutral gray. 10
- MEP equipment is screened from public view by parapets. 10



02 NORTH ELEVATION

SCALE 1"=30'



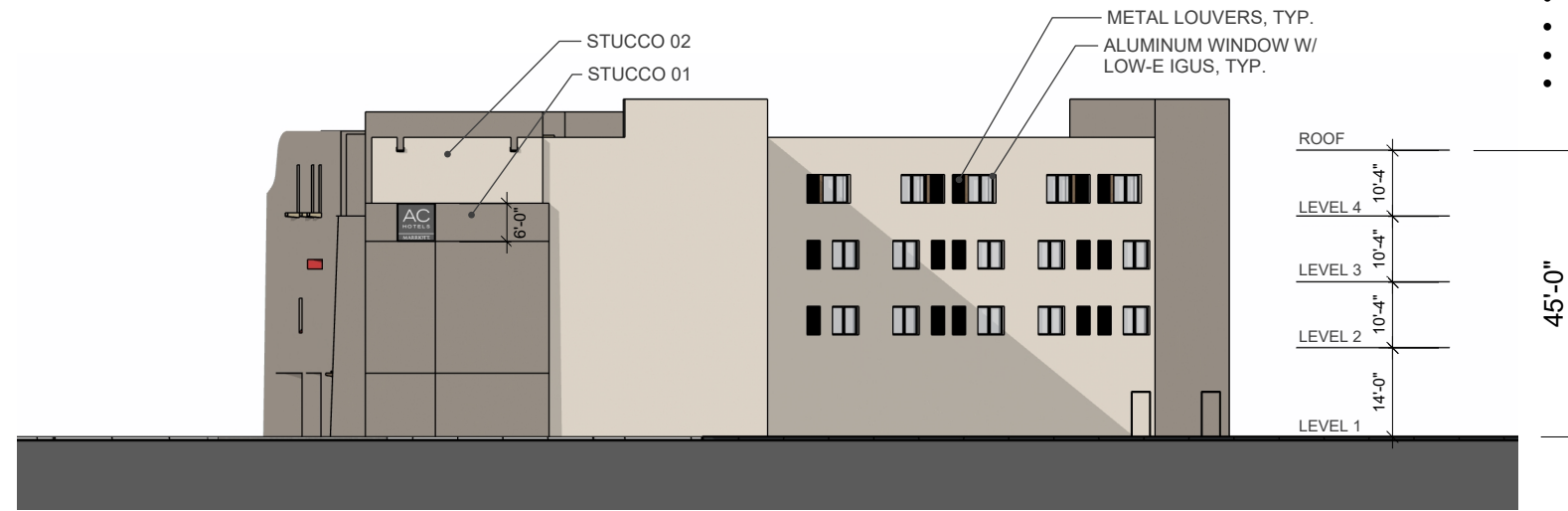
01 WEST ELEVATION (CERRILLOS ROAD)

SCALE 1"=30'



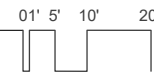
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- Roofing surfaces are entirely concealed from public view. 20
- Roofing material color is entirely concealed from public view. 15
- All windows and doors are recessed a min. 3" from exterior wall surfaces. 20
- The total area of windows and doors is less than 50% of the façade area. 20
- No doors or windows are located less than 2' from the corners. 20
- Window glazing color is neutral gray. 10
- MEP equipment is screened from public view by parapets. 10



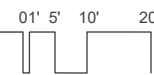
02 SOUTH ELEVATION (CORDOVA ROAD)

SCALE 1"=30'



01 EAST ELEVATION

SCALE 1"=30'





01 NORTHWEST PERSPECTIVE VIEW

**ARCHITECTURAL POINTS ANALYSIS / Santa Fe AC Marriott**

**DATE**

Architectural Design Standard	Points	Actual
<b>WALLS</b>		
Predominant Exterior Surface Material		
Stucco, Adobe	30	<b>30</b>
Brick, natural stone, and integral color masonry	25; 30*	
Concrete and painted CMU	20; 30*	
Metal siding, glass curtain wall, wood siding, simulated materials	10; 30*	
Mirrored glass curtainwall systems	-10	
Color of Predominant Material		
Earthtones, creams, and pastels of earthtone hues	30	<b>30</b>
Pastel colors of non-earthtone hues, whites, grays, grayish greens	15; 30*	
High intensity colors	0; 20*	
metallic colors, glass, black	-10	
Exterior Surface Treatment		
(A) Wall surfaces appear monolithic with at least 75 percent of the total wall area one material and one color. Differing shades of the same general hue shall not be considered different colors. Non-solar fenestration, window and door awnings, applied trim, and accent materials, colors, and decorative bands, with the exception of stucco, masonry or concrete control joints, are used in such a way that they do not give a panelized or prefabricated appearance, produce striped or checkerboard patterns, or exceed 25 percent of the surface area of any façade. Fenestration and/or accent colors on wall surfaces under portals or canopies having a horizontal depth of at least six feet shall be exempt from area calculations	10	<b>10</b>
(B) Wall surfaces do not meet the criteria set forth in paragraph (A) above	-10	
<b>ROOFS</b>		
Form		
(A) Flat roof surfaces entirely concealed from public view by parapets	20	<b>20</b>
(B) Flat roof surfaces not entirely concealed from public view by parapets, uniformly sloping roofs, or any combination of flat and uniformly sloping roofs, having a height, from springline to peak, that does not exceed the average height of the supporting walls and having a slope with greater than or equal to four feet of vertical rise for every 12 feet of horizontal run and less than or equal to 12 feet of vertical rise for every 12 feet of horizontal run	15	
(C) Uniformly sloping roofs or any combination of flat and uniformly sloping roofs, having a height, from springline to peak, that does not exceed the average height of the supporting walls and having a slope with less than four feet of vertical rise for every 12 feet of horizontal run or having a slope with greater than 12 feet of vertical rise for every 12 feet of horizontal run	10	
(D) Any type of sloping roof having a height, from springline to peak, that exceeds the average height of the supporting walls; non-uniformly sloping roofs; or any combination of flat and non-uniformly sloping roofs	-10	
Predominant Material		
(A) All surfaces are concealed from public view	20	<b>20</b>
(B) Standing, flat, or batten seam metal roofing, or membrane, asphalt or gravel surfaces exposed to public view	15	
(C) Flat tiles of clay, concrete or slate	10	
(D) Barrel tiles of clay, concrete, or slate; and asphalt shingles	5	
(E) Wood shingles or shakes and other materials including but not necessarily limited to plastic, fiberglass or metal roof tiles	-10	
Predominant Color		
(A) All surfaces are concealed from public view	15	<b>15</b>
(B) Dark reds, browns, and earthtones, and natural metals including aluminum, zinc, tin, and lead	10	
(C) Low-intensity colors other than those stated above	5	
(D) White	0	
(E) Bright, non-fading, high-intensity colors and any use of multiple colors	-15	

Architectural Design Standard	Points	Actual
<b>BUILDING FORM</b>		
Massing		
(A) One-story buildings with over 10,000 square feet of gross floor area and multi-story buildings with over 20,000 square feet of gross floor area which are designed with wall plane projections or setbacks on each publicly visible façade having a depth of at least three percent of the length of the façade and extending at least 20 percent of the length of the façade	30	<b>0</b>
(B) One-story buildings with less than or equal to 10,000 square feet of gross floor area and multi-story buildings with less than or equal to 20,000 square feet of gross floor area which are designed with either offsetting wall planes or upper story setbacks of at least four horizontal feet, or a recessed entry space or projecting canopy or portal having a depth of at least six horizontal feet, on at least one publicly visible façade	30	
(C) Buildings not utilizing the massing techniques described in paragraphs (A) or (B) above	0	
<b>DOORS &amp; WINDOWS</b>		
Treatment		
(A) More than 50 percent of doors, windows and glazed surfaces, which are not located under portales or canopies having a horizontal depth of at least six feet, have either frames recessed a minimum of two inches, are cased with trim, have divided lites, or have exposed or otherwise articulated lintels	20	<b>20</b>
(B) More than 50 percent of doors, windows and glazed surfaces do not meet the requirements set forth in paragraph (A) above	0	
Area		
(A) All wall surfaces which are not located under portales or canopies having a horizontal depth of at least six feet, and which do not include solar fenestration, have less than or equal to 50 percent openings consisting of doors, windows, glazing and other penetrations	20	<b>20</b>
(B) Wall surfaces do not meet the requirements as set forth in paragraph (A) above	0	
Location		
(A) All doors, windows and glazed surfaces, on structures having a gross floor area greater than 150 square feet, are located at least two feet from outside building corners	20	<b>20</b>
(B) All doors, windows and glazed surfaces, on structures having a gross floor area less than or equal to 150 square feet, have at least a two inch mullion at inside and outside building corners	20	
Glazing		
(A) All glazing is clear or tinted neutral gray	10	<b>10</b>
(B) Any use of colored glazing	0	
(C) Any use of mirrored glazing	-10	
<b>EQUIPMENT</b>		
Screening		
(A) All roof and wall mounted mechanical, electrical, communications, and service equipment, including satellite dishes and vent pipes, are screened from public view by parapets, walls, fences, dense evergreen foliage, or by other means	10	<b>10</b>
(B) All building mounted equipment set forth in paragraph (A) above is either screened; and/or painted to match visually adjacent surfaces	5	
(C) All building mounted equipment set forth in paragraph (A) above is not screened and/or painted to match visually adjacent surfaces	-10	
<b>TOTAL</b>		
REQ'D (R-1 Zoning)		<b>205</b>
PROVIDED		<b>205</b>

City of Santa Fe, New Mexico  
**UTILITY SERVICE APPLICATION**

**\*Fill in all highlighted fields on this application. Applicant must sign and date application.**

- Check one only:**
- Sewer Service Technical Evaluation Request
  - Water Service Technical Evaluation Request
  - Agreement for Metered Service (AMS)
  - Agreement to Construct and Dedicate Public Improvements (ACD)
  - Annexation Application Water Budget
  - Water Offset Program/Water Rights Compliance Evaluation Request

**WORK ORDER #** \_\_\_\_\_

**Applicant Name:** Marriott

**Project Address:** 1000 Cerrillos Road, Santa Fe, NM 87505

**\*Required - Attach a Plat of the Property (legal lot of record and proposed development)**

**Plat Filing Information:** Year 2000 Book 435 Page 37 Township, Range, Section: S26 / T17N / R9E

**Location:** (check one only) Inside Corporate City Limits  Outside Corporate City Limits

**Property Uniform Property Code:** 1-053-098-249-307 **Existing Well:** Yes  No

**Legal Description including lot size:** Amended Plats Lot 8 and 9 of the Boulevard Addition prepared by Turley November 1, 1948/ROW for Paving by Davalos March 1, 1954.

**Short Description of Project:** 150-room hotel

**Construction Start Date:** Spring 2026

**\*RESIDENTIAL PROJECT - Complete the following**

1. Type of project: (i.e. Single Family Residence, Subdivision, Lot split, Apartments) \_\_\_\_\_
2. Total number of lots approved on final plat/development plan: \_\_\_\_\_
3. Total number of homes existing or under construction: \_\_\_\_\_
4. Size of service requested: (5/8", 3/4", 1" or 2") \_\_\_\_\_

**\*Please fill in all categories below that apply for which water service is requested:**

**--- COMPLETED BY APPLICANT ---**

- Number of Lots or Units**
- Single Family Dwelling Unit, lot size less than 6,000 sq. ft.
  - Single Family Dwelling Unit, lot size 6,000-10,890 sq. ft.
  - Single Family Dwelling Unit, lot size greater than 10,890 sq. ft.
  - Mobile Home (in Mobile home park)
  - Accessory Dwelling Unit
  - Apartment/Condominium
  - Senior Complex

**Total**

**--- COMPLETED BY STAFF ---**

Water Use Factors	Annual Water Demand
.15 afy per d.u.	_____
.17 afy per d.u.	_____
.25 afy per d.u.	_____
.17 afy per d.u.	_____
.09 afy per d.u.	_____
.16 afy per d.u.	_____
.12 afy per d.u.	_____

**Total Residential Water Demand \_\_\_\_\_ AFY**

City of Santa Fe, New Mexico  
**UTILITY SERVICE APPLICATION**

**\*COMMERCIAL PROJECT - Complete the following**

Type of Project: (i.e. Office, Retail, Mixed, etc.) Commercial - Lodging

Total gross floor area of building: 80,000 square feet

Total area of lot, tract or parcel: 3.40 acres

Automatic Fire Sprinkler System:  Yes  No

Building Construction Type: \_\_\_\_\_

Building Square Footage: 80,000

Site Plan Attached:  Yes  No

\*Please check all use categories below that are planned for the building and the gross floor areas of each use within the proposed building.

**---- COMPLETED BY APPLICANT ----**

<u>Check Type of Use</u>	<u>Gross Floor Area</u>
<b><u>Commercial</u></b>	

<input type="checkbox"/> Office – Non-medical	_____
<input type="checkbox"/> Medical Office	_____
<input type="checkbox"/> Office – City/State	_____
<input type="checkbox"/> Research and Development Lab	_____
<input type="checkbox"/> Manufacturing – Goods	_____
<input type="checkbox"/> Manufacturing – Consumables	_____
<input type="checkbox"/> Laundromat, Commercial	_____
<input type="checkbox"/> Laundromat, Other	_____
<input type="checkbox"/> Drycleaner	_____
<input type="checkbox"/> Plant Nursery	_____
<input type="checkbox"/> Gyms with showers	_____
<input type="checkbox"/> Gyms without showers	_____
<input type="checkbox"/> Salons	_____
<input type="checkbox"/> Pet Grooming	_____
<input type="checkbox"/> Pet Daycare	_____
<input type="checkbox"/> Retail, Large (Individual stores or shopping areas > 75,000 sq ft)	_____
<input type="checkbox"/> Neighborhood Center/Medium Retail (Individual stores or shopping areas 75,000-25,000 sq ft)	_____
<input type="checkbox"/> Retail, Small (Individual stores or shopping areas < 25,000 sq ft)	_____
<input type="checkbox"/> Gallery	_____
<input type="checkbox"/> Grocery Store	_____
<input type="checkbox"/> Restaurant (full service)	_____
<input type="checkbox"/> Restaurant (limited service)	_____
<input type="checkbox"/> Gasoline Station w/ Car Wash	_____

**---- COMPLETED BY STAFF ----**

<u>Water Use Factors</u>	<u>Annual Water Demand</u>
--------------------------	----------------------------

(0.70 afy per 10,000 s.f.)	_____
(0.72 afy per 10,000 s.f.)	_____
(0.58 afy per 10,000 s.f.)	_____
(1.18 afy per 10,000 s.f.)	_____
(0.21 afy per site)	_____
(2.33 afy per site)	_____
(0.78 afy per machine)	_____
(0.22 afy per machine)	_____
(0.41 afy per site)	_____
(0.56 afy per 10,000 s.f.)	_____
(8.94 afy per site)	_____
(0.77 afy per site)	_____
(0.21 afy per site)	_____
(0.52 afy per site)	_____
(0.11 afy per site)	_____
(0.45 afy per 10,000 s.f.)	_____
(0.43 afy per 10,000 s.f.)	_____
(0.06 afy per site)	_____
(0.60 afy per site)	_____
(1.27 afy per 10,000 s.f.)	_____
(0.02 afy per seat)	_____
(1.63 afy per Site)	_____
(6.56 afy per Site)	_____

\_\_\_\_\_ Gasoline Station \_\_\_\_\_ (0.88 afy per Site) \_\_\_\_\_

### UTILITY SERVICE APPLICATION

**\*\*\*See Attached Preliminary Water Budget\*\*\***

_____ Car Wash (full service)	_____	(5.66 afy per Site)	_____
_____ Car Wash (limited service)	_____	(0.94 afy per Wash Bay)	_____
_____ Auto Repair	_____	(0.12 afy per site)	_____
_____ Car Rental	_____	(0.12 afy per site)	_____
_____ Car Sales	_____	(0.07 afy per 10,000 s.f.)	_____
_____ Self Storage	_____	(0.13 afy per site)	_____
_____ Wholesale, Warehousing	_____	(0.4 afy per 10,000 s.f.)	_____
_____ Industrial, Manufacturing	_____	(applicant estimate of water use)	_____
_____ Church w/ day care or school)	_____	(1.3 afy per Site)	_____
_____ Church w/o day care or school)	_____	(0.6 afy per Site)	_____
<input checked="" type="checkbox"/> Hotel	No. of rooms <u>150</u>	(.13 afy per room)	_____
_____ Motel	No. of rooms _____	(.09 afy per room)	_____

#### **Public Services**

_____ School, Elementary		(0.53 afy per 100 students)	_____
_____ School, Middle or Junior High		(1.68 afy per 100 students)	_____
_____ School, Senior High		(2.64 afy per 100 students)	_____
_____ Schools, Daycare		(0.85 afy per 100 kids)	_____
_____ Places of Worship		(0.15 afy per site)	_____
_____ With Daycare and school		(0.95 afy per site)	_____
_____ Parks		(1.48 afy per acre)	_____
_____ <b>Other</b> (not listed above)	Please attach	(with attachment)	_____

water demand calculations and assumptions used

**Total Floor Area** 80,000

**Total Commercial Water Demand** \_\_\_\_\_ AFY

**Total Residential Water Demand** \_\_\_\_\_ AFY

**TOTAL PROJECT WATER DEMAND** \_\_\_\_\_ AFY

# UTILITY SERVICE APPLICATION

<p>_____</p> <p>_____</p> <p>_____</p> <p>_____</p> <p>_____</p>	<p>_____</p> <p>AGENT: _____</p> <p>Title: _____</p> <p>Mailing Address: _____</p> <p>_____</p> <p>Phone Number: _____</p> <p>Mobile Number: _____</p>
--	--

Information Provided By: Check one: Owner \_\_\_\_\_ Agent

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

Technical Evaluation to be Sent to: Check one: Owner \_\_\_\_\_ Agent \_\_\_\_\_

COMMENTS: \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

## **APPLICANTS, PLEASE NOTE:**

- Ordinance 2008-53, prohibits new connections outside the presumptive city limits including the Agua Fria traditional historic community (AFTHC) unless specific conditions are met. Applications for service outside the presumptive city limits and AFTHC must include documentation showing these conditions are met or the application will be rejected. The documents required are shown below.
- A map of the proposed project in relation to the existing city limits and the presumptive city limits
- A detailed description of the proposed development including the type and size of proposed land uses
- The health, safety and welfare or other legal reason for the connection
- A site water budget
- Documentation from the County of Santa Fe that county water service is not available
- Documentation from the wastewater division regarding sewer availability
- A certified Santa Fe Homes Proposal as set forth in Section 14-8.11 SFCC 1987 if applicable

**Santa Fe AC Marriott Hotel**  
**Preliminary Alternative Water Budget per SFCC 14-8.13(B)(2)(b)**  
**October 13, 2025**

150 Hotel Rooms

	<b>GPY</b>	<b>AFY</b>
Domestic Water Use Per Room*	24,598	0.08

*\*Based on attached water use data from other hotel properties.*

<i>Subtotal Domestic Use for 150 Rooms</i>	<i>3,689,731</i>	<i>11.32</i>
--	------------------	--------------

Landscape Irrigation per Landscape Plans

YEAR 1	126,147	0.39
YEAR 2	126,147	0.39
YEAR 3	110,989	0.34
<b>3-YEAR AVERAGE</b>	<b>121,094</b>	<b>0.37</b>

<i>WATER BUDGET SUBTOTAL</i>	<i>3,810,825</i>	<i>11.69</i>
<b>Credit per SFCC 14-8.13(B)(3)**</b>	<b>TBD</b>	<b>TBD</b>
<b>DEVELOPMENT WATER BUDGET</b>	<b>3,810,825</b>	<b>11.69</b>
9.8% Contingency per SFCC §14-8.13(E)(1)	373,461	1.15
<b>TOTAL WATER DEMAND OFFSET</b>	<b>4,184,286</b>	<b>12.84</b>

*\*\*An IPRA request has been submitted to the City for the 10 year of water use history.*

El Rey Hotel Water Use Data (86 Rooms)											
2021-22			2022-23			2023-24			2024-25		
Read Date	Meter #	Gallons	Read Date	Meter #	Gallons	Read Date	Meter #	Gallons	Read Date	Meter #	Gallons
10/13/2021	4	120,300	10/9/2022	4	124,000	10/8/2023	4	121,600	10/8/2024	4	136,900
	5	79,800		5	84,800		5	75,200		5	65,100
11/12/2021	4	100,600	11/6/2022	4	98,600	11/7/2023	4	120,900	11/9/2024	4	118,300
	5	66,800		5	82,000		5	90,600		5	71,900
12/13/2021	4	89,000	12/4/2022	4	86,100	12/9/2023	4	91,700	12/10/2024	4	84,600
	5	56,600		5	77,600		5	81,000		5	47,500
1/13/2022	4	99,800	1/8/2023	4	109,900	1/8/2024	4	116,400	1/11/2025	4	92,300
	5	69,900		5	84,600		5	101,800		5	60,800
2/13/2022	4	67,600	2/5/2023	4	67,200	2/7/2024	4	75,000	2/10/2025	4	67,600
	5	38,700		5	78,400		5	83,900		5	41,000
3/16/2022	4	105,200	3/12/2023	4	96,400	3/10/2024	4	124,800	3/11/2025	4	89,900
	5	61,800		5	165,900		5	112,800		5	70,100
4/14/2022	4	106,600	4/9/2023	4	96,300	4/9/2024	4	152,100	4/9/2025	4	106,300
	5	56,200		5	130,000		5	120,500		5	81,400
5/15/2022	4	180,600	5/7/2023	4	96,400	5/8/2024	4	166,800	5/9/2025	4	113,000
	5	73,600		5	112,500		5	108,400		5	70,700
6/11/2022	4	144,000	6/4/2023	4	116,800	6/9/2024	4	174,200	6/8/2025	4	139,000
	5	71,100		5	98,500		5	130,600		5	78,700
7/10/2022	4	134,100	7/9/2023	4	205,700	7/9/2024	4	127,700	7/9/2025	4	159,600
	5	86,500		5	117,500		5	101,400		5	94,400
8/7/2022	4	161,700	8/6/2023	4	173,400	8/10/2024	4	145,900	8/9/2025	4	162,700
	5	71,500		5	98,200		5	104,200		5	103,900
9/11/2022	4	150,800	9/10/2023	4	181,100	9/8/2024	4	122,600	9/8/2025	4	134,400
	5	107,200		5	113,400		5	85,500		5	80,200
GPY		2,300,000	GPY		2,695,300	GPY		2,735,600	GPY		2,270,300
AFY		7.06	AFY		8.27	AFY		8.40	AFY		6.97
AFY/ROOM		0.08	AFY/ROOM		0.10	AFY/ROOM		0.10	AFY/ROOM		0.08

AVERAGE AFY	7.67
AVERAGE AFY/ROOM	0.09

Spring Hill Suites Water Use Data (138 Rooms)	
Month	Gallons
Jan-24	266,288
Feb-24	201,960
Mar-24	184,008
Apr-24	219,912
May-24	213,931
Jun-24	208,360
Jul-24	240,108
Aug-24	261,052
Sep-24	297,704
Oct-24	214,676
Nov-24	251,328
Dec-24	217,668
GPY	2,776,995
AFY	8.52
AFY/ROOM	0.06

OVERALL AVERAGE AFY/ROOM	0.08
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November 25, 2025

Leroy Pacheco, P.E.  
City of Santa Fe

**RE: Case #: 2025-11380 - AC Marriott Hotel  
1000 Cerrillos Rd., Santa Fe  
Level 2 Traffic Impact Analysis  
Response to Comments**

Dear Leroy Pacheco,

Per the correspondence dated November 3rd, 2025, please find the following responses addressing the comments listed below:

#### **General Comments**

- 1) **CoSF Comment:** NMDOT does not approve the proposed secondary west driveway as a right-out, left-in-only access. And only supports this driveway as a right-in, right-out only configuration. As per email from Kathleen Garcia, PE, D5 Traffic Engineer dated 10/10/25. All analysis for this driveway shall be reevaluated and included in all references to driveway including in the tables and appendices.

***TW Response:*** *Driveway B is now being evaluated as a Right- In only driveway.*

- 2) **CoSF Comment:** Appendices need to be listed in the Table of Contents

***TW Response:*** *This has been completed. See page v of the report.*

- 3) **CoSF Comment:** Calculations shall be included for the proposed auxiliary lanes.

***TW Response:*** *Right and Left deceleration lane warrants are graphical and are included on the report. Storage lengths are based on the 95<sup>th</sup> percentile queues, which are less than a vehicle and are displayed in the report.*

- 4) **CoSF Comment:** Multimodal Review beginning on page 33 is generally thorough however a paragraph discussing the Deaf Population utilizing pedestrian facilities in the vicinity of the School for the Deaf should be included and any mitigations to improve access for this population.

***TW Response:*** *This has been completed. The study area provides sufficient infrastructure, aside from the recommended improvements, for pedestrians to travel to and from the nearby school. See narrative on page 38 of the report.*

### **Report Comments**

- 1) **CoSF Comment:** The Executive Summary page iv Driveway B needs to be referenced as a Right In/Right Out and evaluated as per NMDOT direction

***TW Response:*** *Driveway B is now being evaluated as a Right- In only driveway.*

- 2) **CoSF Comment:** Page 2 Driveway B shall be analyzed as a Right In/Right Out driveway as per NMDOT direction.

***TW Response:*** *Driveway B is now being evaluated as a Right- In only driveway.*

- 3) **CoSF Comment:** Page 3 Driveway B shall be shown as a Right In/Right Out driveway as per NMDOT direction.

***TW Response:*** *Driveway B is now being evaluated as a Right- In only driveway.*

- 4) **CoSF Comment:** Page 4 No. 5 Driveway B shall be analyzed as a Right In/Right Out driveway as per NMDOT direction.

***TW Response:*** *Driveway B is now being evaluated as a Right- In only driveway.*

- 5) **CoSF Comment:** Page 5 last paragraph shall reflect the most current NMDOT design as per email from Kathleen Garcia, PE, D5 Traffic Engineer dated 10/10/25.

***TW Response:*** *Narrative was inserted to the report to describe the proposed major modifications that the NMDOT plans to make to Cerrillos Rd.*

- 6) **CoSF Comment:** Page 7 Safety Analysis the most recent 3 years were evaluated. This should be 5 years as determined by the COSF.

***TW Response:*** *Safety Analysis was updated to be 5 years.*

- 7) **CoSF Comment:** Page 7 Cerrillos Rd./ST. Francis Dr. first sentence should read 117 crashes. Also since rear end collisions are the major contributing factor and paragraph discussing possible mitigation measures should be included.

***TW Response:*** *Total crashes was updated to reflect the request of 5-year analysis. According to the AASHTOWare Safety Transportation Program, rear-end collision only accounted for 11% of the total crashes reported. The crash history is not a result of the proposed development, which is expected to have minimal to no negative operational impact; therefore, no additional traffic safety recommendations are provided.*

- 8) **CoSF Comment:** Page 8 The Crash Summary Table provided shall be included with all of the required information. The table provided is only partial. Also since rear end collisions are the major contributing factor and paragraph discussing possible mitigation measures should be included.

***TW Response:*** *Received table format and all required information from the CoSF. Safety analysis was updated. According to the AASHTOWare Safety Transportation Program, rear-end collision only accounted for 11% of the total crashes reported. The crash history is not a result of the proposed development, which is expected to have minimal to no negative operational impact; therefore, no additional traffic safety recommendations are provided. Please see pages 8 of the report.*

- 9) **CoSF Comment:** Page 9 The Crash Summary Table provided shall be included with all of the required information. The table provided is only partial.

***TW Response:*** *Received table format and all required information from the CoSF. Safety analysis was updated. Please see page 8 of the report.*

10) **CoSF Comment:** Page 15 Need to show 2027 data first or is this a typo? Also Need to reevaluate with correct Rt. In/Rt. Out Configuration for Driveway B which isn't even shown.

**TW Response:** *Image in question has been fixed.*

11) **CoSF Comment:** Page 16 Where are the rest of the intersections? And Driveway B needs to be evaluated as a Rt In/Rt. Out.

**TW Response:** *Driveway B is now being evaluated as a Right- In only driveway. Five intersections are analyzed in this study, and all are shown in the reference section; see pages 17 through 20 of the report.*

12) **CoSF Comment:** Page 17 the year 2027 needs to be shown first

**TW Response:** *This has been fixed. See page 17 of the report.*

13) **CoSF Comment:** Page 18 Driveway B needs to be evaluated as a Rt. In/Rt. Out

**TW Response:** *Driveway B is now being evaluated as a Right- In only driveway.*

14) **CoSF Comment:** Table 4 2027 AM Peak Hour shows a 520-foot queue length. Why is this so high from the No Build since delay is about the same?

**TW Response:** *The 95th-percentile queue length for the NO BUILD scenario is 515 ft, and for the BUILD scenario it is 520 ft. The formula used in the spreadsheet for that row was not copied correctly. All Synchro summary tables have been checked and corrected as needed.*

15) **CoSF Comment:** Page 28 Driveway B needs to be evaluated as a Rt. In/Rt. Out.

**TW Response:** *Driveway B is now being evaluated as a Right- In only driveway.*

16) **CoSF Comment:** Table 12 Driveway B needs to be evaluated as a Rt. In/Rt. Out.

**TW Response:** *Driveway B is now being evaluated as a Right- In only driveway.*

17) **CoSF Comment:** Page 30 Driveway B needs to be evaluated as a Rt. In/Rt. Out.

**TW Response:** *Driveway B is now being evaluated as a Right- In only driveway.*

18) **CoSF Comment:** Page 30 The proposed Driveway A WB Deceleration Lane shall reflect the NMDOT design.

**TW Response:** *WB deceleration lane was revised to reflect the NMDOT design. See page 32 and A-107 of the report and appendix.*

19) **CoSF Comment:** Driveway B needs to be evaluated as a Rt. In/Rt. Out.

**TW Response:** *Driveway B is now being evaluated as a Right- In only driveway.*

20) **CoSF Comment:** Page 37 Potential mitigations to address rear end collisions shall be included since they are the major contributing factor.

**TW Response:** *The crash history is not a result of the proposed development, which is expected to have minimal to no negative operational impact; therefore, no additional traffic safety recommendations are provided. Please see pages 8 of the report.*

21) **CoSF Comment:** Page 37 Recommendations Driveway B needs to be evaluated as a Rt. In/Rt. Out.

**TW Response:** *Driveway B is now being evaluated as a Rt. In only driveway.*

Accompanying this letter is the revised draft Final Traffic Impact Analysis including the requested adjustments for your review.

Please call me if you have any additional comments or questions regarding the responses presented, pertaining to the subject of the traffic impact study.

Sincerely,



Terry O. Brown, P.E.

JN: 2025080  
RRB/JL/TB/JN

Enclosure/s: Marriott Hotel – Santa Fe Final Traffic Impact Analysis  
City of Santa Fe Transmittal Letter



**Marriott Hotel – Santa Fe**

(1000 Cerrillos Rd.)

**Traffic Impact Analysis**

November 25, 2025

FINAL



A handwritten signature in blue ink that reads "Terry O. Brown".

Terry O. Brown, P.E.  
5571 Midway Park Pl. NE  
Albuquerque, NM 87109  
(505) 858-3100



A handwritten signature in blue ink that reads "Ron R. Bohannon".

Ronald R. Bohannon, P.E.  
5571 Midway Park Pl. NE  
Albuquerque, NM 87109  
(505) 858-3100

**Presented to:**  
City of Santa Fe

Prepared for:  
Titan Development  
6300 Riverside Plaza Lane NW,  
Ste. 200,  
Albuquerque, NM 87120

Marriott Hotel – Santa Fe  
(1000 Cerrillos Rd.)  
Final Traffic Impact Analysis

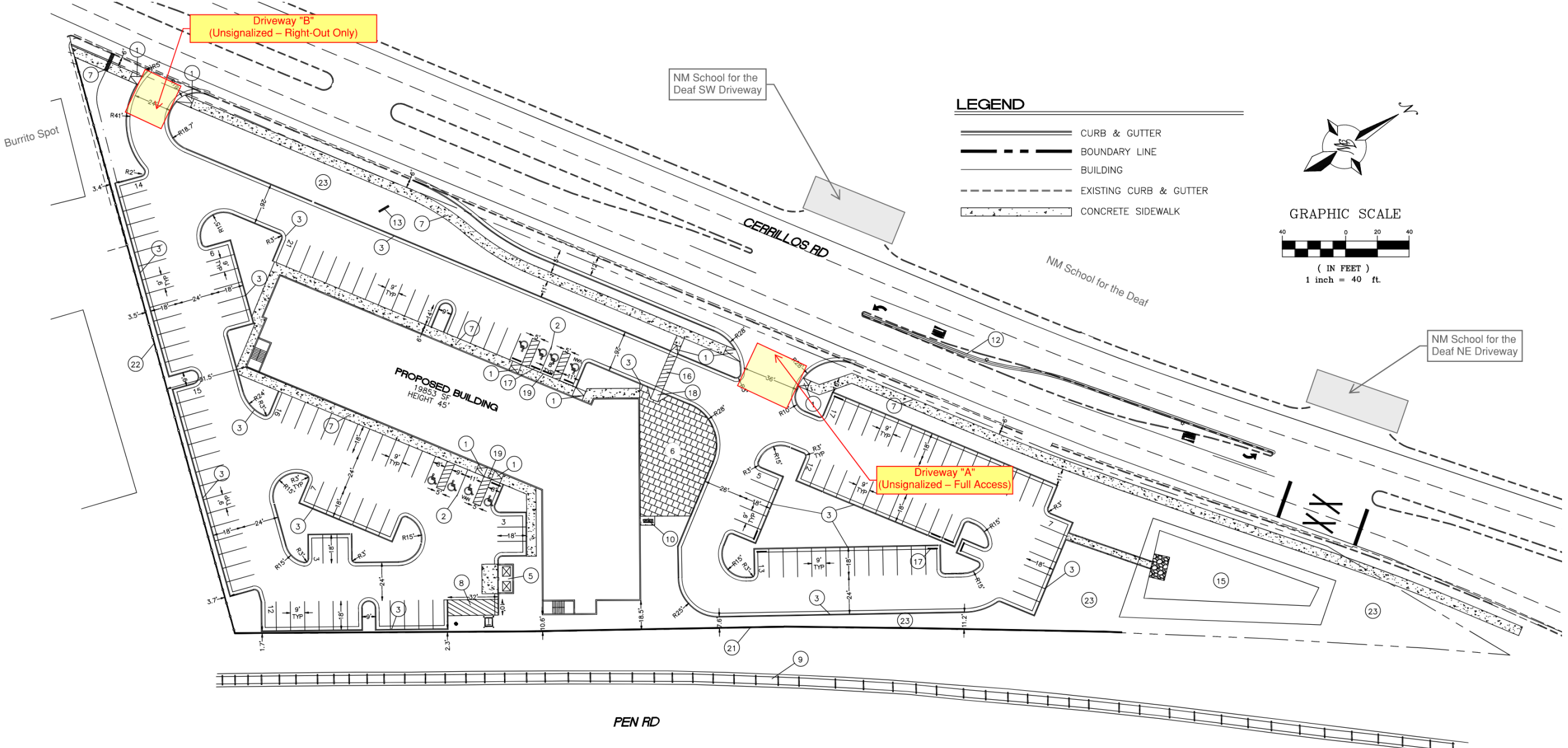
## Executive Summary

The purpose of this Traffic Impact Analysis (TIA) is to evaluate transportation conditions before and after the construction of the proposed Marriott Hotel development, assess the project's impact on the adjacent transportation system, and recommend mitigation measures as needed. The study examines two scenarios: NO BUILD and BUILD for both the 2027 implementation year and the 2037 horizon year. This study has been prepared in accordance with the requirements of the City of Santa Fe (CoSF) Traffic Engineering Department and the scoping meeting held on August 12, 2025.

The project is located within Santa Fe, New Mexico, at 1000 Cerrillos Rd., between St. Francis Drive and Cordova Road, and across the street from the New Mexico School for the Deaf. A vicinity map is provided below.



The project proposes a 150-unit hotel to be built in one phase, with analysis years 2027 (implementation) and 2037 (horizon). The site will have two access points: Driveway “A,” a full-access driveway about 700-ft northeast of Cordova Rd., and Driveway “B,” about 265-ft northeast of Cordova Rd., proposed to operate as a right-out only. See the site plan below. There are two businesses on the site currently and traffic from these businesses are included in the traffic counts that were collected for this study. However, to evaluate the worst-case scenario this traffic study does not reduce the potential traffic expected to be generated by the new hotel to account for the existing traffic at the site.



A summary of the trips expected to be generated by 150-units commercial development, based on the 11th Edition of the Institute of Transportation Engineers (ITE) Trip Generation Manual, is presented below.

## *Marriott Hotel (1000 Cerrillos Rd.)*

### Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

USE (ITE CODE)	24 HR VOL	A. M. PEAK HR.		P. M. PEAK HR.		
DESCRIPTION	GROSS	ENTER	EXIT	ENTER	EXIT	
<b>Summary Sheet</b>						
	Units					
Hotel (310)	150	1,202	38	30	42	41
<b>Total Primary Trips</b>			<b>38</b>	<b>30</b>	<b>42</b>	<b>41</b>

The study area includes two signalized intersections and three unsignalized intersections are listed below:

1. Cerrillos Rd. / St Francis Dr. (Signalized – Full Access except NB and SB Left Turns are prohibited)
2. Cerrillos Rd. / Cordova Rd. (Signalized – Full Access)
3. Cerrillos Rd. / NM School for the Deaf North Driveway (Unsignalized – Full Access)
4. Cerrillos Rd. / NM School for the Deaf South Driveway / Driveway “A” – (Unsignalized – Full Access)
5. Cerrillos Rd. / Driveway “B” (Unsignalized – Proposed as a Right-Out Only)

The analysis of the study area for this project was performed using Synchro 12 software (version 12.2.5.31). The results presented in this Traffic Impact Study are based on HCM7 (Highway Capacity Manual, 7th Edition) reports generated from Synchro 12. A summary of the analysis results is provided in the following table:

### Executive Summary Results Table

Intersection No. / Name	Intersection Operation	Case Evaluation	Implementation Year (2027) Conditions		Horizon Year (2037) Conditions	
			AM Peak LOS - Delay (s)	PM Peak LOS - Delay (s)	AM Peak LOS - Delay (s)	PM Peak LOS - Delay (s)
1 Cerrillos Rd. / St Francis Dr.	Signalized	<i>No Build (Exist. Geo.)</i>	C - 28.4	D - 35.2	C - 30.8	D - 38.0
		<i>Build (Pro. Geo.)</i>	C - 28.6	D - 35.5	C - 31.0	D - 38.3
2 Cerrillos Rd. / Cordova Rd.	Signalized	<i>No Build (Exist. Geo.)</i>	B - 13.5	B - 15.7	B - 14.5	B - 16.7
		<i>Build (Pro. Geo.)</i>	B - 13.8	B - 16.1	B - 14.9	B - 17.1
3 Cerrillos Rd. / NM School for the Deaf Northeast Driveway	Unsignalized	<i>No Build (Exist. Geo.)</i>	B - 11.8	C - 15.9	B - 12.3	C - 17.0
		<i>Build (Pro. Geo.)</i>	B - 11.8	C - 16.1	B - 12.4	C - 17.2
4 Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A”	Unsignalized	<i>No Build (Exist. Geo.)</i>	B - 12.7	C - 16.8	B - 13.3	C - 18.7
		<i>Build (Pro. Geo.)</i>	C - 22.5	C - 21.6	D - 25.5	C - 24.3
5 Cerrillos Rd. / Driveway “B”	Unsignalized	<i>No Build (Exist. Geo.)</i>	N/A	N/A	N/A	N/A
		<i>Build (Pro. Geo.)</i>	B - 12.1	B - 11.7	B - 12.6	B - 12.2

The proposed Marriott Hotel development is anticipated to have minimal impact on the adjacent transportation system in both the 2027 Implementation Year and the 2037 Horizon Year under the BUILD scenario. Traffic analysis confirms that intersection operations within the study area are expected to remain at LOS “D” or better, consistent with the thresholds defined in the CoSF TIA Guidelines. At the two signalized study intersections the addition of anticipated hotel traffic will not change the LOS for either the 2027 or 2037 No Build scenarios and will result in a maximum increase in delay at these two intersections of 0.4 seconds. Crash history indicates a low crash frequency; therefore, given the limited increase in project-generated trips, no safety mitigation measures are warranted.

The access design analysis determined the following:

- A westbound left-turn deceleration lane on Cerrillos Rd. at the northeast access (Driveway “A”) is warranted based on the CoSF TIS Guidelines, with corresponding design recommendations provided below.
- An eastbound right-turn deceleration lane on Cerrillos Rd. at the northeast access (Driveway “A”) is warranted based on the CoSF TIS Guidelines, with corresponding design recommendations provided below.
- The southwest access (Driveway “B”) is recommended to operate as a right-out only driveway to minimize potential vehicle conflict points.

During turning movement counts at Cerrillos Rd. / St. Francis St., it was observed that eastbound queuing on Cerrillos Rd. backed up to the proposed main driveway of the Marriott (also serving the School for the Deaf) coinciding with the passage of the Rail Runner through the signalized intersection. As traffic volumes continue to grow over time, this issue is expected to worsen. During these periods, westbound traffic on Cerrillos Rd. may be unable to turn left into the main driveway, potentially resulting in complaints to the City and/or NMDOT.

In addition, the multimodal review identified a deficiency along Cerrillos Road, where the Pedestrian Level of Traffic Stress (PLTS) is classified as Level 4; related improvement recommendations are provided below.

- All design and construction of the project shall maintain adequate sight distances at driveways and intersections to the extent possible.
- Frontage Improvements
  - Construct a sidewalk along the project frontage in accordance with CoSF design standards for a four-lane Major Arterial, providing a minimum 5-ft setback and a 6-ft sidewalk width.
- Intersection #3: Cerrillos Rd. / NM School for the Deaf Northeast Driveway
  - Extend the existing eastbound left-turn lane to contain a storage length of 136-ft plus a 148/150 reverse-curve transition per NMDOT CN S100681.
- Intersection #4: Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A”
  - Shall operate as an unsignalized full-access driveway.
  - Extend the existing westbound left-turn deceleration lane to contain a storage length of 70-ft plus 150/148-ft reverse-curve transition per NMDOT CN S100681.
  - Construct an eastbound right turn deceleration lane to include a storage length of 120-ft plus 150/150-ft reverse-curve transition.
- Intersection #5: Cerrillos Rd. / Driveway “B”
  - Shall operate as an unsignalized right-out only driveway.

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# Marriott Hotel – Santa Fe (1000 Cerrillos Rd.) Final Traffic Impact Analysis

## Introduction

The purpose of this Traffic Impact Analysis (TIA) is to evaluate transportation conditions before and after the construction of the proposed Marriott Hotel development, assess the project's impact on the adjacent transportation system, and recommend mitigation measures as needed. The study examines two scenarios: NO BUILD and BUILD for both the 2027 implementation year and the 2037 horizon year. This study has been prepared in accordance with the requirements of the City of Santa Fe (CoSF) Traffic Engineering Department and the scoping meeting held on August 12, 2025.

## Description of Proposed Development

The project is located within Santa Fe, New Mexico, at 1000 Cerrillos Rd., between St. Francis Drive and Cordova Road. A vicinity map is provided below.



Figure 1 - Vicinity Map

The proposed commercial development consists of a hotel with 150 units, to be constructed in a single phase. This study evaluates an implementation year of 2027 and a horizon year of 2037.

The subject site is currently zoned C2 (PUD General Commercial), with no plans for rezoning. Currently the site has four driveways. Two of the existing driveways are very wide, one is 78-ft

and the other is 85-ft wide. The hotel project is proposing two driveways, as follows:

- Driveway “A” – An existing full-access driveways on both sides of Cerrillos Road, approximately 700-ft northeast of Cordova Road (centerline to centerline) the driveway to the northwest is for the New Mexico School of the Deaf and the driveway to the southeast is for the proposed project. The driveways are currently stop controlled and will remain that way with the completion of the proposed project.
- Driveway “B” – A driveway on the southeast side of Cerrillos Rd., approximately 265-ft northeast of Cordova Rd. (centerline to centerline). This driveway currently operates as a right-in/right-out access but is proposed as a right-out only.

The site plan showing the proposed access intersections/driveways is presented below (see Appendix Page A-1, for the full site plan).

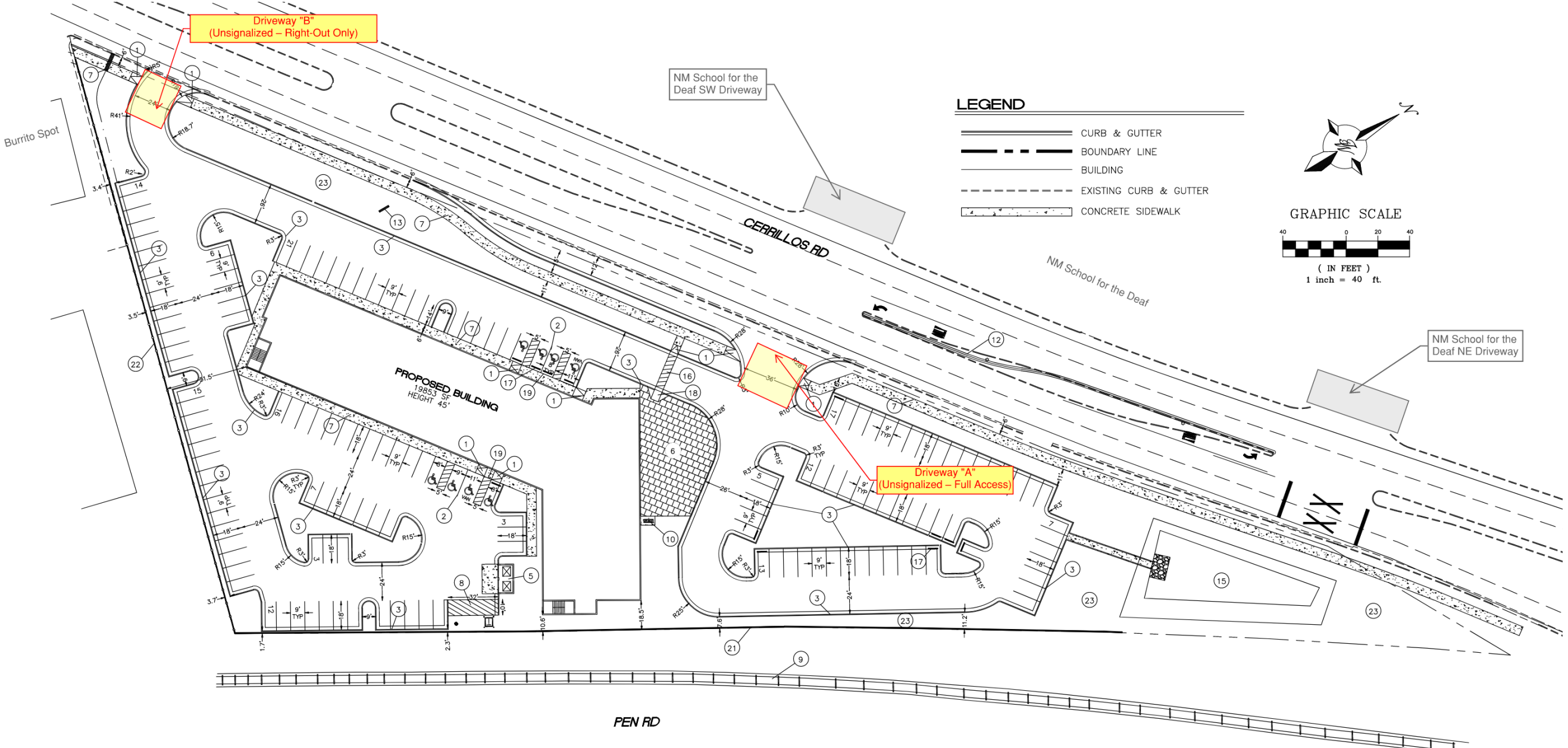


Figure 2 - Site Plan

## Study Area Conditions

### Study Area Definition

The traffic impact analysis scoping meeting was held on August 12, 2025. Attendees included Leroy Pacheco and Phil Gallegos (City of Santa Fe); Jennifer Jenkins and Margaret Ambrosino (Jenkins-Gavin), Ian Robertson, Rafael Castellanos, Josh Rogers (Titan); Ron Bohannon, Derek Bohannon, Jay Nelson, and Terry Brown (Tierra West, LLC).

During the meeting, it was determined that the study area would include the two signalized intersections and three unsignalized intersections that are listed below:

1. Cerrillos Rd. / St Francis Dr. (Signalized – Full Access except NB and SB Left Turns on St. Francis Dr. are prohibited)
2. Cerrillos Rd. / Cordova Rd. (Signalized – Full Access)
3. Cerrillos Rd. / NM School for the Deaf North Driveway (Unsignalized – Full Access)
4. Cerrillos Rd. / NM School for the Deaf South Driveway / Driveway “A” – (Unsignalized – Full Access)
5. Cerrillos Rd. / Driveway “B” (Unsignalized – Proposed as a Right-Out Only)

It was agreed that the implementation year for the project would be 2027 and the horizon year would be 2037. Analysis in the TIS complies with the requirements established in the Scoping Letter (See Appendix Pages A-105 through A-106).

### Existing Land Use

The subject site is currently developed with a furniture store and a tour guide agency. Surrounding parcels within the study area are fully developed with the following:

- New Mexico School for the Deaf – An existing school serving children and youth from birth to age 21, located on Cerrillos Rd., directly across from the subject property.
- Burrito Spot – An existing sit-down restaurant with a drive-thru located off Cerrillos Rd., adjacent to the subject property on the southwest side.

### Other Known Development Activity

New Mexico Department of Transportation Construction Plans CN S100681 – Plans are in place, pending funding, for NM 14 (Cerrillos Road) and adjacent side streets from mile post (MP) 52.04 to MP 53.71. The project includes roadway reconstruction, new traffic signals, overhead lighting, ITS, bus stop improvements (including a new pullout at Second Street), storm drain upgrades, utility relocations, sidewalks, ADA and bicycle facilities, landscaping, retaining walls, access management, new signs and pavement markings, and temporary traffic control during construction.

## Existing Roadway System Characteristics

The Santa Fe MPO roadway classification map and bikeway/trails map, shown below, were used in the analysis. The roadway classifications and characteristics within the study area are as follows:

Cerrillos Rd. is classified as a **Principal Arterial** with a posted speed limit of 35 mph. It is generally a four-lane urban roadway with curb and gutter and 12-ft raised median. 6-ft sidewalks are provided on the northwest side of the roadway, while 6-ft bike lanes are provided on both sides. The bike lanes extend approximately 950 ft northeast of Cordova Rd., after which the roadway transitions to a designated shared bike/vehicle lane with higher traffic volumes and speeds.

The current NMDOT plans (CN S100681) show the following modifications along Cerrillos Rd.:

- Extend existing westbound left-turn lane at Cerrillos Rd. / Cordova Rd.
- Extend existing westbound left-turn lane into Driveway "A" to contain 70-ft of storage and a 150/148-ft reverse-curve transition.
- Extend existing eastbound left-turn lane into NM School for the Deaf NW Driveway to contain 136-ft of storage and a 150/148-ft reverse-curve transition.

St Francis Dr. is classified as a **Principal Arterial** with a posted speed of 35 MPH. It is generally a six-lane urban roadway with curb and gutter and 3-ft raised median. 6-ft sidewalks are provided on both sides of the roadway. No bike lanes are present.

Cordova Rd. is classified as a **Minor Arterial** with a posted speed limit of 35 mph. It is generally a two-lane urban roadway with curb and gutter and a 12-to-13-foot striped median. There is a 6-ft sidewalk on the north side of the road and a 4-ft sidewalk on the south side. The roadway is designated as a shared bike/vehicle lane with higher traffic volumes and speeds.

# Legend

## FUNCTIONAL CLASSIFICATION

- 1 - Interstate
- 3 - Principal Arterial
- 4 - Minor Arterial
- 5 - Major Collector
- 6 - Minor Collector
- Unclassified Roads
- Adjusted Urbanized Area [2013]
- City Boundary
- Tesuque Pueblo
- Agua Fria Traditional Village
- MPO Planning Area
- Santa Fe County Boundary

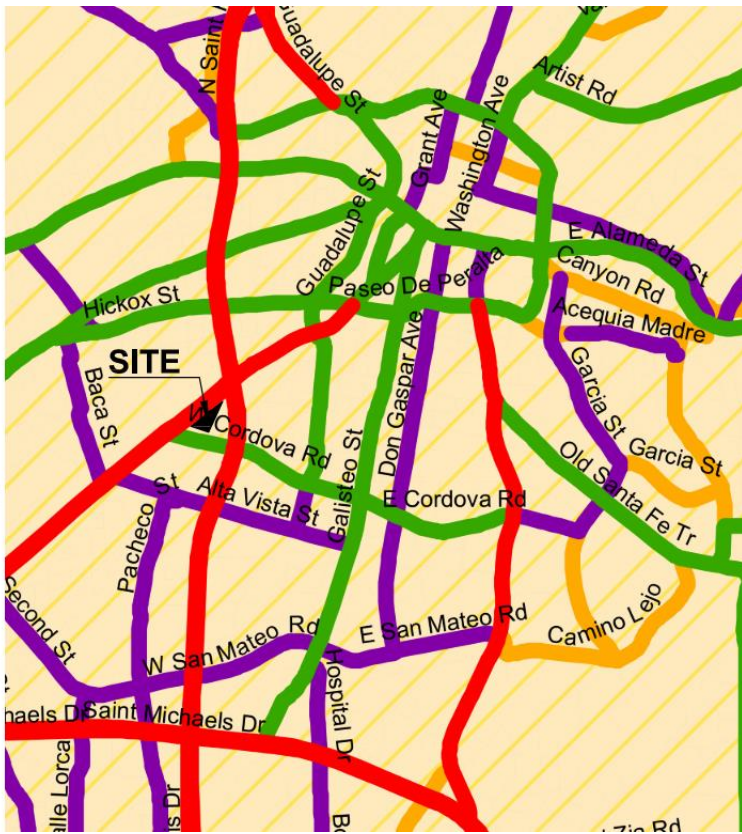


Figure 3 - Portion of the Santa Fe MPO Roadway Classification Map

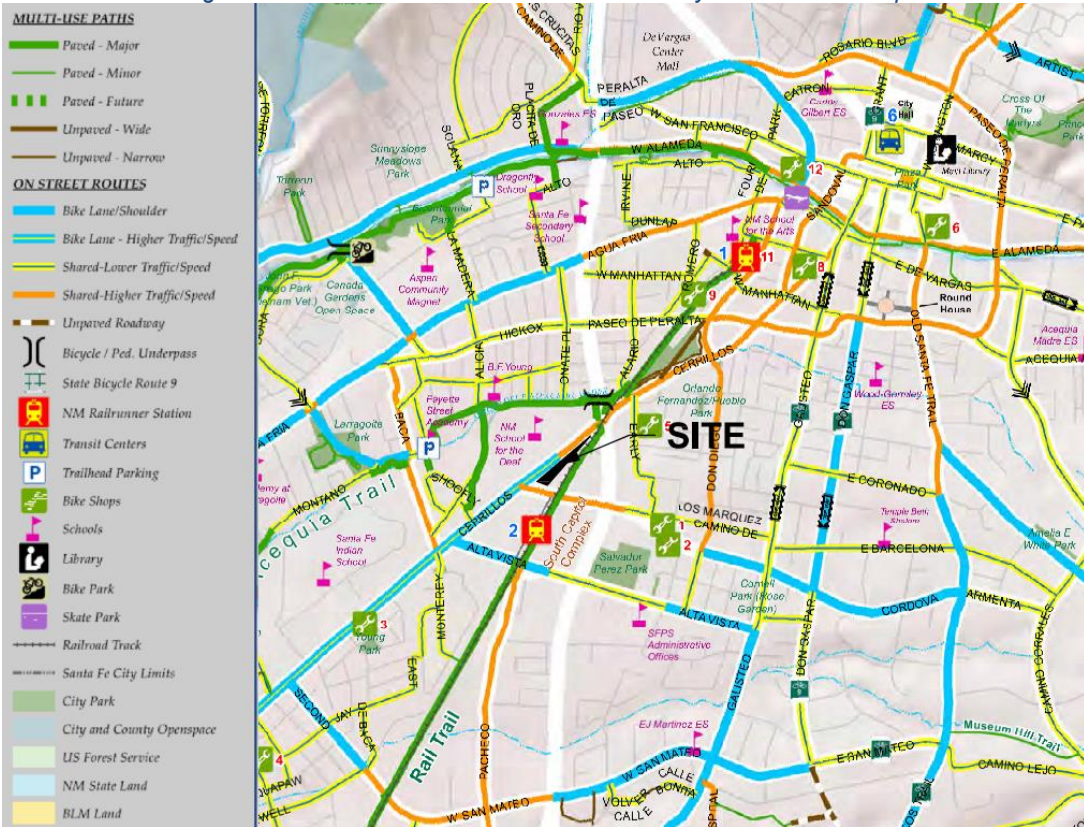


Figure 4 - Portion of the Santa Fe MPO Bikeway and Trails Map

## Traffic Control Devices

Signal timing data for Cerrillos Rd. / St Francis Dr. and Cerrillos Rd. / Cordova Rd. was provided by the CoSF and used in the analysis. The detailed traffic signal timing data is presented in the Appendix on Pages A-80 through A-104.

## Desired Level of Services Per Reviewing Agency

According to the City of Santa Fe, roadways and intersections within the study area must be analyzed with and without the proposed development to assess potential impacts on level of service (LOS) and safety. If an intersection is projected to operate at LOS E or worse, alternatives to mitigate these impacts must be evaluated and documented as part of the study. LOS E may be considered acceptable for left-turn and side-street movements at signalized intersections, provided the overall intersection operates at LOS D or better. For unsignalized movements operating at LOS E or worse, mitigation should be identified if queue lengths exceed available storage or spacing and/or if volume-to-capacity ratios exceed 1.0, indicating congestion concerns.

## Analysis of Existing Conditions

Traffic volumes (turning movement counts) were collected at the intersections targeted for analysis in this study in August 2025 (while school was still in session) and are included on Appendix Pages A – 72 through A - 75. The counts were collected from 7:00 – 9:00 AM and 4:00 – 6:00 PM as directed by the City of Santa Fe in the TIS scoping meeting.

The implementation year for this project is only two years in the future, and the annual growth rate used is 1%. Accordingly, the implementation year NO BUILD analysis should closely approximate the existing conditions analysis. Therefore, a separate existing conditions analysis was not performed.

## Safety Analysis

As requested by CoSF, the most recent five years (2019 through 2023) of available Crash data for study area was obtained from the New Mexico Department of Transportation's (NMDOT's) AASHTOWare Safety Transportation Program. Below is a summary of the intersections that include some of the pertinent data and the graph of the top ten contributing factors and Crash Severity.

### Cerrillos Rd. / St Francis Dr.

As shown in Table 1, a total of 186 crashes were recorded, and less than 1% involved pedestrians. The data indicates an increase in crashes from 2019 to 2023; however, due to the COVID-19 pandemic occurring during most of the analyzed years, the growth trend is inconclusive. A majority of the crashes were front-to-rear, **resulting in rear-end collisions, which accounted for approximately 11% of the total crashes reported.** Most crashes occurred during daylight hours and resulted in property damage only (74%), with no fatalities.

The main contributing factors identified were “other improper driving” (148 crashes) and “following too closely” (66 crashes). Using traffic count data from NMDOT, **the crash rate for the intersection is approximately 1.61 crashes per million entering vehicles (MEV).**

The crash history is not a result of the proposed development, which is expected to have minimal to no negative operational impact; therefore, **no additional traffic safety recommendations are provided.**

Table 1 - Crash Data Summary: Cerrillos Rd. / St Francis Dr.

Cerrillos Rd. / St Francis Dr.		
5 - Year (2019-2023)		
<b>Total Crashes</b>		<b>186</b>
Pedestrian Involved		1
<b>By Year</b>	2023	51
	2022	41
	2021	25
	2020	26
	2019	43
<b>By Type of Crash</b>	Front-to-Rear	52
	Not Available	43
	Sideswipe	21
	Front-to-Side	12
	Front-to-Front	2
	Other Vehicle - From Opposite Direction	0%
	Other Vehicle - From Same Direction/Both Going Straight	3%
	Other Vehicle - From Same Direction/Rear End Collisions	11%
<b>By Lighting Conditions</b>	Day	149
	Dawn/Dusk	2
	Dark	33
	Not Specified	2
	% Dark + Dawn/Dusk	19%
<b>KABCO Crash Severity</b>	O(Property Damage Only)	138
	C(Possible Injury)	37
	B (Minor Injury)	11
	K (Fatal)	0
	A (Serious Injury)	0
	% Property Damage	74%
	% Injury/Fatality	26%
<b>Contributing Factor</b>	Alcohol/Drugs	7
	Animal	0
	Avoid No Contact - Vehicle	6
	Disregarded Traffic Signal	8
	Driver Inattention	25
	Excessive Speed	2
	Failed to Yield Right of Way	11
	Following too Closely	66
	Improper Lane Change	24
	Made Improper Turn	3
	Other - No Driver Error	27
	Speed Too Fast for Conditions	1
	Other Improper Driving	148
	Other Mechanical Defect	4
	% Driver Inattention	7%
	% Following too Closely	18%
	% Diregarded Traffic Signal	2%
% Failed to Yield	3%	

## Cerrillos Rd. / Cordova Rd.

As shown in Table 2, a total of 64 crashes were recorded, and only one involved pedestrians. The data indicates an increase in crashes from 2019 to 2023; however, due to the COVID-19 pandemic occurring during most of the analyzed years, the growth trend is inconclusive. A majority of the crashes were front-to-rear, **resulting in rear-end collisions, which accounted for approximately 8% of the total crashes reported.** Most crashes occurred during daylight hours and resulted in property damage only (66%), with no fatalities.

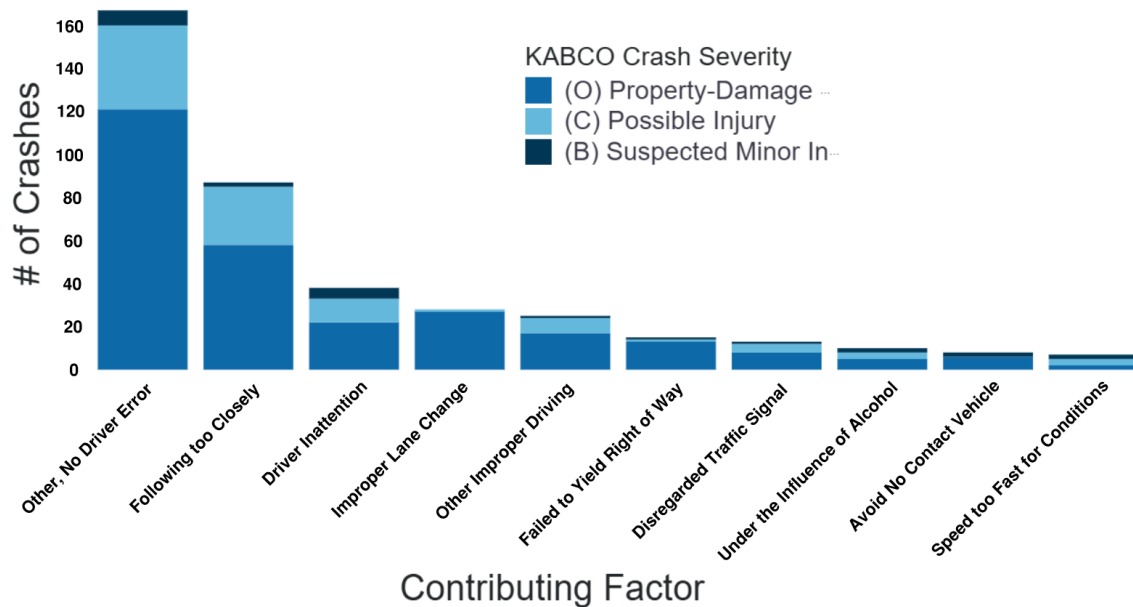
The main contributing factors identified were “other – no driver error” (49 crashes) and “following too closely” (22 crashes). Using traffic count data from NMDOT, the **crash rate for the intersection is approximately 1.84 crashes per million entering vehicles (MEV).**

The crash history is not a result of the proposed development, which is expected to have minimal to no negative operational impact; therefore, **no additional traffic safety recommendations are provided.**

Table 2 - Crash Data Summary: Cerrillos Rd. / Cordova Rd.

Cerrillos Rd. / Cordova Rd. 5 - Year (2019-2023)		
<b>Total Crashes</b>		<b>64</b>
Pedestrian Involved		1
<b>By Year</b>	2023	20
	2022	9
	2021	14
	2020	8
	2019	13
<b>By Type of Crash</b>	Front-to-Rear	24
	Not Available	13
	Front-to-Side	6
	Sideswipe	4
	Front-to-Front	1
	Other Vehicle - From Opposite Direction	0%
	Other Vehicle - From Same Direction/Both Going Straight	5%
	Other Vehicle - From Same Direction/Rear End Collisions	8%
<b>By Lighting Conditions</b>	Day	53
	Dawn/Dusk	5
	Dark	6
	Not Specified	0
	Percent Dark + Dawn/Dusk	17%
<b>KABCO Crash Severity</b>	O(Property Damage Only)	42
	C(Possible Injury)	19
	B (Minor Injury)	3
	K (Fatal)	0
	A (Serious Injury)	0
	% Property Damage	66%
% Injury/Fatality	34%	
<b>Contributing Factor</b>	Alcohol/Drugs	4
	Animal	0
	Avoid No Contact - Vehicle	2
	Disregarded Traffic Signal	5
	Driver Inattention	13
	Excessive Speed	1
	Failed to Yield Right of Way	4
	Following too Closely	22
	Improper Lane Change	4
	Made Improper Turn	1
	Other - No Driver Error	49
	Speed Too Fast for Conditions	3
	Other Improper Driving	5
	Other Mechanical Defect	0
	% Driver Inattention	11%
	% Following too Closely	18%
	% Diregarded Traffic Signal	4%
% Failed to Yield	3%	

Please see the Top 10 Contributing Factors and Crash Severity chart that includes both intersections analyzed within the five-year period below.



## Analysis of Implementation Year and Horizon Year Conditions

### Background Traffic

Background traffic volumes were calculated by applying an annual background growth rate to existing traffic counts. This growth rate, used for both the implementation year and the horizon year, was derived from an analysis of AAWDT volumes from the NMDOT Traffic Count Data System (TCDS) for the period 2014–2024. The resulting trendline indicates a negative annual growth rate, which is likely attributable to the anomalous impacts of the pandemic on travel patterns in Santa Fe, where tourism plays a significant role in driving vehicle volumes. Statewide data show that vehicle travel dropped by as much as 41% at the height of restrictions in April 2020, with reductions persisting at around 18–20% by year’s end in central New Mexico.

Since then, recovery has been strong, with Santa Fe recording more than 3 million visitors in the most recent fiscal year, contributing to increases in both car and air travel. To reflect this rebound and ensure conservative, forward-looking planning that avoids underestimating future demand, a **1% annual growth rate** was applied in the traffic impact analysis, consistent with standard assumptions for recovering urban areas in the region.

### Trip Generation

A Marriott Hotel is proposed with 150-rooms. This analysis used ITE Land Use Code 310 to generate the average weekday traffic, along with the AM and PM Peak Hour project trips. According to the ITE Generation Manual, Hotel (ITE Land Use Code 310) is defined as: “a place of lodging that provides sleeping accommodations and supporting facilities such as a full-service restaurant, cocktail lounge, meeting rooms, banquet room, and convention facilities.”

A summary of the calculated trips expected to be generated by this project is shown in Table 3.

Table 3 - Trip Generation

## *Marriott Hotel (1000 Cerrillos Rd.)*

### Trip Generation Data (ITE Trip Generation Manual - 11th Edition)

USE (ITE CODE)	24 HR VOL	A. M. PEAK HR.		P. M. PEAK HR.	
<i>DESCRIPTION</i>	<i>GROSS</i>	<i>ENTER</i>	<i>EXIT</i>	<i>ENTER</i>	<i>EXIT</i>
<b><u>Summary Sheet</u></b>					
	Units				
Hotel (310)	150	1,202	38	30	42
<b>Total Primary Trips</b>			<b>38</b>	<b>30</b>	<b>42</b>

### Trip Distribution and Assignment

The distribution of project-generated trips was established based on observed travel patterns in the study area and the directional turn movements obtained from existing traffic count data.

The Trip Distribution Map and Trip Assignment Maps for the BUILD condition are provided in Figures Figure 5 – Figure 7 on the following pages.

# Marriott Hotel - Santa Fe, NM

(1000 Cerrillos Rd.)

Trip Distribution Map (%)

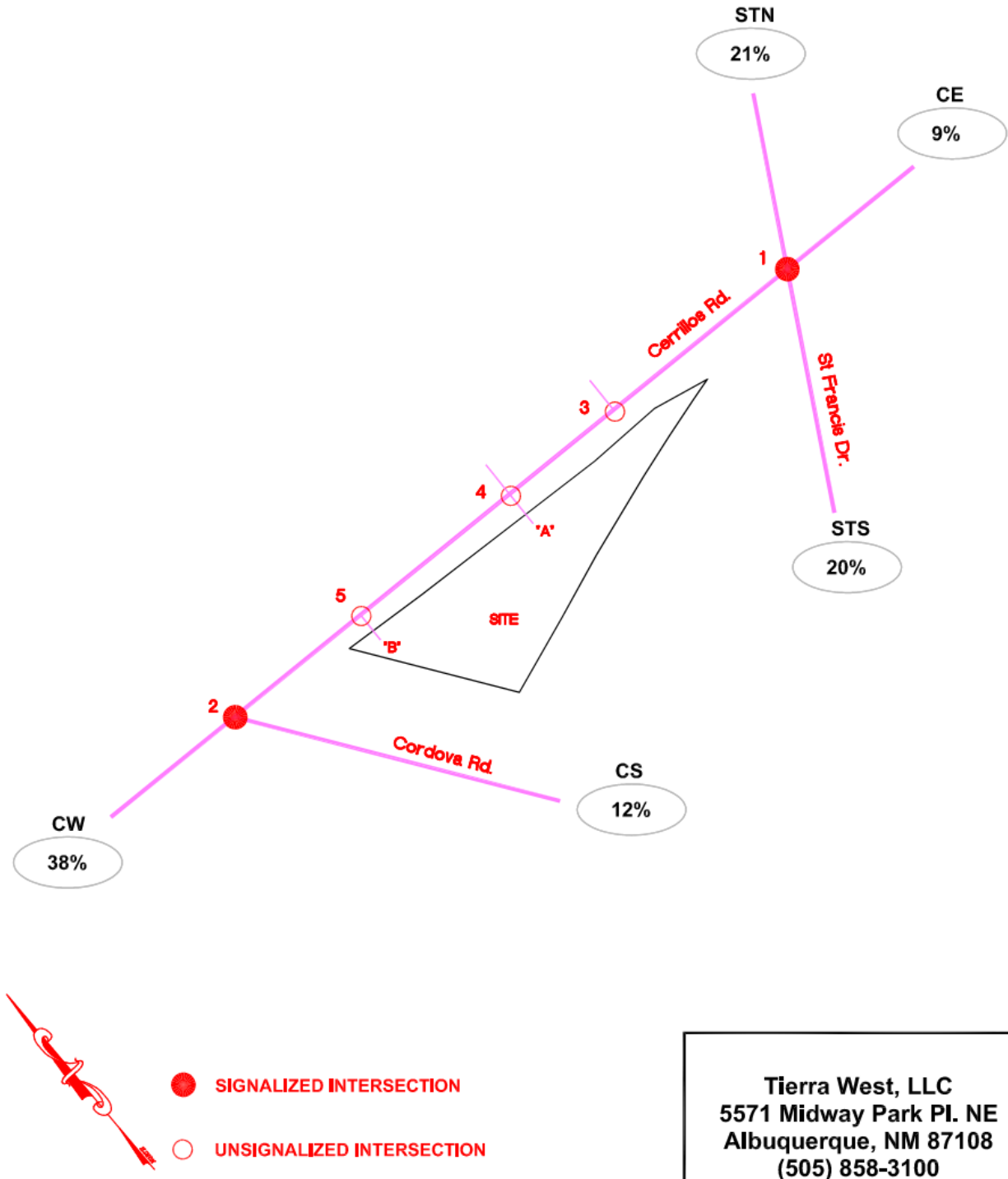
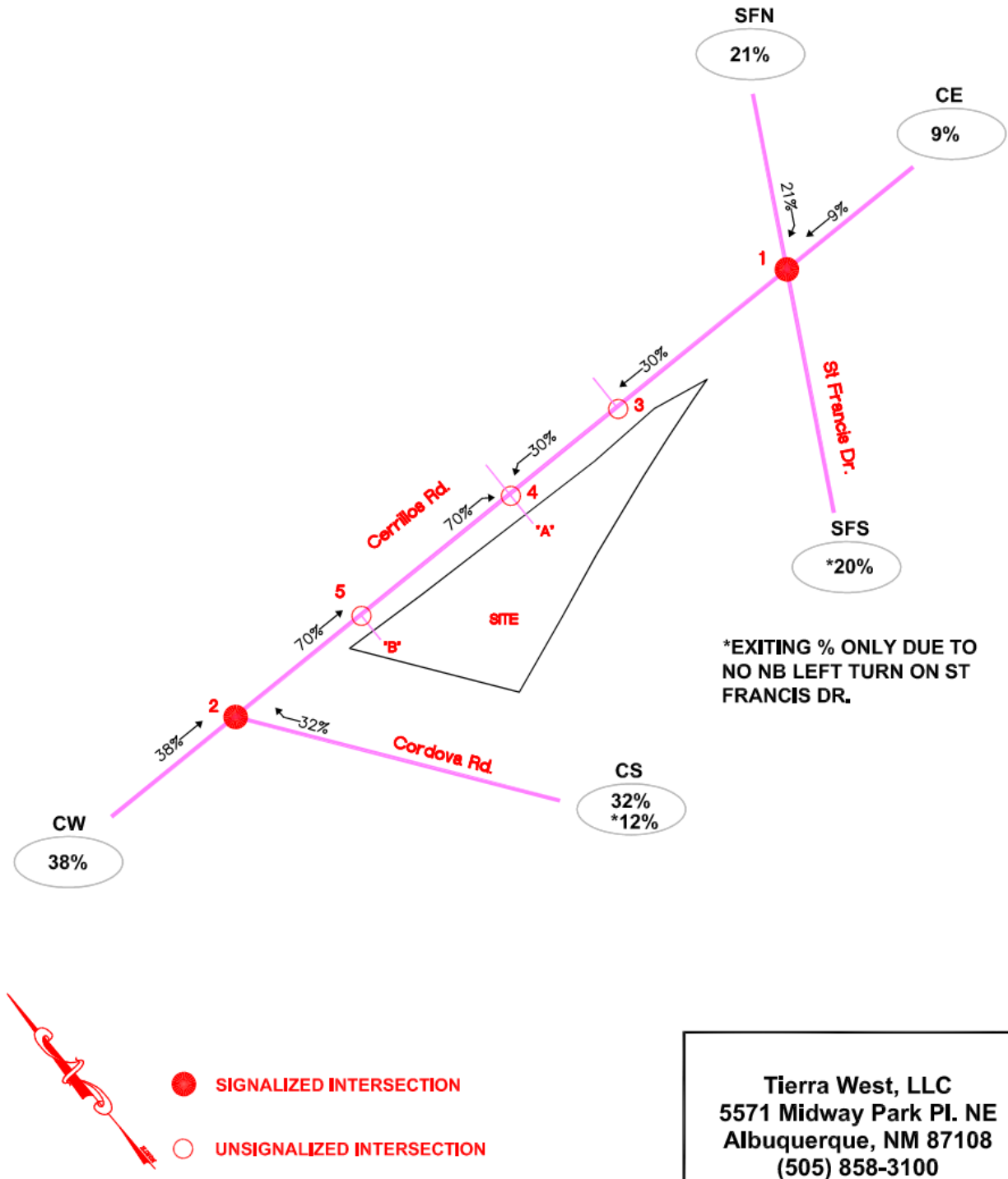


Figure 5 - Trip Distribution Map (%)

# Marriott Hotel - Santa Fe, NM

(1000 Cerrillos Rd.)

Trip Assignments (% Entering)



**Tierra West, LLC**  
 5571 Midway Park Pl. NE  
 Albuquerque, NM 87108  
 (505) 858-3100

Figure 6 - Trip Assignment Map (% Entering)

# Marriott Hotel - Santa Fe, NM

(1000 Cerrillos Rd.)

Trip Assignments (% Exiting)

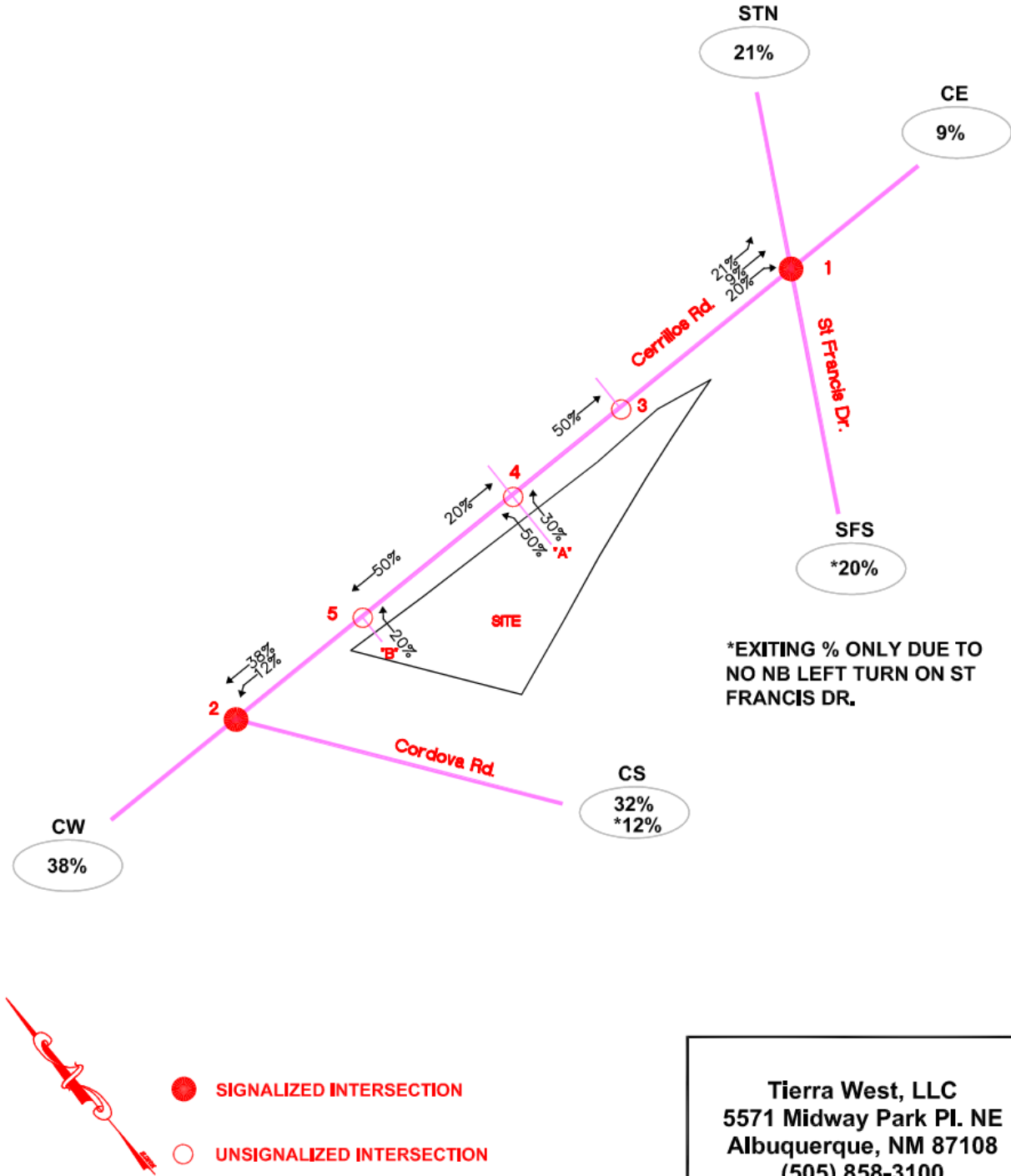


Figure 7 - Trip Assignment Map (% Exiting)

## Traffic Analysis

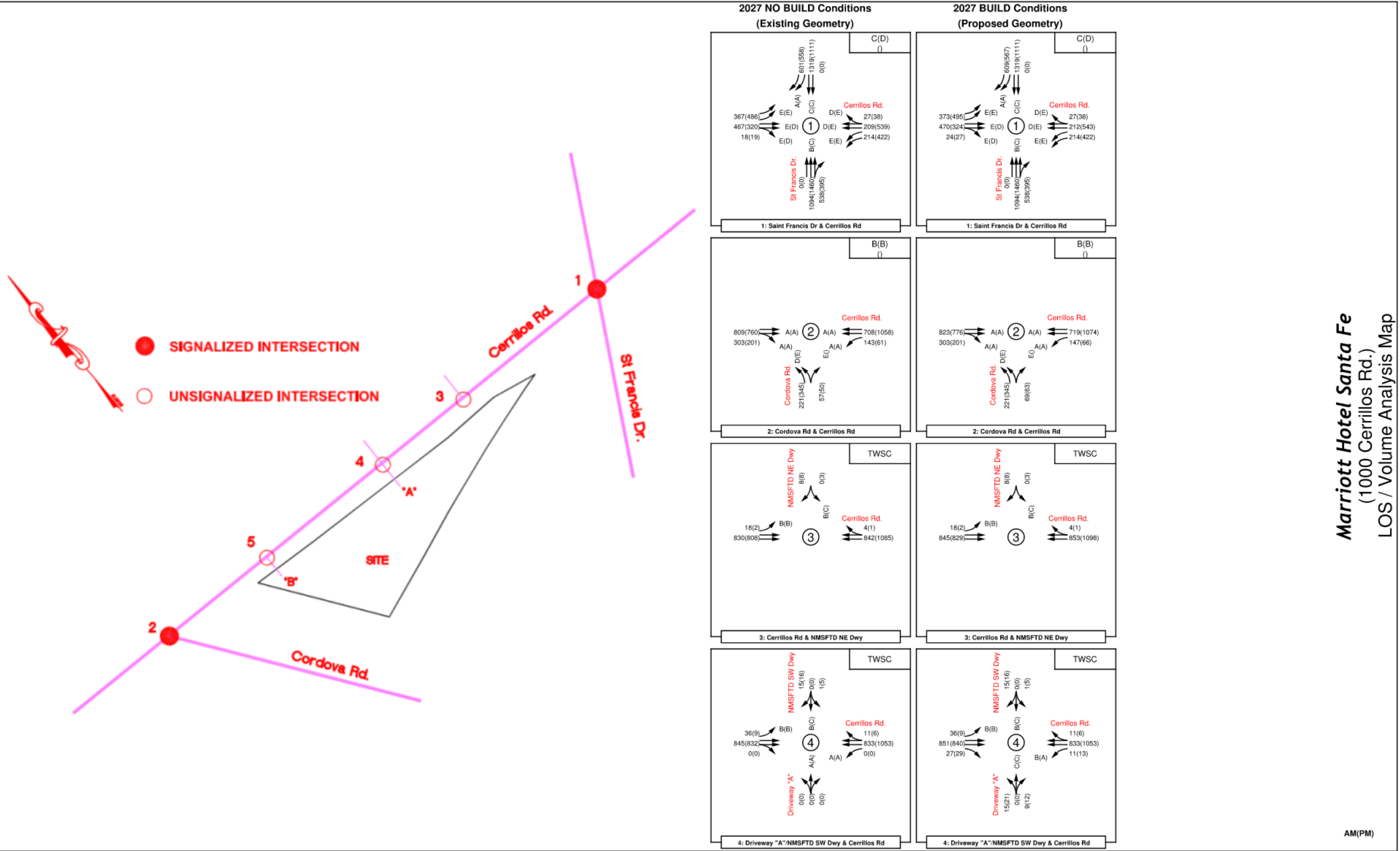
The 2027 and 2037 AM and PM Peak Hour NO BUILD, and BUILD traffic analysis was completed using the observed peak hour factor from the traffic courts. NO BUILD volumes were developed by adjusting existing traffic counts with background traffic growth. BUILD traffic volumes were calculated by adding project-generated trips to the NO BUILD volumes.

The projected turning movement worksheets are provided on Appendix Pages A-8 through A-31.

A capacity analysis was conducted for the following NO BUILD and BUILD conditions during both the AM and PM peak hours:

1. Implementation Year – 2027
2. Horizon Year – 2037

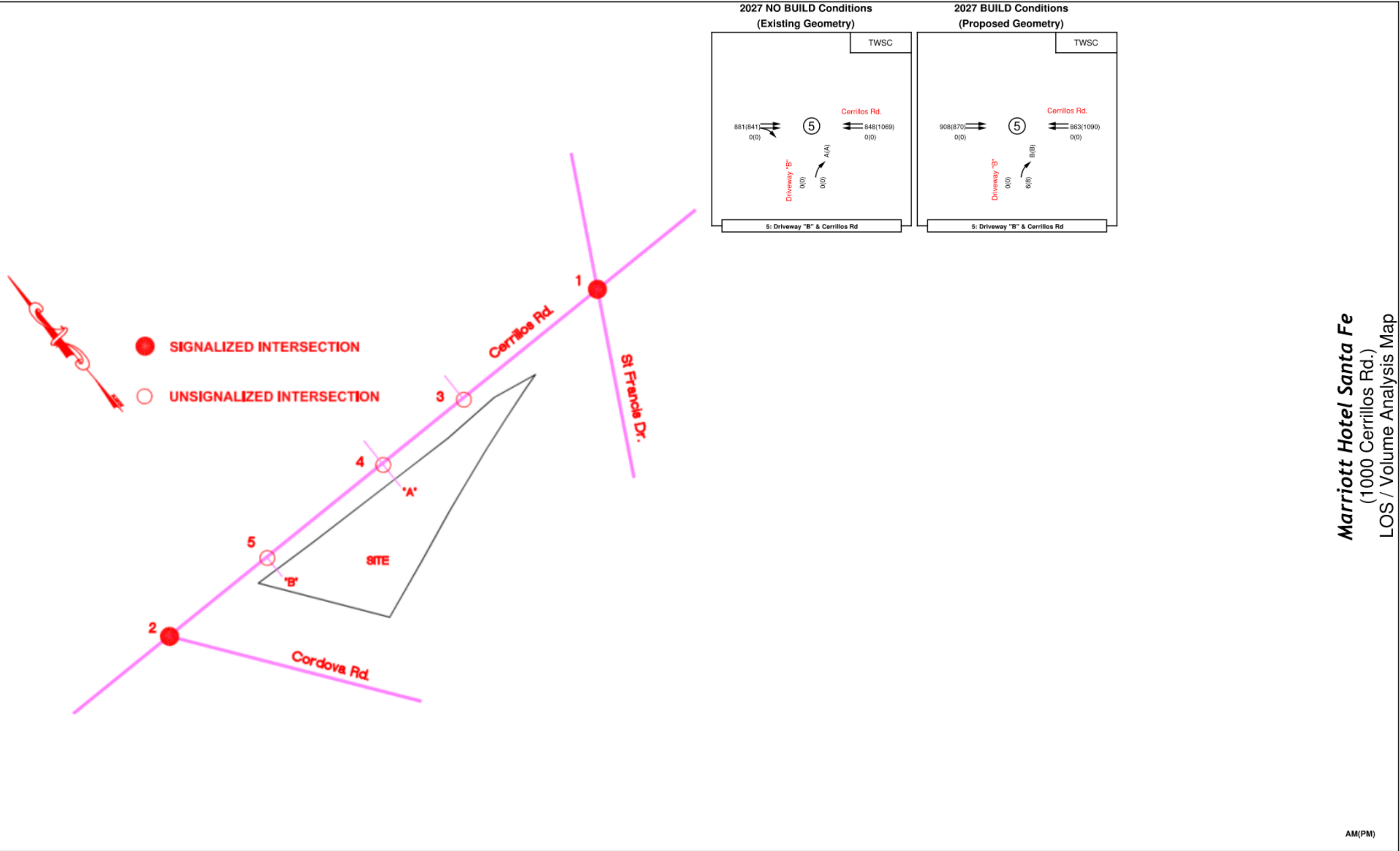
Synchro 12 (version 12.2.5.31) by Trafficware was used to perform capacity and queuing analyses using the HCM 7 methodology for both signalized and unsignalized intersections. The lane/volume analysis maps for the study intersections are provided below, with Figure 8 and Figure 9 presenting the information for the 2027 Implementation Year and Figure 10 and Figure 11 presenting the information for the 2037 Horizon Year.



**Marriott Hotel Santa Fe**  
 (1000 Cerrillos Rd.)  
 LOS / Volume Analysis Map

AM(PM)

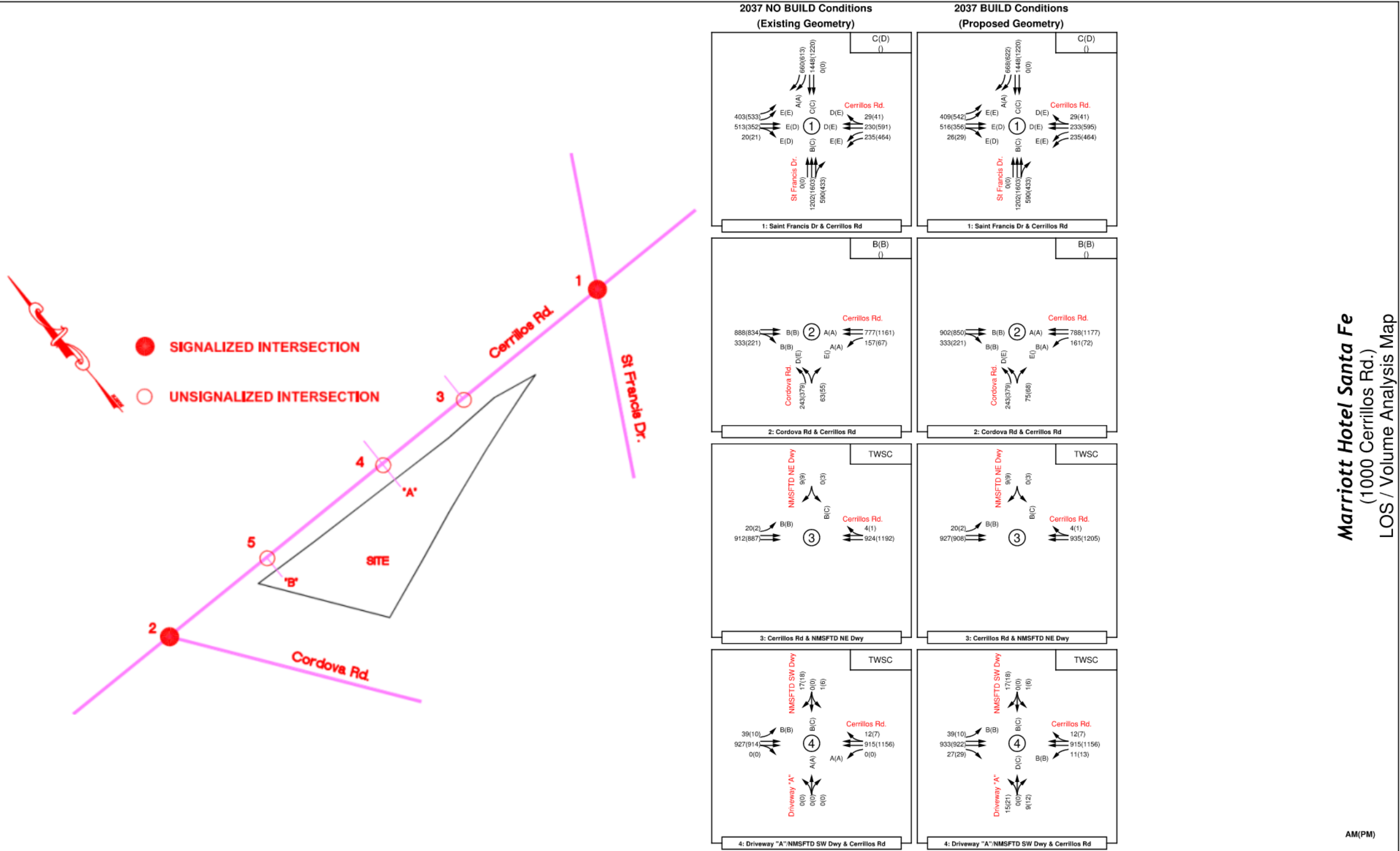
Figure 8 - 2027 Lanes/ Volume Analysis Maps: Intersections 1 - 4



**Marriott Hotel Santa Fe**  
 (1000 Cerrillos Rd.)  
 LOS / Volume Analysis Map

AM(PM)

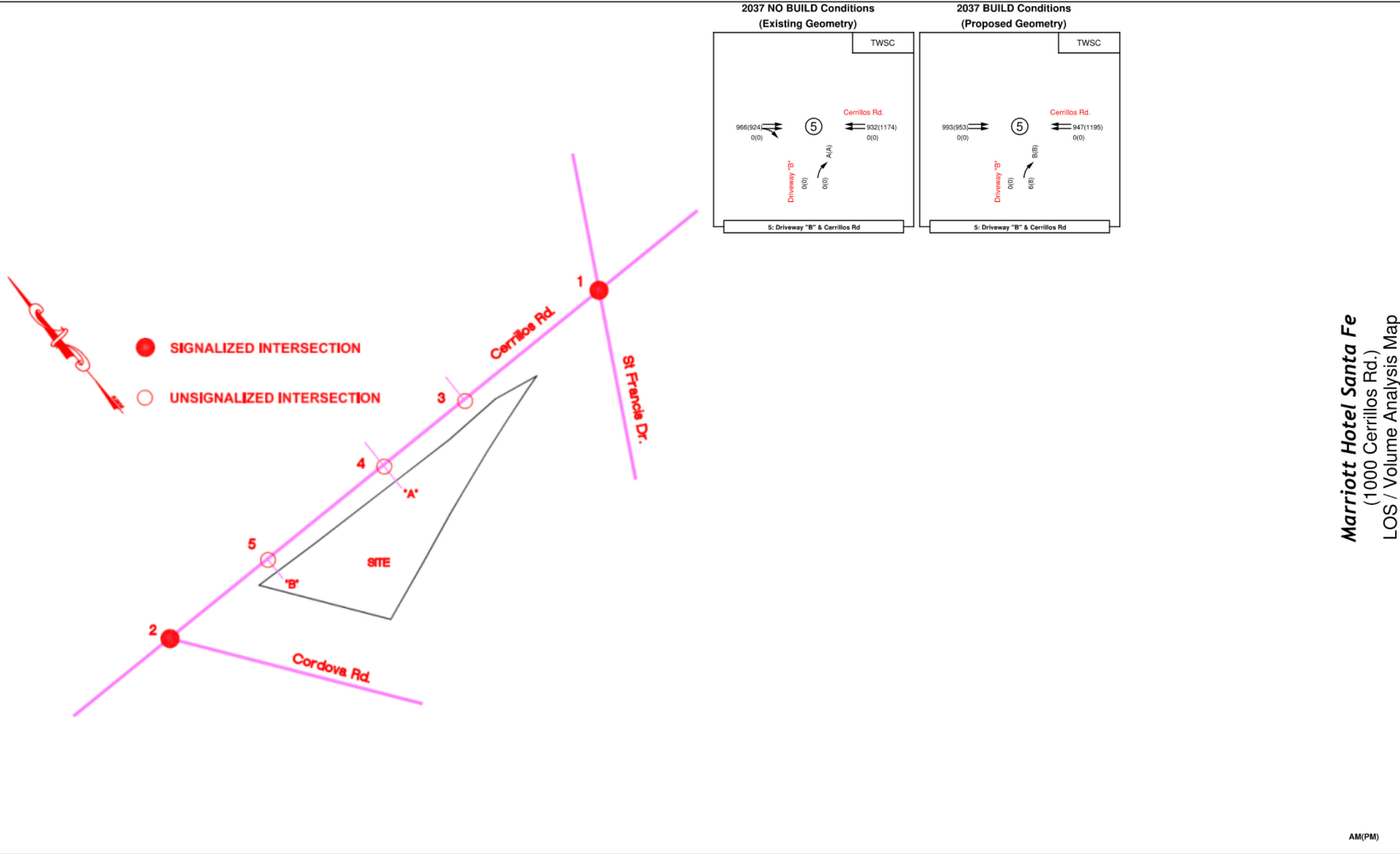
Figure 9 - 2027 Lanes/ Volume Analysis Maps: Intersections 5



**Marriott Hotel Santa Fe**  
 (1000 Cerrillos Rd.)  
 LOS / Volume Analysis Map

AM(PM)

Figure 10 - 2037 Lanes/ Volume Analysis Maps: Intersections 1 - 4



**Marriott Hotel Santa Fe**  
 (1000 Cerrillos Rd.)  
 LOS / Volume Analysis Map

AM(PM)

Figure 11 - 2037 Lanes/ Volume Analysis Maps: Intersections 5

Intersection #1: Cerrillos Rd. / St Francis Dr.



Figure 12 - Aerial Image: Intersection #1

The results of the 2027 and 2037 analysis of Cerrillos Rd. / St Francis Dr. (Signalized – Full Access except northbound and southbound left turns from St. Francis Dr. are prohibited) are summarized in Table 4 and Table 5, as well as attached in Appendix Pages A-32 through A-35 and A-52 through A-55.

Table 4 - 2027 Synchro Summary - Cerrillos Rd. / St Francis Dr.

Signalized

1: Saint Francis Dr & Cerrillos Rd 2027 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB (Saint Francis Dr.)			SB (Saint Francis Dr.)		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing Lane Geometry	2	2>		2	2>			3>			2	2
Queueing Lane Length	270		310	290		365			370			470
<b>AM Peak Hour</b>												
2027 NO BUILD Conditions Volumes	367	467	18	214	209	27	0	1,094	538	0	1,319	601
V/C Ratio	0.84	0.83	0.83	0.78	0.57	0.58	0.00	0.41		0.00	0.72	0.33
Level-of-Service	E	E	E	E	D	D		B			C	A
Control Delay (Seconds)	56.0	57.1	56.9	58.2	51.8	52.0	0.0	15.4	0.0	0.0	21.4	7.5
<b>Intersection LOS</b>	<b>C - 28.4</b>											
95th Percentile Queue (veh)	10.2	12.9	13.3	6.5	6.6	6.9	0.0	10.1	0.0	0.0	20.6	5.5
95th Percentile Queue (ft)	255	323	333	163	165	173	0	253	0	0	515	138
2027 BUILD Conditions Volumes	373	470	24	214	212	27	0	1,094	538	0	1,319	609
V/C Ratio	0.84	0.83	0.83	0.78	0.57	0.58	0.00	0.42		0.00	0.72	0.34
Level-of-Service	E	E	E	E	D	D		B			C	A
Control Delay (Seconds)	56.1	57.3	57.2	58.2	51.8	51.9	0.0	15.6	0.0	0.0	21.7	7.6
<b>Intersection LOS</b>	<b>C - 28.6</b>											
95th Percentile Queue (veh)	10.4	13.1	13.5	6.5	6.7	6.9	0.0	10.1	0.0	0.0	20.8	5.6
95th Percentile Queue (ft)	260	328	338	163	168	173	0	253	0	0	520	140

PM Peak Hour

2027 NO BUILD Conditions Volumes	486	320	19	422	539	38	0	1,460	395	0	1,111	558
V/C Ratio	0.89	0.47	0.47	0.87	0.88	0.88	0.00	0.55		0.00	0.60	0.29
Level-of-Service	E	D	D	E	E	E		C			C	A
Control Delay (Seconds)	66.3	47.5	47.6	64.5	70.5	70.5	0.0	20.5	0.0	0.0	21.9	7.3
<b>Intersection LOS</b>	<b>D - 35.2</b>											
95th Percentile Queue (veh)	14.4	9.0	9.3	12.6	17.0	17.4	0.0	15.6	0.0	0.0	18.2	5.2
95th Percentile Queue (ft)	360	225	233	315	425	435	0	390	0	0	455	130
2027 BUILD Conditions Volumes	495	324	27	422	543	38	0	1,460	395	0	1,111	567
V/C Ratio	0.89	0.48	0.48	0.87	0.88	0.88	0.00	0.55		0.00	0.60	0.30
Level-of-Service	E	D	D	E	E	E		C			C	A
Control Delay (Seconds)	66.5	47.3	47.4	64.5	70.7	70.7	0.0	20.8	0.0	0.0	22.3	7.4
<b>Intersection LOS</b>	<b>D - 35.5</b>											
95th Percentile Queue (veh)	14.6	9.2	9.5	12.6	17.1	17.6	0.0	15.7	0.0	0.0	18.3	5.4
95th Percentile Queue (ft)	365	230	238	315	428	440	0	393	0	0	458	135

Table 5 - 2037 Synchro Summary - Cerrillos Rd. / St Francis Dr.

Signalized

1: Saint Francis Dr & Cerrillos Rd 2037 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB (Saint Francis Dr.)			SB (Saint Francis Dr.)		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing Lane Geometry	2	2>		2	2>			3>			2	2
Queueing Lane Length	270		310	290		365			370			470
<b>AM Peak Hour</b>												
2037 NO BUILD Conditions Volumes	403	513	20	235	230	29	0	1,202	590	0	1,448	660
V/C Ratio	0.85	0.84	0.85	0.80	0.59	0.60	0.00	0.47		0.00	0.82	0.37
Level-of-Service	E	E	E	E	D	D		B			C	A
Control Delay (Seconds)	56.8	58.6	58.4	58.5	51.2	51.4	0.0	17.4	0.0	0.0	26.5	8.2
<b>Intersection LOS</b>	<b>C - 30.8</b>											
95th Percentile Queue (veh)	11.1	14.1	14.6	7.1	7.3	7.5	0.0	11.7	0.0	0.0	25.2	6.5
95th Percentile Queue (ft)	278	353	365	178	183	188	0	293	0	0	630	163
2037 BUILD Conditions Volumes	409	516	26	235	233	29	0	1,202	590	0	1,448	668
V/C Ratio	0.85	0.85	0.85	0.80	0.59	0.60	0.00	0.47		0.00	0.82	0.38
Level-of-Service	E	E	E	E	D	D		B			C	A
Control Delay (Seconds)	56.9	58.8	58.7	58.5	51.2	51.3	0.0	17.6	0.0	0.0	26.8	8.3
<b>Intersection LOS</b>	<b>C - 31.0</b>											
95th Percentile Queue (veh)	11.3	14.4	14.8	7.1	7.3	7.6	0.0	11.7	0.0	0.0	25.4	6.6
95th Percentile Queue (ft)	283	360	370	178	183	190	0	293	0	0	635	165

PM Peak Hour

2037 NO BUILD Conditions Volumes	533	352	21	464	591	41	0	1,603	433	0	1,220	613
V/C Ratio	0.90	0.48	0.49	0.88	0.90	0.90	0.00	0.63		0.00	0.69	0.33
Level-of-Service	E	D	D	E	E	E		C			C	A
Control Delay (Seconds)	67.9	46.4	46.5	65.6	74.0	73.9	0.0	24.2	0.0	0.0	26.3	8.2
<b>Intersection LOS</b>	<b>D - 38.0</b>											
95th Percentile Queue (veh)	15.7	9.6	9.9	13.7	18.8	19.3	0.0	18.6	0.0	0.0	21.9	6.3
95th Percentile Queue (ft)	393	240	248	343	470	483	0	465	0	0	548	158
2037 BUILD Conditions Volumes	542	356	29	464	595	41	0	1,603	433	0	1,220	622
V/C Ratio	0.90	0.49	0.50	0.88	0.90	0.90	0.00	0.64		0.00	0.70	0.33
Level-of-Service	E	D	D	E	E	E		C			C	A
Control Delay (Seconds)	68.3	46.3	46.3	65.6	74.3	74.2	0.0	24.5	0.0	0.0	26.7	8.3
<b>Intersection LOS</b>	<b>D - 38.3</b>											
95th Percentile Queue (veh)	16.0	9.9	10.2	13.7	18.9	19.4	0.0	18.7	0.0	0.0	22.1	6.4
95th Percentile Queue (ft)	400	248	255	343	473	485	0	468	0	0	553	160

The 2027 implementation analysis for Cerrillos Rd. / St Francis Dr., presented in Table 4, indicates that the overall intersection LOS and delays are expected to remain within acceptable ranges under all conditions analyzed. While certain individual movements operate at a LOS lower than “D,” comparison with the NO BUILD condition shows that the **LOS remains unchanged and delays vary only marginally** (less than one second) due to the minimal traffic contribution from the proposed development. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

The 2037 horizon analysis for Cerrillos Rd. / St Francis Dr., presented in Table 5, indicates that the overall intersection LOS and delays are expected to remain within acceptable ranges under all conditions analyzed. While certain individual movements operate at a LOS lower than “D,” comparison with the NO BUILD condition shows that the **LOS remains unchanged and delays vary only marginally** (less than one second) due to the minimal traffic contribution from the proposed development. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

While the calculated 95th percentile queue length at this intersection remains within acceptable limits, the periodic passage of the Rail Runner train (approximately once per hour) is expected to occasionally cause queues exceeding the 95th percentile during affected signal cycles. This condition is discussed in greater detail in the Access Design Specifications section beginning on Page 32 of this report.

Based on the results of the analyses above for the Cerrillos Rd. / St Francis Dr. intersection, **no improvements or mitigation measures are recommended at this location from an operational standpoint.**

Intersection #2: Cerrillos Rd. / Cordova Rd.



Figure 13 - Aerial Image: Intersection #2

The results of the 2027 and 2037 analysis of Cerrillos Rd. / Cordova Rd. (Signalized – Full Access) are summarized in Table 6 and Table 7, as well as attached in Appendix Pages A-36 through A-39 and A-56 through A-59.

Table 6 - 2027 Synchro Summary - Cerrillos Rd. / Cordova Rd.

Signalized

2: Cordova Rd & Cerrillos Rd 2027 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB (Cordova Rd.)		
	L	T	R	L	T	R	L	T	R
Existing Lane Geometry		2>		1	2		2>		
Queuing Lane Length				130					
<b>AM Peak Hour</b>									
2027 NO BUILD Conditions Volumes		809	303	143	708		221		57
V/C Ratio		0.49	0.50	0.41	0.28		0.71		0.83
Level-of-Service		A	A	A	A		D		E
Control Delay (Seconds)		9.3	9.4	7.1	3.6		54.9		60.1
<b>Intersection LOS</b>	<b>B - 13.5</b>								
95th Percentile Queue (veh)		11.1	10.9	1.6	3.6		8.2		8.8
95th Percentile Queue (ft)		278	273	40	90		205		220
2027 BUILD Conditions Volumes		823	303	147	719		221		69
V/C Ratio		0.50	0.51	0.43	0.28		0.72		0.83
Level-of-Service		A	A	A	A		D		E
Control Delay (Seconds)		9.6	9.8	7.6	3.7		54.7		59.9
<b>Intersection LOS</b>	<b>B - 13.8</b>								
95th Percentile Queue (veh)		11.5	11.2	1.7	3.8		8.5		9.1
95th Percentile Queue (ft)		288	280	43	95		213		228

PM Peak Hour

2027 NO BUILD Conditions Volumes		760	201	61	1,058		345		50
V/C Ratio		0.41	0.41	0.15	0.40		0.85		0.00
Level-of-Service		A	A	A	A		E		
Control Delay (Seconds)		8.7	8.8	5.4	5.0		63.3		
<b>Intersection LOS</b>	<b>B - 15.7</b>								
95th Percentile Queue (veh)		9.9	9.7	0.7	7.7		11.8		0.0
95th Percentile Queue (ft)		248	243	18	193		295		0
2027 BUILD Conditions Volumes		776	201	66	1,074		345		63
V/C Ratio		0.42	0.42	0.16	0.41		0.86		0.00
Level-of-Service		A	A	A	A		E		
Control Delay (Seconds)		9.1	9.1	5.7	5.2		93.5		
<b>Intersection LOS</b>	<b>B - 16.1</b>								
95th Percentile Queue (veh)		10.2	10.1	0.8	7.9		12.2		0.0
95th Percentile Queue (ft)		255	253	20	198		305		0

Table 7 - 2037 Synchro Summary - Cerrillos Rd. / Cordova Rd.

Signalized

2: Cordova Rd & Cerrillos Rd 2037 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB (Cordova Rd.)		
	L	T	R	L	T	R	L	T	R
Existing Lane Geometry		2>	0	1	2		2>		0
Queuing Lane Length				130					
<b>AM Peak Hour</b>									
2037 NO BUILD Conditions Volumes		888	333	157	777		243		63
V/C Ratio		0.55	0.56	0.50	0.31		0.72		0.84
Level-of-Service		B	B	A	A		D		E
Control Delay (Seconds)		10.9	11.1	9.8	4.1		54.0		59.5
<b>Intersection LOS</b>	<b>B - 14.5</b>								
95th Percentile Queue (veh)		13.2	13.0	2.3	4.5		8.8		9.5
95th Percentile Queue (ft)		330	325	58	113		220		238
2037 BUILD Conditions Volumes		902	333	161	788		243		75
V/C Ratio		0.56	0.57	0.52	0.31		0.72		0.84
Level-of-Service		B	B	B	A		D		E
Control Delay (Seconds)		11.3	11.6	10.5	4.2		53.8		59.2
<b>Intersection LOS</b>	<b>B - 14.9</b>								
95th Percentile Queue (veh)		13.7	13.5	2.6	4.7		9.1		9.8
95th Percentile Queue (ft)		343	338	65	118		228		245

PM Peak Hour

2037 NO BUILD Conditions Volumes		834	221	67	1,161		379		55
V/C Ratio		0.45	0.46	0.18	0.45		0.86		0.00
Level-of-Service		B	B	A	A		E		
Control Delay (Seconds)		10.0	10.0	6.5	5.8		64.1		
<b>Intersection LOS</b>	<b>B - 16.7</b>								
95th Percentile Queue (veh)		11.5	11.3	0.9	9.1		12.9		0.0
95th Percentile Queue (ft)		288	283	23	228		323		0
2037 BUILD Conditions Volumes		850	221	72	1,177		379		68
V/C Ratio		0.46	0.46	0.20	0.46		0.87		0.00
Level-of-Service		B	B	A	A		E		
Control Delay (Seconds)		10.3	10.4	6.8	6.1		64.4		
<b>Intersection LOS</b>	<b>B - 17.1</b>								
95th Percentile Queue (veh)		11.9	11.7	1.0	9.4		13.2		0.0
95th Percentile Queue (ft)		298	293	25	235		330		0

The 2027 implementation analysis for Cerrillos Rd. / Cordova Rd., presented in Table 6, indicates that the overall intersection LOS and delays are expected to remain within acceptable ranges under all conditions analyzed. While the northbound Cordova Rd. individual movements operate at a LOS lower than “D,” comparison with the NO BUILD condition shows that **the LOS remains unchanged and delays vary only marginally** (less than one second) due to the minimal traffic contribution from the proposed development. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

The 2037 horizon analysis for Cerrillos Rd. / Cordova Rd., presented in Table 7, indicates that the overall intersection LOS and delays are expected to remain within acceptable ranges under all conditions analyzed. While the northbound Cordova Rd. individual movements operate at a LOS lower than “D,” comparison with the NO BUILD condition shows that **the LOS remains unchanged and delays vary only marginally** (less than one second) due to the minimal traffic contribution from the proposed development. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

Based on the results of the analyses above for the Cerrillos Rd. / Cordova Rd. intersection, **no improvements or mitigation measures are recommended at this location.**

### Intersection #3: Cerrillos Rd. / NM School for the Deaf Northeast Driveway



Figure 14 - Aerial Image: Intersection #3

The results of the 2027 and 2037 analysis of Cerrillos Rd. / NM School for the Deaf Northeast Driveway (Unsignalized – Full Access) are summarized in Table 8 and Table 9, as well as attached in Appendix Pages A-40 through A-43 and A-60 through A-63.

Table 8 - 2027 Synchro Summary - Cerrillos Rd. / NM School for the Deaf NE Driveway

Table 9 - 2037 Synchro Summary - Cerrillos Rd. / NM School for the Deaf NE Driveway

Unsignalized

3: Cerrillos Rd & NMSFTD NE Dwy 2027 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			SB (NMSFTD NE Dwy)		
	L	T	R	L	T	R	L	T	R
Existing Lane Geometry	1	2			2>		1>		
<b>AM Peak Hour</b>									
2027 NO BUILD Conditions Volumes	18	830			842	4	0		8
V/C Ratio	0.03						0.02		
Level-of-Service	B						B		
Control Delay (Seconds)	10.1						11.8		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.1						0.1		
95th Percentile Queue (ft)	3						3		
2027 BUILD Conditions Volumes	18	845			853	4	0		8
V/C Ratio	0.03						0.02		
Level-of-Service	B						B		
Control Delay (Seconds)	10.2						11.8		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.1						0.1		
95th Percentile Queue (ft)	3						3		

Unsignalized

3: Cerrillos Rd & NMSFTD NE Dwy 2037 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			SB (NMSFTD NE Dwy)		
	L	T	R	L	T	R	L	T	R
Existing Lane Geometry	1	2			2>		1>		
<b>AM Peak Hour</b>									
2037 NO BUILD Conditions Volumes	20	912			924	4	0		9
V/C Ratio	0.03						0.02		
Level-of-Service	B						B		
Control Delay (Seconds)	10.6						12.3		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.1						0.1		
95th Percentile Queue (ft)	3						3		
2037 BUILD Conditions Volumes	20	927			935	4	0		9
V/C Ratio	0.03						0.02		
Level-of-Service	B						B		
Control Delay (Seconds)	10.6						12.4		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.1						0.1		
95th Percentile Queue (ft)	3						3		

PM Peak Hour

2027 NO BUILD Conditions Volumes	2	808			1,085	1	3		8
V/C Ratio	0.00						0.03		
Level-of-Service	B						C		
Control Delay (Seconds)	11.0						15.9		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.0						0.1		
95th Percentile Queue (ft)	0						3		
2027 BUILD Conditions Volumes	2	829			1,098	1	3		8
V/C Ratio	0.00						0.04		
Level-of-Service	B						C		
Control Delay (Seconds)	11.1						16.1		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.0						0.1		
95th Percentile Queue (ft)	0						3		

PM Peak Hour

2037 NO BUILD Conditions Volumes	2	887			1,192	1	3		9
V/C Ratio	0.00						0.04		
Level-of-Service	B						C		
Control Delay (Seconds)	11.7						17.0		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.0						0.1		
95th Percentile Queue (ft)	0						3		
2037 BUILD Conditions Volumes	2	908			1,205	1	3		9
V/C Ratio	0.00						0.04		
Level-of-Service	B						C		
Control Delay (Seconds)	11.7						17.2		
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)	0.0						0.1		
95th Percentile Queue (ft)	0						3		

The 2027 implementation analysis for Cerrillos Rd. / NM School for the Deaf Northeast Driveway, presented in Table 8, indicates that operations are expected to remain acceptable under all conditions analyzed. All turning movements are anticipated to operate at LOS “C” or better during the 2027 AM and PM peak hours for the BUILD condition. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

The 2037 horizon analysis for Cerrillos Rd. / NM School for the Deaf Northeast Driveway, presented in Table 9, indicates that operations are expected to remain acceptable under all conditions analyzed. All turning movements are anticipated to operate at LOS “C” or better during the 2037 AM and PM peak hours for the BUILD condition. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

Based on the results of the analyses above for the Cerrillos Rd. / NM School for the Deaf Northeast Driveway intersection, **no improvements or mitigation measures are recommended at this location.**

Intersection #4: Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A”



Figure 15 - Aerial Image: Intersection #4

The results of the 2027 and 2037 analysis of Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A” – (Unsignalized – Full Access) are summarized in Table 10 and Table 11, as well as attached in Appendix Pages A-44 through A-47 and A-64 through A-67.

Table 10 - 2027 Synchro Summary - Cerrillos Rd. / NM School for the Deaf South Driveway / Driveway "A"

Unsignalized												
4: Dwy "A"/NMSFTD SW Dwy & Cerrillos Rd 2027 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB ( Driveway "A")			SB (NMSFTD SW Dwy)		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing Lane Geometry	1	2>		1	2>			<1>			<1>	
Proposed Lane Geometry	1	2	1	1	2>			<1>			<1>	
Queuing Lane Length	90			80								
<b>AM Peak Hour</b>												
2027 NO BUILD Conditions Volumes	36	845	0	0	833	11	0	0	0	1	0	15
V/C Ratio	0.06										0.04	
Level-of-Service	B			A				A			B	
Control Delay (Seconds)	10.2			0.0				0.0			12.7	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.2			0.0							0.1	
95th Percentile Queue (ft)	5			0							3	
2027 BUILD Conditions Volumes	36	851	27	11	833	11	15	0	9	1	0	15
V/C Ratio	0.06			0.02				0.12			0.04	
Level-of-Service	B			B				C			B	
Control Delay (Seconds)	10.2			10.2				22.5			12.7	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.2			0.1				0.4			0.1	
95th Percentile Queue (ft)	5			3				10			3	

PM Peak Hour												
2027 NO BUILD Conditions Volumes	9	832	0	0	1,053	6	0	0	0	5	0	16
V/C Ratio	0.02										0.07	
Level-of-Service	B			A				A			C	
Control Delay (Seconds)	10.9			0.0				0.0			16.8	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.0			0.0							0.2	
95th Percentile Queue (ft)	0			0							5	
2027 BUILD Conditions Volumes	9	840	29	13	1,053	6	21	0	12	5	0	16
V/C Ratio	0.02			0.02				0.14			0.07	
Level-of-Service	B			A				C			C	
Control Delay (Seconds)	10.9			10.0				21.6			17.1	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.0			0.1				0.5			0.2	
95th Percentile Queue (ft)	0			3				13			5	

Table 11 - 2037 Synchro Summary - Cerrillos Rd. / NM School for the Deaf South Driveway / Driveway "A"

Unsignalized												
4: Dwy "A"/NMSFTD SW Dwy & Cerrillos Rd 2037 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB ( Driveway "A")			SB (NMSFTD SW Dwy)		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing Lane Geometry	1	2>	0	1	2>	0	0	<1>	0	0	<1>	0
Proposed Lane Geometry	1	2	1	1	2>			<1>			<1>	
Queuing Lane Length	90			80								
<b>AM Peak Hour</b>												
2037 NO BUILD Conditions Volumes	39	927	0	0	915	12	0	0	0	1	0	17
V/C Ratio	0.07										0.04	
Level-of-Service	B			A				A			B	
Control Delay (Seconds)	10.7			0.0				0.0			13.3	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.2			0.0							0.1	
95th Percentile Queue (ft)	5			0							3	
2037 BUILD Conditions Volumes	39	933	27	11	915	12	15	0	9	1	0	17
V/C Ratio	0.07			0.02				0.13			0.04	
Level-of-Service	B			B				D			B	
Control Delay (Seconds)	10.7			10.7				25.5			13.3	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.2			0.1				0.4			0.1	
95th Percentile Queue (ft)	5			3				10			3	

PM Peak Hour												
2037 NO BUILD Conditions Volumes	10	914	0	0	1,156	7	0	0	0	6	0	18
V/C Ratio	0.02										0.09	
Level-of-Service	B			A				A			C	
Control Delay (Seconds)	11.6			0.0				0.0			18.7	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.1			0.0							0.3	
95th Percentile Queue (ft)	3			0							8	
2037 BUILD Conditions Volumes	10	922	29	13	1,156	7	21	0	12	6	0	18
V/C Ratio	0.02			0.02				0.16			0.09	
Level-of-Service	B			B				C			C	
Control Delay (Seconds)	11.6			10.4				24.3			19.3	
<b>Intersection LOS</b>	<b>TWSC</b>											
95th Percentile Queue (veh)	0.1			0.1				0.6			0.3	
95th Percentile Queue (ft)	3			3				15			8	

The 2027 implementation analysis for Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A”, presented in Table 10, indicates that operations are expected to remain acceptable under all conditions analyzed. All turning movements are anticipated to operate at **LOS “C” or better** during the 2027 AM and PM peak hours for the BUILD condition. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

The 2037 horizon analysis for Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A”, presented in Table 11, indicates that operations are expected to remain acceptable under all conditions analyzed. All turning movements are anticipated to operate at **LOS “D” or better** during the 2037 AM and PM peak hours for the BUILD condition. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

Based on the results of the analyses above for the Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A” intersection, **no improvements or mitigation measures are recommended at this location from an operational standpoint.**

#### Intersection #5: Cerrillos Rd. / Driveway “B”

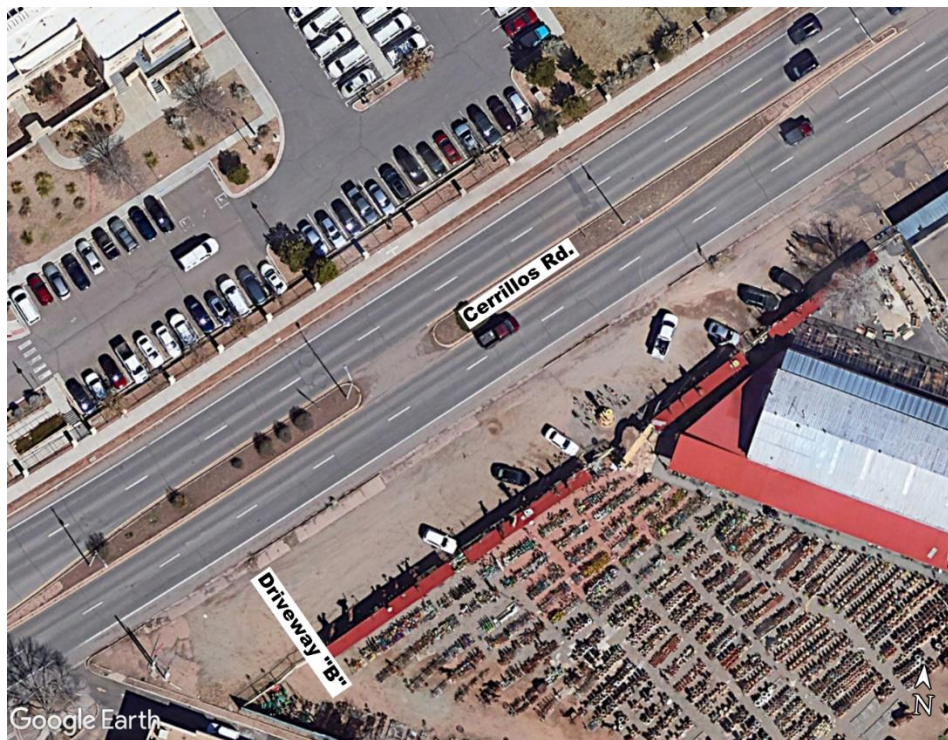


Figure 16 - Aerial Image: Intersection #5

The results of the 2027 and 2037 analysis of Cerrillos Rd. / Driveway “B” (Unsignalized – Proposed as a Right-Out Only) are summarized in Table 12 and Table 13, as well as attached in Appendix Pages A-48 through A-51 and A-68 through A-71.

Table 12 - 2027 Synchro Summary - Cerrillos Rd. / Driveway "B"

Unsignalized

5: Driveway "B" & Cerrillos Rd 2027 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB (Driveway "B")		
	L	T	R	L	T	R	L	T	R
Existing Lane Geometry		2>			2				1
Proposed Lane Geometry		2			2				1
<b>AM Peak Hour</b>									
2027 NO BUILD Conditions Volumes		881	0	0	848		0		0
V/C Ratio									
Level-of-Service									A
Control Delay (Seconds)									0.0
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0
2027 BUILD Conditions Volumes		908	0	0	863		0		6
V/C Ratio									0.01
Level-of-Service									B
Control Delay (Seconds)									12.1
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0

PM Peak Hour

2027 NO BUILD Conditions Volumes		841	0	0	1,069		0		0
V/C Ratio									
Level-of-Service									A
Control Delay (Seconds)									0.0
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0
2027 BUILD Conditions Volumes		870	0	0	1,090		0		8
V/C Ratio									0.02
Level-of-Service									B
Control Delay (Seconds)									11.7
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0

Table 13 - 2037 Synchro Summary - Cerrillos Rd. / Driveway "B"

Unsignalized

5: Driveway "B" & Cerrillos Rd 2037 Conditions	EB (Cerrillos Rd.)			WB (Cerrillos Rd.)			NB (Driveway "B")		
	L	T	R	L	T	R	L	T	R
Existing Lane Geometry		2>	0	0	2		0		1
Proposed Lane Geometry		2			2				1
<b>AM Peak Hour</b>									
2037 NO BUILD Conditions Volumes		966	0	0	932		0		0
V/C Ratio									
Level-of-Service									A
Control Delay (Seconds)									0.0
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0
2037 BUILD Conditions Volumes		993	0	0	947		0		6
V/C Ratio									0.01
Level-of-Service									B
Control Delay (Seconds)									12.6
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0

PM Peak Hour

2037 NO BUILD Conditions Volumes		924	0	0	1,174		0		0
V/C Ratio									
Level-of-Service									A
Control Delay (Seconds)									0.0
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.0
95th Percentile Queue (ft)									0
2037 BUILD Conditions Volumes		953	0	0	1,195		0		8
V/C Ratio									0.02
Level-of-Service									B
Control Delay (Seconds)									12.2
<b>Intersection LOS</b>	<b>TWSC</b>								
95th Percentile Queue (veh)									0.1
95th Percentile Queue (ft)									3

The 2027 implementation analysis for Cerrillos Rd. / Driveway "B", presented in Table 12, indicates that operations are expected to remain acceptable under all conditions analyzed. All turning movements are anticipated to operate at **LOS "B" or better** during the 2027 AM and PM peak hours for the BUILD condition. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

The 2037 horizon analysis for Cerrillos Rd. / Driveway "B", presented in Table 13, indicates that operations are expected to remain acceptable under all conditions analyzed. All turning movements are anticipated to operate at **LOS "B" or better** during the 2037 AM and PM peak hours for the BUILD condition. Furthermore, the 95th-percentile queues are projected to remain within acceptable limits.

Based on the results of the analyses above for the Cerrillos Rd. / Driveway "B" intersection, **no improvements or mitigation measures are recommended at this location from an operational standpoint.**

### Access Design Specifications

**Driveway "A"** – is proposed as an unsignalized full-access intersection. An existing westbound left-turn deceleration lane is present, and the analysis above indicates no operational issues. However, AM and PM peak-hour video footage from Volo Pervidi LLC showed eastbound queuing from Cerrillos Rd. / St. Francis Dr. extending past Driveway "A" due to Rail Runner train activity. Queuing that extended past Driveway "A" occurred four times during the AM peak hour, with an average clearance time of 30 seconds, and four times during the PM peak hour, with an average clearance time of 36 seconds. Each occurrence involved a minimum of two vehicles blocking the median opening.

To mitigate potential queuing delays associated with area growth, it is **recommended to extend the existing westbound left-turn deceleration lane on Cerrillos Rd. at Driveway "A" to contain a storage length of approximately 70-ft plus 150/148 reverse-curve transition**, as per the NMDOT design. In addition, it is recommended to extend the existing eastbound left-turn lane to be approximately 136 -ft plus 148/150 reverse-curve transition to be consistent with the NMDOT design. These improvements are illustrated in Figure 17.

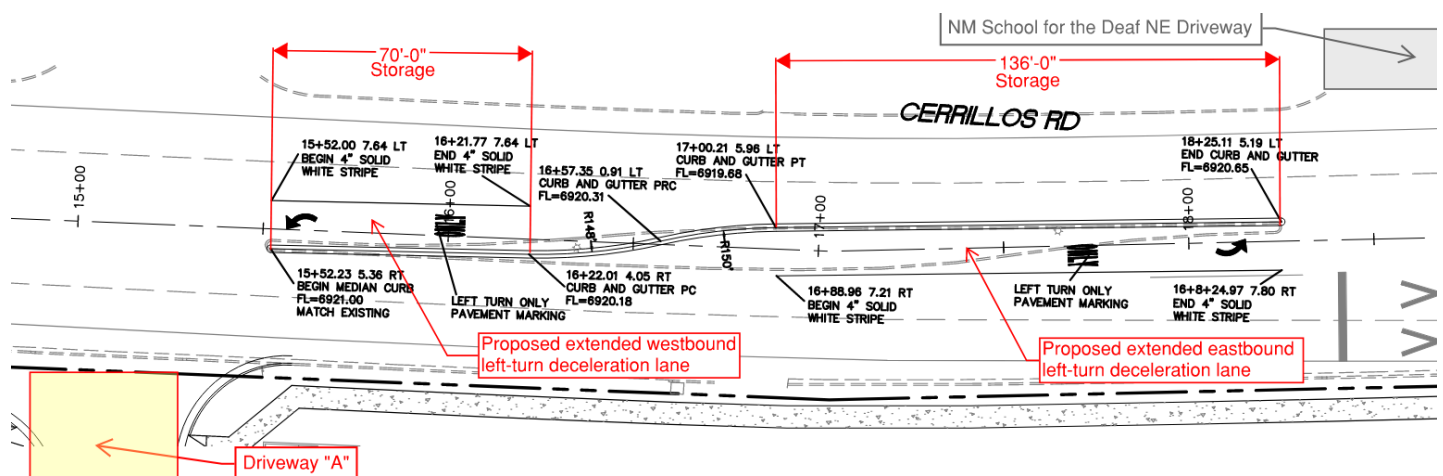
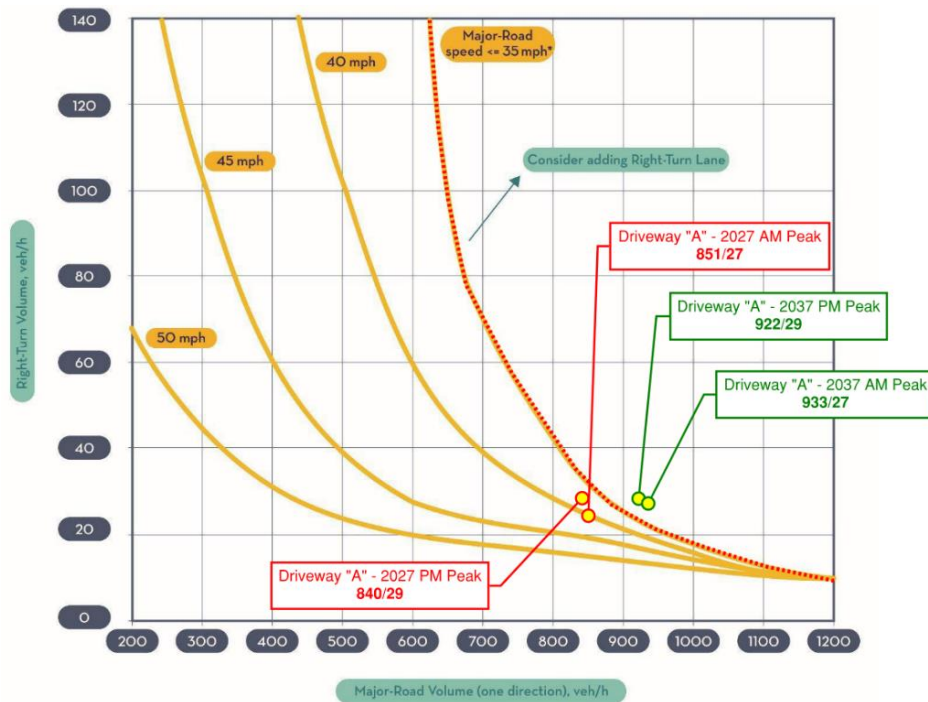


Figure 17 - Proposed Driveway "A" WB Left Turn Deceleration Lane

Per the CoSF TIA Guidelines for Right-Turn Lane Criteria for Four-Plus-Lane Roadways, shown below in Figure 18, an eastbound right-turn lane is **warranted**.



\*Right-turn lanes are not typically considered for roadways posted at 25 mph, but certain circumstances may warrant their consideration:

- Right-turning volume of 50 vph or greater
- At the City Traffic Engineer, or designee's discretion

Figure 18 - Right Turn Lane Criteria: Driveway "A"

According to the CoSF TIA Guidelines, for a 35 MPH speed limit, a right-turn deceleration lane should include a taper plus storage. Table 10 and Table 11 (Page 29) indicate a 95th-percentile queue of 0-ft; therefore, it is recommended to construct a westbound right-turn deceleration lane with a minimum 10-ft width and 50-ft storage plus 80-foot taper (8:1 ratio). However, to account for a majority of the entering trips being eastbound right-ins, it is **recommended to construct an eastbound right turn deceleration lane to include a storage length of 120-ft plus 150/150-ft reverse-curve transition** as shown in Figure 19.

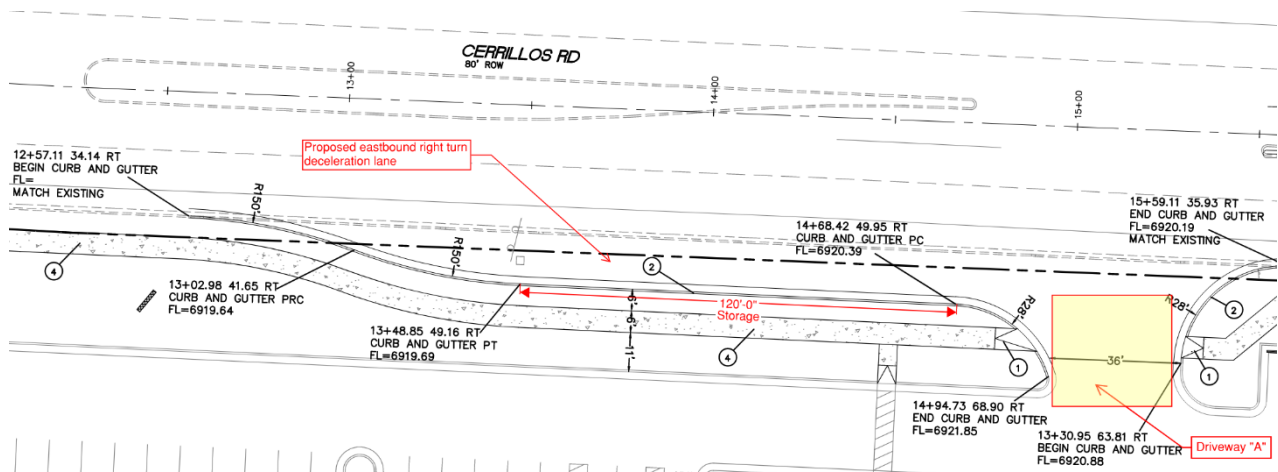
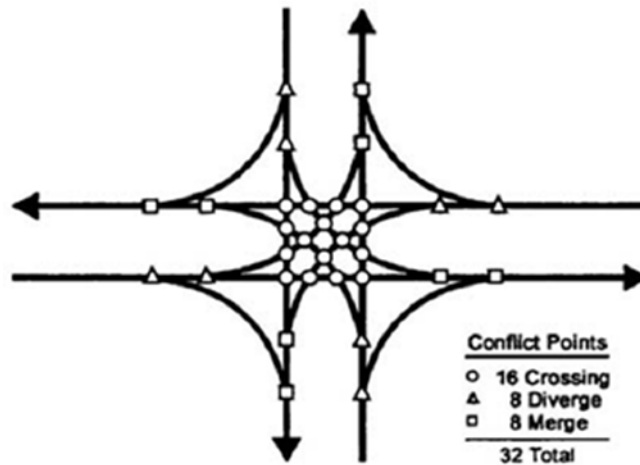


Figure 19 - Proposed Driveway "A" EB Right Turn Deceleration Lane

## Driveway “B” –

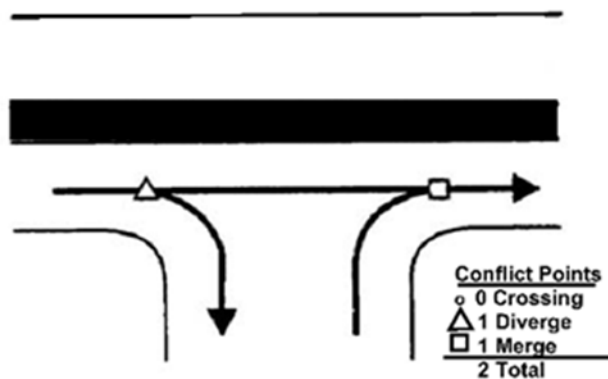
Conflict point analysis, per National Highway institute (NHI) Access Management guidelines, quantifies potential vehicle conflicts at at-grade intersections and driveways. A typical full-access 4-leg intersection contains 32 conflict points (Figure 20).



### Vehicular Conflicts

Figure 20 - 4-Leg Intersection Conflict Points

In contrast, the existing right-in/right-out driveway on the southeast side of Cerrillos Rd., referred to as Driveway “B,” introduces only two conflict points (Figure 21). By modifying the driveway to operate as right-out only, the number of conflict points is further reduced by half, to one (approximately 3% of a full-access intersection).



### Vehicular Conflicts

Figure 21 - Conflict Points with Raised Median

While this method assumes all conflict points carry equal risk, crash data indicate that certain movements contribute disproportionately to crash frequency and severity (see Figure 22). By prohibiting right-turn entering movements, approximately 90% of crash potential can be eliminated. Therefore, **it is recommended that Driveway “B” operates as a right-out only.**

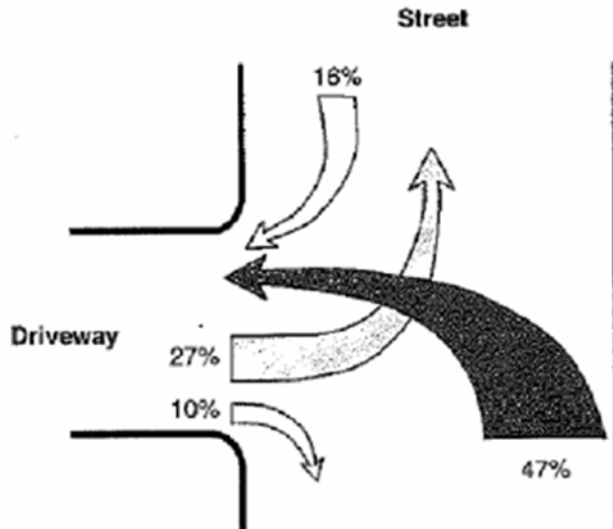


Figure 22 - Percentage of Crashes by Movement

Since right-in and left-in movements are proposed to be restricted at this driveway, no right-turn lane warrants are required as shown in Figure 23.

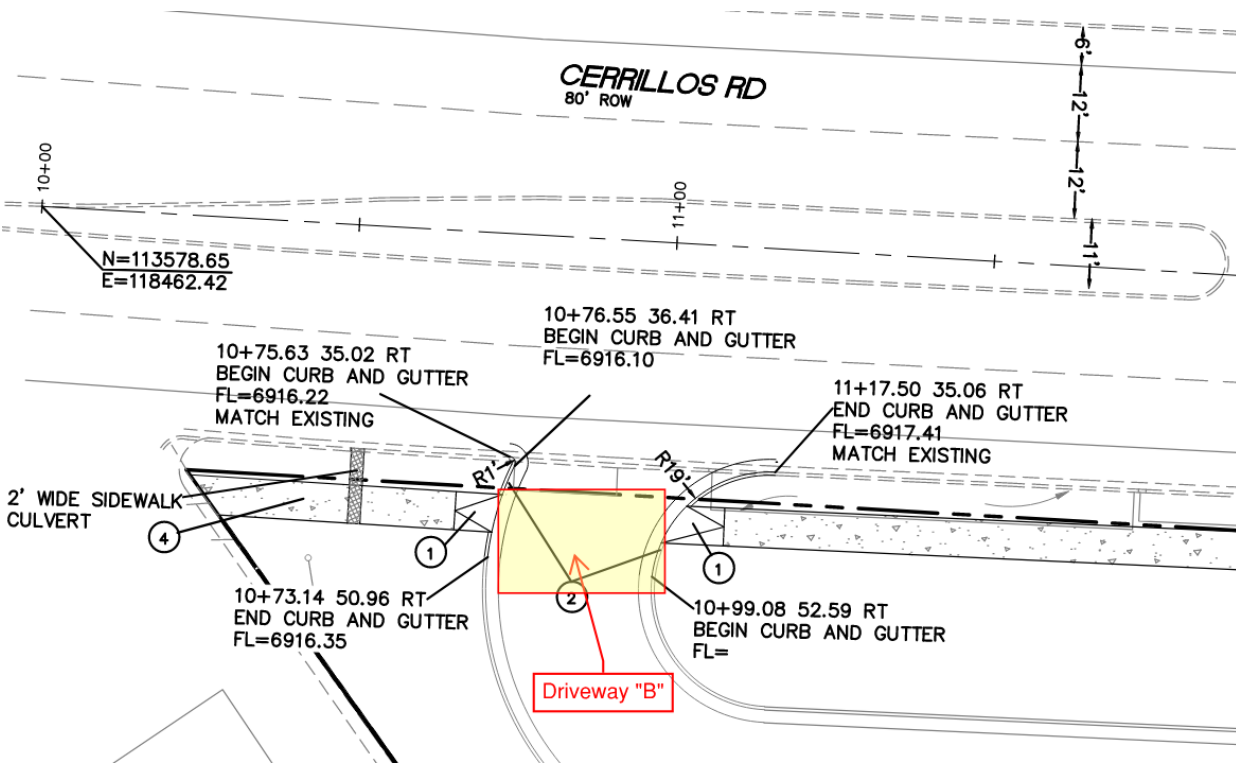


Figure 23 - Proposed Driveway "B"

See Appendix Page A-107 for detailed improvements on Cerrillos Rd.

## Multimodal Review

### Bicycle Evaluation

As noted in the Existing Roadway System Characteristics on Page 5 of this report, Cerrillos Rd. includes 5-ft wide bike lanes on both sides of the roadway from Cordova Rd. to approximately 950-ft northeast, where the facility transitions to a designated shared bike/vehicle lane accommodating higher traffic volumes and speeds. This condition justifies the application of both the Bicycle Level of Traffic Stress: Segments with a Bicycle Facility methodology and the Bicycle Level of Traffic Stress (BLTS): Mixed Traffic Segment methodology outlined in Appendix B of the CoSF TIA Guidelines. As shown in Figure 24 and Figure 25, Cerrillos Rd. exhibits a stress level of **BLTS 4**.

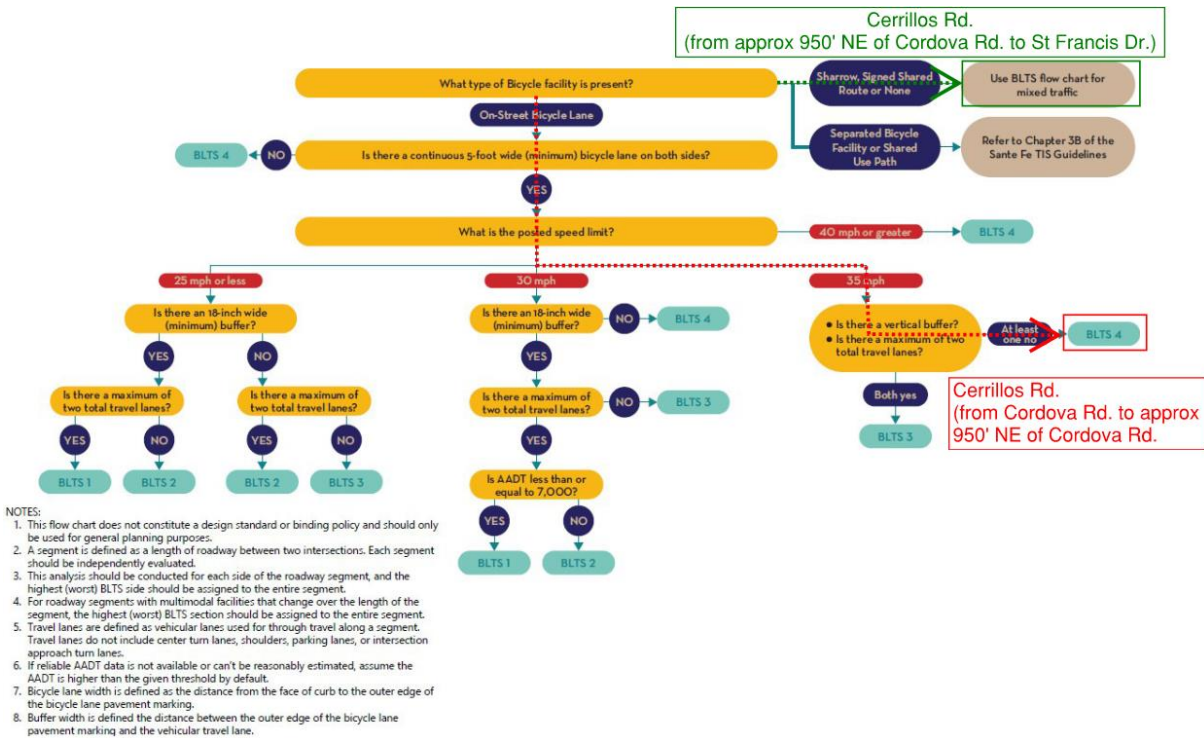


Figure 24 - Bicycle Level of Traffic Stress: Segments with a Bicycle Facility

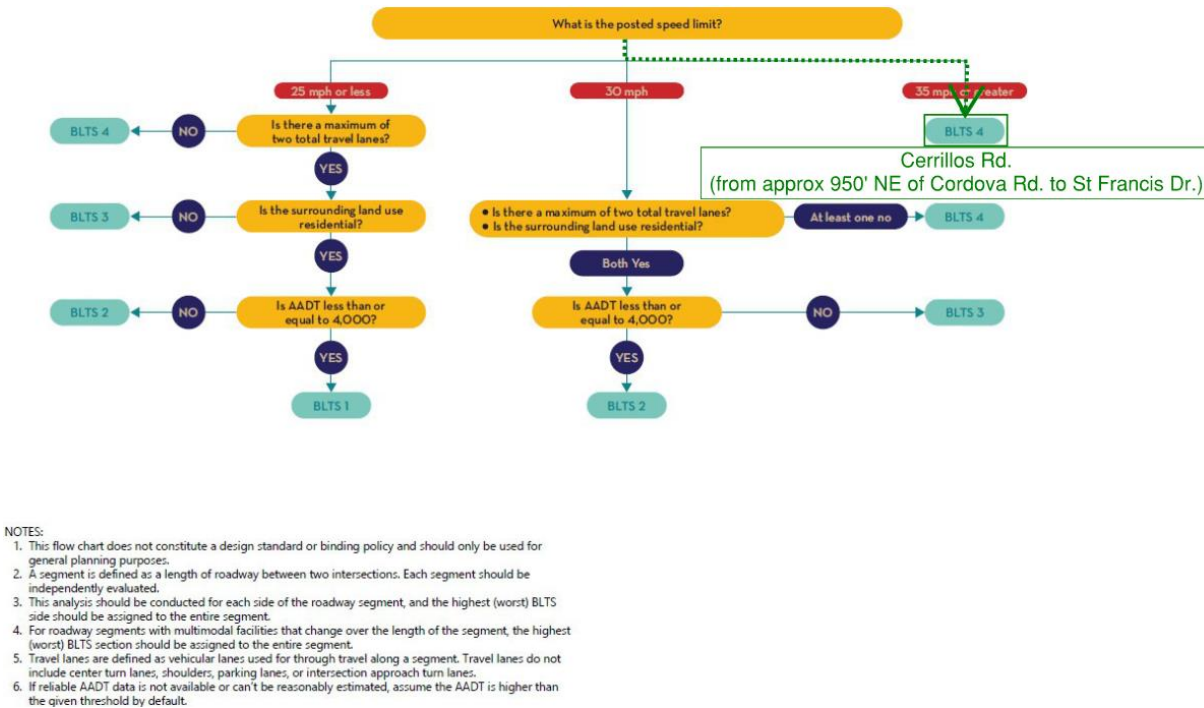
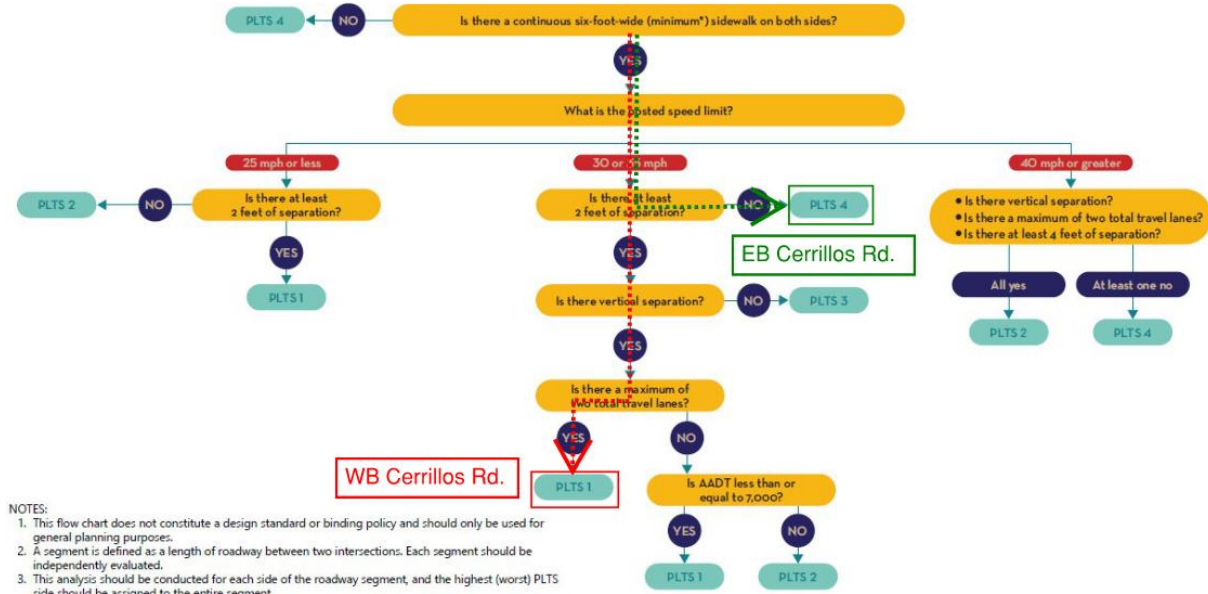


Figure 25 - Bicycle Level of Traffic Stress: Mixed Traffic Segments

## Pedestrian Evaluation

The Pedestrian Level of Traffic Stress (PLTS): Segment methodology outlined in Appendix B of the CoSF TIA guidelines was applied to evaluate the pedestrian level of traffic stress. The results are presented in Figure 26.



- NOTES:
1. This flow chart does not constitute a design standard or binding policy and should only be used for general planning purposes.
  2. A segment is defined as a length of roadway between two intersections. Each segment should be independently evaluated.
  3. This analysis should be conducted for each side of the roadway segment, and the highest (worst) PLTS side should be assigned to the entire segment.
  4. For roadway segments with multimodal facilities that change over the length of the segment, the highest (worst) PLTS section should be assigned to the entire segment.
  5. Travel lanes are defined as vehicular lanes used for through travel along a segment. Travel lanes do not include center turn lanes, shoulders, parking lanes, or intersection approach turn lanes.
  6. Separation is defined as the space between the edge of the adjacent vehicular travel lane and the sidewalk. This may include paved shoulders, bike lanes, planting strips, and on-street parking. Separation does not include curb and gutter.
  7. If reliable AADT data is not available or can't be reasonably estimated, assume the AADT is higher than the given threshold by default.

\*Refer to the Land Use Code for minimum sidewalk width requirement based on roadway classification

Figure 26 - Pedestrian Level of Traffic: Segments

The results above indicate that the westbound Cerrillos Rd. have a have a pedestrian level of traffic stress of **PLTS 1**, while the eastbound currently has a **PLTS 4**. The higher stress level on the eastbound side is attributed to the existing sidewalk lacking the minimum 2-foot separation from the roadway. It is therefore **recommended that the sidewalk fronting the subject property be reconstructed in accordance with the CoSF design standards for a four-lane Major Arterial, providing a minimum 5-foot setback and maintaining a 6-foot sidewalk width.**

As mentioned in this study, the existing New Mexico School for the Deaf is located across the street from the subject site. Westbound Cerrillos Rd. contains existing 6-foot sidewalks, resulting in a PLTS score of 1. Based on field observations, adequate pedestrian infrastructure is provided at the nearby signalized intersections (Cerrillos Rd. / St. Francis Dr. and Cerrillos Rd. / Cordova Rd.) to support school crossings. Therefore, **no additional improvements are recommended.**



## Summary of Deficiencies and Impacts

The proposed Marriott Hotel development is anticipated to have minimal impact on the adjacent transportation system in both the 2027 Implementation Year and the 2037 Horizon Year under the BUILD scenario. Traffic analysis confirms that intersection operations within the study area are expected to remain at LOS “D” or better, consistent with the thresholds defined in the CoSF TIA Guidelines. At the two signalized study intersections the addition of anticipated hotel traffic will not change the LOS for either the 2027 or 2037 No Build scenarios and will result in a maximum increase in delay at these two intersections of 0.4 seconds. Crash history indicates a low crash frequency; therefore, given the limited increase in project-generated trips, no safety mitigation measures are warranted.

The access design analysis determined the following:

- A westbound left-turn deceleration lane on Cerrillos Rd. at the northeast access (Driveway “A”) is warranted based on the CoSF TIS Guidelines, with corresponding design recommendations provided below.
- An eastbound right-turn deceleration lane on Cerrillos Rd. at the northeast access (Driveway “A”) is warranted based on the CoSF TIS Guidelines, with corresponding design recommendations provided below.
- The southwest access (Driveway “B”) is recommended to operate as a right-out only driveway to minimize potential vehicle conflict points.

During turning movement counts at Cerrillos Rd. / St. Francis St., it was observed that eastbound queuing on Cerrillos Rd. backed up to the proposed main driveway of the Marriott (also serving the School for the Deaf) coinciding with the passage of the Rail Runner through the signalized intersection. As traffic volumes continue to grow over time, this issue is expected to worsen. During these periods, westbound traffic on Cerrillos Rd. may be unable to turn left into the main driveway, potentially resulting in complaints to the City and/or NMDOT.

In addition, the multimodal review identified a deficiency along Cerrillos Road, where the Pedestrian Level of Traffic Stress (PLTS) is classified as Level 4; related improvement recommendations are provided below.

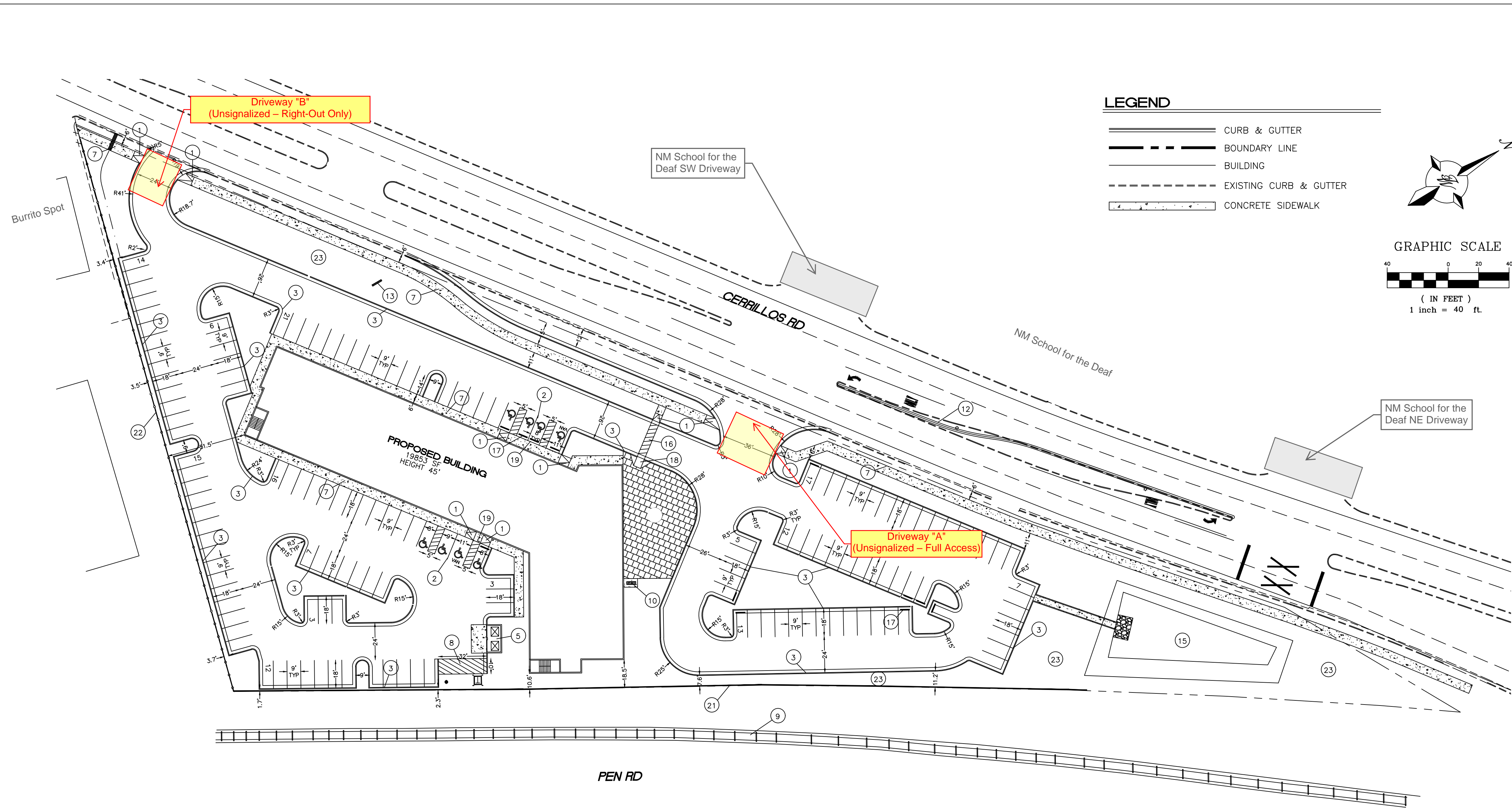
## Recommendations

- All design and construction of the project shall maintain adequate sight distances at driveways and intersections to the extent possible.
- Frontage Improvements
  - Construct a sidewalk along the project frontage in accordance with CoSF design standards for a four-lane Major Arterial, providing a minimum 5-ft setback and a 6-ft sidewalk width.
- Intersection #3: Cerrillos Rd. / NM School for the Deaf Northeast Driveway
  - Extend the existing eastbound left-turn lane to contain a storage length of 136-ft plus a 148/150 reverse-curve transition per NMDOT CN S100681.
- Intersection #4: Cerrillos Rd. / NM School for the Deaf Southwest Driveway / Driveway “A”
  - Shall operate as an unsignalized full-access driveway.
  - Extend the existing westbound left-turn deceleration lane to contain a storage length of 70-ft plus 150/148-ft reverse-curve transition per NMDOT CN S100681.

- Construct an eastbound right turn deceleration lane to include a storage length of 120-ft plus 150/150-ft reverse-curve transition.
- Intersection #5: Cerrillos Rd. / Driveway "B"
  - Shall operate as an unsignalized right-out only driveway.

## APPENDICES

<b>Site Information</b>	
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<b>Background Growth</b>	
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Historic Background Growth Graph	A-3 through A-5
<b>Trip Generation</b>	
Trip Generation Summary & Worksheets	A-6 & A-7
<b>2027 Turning Movement Counts</b>	
Turning Movement Volumes Summary Sheet	A-8 & A-9
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Intersection 2 - Cerrillos Rd. / Cordova Rd.	A-12 & A-13
Intersection 3 - Cerrillos Rd. / NMSFTD NE Dwy	A-14 & A-15
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Intersection 5 - Cerrillos Rd. / Driveway "B"	A-18 & A-19
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VICINITY MAP

**LEGAL DESCRIPTION:**  
 1000 CERRILLOS ROAD, SPECIAL WARRANTY DEED RECORDED IN Bk. 1630, Pg.649 AND PLAT Bk. 435, Pg.37, 1001 AND 1003 CERRILLOS ROAD, WARRANTY DEED RECORDED AS INSTRUMENT #1704193, AND PLAT Bk.651, Pg.03,

**SITE DATA**

PROPOSED USE HOTEL  
 LOT AREA 148122 SF (3.40 ACRES)  
 ZONING C-2  
 ROOFED AREA 19853 GSF  
 LOT COVERAGE 13.4%

**BUILDING SETBACK**

STREET 15'  
 REAR 10'  
 SIDE 0'  
 GFA 79491 SF

PARKING REQUIRED 150 SPACES (1 PER HOTEL ROOM)  
 PARKING PROVIDED 151 SPACES  
 ACCESSIBLE PARKING REQUIRED 8 SPACES (2 VAN)  
 ACCESSIBLE SPACES PROVIDED 8 SPACES (2 VAN)

BICYCLE SPACES REQUIRED 6 (1 PER 15 ROOMS OR 6 IF MORE THAN 75 ROOMS)  
 BICYCLE SPACES PROVIDED 6

OPEN SPACE REQUIRED 20% 0.68 AC  
 OPEN SPACE PROVIDED 25.3% 0.86 AC

**KEYED NOTES**

- 1 UNIDIRECTIONAL ACCESSIBLE RAMP SEE DETAIL SHEET DET-8
- 2 ACCESSIBLE PARKING PER ADA REQUIREMENTS (5)
- 3 STANDARD CURB AND GUTTER, SEE DETAIL SHEET DET-8
- 4 CONCRETE SIDEWALK
- 5 DOUBLE DUMPSTER
- 6 STAMPED CONCRETE, SEE ARCHITECTURAL PLANS FOR PATTERN AND COLOR
- 7 NEW 6" CONCRETE SIDEWALK PER CITY OF SANTA FE STANDARDS
- 8 LOADING ZONE
- 9 EXISTING RAILROAD TRACK
- 10 BICYCLE RACK (6)
- 11 SITE LIGHTING
- 12 MOUNTABLE MEDIAN CURB TYPE A PER NMDOT 609-01-1/1
- 13 MONUMENT SIGN
- 14 DIRECTIONAL SIGN
- 15 DRAINAGE POND
- 16 6" PEDESTRIAN CROSSWALK
- 17 CONCRETE WHEEL STOP (TYP)
- 18 ACCESSIBLE RAMP SEE DETAIL SHEET DET 8
- 19 ZERO CURB
- 20 TRANSFORMER
- 21 8" TALL SCREEN/SOUND ( 560 LF) WALL SEE DETAIL SHEET 11
- 22 WROUGHT IRON FENCE (375 LF) SEE DETAIL SHEET 11
- 23 OPEN SPACE

ENGINEER'S SEAL	1000, 1101, 1103 CERRILLOS RD SANTA FE, NM	DRAWN BY pm
	DEVELOPMENT PLAN	DATE 11-27-25
RONALD R. BOHANNAN P.E. #7868	TIERRA WEST, LLC 5571 MIDWAY PARK PL. NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	DRAWING
		SHEET #
		JOB # 2025080

### Historic Growth Data Table

#### Marriott Hotel (1000 Cerrillos Rd.)

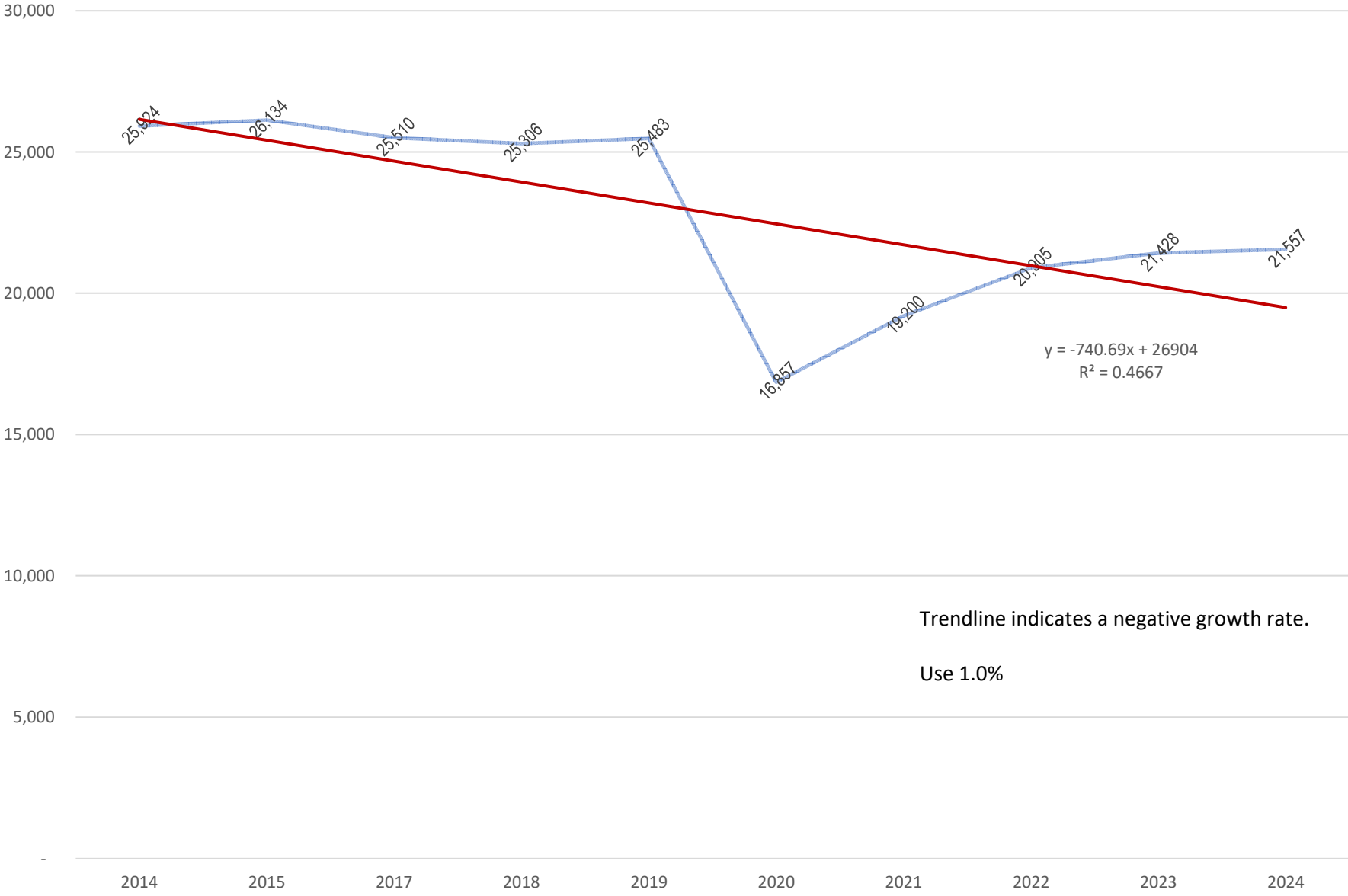
Traffic Flows (AWDT) from Mid-Region Council of Governments

COG ID	Location		2014	2015	2017	2018	2019	2020	2021	2022	2023	2024
Street #1:	<b>Cerrillos Rd.</b>											
	<b>Street:</b>	<b>Location:</b>										
<b>12097</b>	Cerrillos Rd.	Between Cordova Rd and St Fr.	25,924	26,134	25,510	25,306	25,483	16,857	19,200	20,905	21,428	21,557
	<b>Total Intersection Traffic Flows</b>		<b>25,924</b>	<b>26,134</b>	<b>25,510</b>	<b>25,306</b>	<b>25,483</b>	<b>16,857</b>	<b>19,200</b>	<b>20,905</b>	<b>21,428</b>	<b>21,557</b>

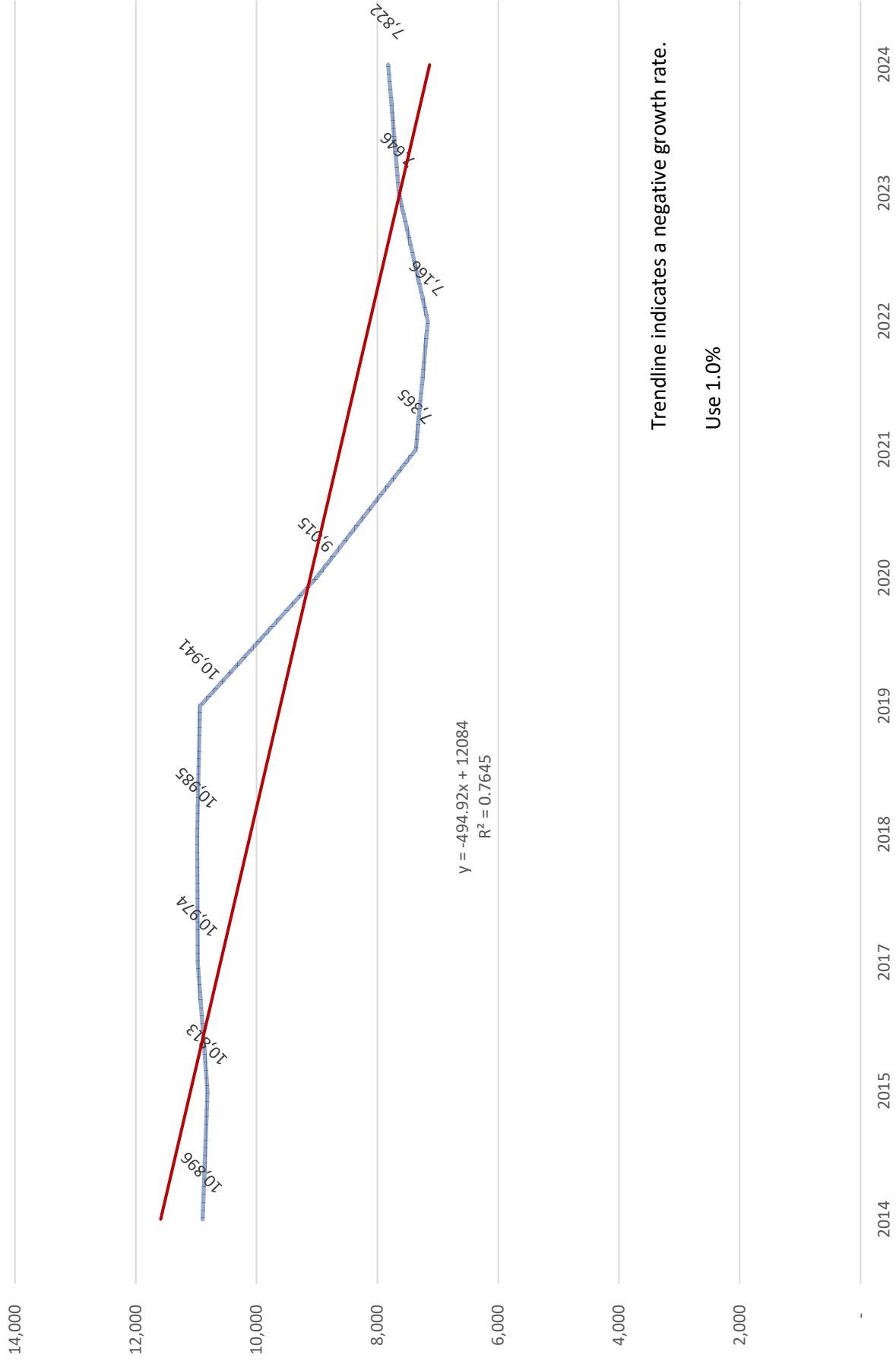
COG ID	Location		2014	2015	2017	2018	2019	2020	2021	2022	2023	2024
Street #2:	<b>Cordova Rd.</b>											
	<b>Street:</b>	<b>From:</b>										
<b>12050</b>	Cordova Rd.	455' east of Cerrillos Rd.	10,896	10,813	10,974	10,985	10,941	9,015	7,365	7,166	7,646	7,822
	<b>Total Intersection Traffic Flows</b>		<b>10,896</b>	<b>10,813</b>	<b>10,974</b>	<b>10,985</b>	<b>10,941</b>	<b>9,015</b>	<b>7,365</b>	<b>7,166</b>	<b>7,646</b>	<b>7,822</b>

COG ID	Location		2014	2015	2017	2018	2019	2020	2021	2022	2023	2024
Street #3:	<b>St Francis Dr.</b>											
	<b>Street:</b>	<b>From:</b>										
<b>4832</b>	St Francis Dr.	765' NW of Cerrillos Rd.	50,637	51,047	50,081	49,680	50,028	40,873	28,524	29,180	29,910	39,184
	<b>Total Intersection Traffic Flows</b>		<b>50,637</b>	<b>51,047</b>	<b>50,081</b>	<b>49,680</b>	<b>50,028</b>	<b>40,873</b>	<b>28,524</b>	<b>29,180</b>	<b>29,910</b>	<b>39,184</b>

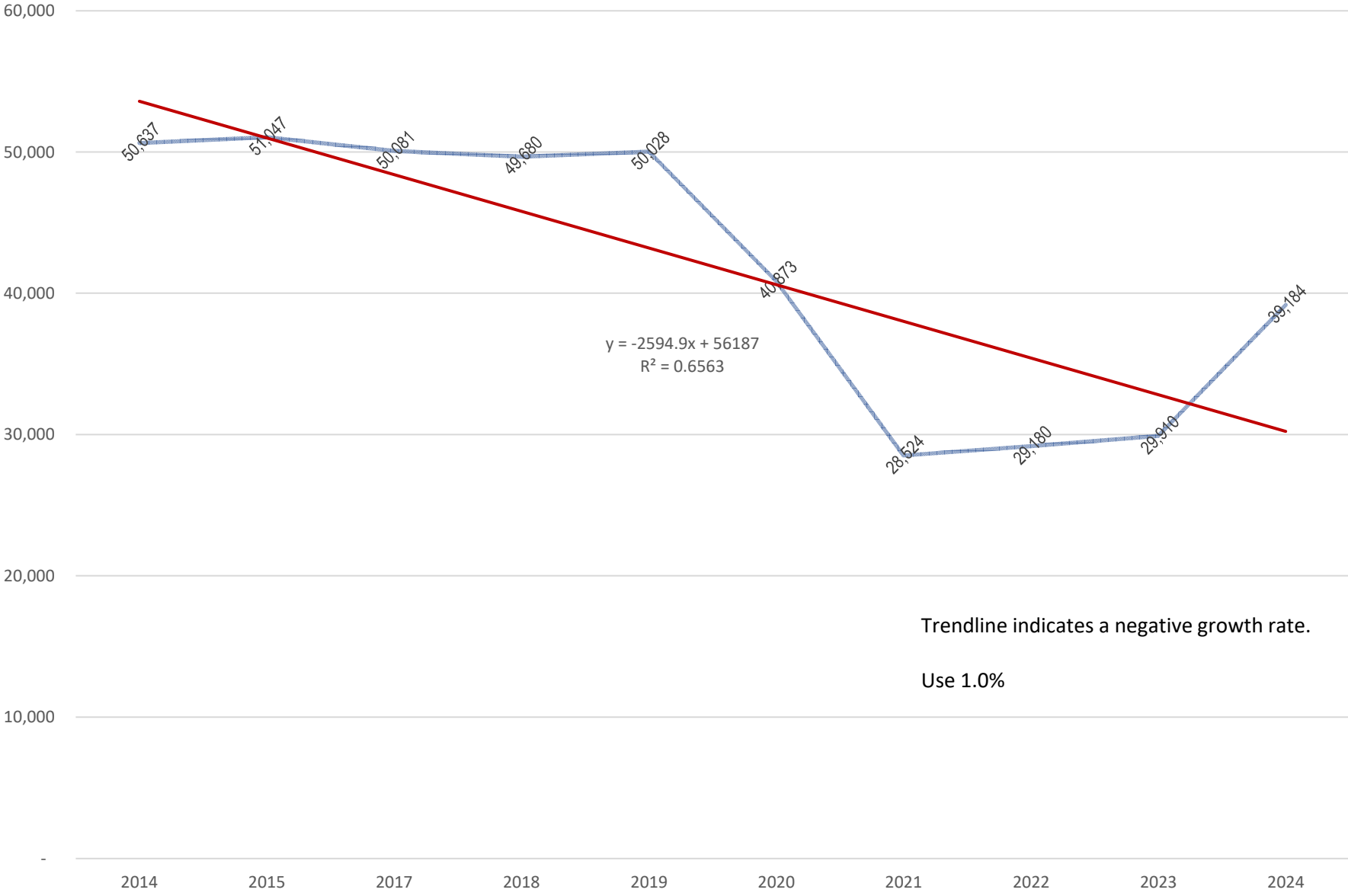
Historic Traffic Flow Graph  
Street #1: Cerrillos Rd.



### Historic Traffic Flow Graph Street #2: Cordova Rd.



Historic Traffic Flow Graph  
Street #3: St Francis Dr.



*Marriott Hotel (1000 Cerrillos Rd.)*

**Trip Generation Data (ITE Trip Generation Manual - 11th Edition)**

USE (ITE CODE)	24 HR VOL	A. M. PEAK HR.		P. M. PEAK HR.	
<i>DESCRIPTION</i>	<i>GROSS</i>	<i>ENTER</i>	<i>EXIT</i>	<i>ENTER</i>	<i>EXIT</i>
<b>Summary Sheet</b>					
	Units				
Hotel (310)	150	1,202	38	30	42
				41	

*Marriott Hotel (1000 Cerrillos Rd.)  
Trip Generation Data (ITE Trip Generation Manual - 11th Edition)*

USE (ITE CODE)	24 HOUR TWO-WAY VOLUME	A. M. PEAK HOUR		P. M. PEAK HOUR		
		GROSS	ENTER	EXIT	ENTER	EXIT
<i>Hotel (310)</i>	<b>150</b>	1,202	38	30	42	41

Units

Rooms

**ITE Trip Generation Equations:**

$$T = 10.84 (X) + -423.51$$

50% Enter,                      50% Exit

$$T = 0.5 (X) + -7.45$$

56% Enter,                      44% Exit

$$T = 0.74 (X) + -27.89$$

51% Enter,                      49% Exit

Comments:

Tract No.

Based on ITE Trip Generation Manual - 11th Edition

**Marriott Hotel (1000 Cerrillos Rd.)**

Projected Turning Movements SUMMARY  
**PROPOSED DEVELOPMENT (2027) - 100% Development**

**INTERSECTION : Summary**

**Cerrillos Rd / St Francis Dr.**

0.92

PHF

		Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
(1)	0.0% H.C.												
<b>Existing (2025)</b>		360	458	18	210	205	26	0	1,073	527	0	1,293	589
<b>2027 (NO BUILD - A.M.)</b>		367	467	18	214	209	27	0	1,094	538	0	1,319	601
<b>2027 (BUILD - A.M.)</b>		<b>373</b>	<b>470</b>	<b>24</b>	<b>214</b>	<b>212</b>	<b>27</b>	<b>0</b>	<b>1,094</b>	<b>538</b>	<b>0</b>	<b>1,319</b>	<b>609</b>

0.95

PHF

		Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
<b>Existing (2025)</b>		476	314	19	414	528	37	0	1,431	387	0	1,089	547
<b>2027 (NO BUILD - P.M.)</b>		486	320	19	422	539	38	0	1,460	395	0	1,111	558
<b>2027 (BUILD - P.M.)</b>		<b>495</b>	<b>324</b>	<b>27</b>	<b>422</b>	<b>543</b>	<b>38</b>	<b>0</b>	<b>1,460</b>	<b>395</b>	<b>0</b>	<b>1,111</b>	<b>567</b>

**Cerrillos Rd / Cordova Rd**

0.91

PHF

		Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
(2)	0.0% H.C.												
<b>Existing (2025)</b>		0	793	297	140	694	0	217	0	56	0	0	0
<b>2027 (NO BUILD - A.M.)</b>		0	809	303	143	708	0	221	0	57	0	0	0
<b>2027 (BUILD - A.M.)</b>		<b>0</b>	<b>823</b>	<b>303</b>	<b>147</b>	<b>719</b>	<b>0</b>	<b>221</b>	<b>0</b>	<b>69</b>	<b>0</b>	<b>0</b>	<b>0</b>

0.94

PHF

		Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
<b>Existing (2025)</b>		0	745	197	60	1,037	0	338	0	49	0	0	0
<b>2027 (NO BUILD - P.M.)</b>		0	760	201	61	1,058	0	345	0	50	0	0	0
<b>2027 (BUILD - P.M.)</b>		<b>0</b>	<b>776</b>	<b>201</b>	<b>66</b>	<b>1,074</b>	<b>0</b>	<b>345</b>	<b>0</b>	<b>63</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Cerrillos Rd / NMSFTD NE Dwy**

0.90

PHF

		Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
(3)	0.0% H.C.												
<b>Existing (2025)</b>		18	814	0	0	825	4	0	0	0	0	0	8
<b>2027 (NO BUILD - A.M.)</b>		18	830	0	0	842	4	0	0	0	0	0	8
<b>2027 (BUILD - A.M.)</b>		<b>18</b>	<b>845</b>	<b>0</b>	<b>0</b>	<b>853</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>

0.94

PHF

		Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
		Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
<b>Existing (2025)</b>		2	792	0	0	1,064	1	0	0	0	3	0	8
<b>2027 (NO BUILD - P.M.)</b>		2	808	0	0	1,085	1	0	0	0	3	0	8
<b>2027 (BUILD - P.M.)</b>		<b>2</b>	<b>829</b>	<b>0</b>	<b>0</b>	<b>1,098</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>8</b>

**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements SUMMARY  
**PROPOSED DEVELOPMENT (2027) - 100% Development**

**INTERSECTION: Summary**

**Cerrillos Rd / Driveway "A"**

0.90

PHF

(4)  
 0.0% H.C.  
**Existing (2025)**  
 2027 (NO BUILD - A.M.)  
 2027 (BUILD - A.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
35	828	0	0	817	11	0	0	0	1	0	15
36	845	0	0	833	11	0	0	0	1	0	15
36	851	27	11	833	11	15	0	9	1	0	15

0.94

PHF

**Existing (2025)**  
 2027 (NO BUILD - P.M.)  
 2027 (BUILD - P.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
9	816	0	0	1,032	6	0	0	0	5	0	16
9	832	0	0	1,053	6	0	0	0	5	0	16
9	840	29	13	1,053	6	21	0	12	5	0	16

**Cerrillos Rd / Driveway "B"**

0.91

PHF

(5)  
 0.0% H.C.  
**Existing (2025)**  
 2027 (NO BUILD - A.M.)  
 2027 (BUILD - A.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
0	0	0	0	0	0	0	0	0	0	0	0
0	881	0	0	848	0	0	0	0	0	0	0
0	908	0	0	863	0	0	0	6	0	0	0

0.94

PHF

**Existing (2025)**  
 2027 (NO BUILD - P.M.)  
 2027 (BUILD - P.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
0	0	0	0	0	0	0	0	0	0	0	0
0	841	0	0	1,069	0	0	0	0	0	0	0
0	870	0	0	1,090	0	0	0	8	0	0	0

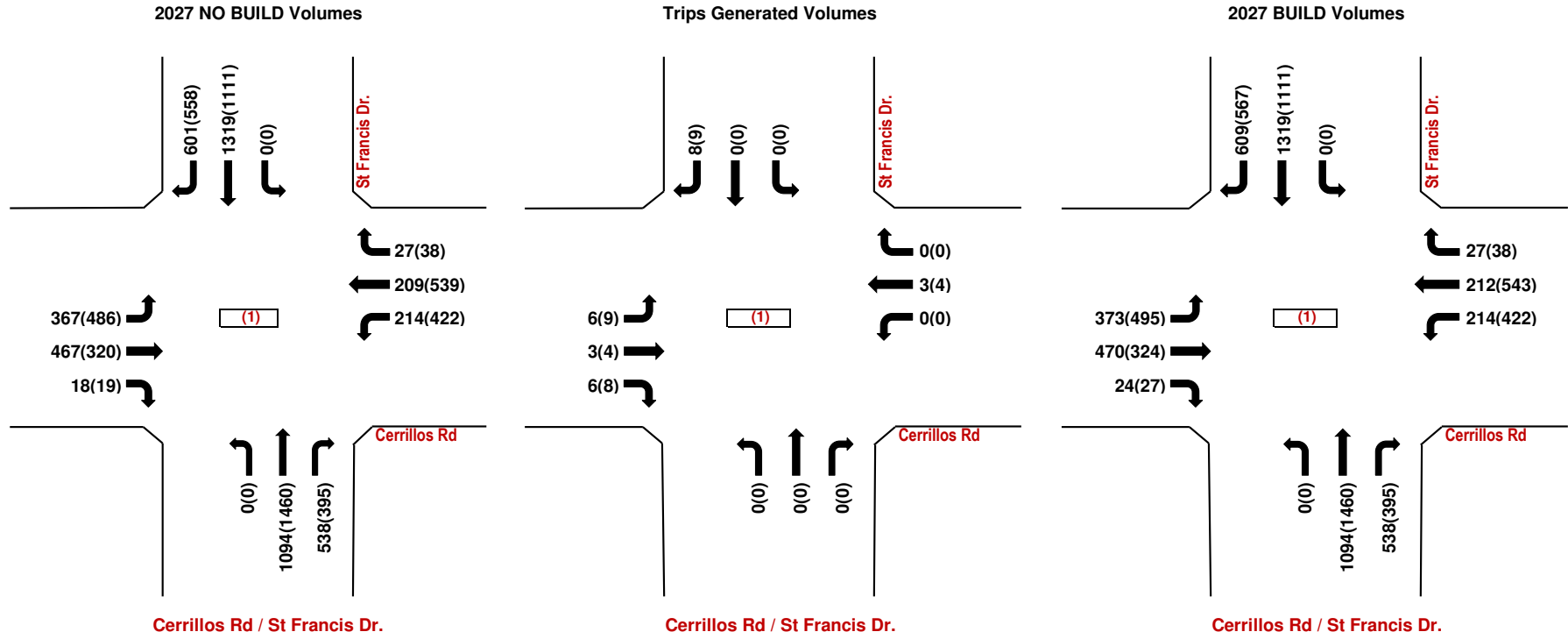
**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / St Francis Dr.**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (1)  
 N-S Street: **St Francis Dr.**  
 Year of Existing Counts 2025  
 Implementation Year 2027

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	360	458	18	210	205	26	0	1,073	527	0	1,293	589
Background Traffic Growth	7	9	0	4	4	1	0	21	11	0	26	12
<i>Subtotal</i>	367	467	18	214	209	27	0	1,094	538	0	1,319	601
<b>Subtotal (NO BUILD - A.M.)</b>	<b>367</b>	<b>467</b>	<b>18</b>	<b>214</b>	<b>209</b>	<b>27</b>	<b>0</b>	<b>1,094</b>	<b>538</b>	<b>0</b>	<b>1,319</b>	<b>601</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	9.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	21.00%
Percent Commercial Trips Generated(Exiting)	21.00%	9.00%	20.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	6	3	6	0	3	0	0	0	0	0	0	8
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>373</b>	<b>470</b>	<b>24</b>	<b>214</b>	<b>212</b>	<b>27</b>	<b>0</b>	<b>1,094</b>	<b>538</b>	<b>0</b>	<b>1,319</b>	<b>609</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>373</b>	<b>470</b>	<b>24</b>	<b>214</b>	<b>212</b>	<b>27</b>	<b>0</b>	<b>1,094</b>	<b>538</b>	<b>0</b>	<b>1,319</b>	<b>609</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	476	314	19	414	528	37	0	1,431	387	0	1,089	547
Background Traffic Growth	10	6	0	8	11	1	0	29	8	0	22	11
<i>Subtotal</i>	486	320	19	422	539	38	0	1,460	395	0	1,111	558
<b>Subtotal (NO BUILD - P.M.)</b>	<b>486</b>	<b>320</b>	<b>19</b>	<b>422</b>	<b>539</b>	<b>38</b>	<b>0</b>	<b>1,460</b>	<b>395</b>	<b>0</b>	<b>1,111</b>	<b>558</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	9.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	21.00%
Percent Commercial Trips Generated(Exiting)	21.00%	9.00%	20.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	9	4	8	0	4	0	0	0	0	0	0	9
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>495</b>	<b>324</b>	<b>27</b>	<b>422</b>	<b>543</b>	<b>38</b>	<b>0</b>	<b>1,460</b>	<b>395</b>	<b>0</b>	<b>1,111</b>	<b>567</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>495</b>	<b>324</b>	<b>27</b>	<b>422</b>	<b>543</b>	<b>38</b>	<b>0</b>	<b>1,460</b>	<b>395</b>	<b>0</b>	<b>1,111</b>	<b>567</b>

Number of Commercial Trips Generated	Entering	Exiting	A.M.	100% Commercial Development
	38	30		
	42	41	P.M.	



AM(PM)

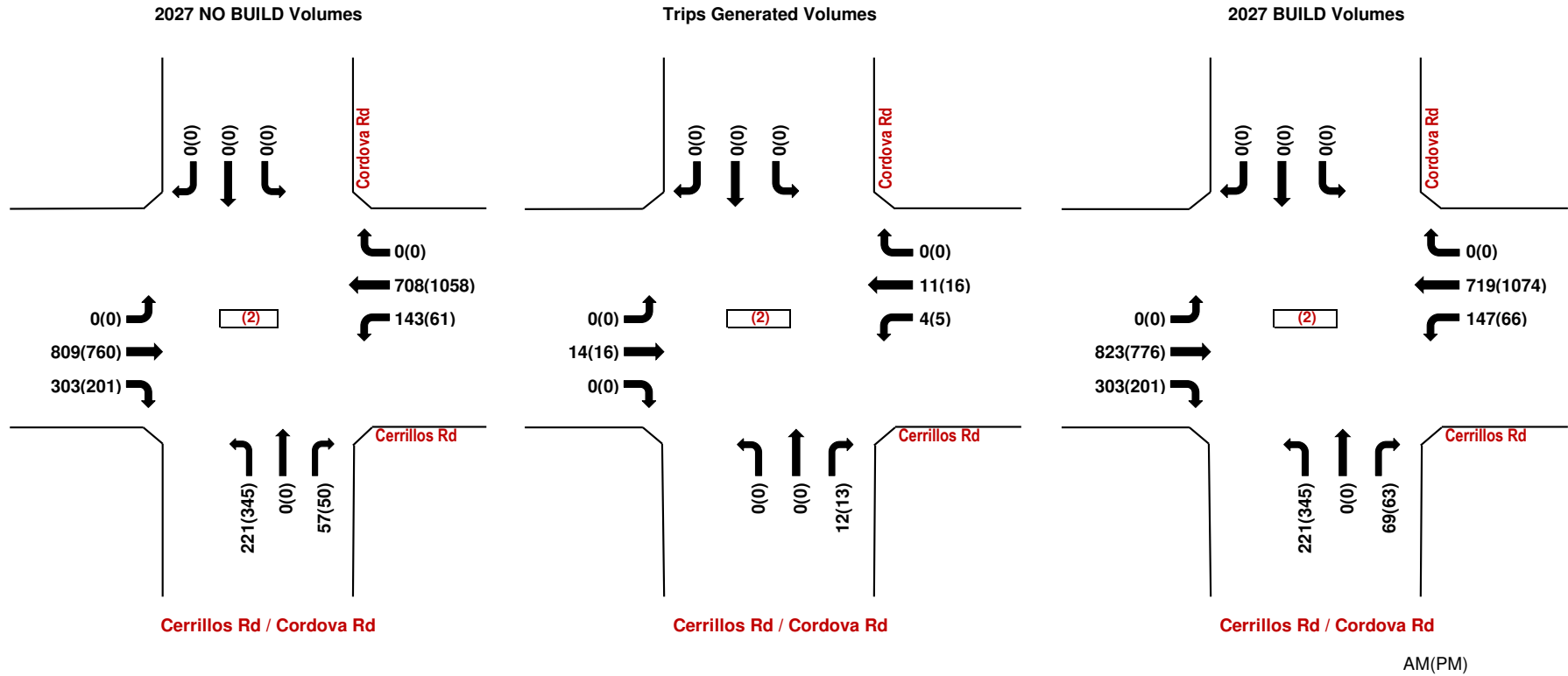
**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / Cordova Rd**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (2)  
 N-S Street: **Cordova Rd**  
 Year of Existing Counts: 2025  
 Implementation Year: **2027**  
 Growth Rates: 1.00%

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	793	297	140	694	0	217	0	56	0	0	0
Background Traffic Growth	0	16	6	3	14	0	4	0	1	0	0	0
Subtotal	0	809	303	143	708	0	221	0	57	0	0	0
<b>Subtotal (NO BUILD - A.M.)</b>	<b>0</b>	<b>809</b>	<b>303</b>	<b>143</b>	<b>708</b>	<b>0</b>	<b>221</b>	<b>0</b>	<b>57</b>	<b>0</b>	<b>0</b>	<b>0</b>
Percent Commercial Trips Generated(Entering)	0.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	32.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	12.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	14	0	4	11	0	0	0	12	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>0</b>	<b>823</b>	<b>303</b>	<b>147</b>	<b>719</b>	<b>0</b>	<b>221</b>	<b>0</b>	<b>69</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>0</b>	<b>823</b>	<b>303</b>	<b>147</b>	<b>719</b>	<b>0</b>	<b>221</b>	<b>0</b>	<b>69</b>	<b>0</b>	<b>0</b>	<b>0</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	745	197	60	1,037	0	338	0	49	0	0	0
Background Traffic Growth	0	15	4	1	21	0	7	0	1	0	0	0
Subtotal	0	760	201	61	1,058	0	345	0	50	0	0	0
<b>Subtotal (NO BUILD - P.M.)</b>	<b>0</b>	<b>760</b>	<b>201</b>	<b>61</b>	<b>1,058</b>	<b>0</b>	<b>345</b>	<b>0</b>	<b>50</b>	<b>0</b>	<b>0</b>	<b>0</b>
Percent Commercial Trips Generated(Entering)	0.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	32.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	12.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	16	0	5	16	0	0	0	13	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>0</b>	<b>776</b>	<b>201</b>	<b>66</b>	<b>1,074</b>	<b>0</b>	<b>345</b>	<b>0</b>	<b>63</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>0</b>	<b>776</b>	<b>201</b>	<b>66</b>	<b>1,074</b>	<b>0</b>	<b>345</b>	<b>0</b>	<b>63</b>	<b>0</b>	<b>0</b>	<b>0</b>

Number of Commercial Trips Generated: Entering 38, Exiting 30 A.M. 100% Commercial Development  
 Entering 42, Exiting 41 P.M.



**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / NMSFTD NE Dwy**

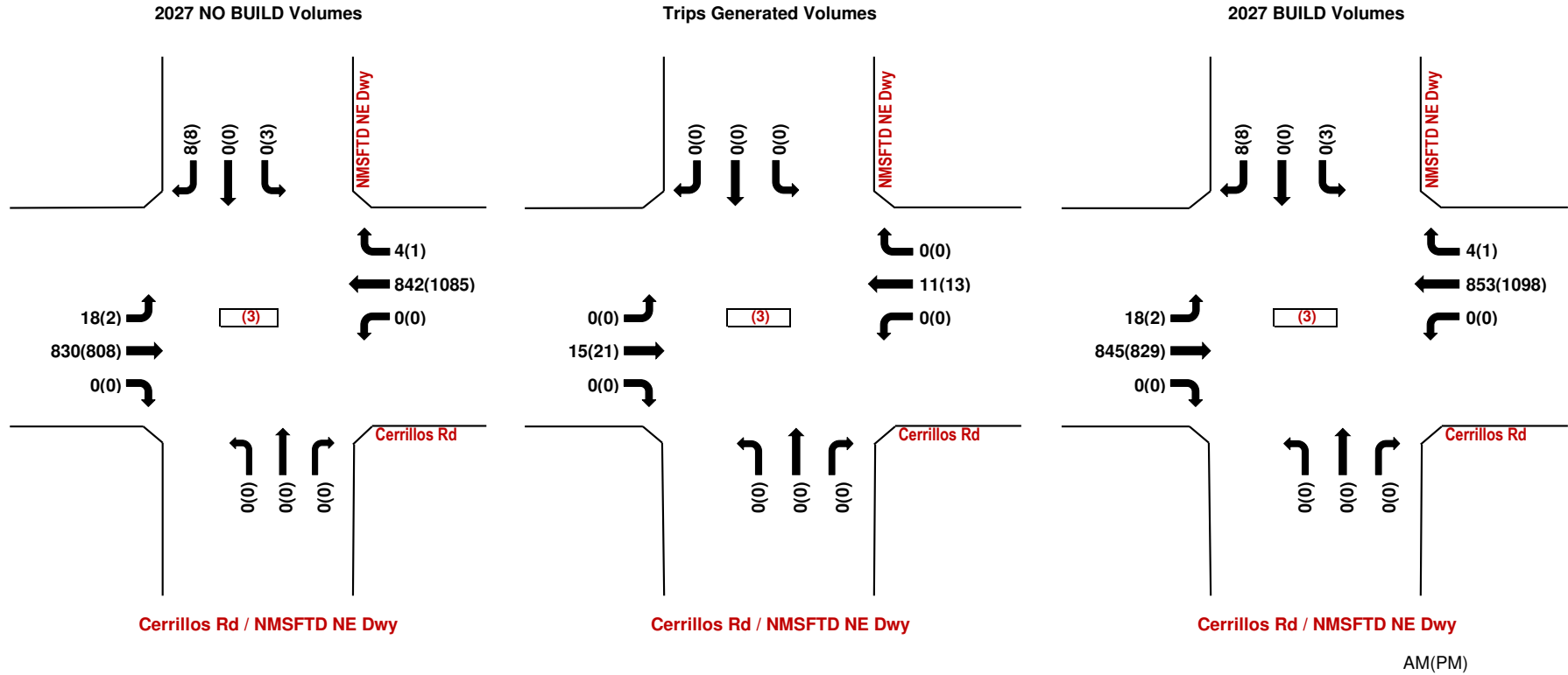
**INTERSECTION :** E-W Street: **Cerrillos Rd** (3)  
 N-S Street: **NMSFTD NE Dwy**  
 Year of Existing Counts: 2025  
 Implementation Year: **2027**  
 Growth Rates: 1.00%

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	18	814	0	0	825	4	0	0	0	0	0	8
Background Traffic Growth	0	16	0	0	17	0	0	0	0	0	0	0
Subtotal	18	830	0	0	842	4	0	0	0	0	0	8
<b>Subtotal (NO BUILD - A.M.)</b>	<b>18</b>	<b>830</b>	<b>0</b>	<b>0</b>	<b>842</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	15	0	0	11	0	0	0	0	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>18</b>	<b>845</b>	<b>0</b>	<b>0</b>	<b>853</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>18</b>	<b>845</b>	<b>0</b>	<b>0</b>	<b>853</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	2	792	0	0	1,064	1	0	0	0	3	0	8
Background Traffic Growth	0	16	0	0	21	0	0	0	0	0	0	0
Subtotal	2	808	0	0	1,085	1	0	0	0	3	0	8
<b>Subtotal (NO BUILD - P.M.)</b>	<b>2</b>	<b>808</b>	<b>0</b>	<b>0</b>	<b>1,085</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>8</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	21	0	0	13	0	0	0	0	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>2</b>	<b>829</b>	<b>0</b>	<b>0</b>	<b>1,098</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>8</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>2</b>	<b>829</b>	<b>0</b>	<b>0</b>	<b>1,098</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>8</b>

Number of Commercial Trips Generated

Entering	38	30	A.M.	100% Commercial Development
Exiting	42	41	P.M.	



**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / Driveway "A"**

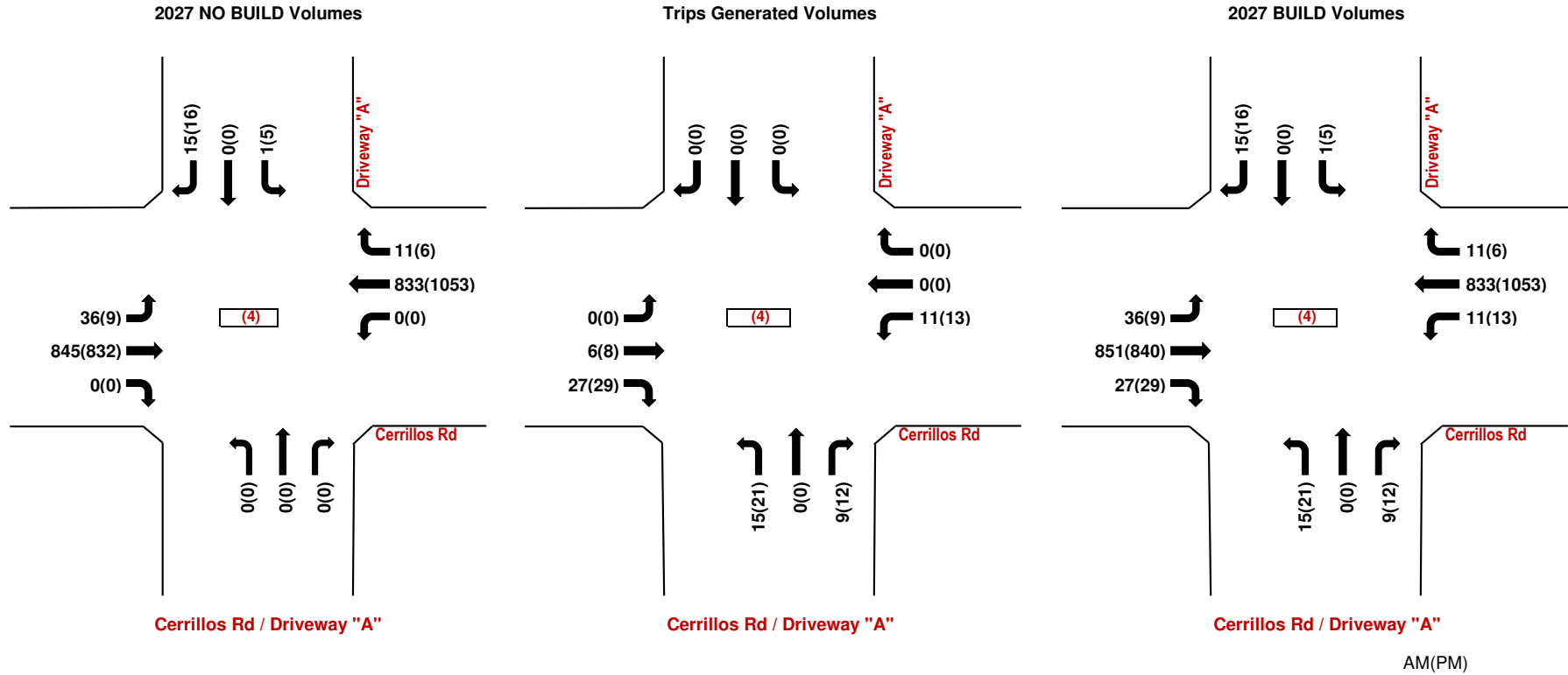
**INTERSECTION :** E-W Street: **Cerrillos Rd** (4)  
 N-S Street: **Driveway "A"**  
 Year of Existing Counts: 2025  
 Implementation Year: **2027**

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	35	828	0	0	817	11	0	0	0	1	0	15
Background Traffic Growth	1	17	0	0	16	0	0	0	0	0	0	0
<i>Subtotal</i>	36	845	0	0	833	11	0	0	0	1	0	15
<b>Subtotal (NO BUILD - A.M.)</b>	<b>36</b>	<b>845</b>	<b>0</b>	<b>0</b>	<b>833</b>	<b>11</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>15</b>
<i>Percent Commercial Trips Generated(Entering)</i>	0.00%	0.00%	70.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
<i>Percent Commercial Trips Generated(Exiting)</i>	0.00%	20.00%	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	30.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	6	27	11	0	0	15	0	9	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>36</b>	<b>851</b>	<b>27</b>	<b>11</b>	<b>833</b>	<b>11</b>	<b>15</b>	<b>0</b>	<b>9</b>	<b>1</b>	<b>0</b>	<b>15</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>36</b>	<b>851</b>	<b>27</b>	<b>11</b>	<b>833</b>	<b>11</b>	<b>15</b>	<b>0</b>	<b>9</b>	<b>1</b>	<b>0</b>	<b>15</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	9	816	0	0	1,032	6	0	0	0	5	0	16
Background Traffic Growth	0	16	0	0	21	0	0	0	0	0	0	0
<i>Subtotal</i>	9	832	0	0	1,053	6	0	0	0	5	0	16
<b>Subtotal (NO BUILD - P.M.)</b>	<b>9</b>	<b>832</b>	<b>0</b>	<b>0</b>	<b>1,053</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>16</b>
<i>Percent Commercial Trips Generated(Entering)</i>	0.00%	0.00%	70.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
<i>Percent Commercial Trips Generated(Exiting)</i>	0.00%	20.00%	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	30.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	8	29	13	0	0	21	0	12	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>9</b>	<b>840</b>	<b>29</b>	<b>13</b>	<b>1,053</b>	<b>6</b>	<b>21</b>	<b>0</b>	<b>12</b>	<b>5</b>	<b>0</b>	<b>16</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>9</b>	<b>840</b>	<b>29</b>	<b>13</b>	<b>1,053</b>	<b>6</b>	<b>21</b>	<b>0</b>	<b>12</b>	<b>5</b>	<b>0</b>	<b>16</b>

Number of Commercial Trips Generated

Entering	38	30	A.M.	100% Commercial Development
Exiting	42	41	P.M.	



AM(PM)

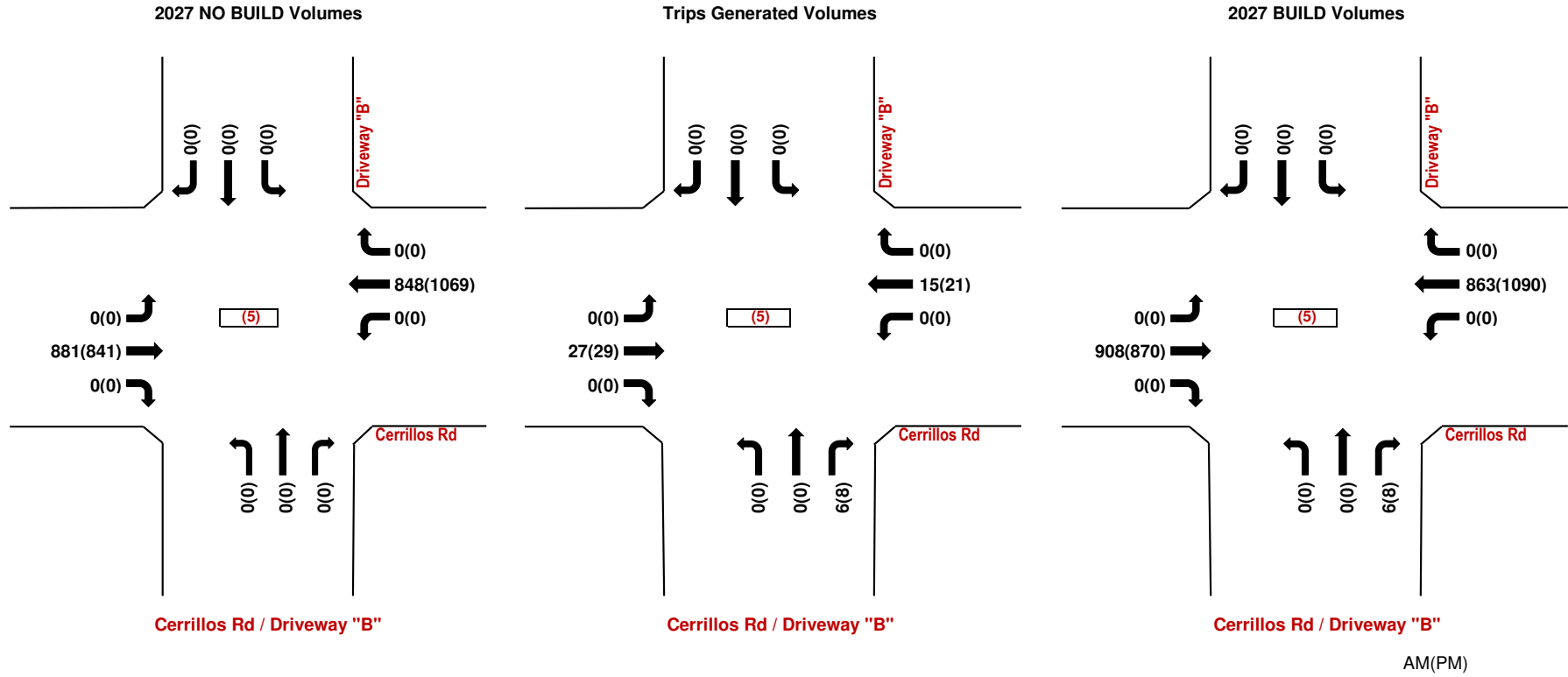
**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / Driveway "B"**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (5)  
 N-S Street: **Driveway "B"**  
 Year of Existing Counts: 2025  
 Implementation Year: **2027**

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	0	0	0	0	0	0	0	0	0	0	0
Background Traffic Growth	0	0	0	0	0	0	0	0	0	0	0	0
Subtotal	0	0	0	0	0	0	0	0	0	0	0	0
<b>Subtotal (NO BUILD - A.M.)</b>	0	<b>881</b>	0	0	<b>848</b>	0	0	0	0	0	0	0
Percent Commercial Trips Generated(Entering)	0.00%	<b>70.00%</b>	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	0.00%	<b>50.00%</b>	0.00%	0.00%	0.00%	<b>20.00%</b>	0.00%	0.00%	0.00%
Total Trips Generated	0	27	0	0	15	0	0	0	6	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	0	<b>908</b>	0	0	<b>863</b>	0	0	0	<b>6</b>	0	0	0
<b>Total AM Peak Hour BUILD Volumes</b>	0	<b>908</b>	0	0	<b>863</b>	0	0	0	<b>6</b>	0	0	0

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	0	0	0	0	0	0	0	0	0	0	0
Background Traffic Growth	0	0	0	0	0	0	0	0	0	0	0	0
Subtotal	0	0	0	0	0	0	0	0	0	0	0	0
<b>Subtotal (NO BUILD - P.M.)</b>	0	<b>841</b>	0	0	<b>1,069</b>	0	0	0	0	0	0	0
Percent Commercial Trips Generated(Entering)	0.00%	<b>70.00%</b>	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	0.00%	<b>50.00%</b>	0.00%	0.00%	0.00%	<b>20.00%</b>	0.00%	0.00%	0.00%
Total Trips Generated	0	29	0	0	21	0	0	0	8	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	0	<b>870</b>	0	0	<b>1,090</b>	0	0	0	<b>8</b>	0	0	0
<b>Total PM Peak Hour BUILD Volumes</b>	0	<b>870</b>	0	0	<b>1,090</b>	0	0	0	<b>8</b>	0	0	0

Number of Commercial Trips Generated	Entering	Exiting	A.M.	100% Commercial Development
	38	30		
	42	41	P.M.	



**Marriott Hotel (1000 Cerrillos Rd.)**

Projected Turning Movements SUMMARY  
**PROPOSED DEVELOPMENT (2037) - 100% Development**

**INTERSECTION : Summary**

**Cerrillos Rd / St Francis Dr.**

0.92

PHF

	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
(1) 0.0% H.C.												
Existing (2025)	360	458	18	210	205	26	0	1,073	527	0	1,293	589
2037 (NO BUILD - A.M.)	403	513	20	235	230	29	0	1,202	590	0	1,448	660
2037 (BUILD - A.M.)	<b>409</b>	<b>516</b>	<b>26</b>	<b>235</b>	<b>233</b>	<b>29</b>	<b>0</b>	<b>1,202</b>	<b>590</b>	<b>0</b>	<b>1,448</b>	<b>668</b>

0.95

PHF

	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing (2025)	476	314	19	414	528	37	0	1,431	387	0	1,089	547
2037 (NO BUILD - P.M.)	533	352	21	464	591	41	0	1,603	433	0	1,220	613
2037 (BUILD - P.M.)	<b>542</b>	<b>356</b>	<b>29</b>	<b>464</b>	<b>595</b>	<b>41</b>	<b>0</b>	<b>1,603</b>	<b>433</b>	<b>0</b>	<b>1,220</b>	<b>622</b>

**Cerrillos Rd / Cordova Rd**

0.91

PHF

	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
(2) 0.0% H.C.												
Existing (2025)	0	793	297	140	694	0	217	0	56	0	0	0
2037 (NO BUILD - A.M.)	0	888	333	157	777	0	243	0	63	0	0	0
2037 (BUILD - A.M.)	<b>0</b>	<b>902</b>	<b>333</b>	<b>161</b>	<b>788</b>	<b>0</b>	<b>243</b>	<b>0</b>	<b>75</b>	<b>0</b>	<b>0</b>	<b>0</b>

0.94

PHF

	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing (2025)	0	745	197	60	1,037	0	338	0	49	0	0	0
2037 (NO BUILD - P.M.)	0	834	221	67	1,161	0	379	0	55	0	0	0
2037 (BUILD - P.M.)	<b>0</b>	<b>850</b>	<b>221</b>	<b>72</b>	<b>1,177</b>	<b>0</b>	<b>379</b>	<b>0</b>	<b>68</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Cerrillos Rd / NMSFTD NE Dwy**

0.90

PHF

	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
(3) 0.0% H.C.												
Existing (2025)	18	814	0	0	825	4	0	0	0	0	0	8
2037 (NO BUILD - A.M.)	20	912	0	0	924	4	0	0	0	0	0	9
2037 (BUILD - A.M.)	<b>20</b>	<b>927</b>	<b>0</b>	<b>0</b>	<b>935</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9</b>

0.94

PHF

	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing (2025)	2	792	0	0	1,064	1	0	0	0	3	0	8
2037 (NO BUILD - P.M.)	2	887	0	0	1,192	1	0	0	0	3	0	9
2037 (BUILD - P.M.)	<b>2</b>	<b>908</b>	<b>0</b>	<b>0</b>	<b>1,205</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>9</b>

**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements SUMMARY  
**PROPOSED DEVELOPMENT (2037) - 100% Development**

**INTERSECTION: Summary**

**Cerrillos Rd / Driveway "A"**

0.90

PHF

(4)  
 0.0% H.C.  
**Existing (2025)**  
 2037 (NO BUILD - A.M.)  
 2037 (BUILD - A.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
35	828	0	0	817	11	0	0	0	1	0	15
39	927	0	0	915	12	0	0	0	1	0	17
<b>39</b>	<b>933</b>	<b>27</b>	<b>11</b>	<b>915</b>	<b>12</b>	<b>15</b>	<b>0</b>	<b>9</b>	<b>1</b>	<b>0</b>	<b>17</b>

0.94

PHF

**Existing (2025)**  
 2037 (NO BUILD - P.M.)  
 2037 (BUILD - P.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
9	816	0	0	1,032	6	0	0	0	5	0	16
10	914	0	0	1,156	7	0	0	0	6	0	18
<b>10</b>	<b>922</b>	<b>29</b>	<b>13</b>	<b>1,156</b>	<b>7</b>	<b>21</b>	<b>0</b>	<b>12</b>	<b>6</b>	<b>0</b>	<b>18</b>

**Cerrillos Rd / Driveway "B"**

0.91

PHF

(5)  
 0.0% H.C.  
**Existing (2025)**  
 2037 (NO BUILD - A.M.)  
 2037 (BUILD - A.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
0	0	0	0	0	0	0	0	0	0	0	0
0	966	0	0	932	0	0	0	0	0	0	0
<b>0</b>	<b>993</b>	<b>0</b>	<b>0</b>	<b>947</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>

0.94

PHF

**Existing (2025)**  
 2037 (NO BUILD - P.M.)  
 2037 (BUILD - P.M.)

Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
0	0	0	0	0	0	0	0	0	0	0	0
0	924	0	0	1,174	0	0	0	0	0	0	0
<b>0</b>	<b>953</b>	<b>0</b>	<b>0</b>	<b>1,195</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>

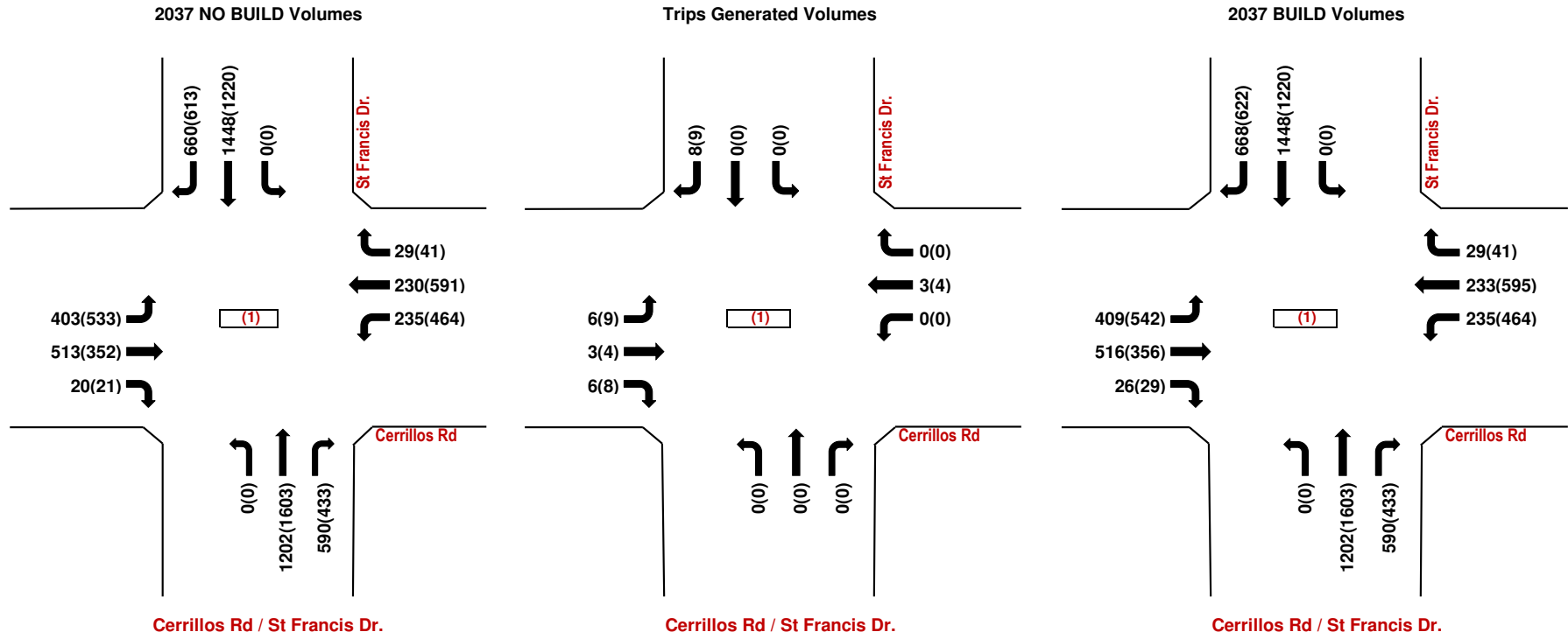
**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / St Francis Dr.**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (1)  
 N-S Street: **St Francis Dr.**  
 Year of Existing Counts 2025  
 Horizon Year 2037

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	360	458	18	210	205	26	0	1,073	527	0	1,293	589
Background Traffic Growth	43	55	2	25	25	3	0	129	63	0	155	71
Subtotal	403	513	20	235	230	29	0	1,202	590	0	1,448	660
<b>Subtotal (NO BUILD - A.M.)</b>	<b>403</b>	<b>513</b>	<b>20</b>	<b>235</b>	<b>230</b>	<b>29</b>	<b>0</b>	<b>1,202</b>	<b>590</b>	<b>0</b>	<b>1,448</b>	<b>660</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	9.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	21.00%
Percent Commercial Trips Generated(Exiting)	21.00%	9.00%	20.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	6	3	6	0	3	0	0	0	0	0	0	8
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>409</b>	<b>516</b>	<b>26</b>	<b>235</b>	<b>233</b>	<b>29</b>	<b>0</b>	<b>1,202</b>	<b>590</b>	<b>0</b>	<b>1,448</b>	<b>668</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>409</b>	<b>516</b>	<b>26</b>	<b>235</b>	<b>233</b>	<b>29</b>	<b>0</b>	<b>1,202</b>	<b>590</b>	<b>0</b>	<b>1,448</b>	<b>668</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (St Francis Dr.)			SB (St Francis Dr.)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	476	314	19	414	528	37	0	1,431	387	0	1,089	547
Background Traffic Growth	57	38	2	50	63	4	0	172	46	0	131	66
Subtotal	533	352	21	464	591	41	0	1,603	433	0	1,220	613
<b>Subtotal (NO BUILD - P.M.)</b>	<b>533</b>	<b>352</b>	<b>21</b>	<b>464</b>	<b>591</b>	<b>41</b>	<b>0</b>	<b>1,603</b>	<b>433</b>	<b>0</b>	<b>1,220</b>	<b>613</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	9.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	21.00%
Percent Commercial Trips Generated(Exiting)	21.00%	9.00%	20.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	9	4	8	0	4	0	0	0	0	0	0	9
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>542</b>	<b>356</b>	<b>29</b>	<b>464</b>	<b>595</b>	<b>41</b>	<b>0</b>	<b>1,603</b>	<b>433</b>	<b>0</b>	<b>1,220</b>	<b>622</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>542</b>	<b>356</b>	<b>29</b>	<b>464</b>	<b>595</b>	<b>41</b>	<b>0</b>	<b>1,603</b>	<b>433</b>	<b>0</b>	<b>1,220</b>	<b>622</b>

Number of Commercial Trips Generated	Entering	Exiting	A.M.	100% Commercial Development
	38	30		
	42	41	P.M.	



AM(PM)

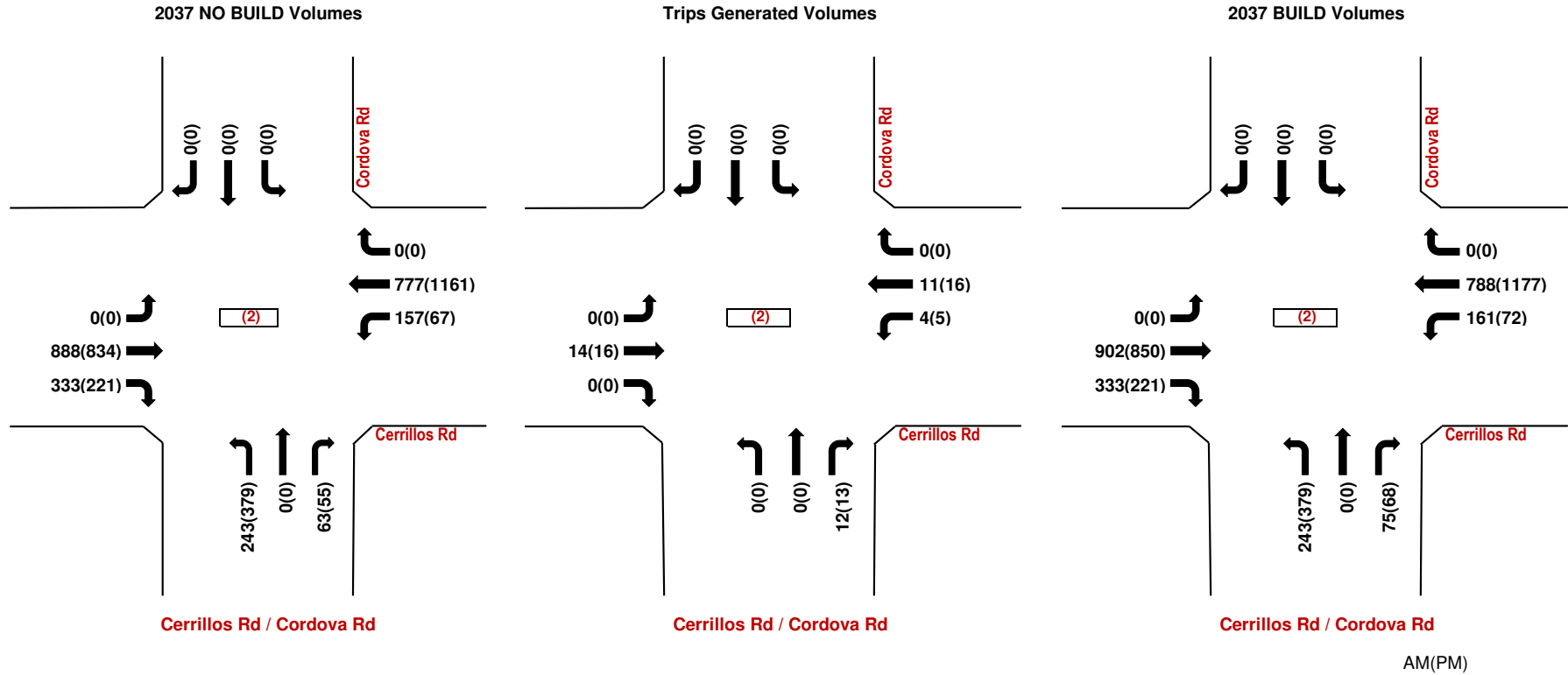
**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / Cordova Rd**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (2)  
 N-S Street: **Cordova Rd**  
 Year of Existing Counts: 2025  
 Horizon Year: 2037

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	793	297	140	694	0	217	0	56	0	0	0
Background Traffic Growth	0	95	36	17	83	0	26	0	7	0	0	0
Subtotal	0	888	333	157	777	0	243	0	63	0	0	0
<b>Subtotal (NO BUILD - A.M.)</b>	<b>0</b>	<b>888</b>	<b>333</b>	<b>157</b>	<b>777</b>	<b>0</b>	<b>243</b>	<b>0</b>	<b>63</b>	<b>0</b>	<b>0</b>	<b>0</b>
Percent Commercial Trips Generated(Entering)	0.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	32.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	12.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	14	0	4	11	0	0	0	12	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>0</b>	<b>902</b>	<b>333</b>	<b>161</b>	<b>788</b>	<b>0</b>	<b>243</b>	<b>0</b>	<b>75</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>0</b>	<b>902</b>	<b>333</b>	<b>161</b>	<b>788</b>	<b>0</b>	<b>243</b>	<b>0</b>	<b>75</b>	<b>0</b>	<b>0</b>	<b>0</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			Northbound (Cordova Rd)			Southbound (Cordova Rd)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	745	197	60	1,037	0	338	0	49	0	0	0
Background Traffic Growth	0	89	24	7	124	0	41	0	6	0	0	0
Subtotal	0	834	221	67	1,161	0	379	0	55	0	0	0
<b>Subtotal (NO BUILD - P.M.)</b>	<b>0</b>	<b>834</b>	<b>221</b>	<b>67</b>	<b>1,161</b>	<b>0</b>	<b>379</b>	<b>0</b>	<b>55</b>	<b>0</b>	<b>0</b>	<b>0</b>
Percent Commercial Trips Generated(Entering)	0.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	32.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	12.00%	38.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	16	0	5	16	0	0	0	13	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>0</b>	<b>850</b>	<b>221</b>	<b>72</b>	<b>1,177</b>	<b>0</b>	<b>379</b>	<b>0</b>	<b>68</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>0</b>	<b>850</b>	<b>221</b>	<b>72</b>	<b>1,177</b>	<b>0</b>	<b>379</b>	<b>0</b>	<b>68</b>	<b>0</b>	<b>0</b>	<b>0</b>

Number of Commercial Trips Generated	Entering	Exiting	A.M.	100% Commercial Development
	38	30		
	42	41	P.M.	



**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / NMSFTD NE Dwy**

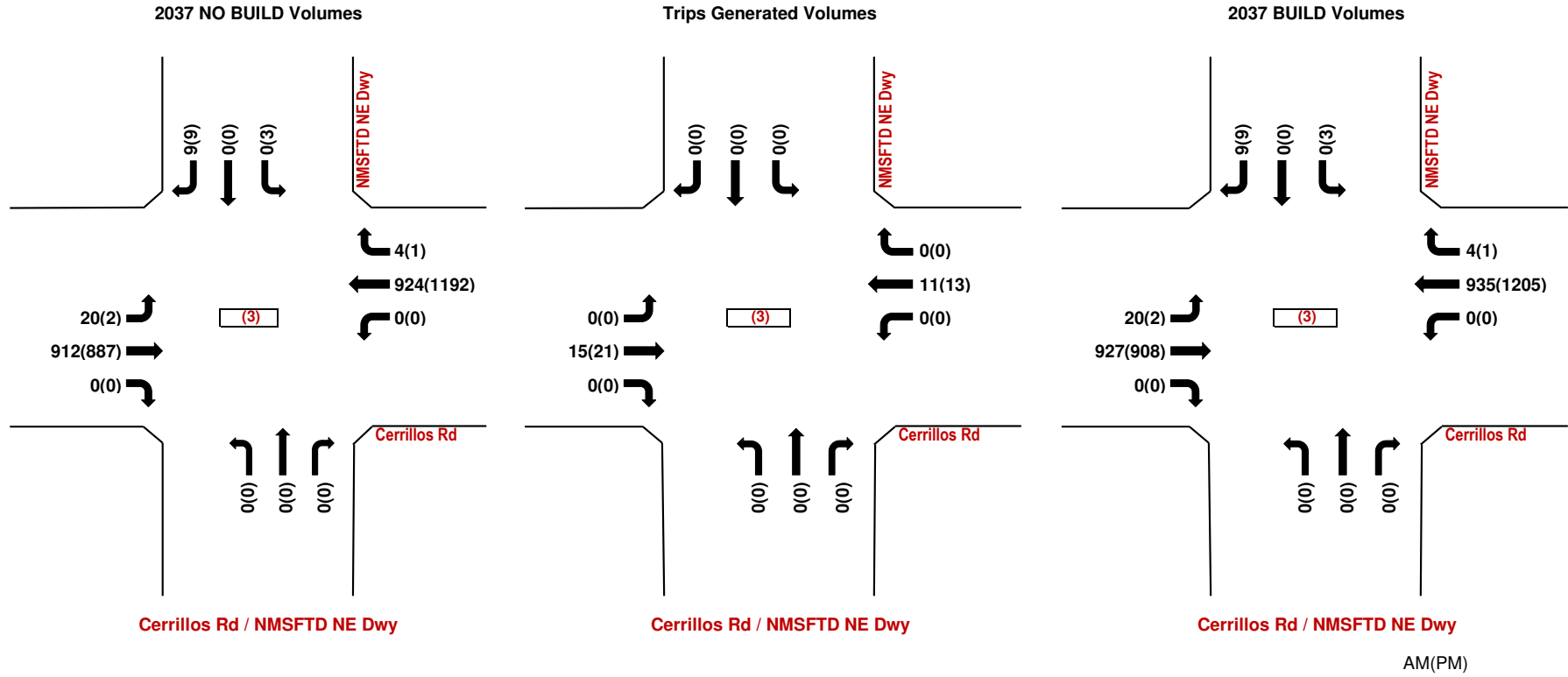
**INTERSECTION :** E-W Street: **Cerrillos Rd** (3)  
 N-S Street: **NMSFTD NE Dwy**  
 Year of Existing Counts: 2025  
 Horizon Year: 2037

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	18	814	0	0	825	4	0	0	0	0	0	8
Background Traffic Growth	2	98	0	0	99	0	0	0	0	0	0	1
Subtotal	20	912	0	0	924	4	0	0	0	0	0	9
<b>Subtotal (NO BUILD - A.M.)</b>	<b>20</b>	<b>912</b>	<b>0</b>	<b>0</b>	<b>924</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	15	0	0	11	0	0	0	0	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>20</b>	<b>927</b>	<b>0</b>	<b>0</b>	<b>935</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>20</b>	<b>927</b>	<b>0</b>	<b>0</b>	<b>935</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (NMSFTD NE Dwy)			SB (NMSFTD NE Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	2	792	0	0	1,064	1	0	0	0	3	0	8
Background Traffic Growth	0	95	0	0	128	0	0	0	0	0	0	1
Subtotal	2	887	0	0	1,192	1	0	0	0	3	0	9
<b>Subtotal (NO BUILD - P.M.)</b>	<b>2</b>	<b>887</b>	<b>0</b>	<b>0</b>	<b>1,192</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>9</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	0.00%	0.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	21	0	0	13	0	0	0	0	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>2</b>	<b>908</b>	<b>0</b>	<b>0</b>	<b>1,205</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>9</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>2</b>	<b>908</b>	<b>0</b>	<b>0</b>	<b>1,205</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>9</b>

Number of Commercial Trips Generated

Entering	38	30	A.M.	100% Commercial Development
Exiting	42	41	P.M.	



**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / Driveway "A"**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (4)  
 N-S Street: **Driveway "A"**  
 Year of Existing Counts: 2025  
 Horizon Year: 2037

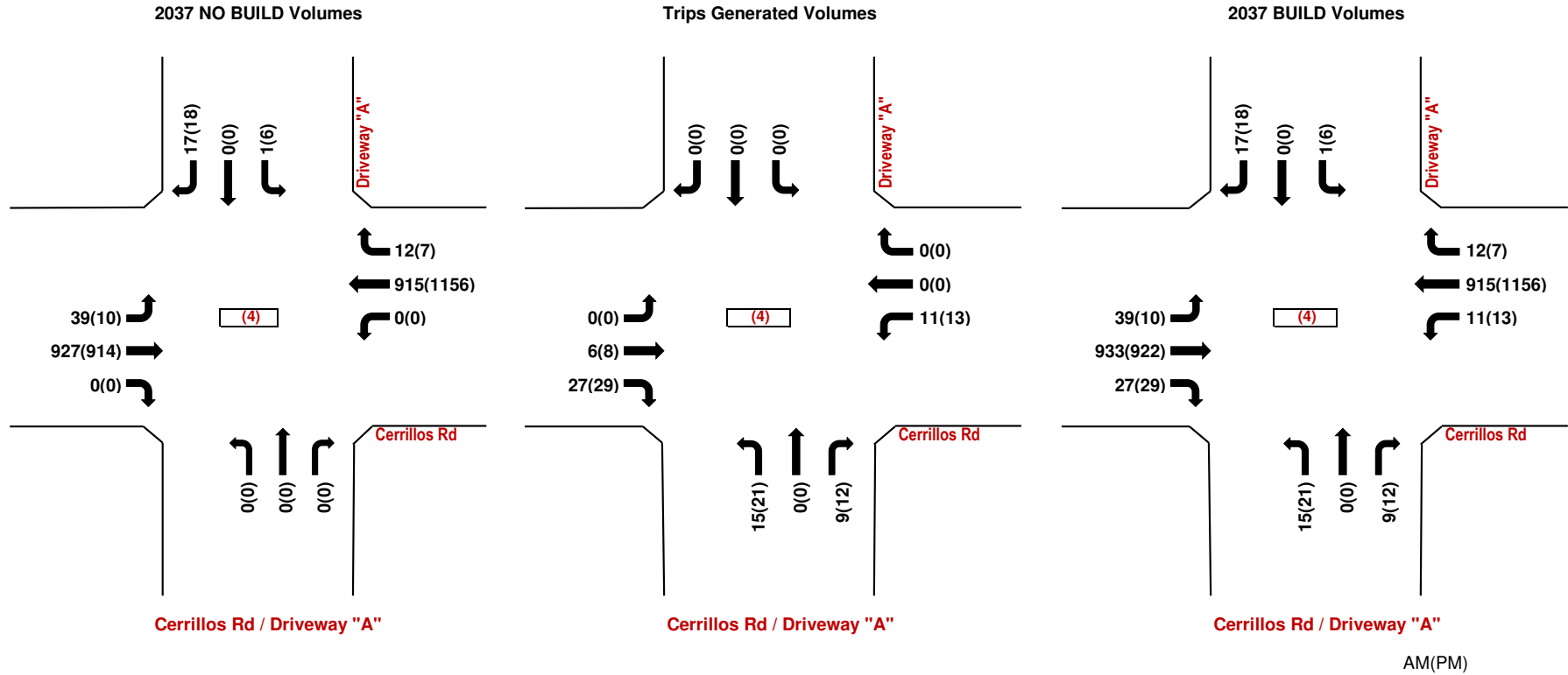
**Growth Rates** 1.00% 1.00% 1.00% 1.00%

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	35	828	0	0	817	11	0	0	0	1	0	15
Background Traffic Growth	4	99	0	0	98	1	0	0	0	0	0	2
Subtotal	39	927	0	0	915	12	0	0	0	1	0	17
<b>Subtotal (NO BUILD - A.M.)</b>	<b>39</b>	<b>927</b>	<b>0</b>	<b>0</b>	<b>915</b>	<b>12</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>17</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	70.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	20.00%	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	30.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	6	27	11	0	0	15	0	9	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	<b>39</b>	<b>933</b>	<b>27</b>	<b>11</b>	<b>915</b>	<b>12</b>	<b>15</b>	<b>0</b>	<b>9</b>	<b>1</b>	<b>0</b>	<b>17</b>
<b>Total AM Peak Hour BUILD Volumes</b>	<b>39</b>	<b>933</b>	<b>27</b>	<b>11</b>	<b>915</b>	<b>12</b>	<b>15</b>	<b>0</b>	<b>9</b>	<b>1</b>	<b>0</b>	<b>17</b>

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "A")			SB (NMSFTD SW Dwy)		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	9	816	0	0	1,032	6	0	0	0	5	0	16
Background Traffic Growth	1	98	0	0	124	1	0	0	0	1	0	2
Subtotal	10	914	0	0	1,156	7	0	0	0	6	0	18
<b>Subtotal (NO BUILD - P.M.)</b>	<b>10</b>	<b>914</b>	<b>0</b>	<b>0</b>	<b>1,156</b>	<b>7</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>18</b>
Percent Commercial Trips Generated(Entering)	0.00%	0.00%	70.00%	30.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	20.00%	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	30.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	8	29	13	0	0	21	0	12	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	<b>10</b>	<b>922</b>	<b>29</b>	<b>13</b>	<b>1,156</b>	<b>7</b>	<b>21</b>	<b>0</b>	<b>12</b>	<b>6</b>	<b>0</b>	<b>18</b>
<b>Total PM Peak Hour BUILD Volumes</b>	<b>10</b>	<b>922</b>	<b>29</b>	<b>13</b>	<b>1,156</b>	<b>7</b>	<b>21</b>	<b>0</b>	<b>12</b>	<b>6</b>	<b>0</b>	<b>18</b>

Number of Commercial Trips Generated

Entering	38	30	A.M.	100% Commercial Development
Exiting	42	41	P.M.	



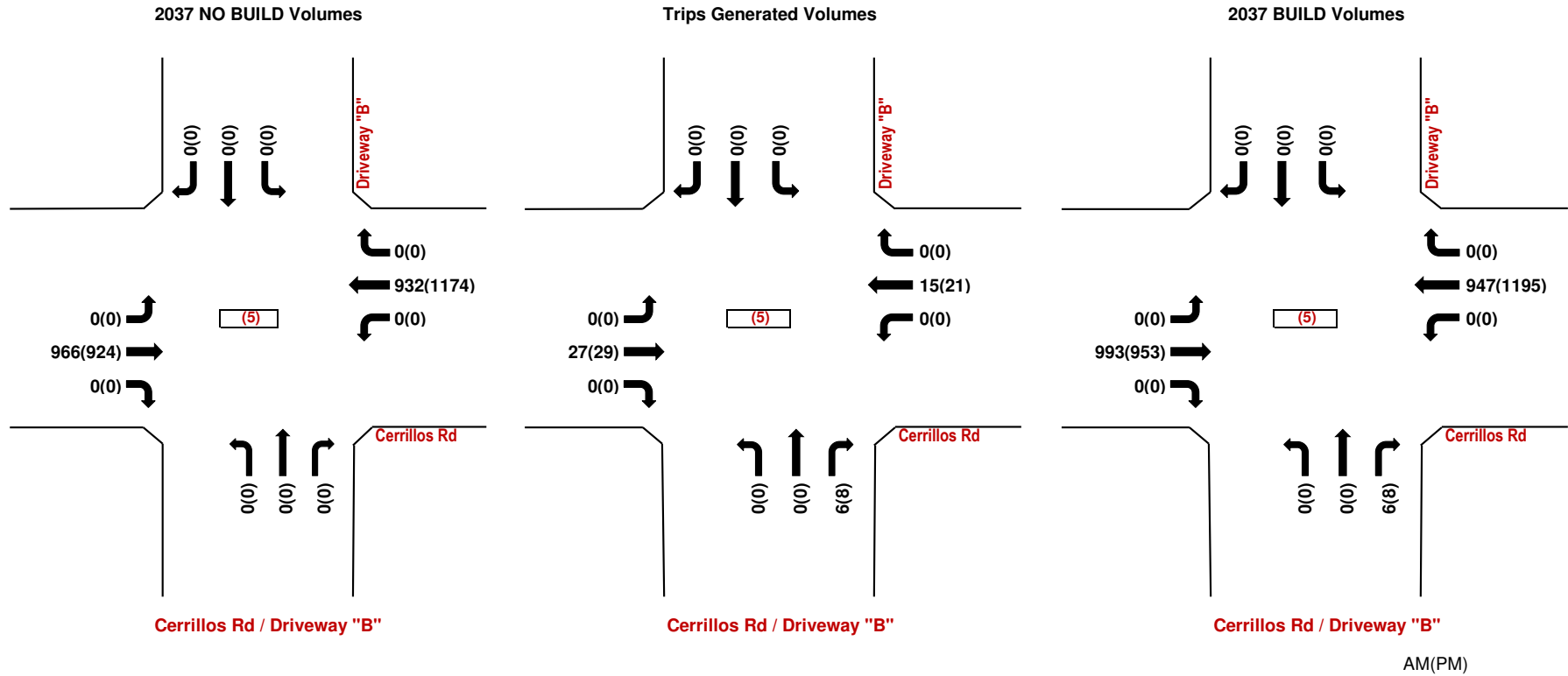
**Marriott Hotel (1000 Cerrillos Rd.)**  
 Projected Turning Movements Worksheet  
**Cerrillos Rd / Driveway "B"**

**INTERSECTION :** E-W Street: **Cerrillos Rd** (5)  
 N-S Street: **Driveway "B"**  
 Year of Existing Counts: 2025  
 Horizon Year: 2037

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	0	0	0	0	0	0	0	0	0	0	0
Background Traffic Growth	0	0	0	0	0	0	0	0	0	0	0	0
Subtotal	0	0	0	0	0	0	0	0	0	0	0	0
<b>Subtotal (NO BUILD - A.M.)</b>	0	966	0	0	932	0	0	0	0	0	0	0
Percent Commercial Trips Generated(Entering)	0.00%	70.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	27	0	0	15	0	0	0	6	0	0	0
<b>Subtotal AM Pk Hr. BUILD Volumes</b>	0	993	0	0	947	0	0	0	6	0	0	0
<b>Total AM Peak Hour BUILD Volumes</b>	0	993	0	0	947	0	0	0	6	0	0	0

	1.00%			1.00%			1.00%			1.00%		
	Eastbound (Cerrillos Rd)			Westbound (Cerrillos Rd)			NB (Driveway "B")			SB (Driveway "B")		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Existing Volumes	0	0	0	0	0	0	0	0	0	0	0	0
Background Traffic Growth	0	0	0	0	0	0	0	0	0	0	0	0
Subtotal	0	0	0	0	0	0	0	0	0	0	0	0
<b>Subtotal (NO BUILD - P.M.)</b>	0	924	0	0	1,174	0	0	0	0	0	0	0
Percent Commercial Trips Generated(Entering)	0.00%	70.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Percent Commercial Trips Generated(Exiting)	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%
Total Trips Generated	0	29	0	0	21	0	0	0	8	0	0	0
<b>Subtotal PM Pk Hr. BUILD Volumes</b>	0	953	0	0	1,195	0	0	0	8	0	0	0
<b>Total PM Peak Hour BUILD Volumes</b>	0	953	0	0	1,195	0	0	0	8	0	0	0

Number of Commercial Trips Generated	Entering	Exiting	A.M.	100% Commercial Development
	38	30		
	42	41	P.M.	



Timings

1: Saint Francis Dr & Cerrillos Rd

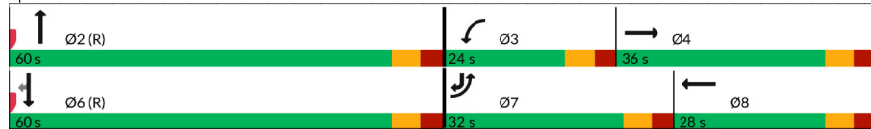
11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (vph)	367	467	214	209	1094	1319	601
Future Volume (vph)	367	467	214	209	1094	1319	601
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	10.0	10.0	3.0	10.0	10.0	10.0	10.0
Minimum Split (s)	17.0	25.0	10.0	25.0	25.0	25.0	17.0
Total Split (s)	32.0	36.0	24.0	28.0	60.0	60.0	32.0
Total Split (%)	26.7%	30.0%	20.0%	23.3%	50.0%	50.0%	26.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lead	Lag	Lead	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	20.4	23.5	13.4	16.5	62.1	62.1	89.5
Actuated g/C Ratio	0.17	0.20	0.11	0.14	0.52	0.52	0.75
v/c Ratio	0.68	0.76	0.61	0.53	0.69	0.78	0.30
Control Delay (s/veh)	45.5	57.6	57.4	49.7	22.7	28.7	2.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	45.5	57.6	57.4	49.7	22.7	28.7	2.7
LOS	D	E	E	D	C	C	A
Approach Delay (s/veh)		52.4		53.4	22.7	20.5	
Approach LOS		D		D	C	C	

Intersection Summary

Cycle Length: 120
Actuated Cycle Length: 120
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 90
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.78
Intersection Signal Delay (s/veh): 29.9
Intersection LOS: C
Intersection Capacity Utilization 73.5%
ICU Level of Service D
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘	↖	→	↙	←	↑	↓	↘
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR		
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗	↖↗		↖↗	↖↗		
Traffic Volume (veh/h)	367	467	18	214	209	27	0	1094	538	0	1319	601		
Future Volume (veh/h)	367	467	18	214	209	27	0	1094	538	0	1319	601		
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0		
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00	1.00	1.00	1.00	1.00		
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Work Zone On Approach	No			No			No			No				
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870		
Adj Flow Rate, veh/h	399	508	20	233	227	29	0	1189	0	0	1434	653		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92		
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2		
Cap, veh/h	476	612	24	298	395	50	0	2875		0	2001	1955		
Arrive On Green	0.14	0.18	0.18	0.09	0.12	0.12	0.00	0.56	0.00	0.00	0.56	0.56		
Sat Flow, veh/h	3456	3486	137	3456	3174	401	0	5443	0	0	3647	2790		
Grp Volume(v), veh/h	399	259	269	233	126	130	0	1189	0	0	1434	653		
Grp Sat Flow(s),veh/h/ln	1728	1777	1846	1728	1777	1798	0	1702	0	0	1777	1395		
Q Serve(g_s), s	13.5	16.8	16.9	7.9	8.0	8.2	0.0	15.9	0.0	0.0	35.5	11.0		
Cycle Q Clear(g_c), s	13.5	16.8	16.9	7.9	8.0	8.2	0.0	15.9	0.0	0.0	35.5	11.0		
Prop In Lane	1.00		0.07	1.00		0.22	0.00		0.00	0.00		1.00		
Lane Grp Cap(c), veh/h	476	312	324	298	221	224	0	2875		0	2001	1955		
V/C Ratio(X)	0.84	0.83	0.83	0.78	0.57	0.58	0.00	0.41		0.00	0.72	0.33		
Avail Cap(c_a), veh/h	720	429	446	490	311	315	0	2875		0	2001	1955		
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00		
Uniform Delay (d), s/veh	50.4	47.7	47.7	53.7	49.5	49.6	0.0	14.9	0.0	0.0	19.2	7.0		
Incr Delay (d2), s/veh	5.5	9.3	9.2	4.5	2.3	2.4	0.0	0.4	0.0	0.0	2.2	0.5		
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
%ile BackOfQ(95%),veh/ln	10.2	12.9	13.3	6.5	6.6	6.9	0.0	10.1	0.0	0.0	20.6	5.5		
Unsig. Movement Delay, s/veh														
LnGrp Delay(d), s/veh	56.0	57.1	56.9	58.2	51.8	52.0	0.0	15.4	0.0	0.0	21.4	7.5		
LnGrp LOS	E	E	E	E	D	D		B			C	A		
Approach Vol, veh/h		927			489			1189			2087			
Approach Delay, s/veh		56.5			54.9			15.4			17.1			
Approach LOS		E			D			B			B			
Timer - Assigned Phs	2	3	4		6	7	8							
Phs Duration (G+Y+Rc), s	74.6	17.4	28.1		74.6	23.5	21.9							
Change Period (Y+Rc), s	7.0	7.0	7.0		7.0	7.0	7.0							
Max Green Setting (Gmax), s	53.0	17.0	29.0		53.0	25.0	21.0							
Max Q Clear Time (g_c+I1), s	17.9	9.9	18.9		37.5	15.5	10.2							
Green Ext Time (p_c), s	10.4	0.4	2.2		11.1	1.0	1.0							

Intersection Summary

HCM 7th Control Delay, s/veh	28.4
HCM 7th LOS	C

Notes

Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (vph)	373	470	214	212	1094	1319	609
Future Volume (vph)	373	470	214	212	1094	1319	609
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	10.0	10.0	3.0	10.0	10.0	10.0	10.0
Minimum Split (s)	17.0	25.0	10.0	25.0	25.0	25.0	17.0
Total Split (s)	32.0	36.0	24.0	28.0	60.0	60.0	32.0
Total Split (%)	26.7%	30.0%	20.0%	23.3%	50.0%	50.0%	26.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lead	Lag	Lead	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	20.6	23.7	13.4	16.6	61.9	61.9	89.4
Actuated g/C Ratio	0.17	0.20	0.11	0.14	0.52	0.52	0.75
v/c Ratio	0.69	0.77	0.61	0.53	0.69	0.79	0.31
Control Delay (s/veh)	45.8	56.9	57.4	49.7	22.9	28.9	2.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	45.8	56.9	57.4	49.7	22.9	28.9	2.8
LOS	D	E	E	D	C	C	A
Approach Delay (s/veh)		52.1		53.4	22.9	20.7	
Approach LOS		D		D	C	C	

Intersection Summary

Cycle Length: 120
Actuated Cycle Length: 120
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 90
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.79
Intersection Signal Delay (s/veh): 30.0
Intersection LOS: C
Intersection Capacity Utilization 73.8%
ICU Level of Service D
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↖	↙	↑	↘	↘	↓	↖	
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗	↖↗		↖↗	↖↗
Traffic Volume (veh/h)	373	470	24	214	212	27	0	1094	538	0	1319	609
Future Volume (veh/h)	373	470	24	214	212	27	0	1094	538	0	1319	609
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00	1.00	1.00	1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870
Adj Flow Rate, veh/h	405	511	26	233	230	29	0	1189	0	0	1434	662
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2
Cap, veh/h	482	614	31	298	398	50	0	2861		0	1991	1952
Arrive On Green	0.14	0.18	0.18	0.09	0.13	0.13	0.00	0.56	0.00	0.00	0.56	0.56
Sat Flow, veh/h	3456	3441	175	3456	3180	396	0	5443	0	0	3647	2790
Grp Volume(v), veh/h	405	263	274	233	127	132	0	1189	0	0	1434	662
Grp Sat Flow(s),veh/h/ln	1728	1777	1839	1728	1777	1799	0	1702	0	0	1777	1395
Q Serve(g_s), s	13.7	17.2	17.2	7.9	8.1	8.3	0.0	16.0	0.0	0.0	35.7	11.2
Cycle Q Clear(g_c), s	13.7	17.2	17.2	7.9	8.1	8.3	0.0	16.0	0.0	0.0	35.7	11.2
Prop In Lane	1.00		0.10	1.00		0.22	0.00		0.00	0.00		1.00
Lane Grp Cap(c), veh/h	482	317	328	298	223	225	0	2861		0	1991	1952
V/C Ratio(X)	0.84	0.83	0.83	0.78	0.57	0.58	0.00	0.42		0.00	0.72	0.34
Avail Cap(c_a), veh/h	720	429	444	490	311	315	0	2861		0	1991	1952
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00
Uniform Delay (d), s/veh	50.3	47.5	47.6	53.7	49.5	49.5	0.0	15.1	0.0	0.0	19.4	7.1
Incr Delay (d2), s/veh	5.7	9.7	9.7	4.5	2.3	2.4	0.0	0.4	0.0	0.0	2.3	0.5
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	10.4	13.1	13.5	6.5	6.7	6.9	0.0	10.1	0.0	0.0	20.8	5.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	56.1	57.3	57.2	58.2	51.8	51.9	0.0	15.6	0.0	0.0	21.7	7.6
LnGrp LOS	E	E	E	E	D	D		B			C	A
Approach Vol, veh/h		942			492			1189			2096	
Approach Delay, s/veh		56.8			54.8			15.6			17.3	
Approach LOS		E			D			B			B	

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	74.2	17.4	28.4	74.2	23.7	22.0
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0
Max Green Setting (Gmax), s	53.0	17.0	29.0	53.0	25.0	21.0
Max Q Clear Time (g_c+I1), s	18.0	9.9	19.2	37.7	15.7	10.3
Green Ext Time (p_c), s	10.4	0.4	2.2	11.1	1.0	1.0

Intersection Summary

HCM 7th Control Delay, s/veh	28.6
HCM 7th LOS	C

Notes

Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↖	↖↗	↖↖	↖↗	↖↗	↖↗	↖↖
Traffic Volume (vph)	486	320	422	539	1460	1111	558
Future Volume (vph)	486	320	422	539	1460	1111	558
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5	9.5	22.5	22.5	22.5	9.5
Total Split (s)	32.0	34.0	32.0	34.0	69.0	69.0	32.0
Total Split (%)	23.7%	25.2%	23.7%	25.2%	51.1%	51.1%	23.7%
Yellow Time (s)	3.5	3.0	3.5	3.0	3.0	3.0	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.0	4.5	4.0	4.0	4.0	4.5
Lead/Lag	Lead	Lag	Lead	Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	25.2	30.2	22.5	27.6	69.7	69.7	98.9
Actuated g/C Ratio	0.19	0.22	0.17	0.20	0.52	0.52	0.73
v/c Ratio	0.80	0.45	0.77	0.84	0.76	0.64	0.29
Control Delay (s/veh)	68.0	37.8	63.3	62.8	28.1	26.5	6.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	68.0	37.8	63.3	62.8	28.1	26.5	6.0
LOS	E	D	E	E	C	C	A
Approach Delay (s/veh)		55.6		63.0	28.1	19.6	
Approach LOS		E		E	C	B	

Intersection Summary

Cycle Length: 135
Actuated Cycle Length: 135
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 80
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.84
Intersection Signal Delay (s/veh): 36.2
Intersection LOS: D
Intersection Capacity Utilization 77.0%
ICU Level of Service D
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘	↙	↘	↑	↓	↘
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↖	↖↗		↖↖	↖↗			↖↗	↖↗	↖↗	↖↗	↖↖
Traffic Volume (veh/h)	486	320	19	422	539	38	0	1460	395	0	1111	558
Future Volume (veh/h)	486	320	19	422	539	38	0	1460	395	0	1111	558
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00	1.00	1.00	1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No				No				No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870
Adj Flow Rate, veh/h	512	337	20	444	567	40	0	1537	0	0	1169	587
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2
Cap, veh/h	578	719	43	513	647	46	0	2799	0	0	1948	1996
Arrive On Green	0.17	0.21	0.21	0.15	0.19	0.19	0.00	0.55	0.00	0.00	0.55	0.55
Sat Flow, veh/h	3456	3409	202	3456	3367	237	0	5443	0	0	3647	2790
Grp Volume(v), veh/h	512	175	182	444	299	308	0	1537	0	0	1169	587
Grp Sat Flow(s),veh/h/ln	1728	1777	1834	1728	1777	1828	0	1702	0	0	1777	1395
Q Serve(g_s), s	19.6	11.6	11.7	16.9	22.0	22.1	0.0	26.3	0.0	0.0	29.9	10.2
Cycle Q Clear(g_c), s	19.6	11.6	11.7	16.9	22.0	22.1	0.0	26.3	0.0	0.0	29.9	10.2
Prop In Lane	1.00		0.11	1.00			0.13	0.00			0.00	1.00
Lane Grp Cap(c), veh/h	578	375	387	513	341	351	0	2799	0	0	1948	1996
V/C Ratio(X)	0.89	0.47	0.47	0.87	0.88	0.88	0.00	0.55	0.00	0.00	0.60	0.29
Avail Cap(c_a), veh/h	704	395	408	704	395	406	0	2799	0	0	1948	1996
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00
Uniform Delay (d), s/veh	54.9	46.6	46.7	56.2	53.0	53.0	0.0	19.7	0.0	0.0	20.5	6.9
Incr Delay (d2), s/veh	11.3	0.9	0.9	8.4	17.5	17.5	0.0	0.8	0.0	0.0	1.4	0.4
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	14.4	9.0	9.3	12.6	17.0	17.4	0.0	15.6	0.0	0.0	18.2	5.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	66.3	47.5	47.6	64.5	70.5	70.5	0.0	20.5	0.0	0.0	21.9	7.3
LnGrp LOS	E	D	D	E	E	E		C			C	A
Approach Vol, veh/h		869			1051			1537			1756	
Approach Delay, s/veh		58.6			68.0			20.5			17.0	
Approach LOS		E			E			C			B	

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	78.0	24.5	32.5	78.0	27.1	29.9
Change Period (Y+Rc), s	4.0	4.5	4.0	4.0	4.5	4.0
Max Green Setting (Gmax), s	65.0	27.5	30.0	65.0	27.5	30.0
Max Q Clear Time (g_c+I1), s	28.3	18.9	13.7	31.9	21.6	24.1
Green Ext Time (p_c), s	15.1	1.1	1.7	14.3	1.0	1.8

Intersection Summary	
HCM 7th Control Delay, s/veh	35.2
HCM 7th LOS	D

Notes  
 Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗↖↗	↖↗	↖↗
Traffic Volume (vph)	495	324	422	543	1460	1111	567
Future Volume (vph)	495	324	422	543	1460	1111	567
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5	9.5	22.5	22.5	22.5	9.5
Total Split (s)	32.0	34.0	32.0	34.0	69.0	69.0	32.0
Total Split (%)	23.7%	25.2%	23.7%	25.2%	51.1%	51.1%	23.7%
Yellow Time (s)	3.5	3.0	3.5	3.0	3.0	3.0	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.0	4.5	4.0	4.0	4.0	4.5
Lead/Lag	Lead	Lag	Lead	Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	25.4	30.5	22.5	27.7	69.4	69.4	98.8
Actuated g/C Ratio	0.19	0.23	0.17	0.21	0.51	0.51	0.73
v/c Ratio	0.81	0.46	0.77	0.85	0.76	0.64	0.29
Control Delay (s/veh)	69.1	37.7	63.3	63.0	28.4	26.7	6.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	69.1	37.7	63.3	63.0	28.4	26.7	6.1
LOS	E	D	E	E	C	C	A
Approach Delay (s/veh)		56.1		63.1	28.4	19.7	
Approach LOS		E		E	C	B	

Intersection Summary

Cycle Length: 135
Actuated Cycle Length: 135
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 80
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.85
Intersection Signal Delay (s/veh): 36.5
Intersection LOS: D
Intersection Capacity Utilization 77.4%
ICU Level of Service D
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘	↙	↘	↙	↘	↙	↘
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗↖↗			↖↗	↖↗	
Traffic Volume (veh/h)	495	324	27	422	543	38	0	1460	395	0	1111	567	
Future Volume (veh/h)	495	324	27	422	543	38	0	1460	395	0	1111	567	
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0	
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00	1.00	1.00	1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Work Zone On Approach	No			No				No				No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870	
Adj Flow Rate, veh/h	521	341	28	444	572	40	0	1537	0	0	1169	597	
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2	
Cap, veh/h	587	714	58	513	651	45	0	2780	0	0	1935	1992	
Arrive On Green	0.17	0.21	0.21	0.15	0.19	0.19	0.00	0.54	0.00	0.00	0.54	0.54	
Sat Flow, veh/h	3456	3327	272	3456	3370	235	0	5443	0	0	3647	2790	
Grp Volume(v), veh/h	521	181	188	444	301	311	0	1537	0	0	1169	597	
Grp Sat Flow(s),veh/h/ln	1728	1777	1821	1728	1777	1828	0	1702	0	0	1777	1395	
Q Serve(g_s), s	19.9	12.0	12.2	16.9	22.2	22.3	0.0	26.5	0.0	0.0	30.1	10.5	
Cycle Q Clear(g_c), s	19.9	12.0	12.2	16.9	22.2	22.3	0.0	26.5	0.0	0.0	30.1	10.5	
Prop In Lane	1.00			0.15	1.00		0.13	0.00			0.00	1.00	
Lane Grp Cap(c), veh/h	587	381	391	513	343	353	0	2780	0	0	1935	1992	
V/C Ratio(X)	0.89	0.48	0.48	0.87	0.88	0.88	0.00	0.55	0.00	0.00	0.60	0.30	
Avail Cap(c_a), veh/h	704	395	405	704	395	406	0	2780	0	0	1935	1992	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00	
Uniform Delay (d), s/veh	54.8	46.4	46.4	56.2	52.9	52.9	0.0	20.0	0.0	0.0	20.9	7.0	
Incr Delay (d2), s/veh	11.7	0.9	0.9	8.4	17.8	17.8	0.0	0.8	0.0	0.0	1.4	0.4	
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
%ile BackOfQ(95%),veh/ln	14.6	9.2	9.5	12.6	17.1	17.6	0.0	15.7	0.0	0.0	18.3	5.4	
Unsig. Movement Delay, s/veh													
LnGrp Delay(d), s/veh	66.5	47.3	47.4	64.5	70.7	70.7	0.0	20.8	0.0	0.0	22.3	7.4	
LnGrp LOS	E	D	D	E	E	E		C			C	A	
Approach Vol, veh/h		890			1056			1537				1766	
Approach Delay, s/veh		58.6			68.1			20.8				17.2	
Approach LOS		E			E			C				B	

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	77.5	24.5	33.0	77.5	27.4	30.1
Change Period (Y+Rc), s	4.0	4.5	4.0	4.0	4.5	4.0
Max Green Setting (Gmax), s	65.0	27.5	30.0	65.0	27.5	30.0
Max Q Clear Time (g_c+I1), s	28.5	18.9	14.2	32.1	21.9	24.3
Green Ext Time (p_c), s	15.1	1.1	1.8	14.4	1.0	1.8

Intersection Summary	
HCM 7th Control Delay, s/veh	35.5
HCM 7th LOS	D

Notes  
 Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↑	↑↑	↑↑
Traffic Volume (vph)	809	143	708	221
Future Volume (vph)	809	143	708	221
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	70.0	18.0	88.0	32.0
Total Split (%)	58.3%	15.0%	73.3%	26.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	81.1	95.1	94.1	15.4
Actuated g/C Ratio	0.68	0.79	0.78	0.13
v/c Ratio	0.53	0.44	0.28	0.68
Control Delay (s/veh)	10.7	8.1	3.0	53.0
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	10.7	8.1	3.0	53.0
LOS	B	A	A	D
Approach Delay (s/veh)	10.7		3.9	53.0
Approach LOS	B		A	D

Intersection Summary	
Cycle Length: 120	
Actuated Cycle Length: 120	
Offset: 110 (92%), Referenced to phase 2:EBT and 6:WBTL, Start of Green	
Natural Cycle: 60	
Control Type: Actuated-Coordinated	
Maximum v/c Ratio: 0.68	
Intersection Signal Delay (s/veh): 13.4	Intersection LOS: B
Intersection Capacity Utilization 61.0%	ICU Level of Service B
Analysis Period (min) 15	

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑↑	
Traffic Volume (veh/h)	809	303	143	708	221	57
Future Volume (veh/h)	809	303	143	708	221	57
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No					
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	889	333	157	778	153	159
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1793	669	384	2811	216	192
Arrive On Green	0.71	0.71	0.04	0.79	0.12	0.12
Sat Flow, veh/h	2626	945	1781	3647	1781	1585
Grp Volume(v), veh/h	623	599	157	778	153	159
Grp Sat Flow(s),veh/h/ln	1777	1700	1781	1777	1781	1585
Q Serve(g_s), s	18.9	19.1	2.7	7.0	9.9	11.8
Cycle Q Clear(g_c), s	18.9	19.1	2.7	7.0	9.9	11.8
Prop In Lane	0.56					
Lane Grp Cap(c), veh/h	1258	1204	384	2811	216	192
V/C Ratio(X)	0.49	0.50	0.41	0.28	0.71	0.83
Avail Cap(c_a), veh/h	1258	1204	504	2811	408	363
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	7.9	7.9	6.4	3.4	50.7	51.5
Incr Delay (d2), s/veh	1.4	1.5	0.7	0.2	4.2	8.7
Initial Q Delay(d3), s/veh	0.0					
%ile BackOfQ(95%),veh/ln	11.1	10.9	1.6	3.6	8.2	8.8
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	9.3	9.4	7.1	3.6	54.9	60.1
LnGrp LOS	A	A	A	A	D	E
Approach Vol, veh/h	1222		935		312	
Approach Delay, s/veh	9.3		4.2		57.6	
Approach LOS	A		A		E	

Timer - Assigned Phs	1	2	6	8
Phs Duration (G+Y+Rc), s	10.0	91.0	100.9	19.1
Change Period (Y+Rc), s	5.0	6.0	6.0	4.5
Max Green Setting (Gmax), s	13.0	64.0	82.0	27.5
Max Q Clear Time (g_c+I1), s	4.7	21.1	9.0	13.8
Green Ext Time (p_c), s	0.2	10.9	6.3	0.8

Intersection Summary	
HCM 7th Control Delay, s/veh	13.5
HCM 7th LOS	B

Notes  
User approved volume balancing among the lanes for turning movement.

Timings

2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↑	↑↑	↑↑
Traffic Volume (vph)	823	147	719	221
Future Volume (vph)	823	147	719	221
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	70.0	18.0	88.0	32.0
Total Split (%)	58.3%	15.0%	73.3%	26.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	80.6	94.9	93.9	15.6
Actuated g/C Ratio	0.67	0.79	0.78	0.13
v/c Ratio	0.54	0.46	0.29	0.69
Control Delay (s/veh)	11.2	8.9	3.1	52.0
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	11.2	8.9	3.1	52.0
LOS	B	A	A	D
Approach Delay (s/veh)	11.2		4.1	52.0
Approach LOS	B		A	D

Intersection Summary

Cycle Length: 120
Actuated Cycle Length: 120
Offset: 110 (92%), Referenced to phase 2:EBT and 6:WBTL, Start of Green
Natural Cycle: 60
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.69
Intersection Signal Delay (s/veh): 13.7
Intersection LOS: B
Intersection Capacity Utilization 62.0%
ICU Level of Service B
Analysis Period (min) 15

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary

2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑↑	
Traffic Volume (veh/h)	823	303	147	719	221	69
Future Volume (veh/h)	823	303	147	719	221	69
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	904	333	162	790	160	165
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1790	656	378	2797	223	199
Arrive On Green	0.70	0.70	0.04	0.79	0.13	0.13
Sat Flow, veh/h	2639	933	1781	3647	1781	1585
Grp Volume(v), veh/h	630	607	162	790	160	165
Grp Sat Flow(s),veh/h/ln	1777	1702	1781	1777	1781	1585
Q Serve(g_s), s	19.5	19.8	2.9	7.3	10.4	12.2
Cycle Q Clear(g_c), s	19.5	19.8	2.9	7.3	10.4	12.2
Prop In Lane	0.55					
Lane Grp Cap(c), veh/h	1249	1197	378	2797	223	199
V/C Ratio(X)	0.50	0.51	0.43	0.28	0.72	0.83
Avail Cap(c_a), veh/h	1249	1197	495	2797	408	363
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	8.2	8.2	6.8	3.5	50.4	51.2
Incr Delay (d2), s/veh	1.5	1.5	0.8	0.3	4.3	8.6
Initial Q Delay(d3), s/veh	0.0					
%ile BackOfQ(95%),veh/ln	11.5	11.2	1.7	3.8	8.5	9.1
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	9.6	9.8	7.6	3.7	54.7	59.9
LnGrp LOS	A	A	A	A	D	E
Approach Vol, veh/h	1237		952		325	
Approach Delay, s/veh	9.7		4.4		57.3	
Approach LOS	A		A		E	

Timer - Assigned Phs	1	2	6	8
Phs Duration (G+Y+Rc), s	10.1	90.4	100.5	19.5
Change Period (Y+Rc), s	5.0	6.0	6.0	4.5
Max Green Setting (Gmax), s	13.0	64.0	82.0	27.5
Max Q Clear Time (g_c+I1), s	4.9	21.8	9.3	14.2
Green Ext Time (p_c), s	0.2	11.1	6.5	0.8

Intersection Summary

HCM 7th Control Delay, s/veh	13.8
HCM 7th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

Timings  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↔	↑↑	↔
Traffic Volume (vph)	760	61	1058	345
Future Volume (vph)	760	61	1058	345
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	85.0	18.0	103.0	32.0
Total Split (%)	63.0%	13.3%	76.3%	23.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	93.4	104.0	103.0	21.5
Actuated g/C Ratio	0.69	0.77	0.76	0.16
v/c Ratio	0.43	0.16	0.42	0.77
Control Delay (s/veh)	10.2	8.7	11.3	62.2
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	10.2	8.7	11.3	62.2
LOS	B	A	B	E
Approach Delay (s/veh)	10.2		11.1	62.2
Approach LOS	B		B	E

Intersection Summary

Cycle Length: 135
Actuated Cycle Length: 135
Offset: 65 (48%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle: 60
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.77
Intersection Signal Delay (s/veh): 18.9
Intersection LOS: B
Intersection Capacity Utilization 55.9%
ICU Level of Service B
Analysis Period (min) 15

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↔	↑↑	↔	
Traffic Volume (veh/h)	760	201	61	1058	345	50
Future Volume (veh/h)	760	201	61	1058	345	50
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No					
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	809	214	65	1126	416	0
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1985	525	443	2790	489	217
Arrive On Green	0.71	0.71	0.03	0.79	0.14	0.00
Sat Flow, veh/h	2873	735	1781	3647	3563	1585
Grp Volume(v), veh/h	517	506	65	1126	416	0
Grp Sat Flow(s),veh/h/ln	1777	1738	1781	1777	1781	1585
Q Serve(g_s), s	15.8	15.8	1.2	13.5	15.4	0.0
Cycle Q Clear(g_c), s	15.8	15.8	1.2	13.5	15.4	0.0
Prop In Lane	0.42					
Lane Grp Cap(c), veh/h	1269	1241	443	2790	489	217
V/C Ratio(X)	0.41	0.41	0.15	0.40	0.85	0.00
Avail Cap(c_a), veh/h	1269	1241	554	2790	726	323
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	7.8	7.8	5.3	4.6	56.9	0.0
Incr Delay (d2), s/veh	1.0	1.0	0.2	0.4	6.4	0.0
Initial Q Delay(d3), s/veh	0.0					
%ile BackOfQ(95%),veh/ln	9.9	9.7	0.7	7.7	11.8	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	8.7	8.8	5.4	5.0	63.3	0.0
LnGrp LOS	A	A	A	A	E	
Approach Vol, veh/h	1023		1191		416	
Approach Delay, s/veh	8.8		5.0		63.3	
Approach LOS	A		A		E	

Timer - Assigned Phs	1	2	6	8
Phs Duration (G+Y+Rc), s	9.6	102.4	112.0	23.0
Change Period (Y+Rc), s	5.0	6.0	6.0	4.5
Max Green Setting (Gmax), s	13.0	79.0	97.0	27.5
Max Q Clear Time (g_c+I1), s	3.2	17.8	15.5	17.4
Green Ext Time (p_c), s	0.1	8.4	11.0	1.1

Intersection Summary

HCM 7th Control Delay, s/veh	15.7
HCM 7th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

Timings  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↑	↑↑	↑↑
Traffic Volume (vph)	776	66	1074	345
Future Volume (vph)	776	66	1074	345
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	85.0	18.0	103.0	32.0
Total Split (%)	63.0%	13.3%	76.3%	23.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	92.8	103.5	102.5	22.0
Actuated g/C Ratio	0.69	0.77	0.76	0.16
v/c Ratio	0.44	0.18	0.43	0.77
Control Delay (s/veh)	10.6	8.9	11.6	61.8
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	10.6	8.9	11.6	61.8
LOS	B	A	B	E
Approach Delay (s/veh)	10.6		11.5	61.8
Approach LOS	B		B	E

Intersection Summary	
Cycle Length: 135	
Actuated Cycle Length: 135	
Offset: 65 (48%), Referenced to phase 2:EBT and 6:WBT, Start of Green	
Natural Cycle: 60	
Control Type: Actuated-Coordinated	
Maximum v/c Ratio: 0.77	
Intersection Signal Delay (s/veh): 19.3	Intersection LOS: B
Intersection Capacity Utilization 56.8%	ICU Level of Service B
Analysis Period (min) 15	

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑↑	
Traffic Volume (veh/h)	776	201	66	1074	345	63
Future Volume (veh/h)	776	201	66	1074	345	63
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No					
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	826	214	70	1143	430	0
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1983	514	433	2776	503	224
Arrive On Green	0.71	0.71	0.03	0.78	0.14	0.00
Sat Flow, veh/h	2887	724	1781	3647	3563	1585
Grp Volume(v), veh/h	525	515	70	1143	430	0
Grp Sat Flow(s),veh/h/ln	1777	1740	1781	1777	1781	1585
Q Serve(g_s), s	16.4	16.5	1.3	14.0	15.9	0.0
Cycle Q Clear(g_c), s	16.4	16.5	1.3	14.0	15.9	0.0
Prop In Lane	0.42					
Lane Grp Cap(c), veh/h	1261	1235	433	2776	503	224
V/C Ratio(X)	0.42	0.42	0.16	0.41	0.86	0.00
Avail Cap(c_a), veh/h	1261	1235	544	2776	726	323
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	8.1	8.1	5.5	4.8	56.6	0.0
Incr Delay (d2), s/veh	1.0	1.0	0.2	0.5	6.9	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	10.2	10.1	0.8	7.9	12.2	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	9.1	9.1	5.7	5.2	63.5	0.0
LnGrp LOS	A	A	A	A	E	
Approach Vol, veh/h	1040		1213		430	
Approach Delay, s/veh	9.1		5.2		63.5	
Approach LOS	A		A		E	
Timer - Assigned Phs	1	2	6		8	
Phs Duration (G+Y+Rc), s	9.6	101.8	111.5		23.5	
Change Period (Y+Rc), s	5.0	6.0	6.0		4.5	
Max Green Setting (Gmax), s	13.0	79.0	97.0		27.5	
Max Q Clear Time (g_c+I1), s	3.3	18.5	16.0		17.9	
Green Ext Time (p_c), s	0.1	8.6	11.3		1.1	

Intersection Summary	
HCM 7th Control Delay, s/veh	16.1
HCM 7th LOS	B

Notes  
User approved volume balancing among the lanes for turning movement.

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↵	↑↑	↑↑		↵	
Traffic Vol, veh/h	18	830	842	4	0	8
Future Vol, veh/h	18	830	842	4	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	20	922	936	4	0	9

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	940	0	-	0	1439 470
Stage 1	-	-	-	-	938 -
Stage 2	-	-	-	-	501 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	725	-	-	-	124 540
Stage 1	-	-	-	-	341 -
Stage 2	-	-	-	-	574 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	725	-	-	-	120 540
Mov Cap-2 Maneuver	-	-	-	-	242 -
Stage 1	-	-	-	-	332 -
Stage 2	-	-	-	-	574 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.21	0	11.78
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	725	-	-	-	540
HCM Lane V/C Ratio	0.028	-	-	-	0.016
HCM Ctrl Dly (s/v)	10.1	-	-	-	11.8
HCM Lane LOS	B	-	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↘	↑↑	↑↑		↘	
Traffic Vol, veh/h	18	845	853	4	0	8
Future Vol, veh/h	18	845	853	4	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	20	939	948	4	0	9

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	952	0	-	0	1459 476
Stage 1	-	-	-	-	950 -
Stage 2	-	-	-	-	509 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	717	-	-	-	120 535
Stage 1	-	-	-	-	336 -
Stage 2	-	-	-	-	568 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	717	-	-	-	117 535
Mov Cap-2 Maneuver	-	-	-	-	237 -
Stage 1	-	-	-	-	327 -
Stage 2	-	-	-	-	568 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.21	0	11.84
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	717	-	-	-	535
HCM Lane V/C Ratio	0.028	-	-	-	0.017
HCM Ctrl Dly (s/v)	10.2	-	-	-	11.8
HCM Lane LOS	B	-	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↵	↑↑	↑↑		↵	
Traffic Vol, veh/h	2	808	1085	1	3	8
Future Vol, veh/h	2	808	1085	1	3	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	860	1154	1	3	9

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1155	0	-	0	1589 578
Stage 1	-	-	-	-	1155 -
Stage 2	-	-	-	-	434 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	600	-	-	-	98 459
Stage 1	-	-	-	-	262 -
Stage 2	-	-	-	-	621 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	600	-	-	-	98 459
Mov Cap-2 Maneuver	-	-	-	-	203 -
Stage 1	-	-	-	-	261 -
Stage 2	-	-	-	-	621 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.03	0	15.91
HCM LOS			C

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	600	-	-	-	342
HCM Lane V/C Ratio	0.004	-	-	-	0.034
HCM Ctrl Dly (s/v)	11	-	-	-	15.9
HCM Lane LOS	B	-	-	-	C
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↙	↑↑	↑↑		↘	
Traffic Vol, veh/h	2	829	1098	1	3	8
Future Vol, veh/h	2	829	1098	1	3	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	882	1168	1	3	9

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	1169	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.14	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.22	-	-
Pot Cap-1 Maneuver	593	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	593	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.03	0	16.08
HCM LOS			C

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	593	-	-	-	337
HCM Lane V/C Ratio	0.004	-	-	-	0.035
HCM Ctrl Dly (s/v)	11.1	-	-	-	16.1
HCM Lane LOS	B	-	-	-	C
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection												
Int Delay, s/veh	0.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕		↵	↕			↕			↕	
Traffic Vol, veh/h	36	845	0	0	833	11	0	0	0	1	0	15
Future Vol, veh/h	36	845	0	0	833	11	0	0	0	1	0	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	-	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	40	939	0	0	926	12	0	0	0	1	0	17

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	938	0	0	939	0	0	1482	1957	469	1481	1951	469
Stage 1	-	-	-	-	-	-	1019	1019	-	932	932	-
Stage 2	-	-	-	-	-	-	463	938	-	549	1019	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	726	-	-	726	-	-	87	63	540	87	64	541
Stage 1	-	-	-	-	-	-	254	313	-	287	343	-
Stage 2	-	-	-	-	-	-	549	341	-	487	313	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	726	-	-	726	-	-	80	60	540	82	60	541
Mov Cap-2 Maneuver	-	-	-	-	-	-	178	163	-	196	172	-
Stage 1	-	-	-	-	-	-	240	295	-	287	343	-
Stage 2	-	-	-	-	-	-	532	341	-	460	295	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0.42	0	0	12.67
HCM LOS			A	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	726	-	-	726	-	-	487
HCM Lane V/C Ratio	-	0.055	-	-	-	-	-	0.036
HCM Ctrl Dly (s/v)	0	10.2	-	-	0	-	-	12.7
HCM Lane LOS	A	B	-	-	A	-	-	B
HCM 95th %tile Q(veh)	-	0.2	-	-	0	-	-	0.1

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑↑	↗	↙	↑↑			↕			↕	
Traffic Vol, veh/h	36	851	27	11	833	11	15	0	9	1	0	15
Future Vol, veh/h	36	851	27	11	833	11	15	0	9	1	0	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	120	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	40	946	30	12	926	12	17	0	10	1	0	17

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	938	0	0	976	0	0	1513	1988	473	1509	2012	469
Stage 1	-	-	-	-	-	-	1026	1026	-	956	956	-
Stage 2	-	-	-	-	-	-	487	962	-	553	1056	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	726	-	-	703	-	-	82	60	538	83	58	541
Stage 1	-	-	-	-	-	-	251	310	-	277	335	-
Stage 2	-	-	-	-	-	-	531	332	-	485	300	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	726	-	-	703	-	-	74	56	538	76	54	541
Mov Cap-2 Maneuver	-	-	-	-	-	-	173	158	-	185	160	-
Stage 1	-	-	-	-	-	-	238	293	-	272	329	-
Stage 2	-	-	-	-	-	-	505	327	-	450	284	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0.4	0.13	22.53	12.74
HCM LOS			C	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	232	726	-	-	703	-	-	483
HCM Lane V/C Ratio	0.115	0.055	-	-	0.017	-	-	0.037
HCM Ctrl Dly (s/v)	22.5	10.2	-	-	10.2	-	-	12.7
HCM Lane LOS	C	B	-	-	B	-	-	B
HCM 95th %tile Q(veh)	0.4	0.2	-	-	0.1	-	-	0.1

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕		↵	↕			↕			↕	↕
Traffic Vol, veh/h	9	832	0	0	1053	6	0	0	0	5	0	16
Future Vol, veh/h	9	832	0	0	1053	6	0	0	0	5	0	16
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	-	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	885	0	0	1120	6	0	0	0	5	0	17

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1127	0	0	885	0	0	1464	2031	443	1585	2028	563
Stage 1	-	-	-	-	-	-	904	904	-	1123	1123	-
Stage 2	-	-	-	-	-	-	560	1127	-	462	904	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	616	-	-	760	-	-	90	57	563	73	57	469
Stage 1	-	-	-	-	-	-	298	354	-	219	279	-
Stage 2	-	-	-	-	-	-	480	278	-	549	354	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	616	-	-	760	-	-	85	56	563	72	56	469
Mov Cap-2 Maneuver	-	-	-	-	-	-	199	163	-	168	167	-
Stage 1	-	-	-	-	-	-	293	348	-	219	279	-
Stage 2	-	-	-	-	-	-	463	278	-	541	348	-

Approach	EB			WB			NB			SB		
HCM Ctrl Dly, s/v	0.12			0			0			16.75		
HCM LOS							A			C		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	616	-	-	760	-	-	329
HCM Lane V/C Ratio	-	0.016	-	-	-	-	-	0.068
HCM Ctrl Dly (s/v)	0	10.9	-	-	0	-	-	16.8
HCM Lane LOS		A	B	-	-	A	-	C
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-	0.2

Intersection												
Int Delay, s/veh	0.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑↑	↗	↙	↑↑			↕			↕	
Traffic Vol, veh/h	9	840	29	13	1053	6	21	0	12	5	0	16
Future Vol, veh/h	9	840	29	13	1053	6	21	0	12	5	0	16
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	120	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	894	31	14	1120	6	22	0	13	5	0	17

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1127	0	0	924	0	0	1501	2067	447	1617	2095	563
Stage 1	-	-	-	-	-	-	913	913	-	1151	1151	-
Stage 2	-	-	-	-	-	-	588	1154	-	466	944	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	616	-	-	735	-	-	84	54	559	69	52	469
Stage 1	-	-	-	-	-	-	295	351	-	211	271	-
Stage 2	-	-	-	-	-	-	462	270	-	546	339	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	616	-	-	735	-	-	78	52	559	65	50	469
Mov Cap-2 Maneuver	-	-	-	-	-	-	192	157	-	157	155	-
Stage 1	-	-	-	-	-	-	290	345	-	207	266	-
Stage 2	-	-	-	-	-	-	437	265	-	525	334	-

Approach	EB			WB			NB			SB		
HCM Ctrl Dly, s/v	0.11			0.12			21.59			17.14		
HCM LOS							C			C		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	252	616	-	-	735	-	-	319
HCM Lane V/C Ratio	0.139	0.016	-	-	0.019	-	-	0.07
HCM Ctrl Dly (s/v)	21.6	10.9	-	-	10	-	-	17.1
HCM Lane LOS	C	B	-	-	A	-	-	C
HCM 95th %tile Q(veh)	0.5	0	-	-	0.1	-	-	0.2

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	881	0	0	848	0	0
Future Vol, veh/h	881	0	0	848	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	968	0	0	932	0	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	-	-	484
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	0	-	0	529
Stage 1	-	0	-	0	-
Stage 2	-	0	-	0	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	529
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT
Capacity (veh/h)	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-
HCM Ctrl Dly (s/v)	0	-	-	-
HCM Lane LOS	A	-	-	-
HCM 95th %tile Q(veh)	-	-	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	908	0	0	863	0	6
Future Vol, veh/h	908	0	0	863	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	998	0	0	948	0	7

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	12.05
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	517	-	-
HCM Lane V/C Ratio	0.013	-	-
HCM Ctrl Dly (s/v)	12.1	-	-
HCM Lane LOS	B	-	-
HCM 95th %tile Q(veh)	0	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	841	0	0	1069	0	0
Future Vol, veh/h	841	0	0	1069	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	895	0	0	1137	0	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	-	-	447
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	0	-	0	559
Stage 1	-	0	-	0	-
Stage 2	-	0	-	0	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	559
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT
Capacity (veh/h)	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-
HCM Ctrl Dly (s/v)	0	-	-	-
HCM Lane LOS	A	-	-	-
HCM 95th %tile Q(veh)	-	-	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	870	0	0	1090	0	8
Future Vol, veh/h	870	0	0	1090	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	926	0	0	1160	0	9

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	11.7
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	546	-	-
HCM Lane V/C Ratio	0.016	-	-
HCM Ctrl Dly (s/v)	11.7	-	-
HCM Lane LOS	B	-	-
HCM 95th %tile Q(veh)	0	-	-

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (vph)	403	513	235	230	1202	1448	660
Future Volume (vph)	403	513	235	230	1202	1448	660
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	10.0	10.0	3.0	10.0	10.0	10.0	10.0
Minimum Split (s)	17.0	25.0	10.0	25.0	25.0	25.0	17.0
Total Split (s)	32.0	36.0	24.0	28.0	60.0	60.0	32.0
Total Split (%)	26.7%	30.0%	20.0%	23.3%	50.0%	50.0%	26.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lead	Lag	Lead	Lag		Lead	
Lead-Lag Optimize?	Yes	Yes	Yes	Yes		Yes	
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	21.8	25.0	14.0	17.2	60.0	60.0	88.8
Actuated g/C Ratio	0.18	0.21	0.12	0.14	0.50	0.50	0.74
v/c Ratio	0.70	0.79	0.64	0.56	0.78	0.89	0.34
Control Delay (s/veh)	42.7	56.6	57.9	50.1	26.6	35.7	3.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	42.7	56.6	57.9	50.1	26.6	35.7	3.6
LOS	D	E	E	D	C	D	A
Approach Delay (s/veh)		50.6		53.8	26.6	25.6	
Approach LOS		D		D	C	C	

Intersection Summary

Cycle Length: 120
Actuated Cycle Length: 120
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 90
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.89
Intersection Signal Delay (s/veh): 33.0
Intersection LOS: C
Intersection Capacity Utilization 79.0%
ICU Level of Service D
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘	↖	→	↙	←	↑	↓	↘
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR		
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗	↖↗		↖↗	↖↗		
Traffic Volume (veh/h)	403	513	20	235	230	29	0	1202	590	0	1448	660		
Future Volume (veh/h)	403	513	20	235	230	29	0	1202	590	0	1448	660		
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0		
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00		1.00		1.00		
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Work Zone On Approach	No			No			No			No				
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870		
Adj Flow Rate, veh/h	438	558	22	255	250	32	0	1307	0	0	1574	717		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92		
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2		
Cap, veh/h	515	661	26	320	423	54	0	2772		0	1929	1930		
Arrive On Green	0.15	0.19	0.19	0.09	0.13	0.13	0.00	0.54	0.00	0.00	0.54	0.54		
Sat Flow, veh/h	3456	3485	137	3456	3173	402	0	5443	0	0	3647	2790		
Grp Volume(v), veh/h	438	284	296	255	139	143	0	1307	0	0	1574	717		
Grp Sat Flow(s),veh/h/ln	1728	1777	1846	1728	1777	1798	0	1702	0	0	1777	1395		
Q Serve(g_s), s	14.8	18.5	18.6	8.7	8.8	9.0	0.0	18.9	0.0	0.0	43.6	12.8		
Cycle Q Clear(g_c), s	14.8	18.5	18.6	8.7	8.8	9.0	0.0	18.9	0.0	0.0	43.6	12.8		
Prop In Lane	1.00		0.07	1.00		0.22	0.00		0.00	0.00		1.00		
Lane Grp Cap(c), veh/h	515	337	350	320	237	240	0	2772		0	1929	1930		
V/C Ratio(X)	0.85	0.84	0.85	0.80	0.59	0.60	0.00	0.47		0.00	0.82	0.37		
Avail Cap(c_a), veh/h	720	429	446	490	311	315	0	2772		0	1929	1930		
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00		
Uniform Delay (d), s/veh	49.8	46.9	46.9	53.3	48.9	49.0	0.0	16.9	0.0	0.0	22.5	7.7		
Incr Delay (d2), s/veh	7.0	11.6	11.4	5.2	2.3	2.4	0.0	0.6	0.0	0.0	3.9	0.6		
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
%ile BackOfQ(95%),veh/ln	11.1	14.1	14.6	7.1	7.3	7.5	0.0	11.7	0.0	0.0	25.2	6.5		
Unsig. Movement Delay, s/veh														
LnGrp Delay(d), s/veh	56.8	58.6	58.4	58.5	51.2	51.4	0.0	17.4	0.0	0.0	26.5	8.2		
LnGrp LOS	E	E	E	E	D	D		B			C	A		
Approach Vol, veh/h		1018			537			1307			2291			
Approach Delay, s/veh		57.7			54.7			17.4			20.8			
Approach LOS		E			D			B			C			

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	72.1	18.1	29.7	72.1	24.9	23.0
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0
Max Green Setting (Gmax), s	53.0	17.0	29.0	53.0	25.0	21.0
Max Q Clear Time (g_c+I1), s	20.9	10.7	20.6	45.6	16.8	11.0
Green Ext Time (p_c), s	11.5	0.4	2.2	6.3	1.0	1.0

Intersection Summary

HCM 7th Control Delay, s/veh	30.8
HCM 7th LOS	C

Notes

Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (vph)	409	516	235	233	1202	1448	668
Future Volume (vph)	409	516	235	233	1202	1448	668
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	10.0	10.0	3.0	10.0	10.0	10.0	10.0
Minimum Split (s)	17.0	25.0	10.0	25.0	25.0	25.0	17.0
Total Split (s)	32.0	36.0	24.0	28.0	60.0	60.0	32.0
Total Split (%)	26.7%	30.0%	20.0%	23.3%	50.0%	50.0%	26.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Lead/Lag	Lead	Lag	Lead	Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	21.9	25.2	14.0	17.3	59.8	59.8	88.7
Actuated g/C Ratio	0.18	0.21	0.12	0.14	0.50	0.50	0.74
v/c Ratio	0.71	0.79	0.64	0.56	0.79	0.89	0.34
Control Delay (s/veh)	43.0	55.8	57.9	50.0	26.9	36.1	3.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	43.0	55.8	57.9	50.0	26.9	36.1	3.7
LOS	D	E	E	D	C	D	A
Approach Delay (s/veh)		50.3		53.8	26.9	25.9	
Approach LOS		D		D	C	C	

Intersection Summary

Cycle Length: 120
Actuated Cycle Length: 120
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 90
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.89
Intersection Signal Delay (s/veh): 33.1
Intersection LOS: C
Intersection Capacity Utilization 79.3%
ICU Level of Service D
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘	↙	↘	↙	↘	
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (veh/h)	409	516	26	235	233	29	0	1202	590	0	1448	668
Future Volume (veh/h)	409	516	26	235	233	29	0	1202	590	0	1448	668
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00		1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870
Adj Flow Rate, veh/h	445	561	28	255	253	32	0	1307	0	0	1574	726
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2
Cap, veh/h	521	662	33	320	425	53	0	2758		0	1920	1928
Arrive On Green	0.15	0.19	0.19	0.09	0.13	0.13	0.00	0.54	0.00	0.00	0.54	0.54
Sat Flow, veh/h	3456	3445	172	3456	3178	398	0	5443	0	0	3647	2790
Grp Volume(v), veh/h	445	289	300	255	140	145	0	1307	0	0	1574	726
Grp Sat Flow(s),veh/h/ln	1728	1777	1839	1728	1777	1799	0	1702	0	0	1777	1395
Q Serve(g_s), s	15.1	18.8	18.9	8.7	8.9	9.1	0.0	19.0	0.0	0.0	43.9	13.0
Cycle Q Clear(g_c), s	15.1	18.8	18.9	8.7	8.9	9.1	0.0	19.0	0.0	0.0	43.9	13.0
Prop In Lane	1.00		0.09	1.00		0.22	0.00		0.00	0.00		1.00
Lane Grp Cap(c), veh/h	521	341	353	320	238	241	0	2758		0	1920	1928
V/C Ratio(X)	0.85	0.85	0.85	0.80	0.59	0.60	0.00	0.47		0.00	0.82	0.38
Avail Cap(c_a), veh/h	720	429	445	490	311	315	0	2758		0	1920	1928
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00
Uniform Delay (d), s/veh	49.7	46.8	46.8	53.3	48.9	48.9	0.0	17.0	0.0	0.0	22.8	7.7
Incr Delay (d2), s/veh	7.3	12.1	11.9	5.2	2.3	2.4	0.0	0.6	0.0	0.0	4.1	0.6
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	11.3	14.4	14.8	7.1	7.3	7.6	0.0	11.7	0.0	0.0	25.4	6.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	56.9	58.8	58.7	58.5	51.2	51.3	0.0	17.6	0.0	0.0	26.8	8.3
LnGrp LOS	E	E	E	E	D	D		B			C	A
Approach Vol, veh/h		1034			540			1307			2300	
Approach Delay, s/veh		58.0			54.7			17.6			21.0	
Approach LOS		E			D			B			C	

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	71.8	18.1	30.1	71.8	25.1	23.1
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0
Max Green Setting (Gmax), s	53.0	17.0	29.0	53.0	25.0	21.0
Max Q Clear Time (g_c+H1), s	21.0	10.7	20.9	45.9	17.1	11.1
Green Ext Time (p_c), s	11.5	0.4	2.2	6.1	1.0	1.0

Intersection Summary	
HCM 7th Control Delay, s/veh	31.0
HCM 7th LOS	C

Notes  
 Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (vph)	533	352	464	591	1603	1220	613
Future Volume (vph)	533	352	464	591	1603	1220	613
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5	9.5	22.5	22.5	22.5	9.5
Total Split (s)	32.0	34.0	32.0	34.0	69.0	69.0	32.0
Total Split (%)	23.7%	25.2%	23.7%	25.2%	51.1%	51.1%	23.7%
Yellow Time (s)	3.5	3.0	3.5	3.0	3.0	3.0	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.0	4.5	4.0	4.0	4.0	4.5
Lead/Lag	Lead	Lag	Lead	Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	26.2	31.2	23.8	28.7	67.5	67.5	97.8
Actuated g/C Ratio	0.19	0.23	0.18	0.21	0.50	0.50	0.72
v/c Ratio	0.84	0.48	0.81	0.89	0.86	0.73	0.32
Control Delay (s/veh)	73.0	37.1	64.1	65.8	33.5	30.1	6.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	73.0	37.1	64.1	65.8	33.5	30.1	6.8
LOS	E	D	E	E	C	C	A
Approach Delay (s/veh)		58.2		65.1	33.5	22.3	
Approach LOS		E		E	C	C	

Intersection Summary	
Cycle Length: 135	
Actuated Cycle Length: 135	
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green	
Natural Cycle: 90	
Control Type: Actuated-Coordinated	
Maximum v/c Ratio: 0.89	
Intersection Signal Delay (s/veh): 39.7	Intersection LOS: D
Intersection Capacity Utilization 83.5%	ICU Level of Service E
Analysis Period (min) 15	

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↖	↙	↑	↘	↘	↓	↖	
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗	↖↗		↖↗	↖↗
Traffic Volume (veh/h)	533	352	21	464	591	41	0	1603	433	0	1220	613
Future Volume (veh/h)	533	352	21	464	591	41	0	1603	433	0	1220	613
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00	1.00	1.00	1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870
Adj Flow Rate, veh/h	561	371	22	488	622	43	0	1687	0	0	1284	645
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2
Cap, veh/h	623	765	45	556	692	48	0	2666		0	1856	1959
Arrive On Green	0.18	0.22	0.22	0.16	0.21	0.21	0.00	0.52	0.00	0.00	0.52	0.52
Sat Flow, veh/h	3456	3409	201	3456	3372	233	0	5443	0	0	3647	2790
Grp Volume(v), veh/h	561	193	200	488	327	338	0	1687	0	0	1284	645
Grp Sat Flow(s),veh/h/ln	1728	1777	1834	1728	1777	1828	0	1702	0	0	1777	1395
Q Serve(g_s), s	21.4	12.7	12.8	18.6	24.2	24.3	0.0	31.8	0.0	0.0	36.5	12.1
Cycle Q Clear(g_c), s	21.4	12.7	12.8	18.6	24.2	24.3	0.0	31.8	0.0	0.0	36.5	12.1
Prop In Lane	1.00		0.11	1.00		0.13	0.00		0.00	0.00		1.00
Lane Grp Cap(c), veh/h	623	399	412	556	364	375	0	2666		0	1856	1959
V/C Ratio(X)	0.90	0.48	0.49	0.88	0.90	0.90	0.00	0.63		0.00	0.69	0.33
Avail Cap(c_a), veh/h	704	399	412	704	395	406	0	2666		0	1856	1959
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00
Uniform Delay (d), s/veh	54.2	45.5	45.6	55.4	52.3	52.3	0.0	23.0	0.0	0.0	24.1	7.8
Incr Delay (d2), s/veh	13.7	0.9	0.9	10.2	21.7	21.6	0.0	1.2	0.0	0.0	2.1	0.5
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	15.7	9.6	9.9	13.7	18.8	19.3	0.0	18.6	0.0	0.0	21.9	6.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	67.9	46.4	46.5	65.6	74.0	73.9	0.0	24.2	0.0	0.0	26.3	8.2
LnGrp LOS	E	D	D	E	E	E		C			C	A
Approach Vol, veh/h		954			1153			1687			1929	
Approach Delay, s/veh		59.0			70.4			24.2			20.2	
Approach LOS		E			E			C			C	

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	74.5	26.2	34.3	74.5	28.8	31.7
Change Period (Y+Rc), s	4.0	4.5	4.0	4.0	4.5	4.0
Max Green Setting (Gmax), s	65.0	27.5	30.0	65.0	27.5	30.0
Max Q Clear Time (g_c+I1), s	33.8	20.6	14.8	38.5	23.4	26.3
Green Ext Time (p_c), s	15.9	1.1	1.9	14.6	0.9	1.4

Intersection Summary	
HCM 7th Control Delay, s/veh	38.0
HCM 7th LOS	D

Notes  
 Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↑	↓	↘
Lane Group	EBL	EBT	WBL	WBT	NBT	SBT	SBR
Lane Configurations	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗	↖↗
Traffic Volume (vph)	542	356	464	595	1603	1220	622
Future Volume (vph)	542	356	464	595	1603	1220	622
Turn Type	Prot	NA	Prot	NA	NA	NA	pm+ov
Protected Phases	7	4	3	8	2	6	7
Permitted Phases							6
Detector Phase	7	4	3	8	2	6	7
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5	9.5	22.5	22.5	22.5	9.5
Total Split (s)	32.0	34.0	32.0	34.0	69.0	69.0	32.0
Total Split (%)	23.7%	25.2%	23.7%	25.2%	51.1%	51.1%	23.7%
Yellow Time (s)	3.5	3.0	3.5	3.0	3.0	3.0	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.0	4.5	4.0	4.0	4.0	4.5
Lead/Lag	Lead	Lag	Lead	Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes	Yes			Yes
Recall Mode	None	None	None	None	C-Max	C-Max	None
Act Effct Green (s)	26.4	31.4	23.8	28.8	67.3	67.3	97.7
Actuated g/C Ratio	0.20	0.23	0.18	0.21	0.50	0.50	0.72
v/c Ratio	0.85	0.50	0.81	0.89	0.86	0.73	0.32
Control Delay (s/veh)	73.9	37.1	64.1	66.0	33.8	30.2	6.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	73.9	37.1	64.1	66.0	33.8	30.2	6.9
LOS	E	D	E	E	C	C	A
Approach Delay (s/veh)		58.6		65.2	33.8	22.3	
Approach LOS		E		E	C	C	

Intersection Summary

Cycle Length: 135
Actuated Cycle Length: 135
Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
Natural Cycle: 90
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.89
Intersection Signal Delay (s/veh): 40.0
Intersection LOS: D
Intersection Capacity Utilization 83.8%
ICU Level of Service E
Analysis Period (min) 15

Splits and Phases: 1: Saint Francis Dr & Cerrillos Rd



HCM 7th Signalized Intersection Summary

1: Saint Francis Dr & Cerrillos Rd

11/18/2025

	↖	→	↙	←	↗	↘	↑	↖	↙	↓	↘	
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↖↗		↖↗	↖↗			↖↗	↖↗		↖↗	↖↗
Traffic Volume (veh/h)	542	356	29	464	595	41	0	1603	433	0	1220	622
Future Volume (veh/h)	542	356	29	464	595	41	0	1603	433	0	1220	622
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00			1.00	1.00			1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	0	1870	1870
Adj Flow Rate, veh/h	571	375	31	488	626	43	0	1687	0	0	1284	655
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	0	2	2
Cap, veh/h	631	757	62	556	695	48	0	2649		0	1844	1957
Arrive On Green	0.18	0.23	0.23	0.16	0.21	0.21	0.00	0.52	0.00	0.00	0.52	0.52
Sat Flow, veh/h	3456	3324	274	3456	3374	231	0	5443	0	0	3647	2790
Grp Volume(v), veh/h	571	200	206	488	329	340	0	1687	0	0	1284	655
Grp Sat Flow(s),veh/h/ln	1728	1777	1821	1728	1777	1829	0	1702	0	0	1777	1395
Q Serve(g_s), s	21.8	13.2	13.3	18.6	24.4	24.5	0.0	32.1	0.0	0.0	36.7	12.4
Cycle Q Clear(g_c), s	21.8	13.2	13.3	18.6	24.4	24.5	0.0	32.1	0.0	0.0	36.7	12.4
Prop In Lane	1.00		0.15	1.00		0.13	0.00		0.00	0.00		1.00
Lane Grp Cap(c), veh/h	631	405	415	556	366	376	0	2649		0	1844	1957
V/C Ratio(X)	0.90	0.49	0.50	0.88	0.90	0.90	0.00	0.64		0.00	0.70	0.33
Avail Cap(c_a), veh/h	704	405	415	704	395	406	0	2649		0	1844	1957
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	0.00	0.00	1.00	1.00
Uniform Delay (d), s/veh	54.0	45.3	45.4	55.4	52.3	52.3	0.0	23.3	0.0	0.0	24.5	7.9
Incr Delay (d2), s/veh	14.2	0.9	0.9	10.2	22.0	21.9	0.0	1.2	0.0	0.0	2.2	0.5
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	16.0	9.9	10.2	13.7	18.9	19.4	0.0	18.7	0.0	0.0	22.1	6.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	68.3	46.3	46.3	65.6	74.3	74.2	0.0	24.5	0.0	0.0	26.7	8.3
LnGrp LOS	E	D	D	E	E	E		C			C	A
Approach Vol, veh/h		977			1157			1687			1939	
Approach Delay, s/veh		59.1			70.6			24.5			20.5	
Approach LOS		E			E			C			C	

Timer - Assigned Phs	2	3	4	6	7	8
Phs Duration (G+Y+Rc), s	74.0	26.2	34.8	74.0	29.2	31.8
Change Period (Y+Rc), s	4.0	4.5	4.0	4.0	4.5	4.0
Max Green Setting (Gmax), s	65.0	27.5	30.0	65.0	27.5	30.0
Max Q Clear Time (g_c+I1), s	34.1	20.6	15.3	38.7	23.8	26.5
Green Ext Time (p_c), s	15.9	1.1	2.0	14.6	0.8	1.3

Intersection Summary	
HCM 7th Control Delay, s/veh	38.3
HCM 7th LOS	D

Notes  
 Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.

Timings  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↑	↑↑	↑↑
Traffic Volume (vph)	888	157	777	243
Future Volume (vph)	888	157	777	243
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	70.0	18.0	88.0	32.0
Total Split (%)	58.3%	15.0%	73.3%	26.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	78.5	94.1	93.1	16.4
Actuated g/C Ratio	0.65	0.78	0.78	0.14
v/c Ratio	0.60	0.53	0.31	0.70
Control Delay (s/veh)	13.4	14.3	3.3	53.1
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	13.4	14.3	3.3	53.1
LOS	B	B	A	D
Approach Delay (s/veh)	13.4		5.2	53.1
Approach LOS	B		A	D

Intersection Summary	
Cycle Length: 120	
Actuated Cycle Length: 120	
Offset: 110 (92%), Referenced to phase 2:EBT and 6:WBTL, Start of Green	
Natural Cycle: 65	
Control Type: Actuated-Coordinated	
Maximum v/c Ratio: 0.70	
Intersection Signal Delay (s/veh): 15.2	Intersection LOS: B
Intersection Capacity Utilization 65.7%	ICU Level of Service C
Analysis Period (min) 15	

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑↑	
Traffic Volume (veh/h)	888	333	157	777	243	63
Future Volume (veh/h)	888	333	157	777	243	63
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No					
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	976	366	173	854	168	175
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1760	654	344	2775	235	209
Arrive On Green	0.69	0.69	0.04	0.78	0.13	0.13
Sat Flow, veh/h	2629	942	1781	3647	1781	1585
Grp Volume(v), veh/h	681	661	173	854	168	175
Grp Sat Flow(s),veh/h/ln	1777	1701	1781	1777	1781	1585
Q Serve(g_s), s	22.8	23.3	3.2	8.3	10.8	12.9
Cycle Q Clear(g_c), s	22.8	23.3	3.2	8.3	10.8	12.9
Prop In Lane	1.00					
Lane Grp Cap(c), veh/h	1233	1181	344	2775	235	209
V/C Ratio(X)	0.55	0.56	0.50	0.31	0.72	0.84
Avail Cap(c_a), veh/h	1233	1181	456	2775	408	363
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	9.1	9.2	8.6	3.8	49.9	50.8
Incr Delay (d2), s/veh	1.8	1.9	1.1	0.3	4.0	8.6
Initial Q Delay(d3), s/veh	0.0					
%ile BackOfQ(95%),veh/ln	13.2	13.0	2.3	4.5	8.8	9.5
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	10.9	11.1	9.8	4.1	54.0	59.5
LnGrp LOS	B	B	A	A	D	E
Approach Vol, veh/h	1342		1027		343	
Approach Delay, s/veh	11.0		5.0		56.8	
Approach LOS	B		A		E	

Timer - Assigned Phs	1	2	6	8
Phs Duration (G+Y+Rc), s	10.4	89.3	99.7	20.3
Change Period (Y+Rc), s	5.0	6.0	6.0	4.5
Max Green Setting (Gmax), s	13.0	64.0	82.0	27.5
Max Q Clear Time (g_c+I1), s	5.2	25.3	10.3	14.9
Green Ext Time (p_c), s	0.3	12.5	7.2	0.9

Intersection Summary	
HCM 7th Control Delay, s/veh	14.5
HCM 7th LOS	B

Notes  
User approved volume balancing among the lanes for turning movement.

Timings

2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↔	↑↑	↔
Traffic Volume (vph)	902	161	788	243
Future Volume (vph)	902	161	788	243
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	70.0	18.0	88.0	32.0
Total Split (%)	58.3%	15.0%	73.3%	26.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	77.9	93.8	92.8	16.7
Actuated g/C Ratio	0.65	0.78	0.77	0.14
v/c Ratio	0.61	0.55	0.32	0.71
Control Delay (s/veh)	14.0	15.6	3.4	52.3
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	14.0	15.6	3.4	52.3
LOS	B	B	A	D
Approach Delay (s/veh)	14.0		5.5	52.3
Approach LOS	B		A	D

Intersection Summary

Cycle Length: 120
Actuated Cycle Length: 120
Offset: 110 (92%), Referenced to phase 2:EBT and 6:WBTL, Start of Green
Natural Cycle: 65
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.71
Intersection Signal Delay (s/veh): 15.6
Intersection LOS: B
Intersection Capacity Utilization 66.7%
ICU Level of Service C
Analysis Period (min) 15

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary

2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↔	↑↑	↔	
Traffic Volume (veh/h)	902	333	161	788	243	75
Future Volume (veh/h)	902	333	161	788	243	75
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No					
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	991	366	177	866	175	181
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1756	642	338	2761	242	215
Arrive On Green	0.69	0.69	0.05	0.78	0.14	0.14
Sat Flow, veh/h	2641	932	1781	3647	1781	1585
Grp Volume(v), veh/h	688	689	177	866	175	181
Grp Sat Flow(s),veh/h/ln	1777	1703	1781	1777	1781	1585
Q Serve(g_s), s	23.6	24.1	3.3	8.6	11.3	13.4
Cycle Q Clear(g_c), s	23.6	24.1	3.3	8.6	11.3	13.4
Prop In Lane	1.00					
Lane Grp Cap(c), veh/h	1224	1173	338	2761	242	215
V/C Ratio(X)	0.56	0.57	0.52	0.31	0.72	0.84
Avail Cap(c_a), veh/h	1224	1173	449	2761	408	363
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	9.5	9.6	9.2	3.9	49.7	50.6
Incr Delay (d2), s/veh	1.9	2.0	1.3	0.3	4.1	8.6
Initial Q Delay(d3), s/veh	0.0					
%ile BackOfQ(95%),veh/ln	13.7	13.5	2.6	4.7	9.1	9.8
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	11.3	11.6	10.5	4.2	53.8	59.2
LnGrp LOS	B	B	B	A	D	E
Approach Vol, veh/h	1357		1043		356	
Approach Delay, s/veh	11.5		5.3		56.6	
Approach LOS	B		A		E	

Timer - Assigned Phs	1	2	6	8
Phs Duration (G+Y+Rc), s	10.5	88.7	99.2	20.8
Change Period (Y+Rc), s	5.0	6.0	6.0	4.5
Max Green Setting (Gmax), s	13.0	64.0	82.0	27.5
Max Q Clear Time (g_c+I1), s	5.3	26.1	10.6	15.4
Green Ext Time (p_c), s	0.3	12.6	7.3	0.9

Intersection Summary

HCM 7th Control Delay, s/veh	14.9
HCM 7th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

Timings

2: Cordova Rd & Cerrillos Rd

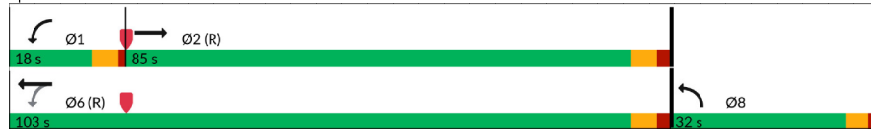
11/18/2025

	→	↖	←	↗
Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↖	↑↑	↗
Traffic Volume (vph)	834	67	1161	379
Future Volume (vph)	834	67	1161	379
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	85.0	18.0	103.0	32.0
Total Split (%)	63.0%	13.3%	76.3%	23.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	91.8	102.6	101.6	22.9
Actuated g/C Ratio	0.68	0.76	0.75	0.17
v/c Ratio	0.48	0.20	0.46	0.79
Control Delay (s/veh)	11.6	9.7	13.3	62.5
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	11.6	9.7	13.3	62.5
LOS	B	A	B	E
Approach Delay (s/veh)	11.6		13.1	62.5
Approach LOS	B		B	E

Intersection Summary

Cycle Length: 135
Actuated Cycle Length: 135
Offset: 65 (48%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle: 60
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.79
Intersection Signal Delay (s/veh): 20.4
Intersection LOS: C
Intersection Capacity Utilization 59.7%
ICU Level of Service B
Analysis Period (min) 15

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary

2: Cordova Rd & Cerrillos Rd

11/18/2025

	→	↖	←	↗	↘	
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↗	
Traffic Volume (veh/h)	834	221	67	1161	379	55
Future Volume (veh/h)	834	221	67	1161	379	55
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No	No	No	No	No	No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	887	235	71	1235	458	0
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1951	516	397	2748	530	236
Arrive On Green	0.70	0.70	0.03	0.77	0.15	0.00
Sat Flow, veh/h	2873	736	1781	3647	3563	1585
Grp Volume(v), veh/h	567	555	71	1235	458	0
Grp Sat Flow(s),veh/h/ln	1777	1738	1781	1777	1781	1585
Q Serve(g_s), s	18.9	18.9	1.4	16.3	17.0	0.0
Cycle Q Clear(g_c), s	18.9	18.9	1.4	16.3	17.0	0.0
Prop In Lane		0.42	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	1247	1220	397	2748	530	236
V/C Ratio(X)	0.45	0.46	0.18	0.45	0.86	0.00
Avail Cap(c_a), veh/h	1247	1220	507	2748	726	323
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	8.8	8.8	6.2	5.3	56.1	0.0
Incr Delay (d2), s/veh	1.2	1.2	0.2	0.5	8.0	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	11.5	11.3	0.9	9.1	12.9	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	10.0	10.0	6.5	5.8	64.1	0.0
LnGrp LOS	B	B	A	A	E	
Approach Vol, veh/h	1122			1306	458	
Approach Delay, s/veh	10.0			5.9	64.1	
Approach LOS	B			A	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	9.7	100.7			110.4	24.6
Change Period (Y+Rc), s	5.0	6.0			6.0	4.5
Max Green Setting (Gmax), s	13.0	79.0			97.0	27.5
Max Q Clear Time (g_c+I1), s	3.4	20.9			18.3	19.0
Green Ext Time (p_c), s	0.1	9.7			12.9	1.1

Intersection Summary

HCM 7th Control Delay, s/veh	16.7
HCM 7th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

Timings  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Lane Group	EBT	WBL	WBT	NBL
Lane Configurations	↑↑	↑	↑↑	↑↑
Traffic Volume (vph)	850	72	1177	379
Future Volume (vph)	850	72	1177	379
Turn Type	NA	pm+pt	NA	Prot
Protected Phases	2	1	6	8
Permitted Phases	6			
Detector Phase	2	1	6	8
Switch Phase				
Minimum Initial (s)	15.0	5.0	15.0	5.0
Minimum Split (s)	24.0	10.0	24.0	22.5
Total Split (s)	85.0	18.0	103.0	32.0
Total Split (%)	63.0%	13.3%	76.3%	23.7%
Yellow Time (s)	4.0	4.0	4.0	3.5
All-Red Time (s)	2.0	1.0	2.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	5.0	6.0	4.5
Lead/Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes		
Recall Mode	C-Max	None	C-Max	None
Act Effct Green (s)	91.3	102.2	101.2	23.3
Actuated g/C Ratio	0.68	0.76	0.75	0.17
v/c Ratio	0.49	0.22	0.47	0.80
Control Delay (s/veh)	11.9	9.9	13.6	62.4
Queue Delay	0.0	0.0	0.0	0.0
Total Delay (s/veh)	11.9	9.9	13.6	62.4
LOS	B	A	B	E
Approach Delay (s/veh)	11.9		13.4	62.4
Approach LOS	B		B	E

Intersection Summary

Cycle Length: 135
Actuated Cycle Length: 135
Offset: 65 (48%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle: 60
Control Type: Actuated-Coordinated
Maximum v/c Ratio: 0.80
Intersection Signal Delay (s/veh): 20.7
Intersection LOS: C
Intersection Capacity Utilization 60.6%
ICU Level of Service B
Analysis Period (min) 15

Splits and Phases: 2: Cordova Rd & Cerrillos Rd



HCM 7th Signalized Intersection Summary  
2: Cordova Rd & Cerrillos Rd

11/18/2025

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑↑	
Traffic Volume (veh/h)	850	221	72	1177	379	68
Future Volume (veh/h)	850	221	72	1177	379	68
Initial Q (Qb), veh	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00					
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No					
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	904	235	77	1252	470	0
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1949	506	389	2736	542	241
Arrive On Green	0.70	0.70	0.03	0.77	0.15	0.00
Sat Flow, veh/h	2885	725	1781	3647	3563	1585
Grp Volume(v), veh/h	575	564	77	1252	470	0
Grp Sat Flow(s),veh/h/ln	1777	1740	1781	1777	1781	1585
Q Serve(g_s), s	19.5	19.6	1.5	16.9	17.4	0.0
Cycle Q Clear(g_c), s	19.5	19.6	1.5	16.9	17.4	0.0
Prop In Lane	0.42					
Lane Grp Cap(c), veh/h	1240	1214	389	2736	542	241
V/C Ratio(X)	0.46	0.46	0.20	0.46	0.87	0.00
Avail Cap(c_a), veh/h	1240	1214	498	2736	726	323
HCM Platoon Ratio	1.00					
Upstream Filter(I)	1.00					
Uniform Delay (d), s/veh	9.1	9.1	6.5	5.5	55.9	0.0
Incr Delay (d2), s/veh	1.2	1.3	0.2	0.6	8.5	0.0
Initial Q Delay(d3), s/veh	0.0					
%ile BackOfQ(95%),veh/ln	11.9	11.7	1.0	9.4	13.2	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	10.3	10.4	6.8	6.1	64.4	0.0
LnGrp LOS	B	B	A	A	E	
Approach Vol, veh/h	1139		1329		470	
Approach Delay, s/veh	10.4		6.1		64.4	
Approach LOS	B		A		E	

Timer - Assigned Phs	1	2	6	8
Phs Duration (G+Y+Rc), s	9.7	100.2	110.0	25.0
Change Period (Y+Rc), s	5.0	6.0	6.0	4.5
Max Green Setting (Gmax), s	13.0	79.0	97.0	27.5
Max Q Clear Time (g_c+H1), s	3.5	21.6	18.9	19.4
Green Ext Time (p_c), s	0.1	10.0	13.2	1.2

Intersection Summary

HCM 7th Control Delay, s/veh	17.1
HCM 7th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↵	↑↑	↑↑		↵	
Traffic Vol, veh/h	20	912	924	4	0	9
Future Vol, veh/h	20	912	924	4	0	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	1013	1027	4	0	10

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1031	0	-	0	1580 516
Stage 1	-	-	-	-	1029 -
Stage 2	-	-	-	-	551 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	670	-	-	-	100 504
Stage 1	-	-	-	-	306 -
Stage 2	-	-	-	-	541 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	670	-	-	-	96 504
Mov Cap-2 Maneuver	-	-	-	-	214 -
Stage 1	-	-	-	-	295 -
Stage 2	-	-	-	-	541 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.23	0	12.28
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	670	-	-	-	504
HCM Lane V/C Ratio	0.033	-	-	-	0.02
HCM Ctrl Dly (s/v)	10.6	-	-	-	12.3
HCM Lane LOS	B	-	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↵	↑↑	↑↑		↵	
Traffic Vol, veh/h	20	927	935	4	0	9
Future Vol, veh/h	20	927	935	4	0	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	1030	1039	4	0	10

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1043	0	-	0	1601 522
Stage 1	-	-	-	-	1041 -
Stage 2	-	-	-	-	559 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	662	-	-	-	97 500
Stage 1	-	-	-	-	301 -
Stage 2	-	-	-	-	536 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	662	-	-	-	93 500
Mov Cap-2 Maneuver	-	-	-	-	210 -
Stage 1	-	-	-	-	291 -
Stage 2	-	-	-	-	536 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.22	0	12.35
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	662	-	-	-	500
HCM Lane V/C Ratio	0.034	-	-	-	0.02
HCM Ctrl Dly (s/v)	10.6	-	-	-	12.4
HCM Lane LOS	B	-	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↘	↑↑	↑↑		↘	
Traffic Vol, veh/h	2	887	1192	1	3	9
Future Vol, veh/h	2	887	1192	1	3	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	944	1268	1	3	10

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1269	0	-	0	1745 635
Stage 1	-	-	-	-	1269 -
Stage 2	-	-	-	-	476 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	543	-	-	-	77 421
Stage 1	-	-	-	-	228 -
Stage 2	-	-	-	-	591 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	543	-	-	-	77 421
Mov Cap-2 Maneuver	-	-	-	-	176 -
Stage 1	-	-	-	-	227 -
Stage 2	-	-	-	-	591 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.03	0	17
HCM LOS			C

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	543	-	-	-	313
HCM Lane V/C Ratio	0.004	-	-	-	0.041
HCM Ctrl Dly (s/v)	11.7	-	-	-	17
HCM Lane LOS	B	-	-	-	C
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↘	↑↑	↑↑		↘	
Traffic Vol, veh/h	2	908	1205	1	3	9
Future Vol, veh/h	2	908	1205	1	3	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	966	1282	1	3	10

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1283	0	-	0	1770 641
Stage 1	-	-	-	-	1282 -
Stage 2	-	-	-	-	487 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	537	-	-	-	75 417
Stage 1	-	-	-	-	224 -
Stage 2	-	-	-	-	583 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	537	-	-	-	74 417
Mov Cap-2 Maneuver	-	-	-	-	173 -
Stage 1	-	-	-	-	223 -
Stage 2	-	-	-	-	583 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0.03	0	17.18
HCM LOS			C

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	537	-	-	-	308
HCM Lane V/C Ratio	0.004	-	-	-	0.041
HCM Ctrl Dly (s/v)	11.7	-	-	-	17.2
HCM Lane LOS	B	-	-	-	C
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection												
Int Delay, s/veh	0.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕		↵	↕			↕			↕	
Traffic Vol, veh/h	39	927	0	0	915	12	0	0	0	1	0	17
Future Vol, veh/h	39	927	0	0	915	12	0	0	0	1	0	17
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	-	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	43	1030	0	0	1017	13	0	0	0	1	0	19

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1030	0	0	1030	0	0	1625	2147	515	1625	2140	515
Stage 1	-	-	-	-	-	-	1117	1117	-	1023	1023	-
Stage 2	-	-	-	-	-	-	508	1030	-	602	1117	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	670	-	-	670	-	-	68	48	505	68	48	505
Stage 1	-	-	-	-	-	-	221	281	-	252	311	-
Stage 2	-	-	-	-	-	-	515	309	-	453	281	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	670	-	-	670	-	-	61	45	505	64	45	505
Mov Cap-2 Maneuver	-	-	-	-	-	-	153	141	-	171	150	-
Stage 1	-	-	-	-	-	-	207	263	-	252	311	-
Stage 2	-	-	-	-	-	-	496	309	-	424	263	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0.43	0	0	13.27
HCM LOS			A	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	670	-	-	670	-	-	455
HCM Lane V/C Ratio	-	0.065	-	-	-	-	-	0.044
HCM Ctrl Dly (s/v)	0	10.7	-	-	0	-	-	13.3
HCM Lane LOS	A	B	-	-	A	-	-	B
HCM 95th %tile Q(veh)	-	0.2	-	-	0	-	-	0.1

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑↑	↗	↙	↑↑			↕			↕	
Traffic Vol, veh/h	39	933	27	11	915	12	15	0	9	1	0	17
Future Vol, veh/h	39	933	27	11	915	12	15	0	9	1	0	17
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	120	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	43	1037	30	12	1017	13	17	0	10	1	0	19

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1030	0	0	1067	0	0	1656	2178	518	1653	2201	515
Stage 1	-	-	-	-	-	-	1123	1123	-	1048	1048	-
Stage 2	-	-	-	-	-	-	533	1054	-	605	1153	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	670	-	-	649	-	-	64	46	502	65	44	505
Stage 1	-	-	-	-	-	-	219	279	-	244	303	-
Stage 2	-	-	-	-	-	-	498	301	-	451	270	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	670	-	-	649	-	-	57	42	502	58	41	505
Mov Cap-2 Maneuver	-	-	-	-	-	-	148	136	-	161	140	-
Stage 1	-	-	-	-	-	-	205	261	-	239	297	-
Stage 2	-	-	-	-	-	-	471	295	-	414	253	-

Approach	EB			WB			NB			SB		
HCM Ctrl Dly, s/v	0.42			0.12			25.55			13.35		
HCM LOS							D			B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	202	670	-	-	649	-	-	451
HCM Lane V/C Ratio	0.132	0.065	-	-	0.019	-	-	0.044
HCM Ctrl Dly (s/v)	25.5	10.7	-	-	10.7	-	-	13.3
HCM Lane LOS	D	B	-	-	B	-	-	B
HCM 95th %tile Q(veh)	0.4	0.2	-	-	0.1	-	-	0.1

Intersection												
Int Delay, s/veh	0.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕		↵	↕			↕			↕	
Traffic Vol, veh/h	10	914	0	0	1156	7	0	0	0	6	0	18
Future Vol, veh/h	10	914	0	0	1156	7	0	0	0	6	0	18
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	-	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	11	972	0	0	1230	7	0	0	0	6	0	19

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1237	0	0	972	0	0	1609	2231	486	1741	2227	619
Stage 1	-	-	-	-	-	-	994	994	-	1234	1234	-
Stage 2	-	-	-	-	-	-	615	1237	-	507	994	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	559	-	-	705	-	-	70	42	527	56	42	432
Stage 1	-	-	-	-	-	-	263	321	-	187	247	-
Stage 2	-	-	-	-	-	-	445	246	-	516	321	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	559	-	-	705	-	-	66	41	527	54	42	432
Mov Cap-2 Maneuver	-	-	-	-	-	-	173	141	-	143	145	-
Stage 1	-	-	-	-	-	-	258	315	-	187	247	-
Stage 2	-	-	-	-	-	-	426	246	-	506	315	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0.13	0	0	18.75
HCM LOS			A	C

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	559	-	-	705	-	-	287
HCM Lane V/C Ratio	-	0.019	-	-	-	-	-	0.089
HCM Ctrl Dly (s/v)	0	11.6	-	-	0	-	-	18.7
HCM Lane LOS	A	B	-	-	A	-	-	C
HCM 95th %tile Q(veh)	-	0.1	-	-	0	-	-	0.3

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑↑	↗	↙	↑↑			↕			↕	
Traffic Vol, veh/h	10	922	29	13	1156	7	21	0	12	6	0	18
Future Vol, veh/h	10	922	29	13	1156	7	21	0	12	6	0	18
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	50	-	120	50	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	11	981	31	14	1230	7	22	0	13	6	0	19

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1237	0	0	1012	0	0	1645	2267	490	1773	2294	619
Stage 1	-	-	-	-	-	-	1002	1002	-	1261	1261	-
Stage 2	-	-	-	-	-	-	643	1265	-	512	1033	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	559	-	-	681	-	-	66	40	524	53	39	432
Stage 1	-	-	-	-	-	-	260	318	-	180	240	-
Stage 2	-	-	-	-	-	-	429	239	-	513	308	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	559	-	-	681	-	-	60	39	524	49	37	432
Mov Cap-2 Maneuver	-	-	-	-	-	-	167	136	-	134	135	-
Stage 1	-	-	-	-	-	-	255	312	-	176	235	-
Stage 2	-	-	-	-	-	-	401	234	-	491	302	-

Approach	EB			WB			NB			SB		
HCM Ctrl Dly, s/v	0.12			0.11			24.26			19.25		
HCM LOS							C			C		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	222	559	-	-	681	-	-	278
HCM Lane V/C Ratio	0.158	0.019	-	-	0.02	-	-	0.092
HCM Ctrl Dly (s/v)	24.3	11.6	-	-	10.4	-	-	19.3
HCM Lane LOS	C	B	-	-	B	-	-	C
HCM 95th %tile Q(veh)	0.6	0.1	-	-	0.1	-	-	0.3

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	966	0	0	932	0	0
Future Vol, veh/h	966	0	0	932	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1062	0	0	1024	0	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	-	-	531
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	0	-	0	493
Stage 1	-	0	-	0	-
Stage 2	-	0	-	0	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	493
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT
Capacity (veh/h)	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-
HCM Ctrl Dly (s/v)	0	-	-	-
HCM Lane LOS	A	-	-	-
HCM 95th %tile Q(veh)	-	-	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	993	0	0	947	0	6
Future Vol, veh/h	993	0	0	947	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1091	0	0	1041	0	7

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	12.57
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	482	-	-
HCM Lane V/C Ratio	0.014	-	-
HCM Ctrl Dly (s/v)	12.6	-	-
HCM Lane LOS	B	-	-
HCM 95th %tile Q(veh)	0	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	924	0	0	1174	0	0
Future Vol, veh/h	924	0	0	1174	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	983	0	0	1249	0	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	-	-	491
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	0	-	0	523
Stage 1	-	0	-	0	-
Stage 2	-	0	-	0	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	523
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT
Capacity (veh/h)	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-
HCM Ctrl Dly (s/v)	0	-	-	-
HCM Lane LOS	A	-	-	-
HCM 95th %tile Q(veh)	-	-	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	953	0	0	1195	0	8
Future Vol, veh/h	953	0	0	1195	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1014	0	0	1271	0	9

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0	12.17
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	511	-	-
HCM Lane V/C Ratio	0.017	-	-
HCM Ctrl Dly (s/v)	12.2	-	-
HCM Lane LOS	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-

## Traffic Count Data Sheet

Year Counts Taken: **2025**      E-W Street **Cerrillos Rd.**      Speed Limit (Cerrillos Rd.)= **35**  
 N-S Street: **St Francis Rd.**      Speed Limit (St Francis Rd.)= **35**  
Signalized 8/19/25

Begin Time	End Time	Eastbound (Cerrillos Rd.)			Westbound (Cerrillos Rd.)			Northbound (St Francis Rd.)			Southbound (St Francis Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
7:00 AM	7:15 AM	63	43	1	20	17	1	0	181	47	0	183	71
7:15 AM	7:30 AM	56	79	2	30	29	4	0	188	91	0	224	94
7:30 AM	7:45 AM	72	88	1	23	37	6	0	235	111	0	270	152
7:45 AM	8:00 AM	<b>88</b>	<b>143</b>	<b>3</b>	<b>46</b>	<b>42</b>	<b>5</b>	<b>0</b>	<b>286</b>	<b>139</b>	<b>0</b>	<b>280</b>	<b>184</b>
8:00 AM	8:15 AM	<b>121</b>	<b>97</b>	<b>6</b>	<b>64</b>	<b>65</b>	<b>10</b>	<b>0</b>	<b>266</b>	<b>125</b>	<b>0</b>	<b>368</b>	<b>178</b>
8:15 AM	8:30 AM	<b>70</b>	<b>106</b>	<b>6</b>	<b>53</b>	<b>54</b>	<b>3</b>	<b>0</b>	<b>266</b>	<b>124</b>	<b>0</b>	<b>355</b>	<b>119</b>
8:30 AM	8:45 AM	<b>81</b>	<b>112</b>	<b>3</b>	<b>47</b>	<b>44</b>	<b>8</b>	<b>0</b>	<b>255</b>	<b>139</b>	<b>0</b>	<b>290</b>	<b>108</b>
8:45 AM	9:00 AM	85	83	2	42	50	9	0	233	130	0	271	122

<b>Peak Hour Vol. (AM)</b>	<b>360</b>	<b>458</b>	<b>18</b>	<b>210</b>	<b>205</b>	<b>26</b>	<b>0</b>	<b>1073</b>	<b>527</b>	<b>0</b>	<b>1293</b>	<b>589</b>
% of Total Traffic	7.6%	9.6%	0.4%	4.4%	4.3%	0.5%	0.0%	22.5%	11.1%	0.0%	27.2%	12.4%
% Directional		17.6%			9.3%	<b>PHF = 0.92</b>		33.6%			39.5%	

Begin Time	End Time	Eastbound (Cerrillos Rd.)			Westbound (Cerrillos Rd.)			Northbound (St Francis Rd.)			Southbound (St Francis Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
4:00 PM	4:15 PM	124	84	5	96	123	7	0	293	102	0	241	132
4:15 PM	4:30 PM	93	80	7	81	97	4	0	314	81	0	247	146
4:30 PM	4:45 PM	<b>128</b>	<b>70</b>	<b>5</b>	<b>99</b>	<b>133</b>	<b>8</b>	<b>0</b>	<b>353</b>	<b>93</b>	<b>0</b>	<b>260</b>	<b>154</b>
4:45 PM	5:00 PM	<b>126</b>	<b>79</b>	<b>1</b>	<b>85</b>	<b>107</b>	<b>6</b>	<b>0</b>	<b>354</b>	<b>104</b>	<b>0</b>	<b>268</b>	<b>108</b>
5:00 PM	5:15 PM	<b>101</b>	<b>78</b>	<b>9</b>	<b>98</b>	<b>130</b>	<b>8</b>	<b>0</b>	<b>383</b>	<b>89</b>	<b>0</b>	<b>286</b>	<b>142</b>
5:15 PM	5:30 PM	<b>121</b>	<b>87</b>	<b>4</b>	<b>132</b>	<b>158</b>	<b>15</b>	<b>0</b>	<b>341</b>	<b>101</b>	<b>0</b>	<b>275</b>	<b>143</b>
5:30 PM	5:45 PM	116	89	6	85	91	3	0	300	97	0	286	156
5:45 PM	6:00 PM	103	76	7	75	77	7	0	268	103	0	291	121

<b>Peak Hour Vol. (PM)</b>	<b>476</b>	<b>314</b>	<b>19</b>	<b>414</b>	<b>528</b>	<b>37</b>	<b>0</b>	<b>1431</b>	<b>387</b>	<b>0</b>	<b>1089</b>	<b>547</b>
% of Total Traffic	10.0%	6.6%	0.4%	8.7%	11.1%	0.8%	0.0%	30.1%	8.1%	0.0%	22.9%	11.5%
% Directional		17.0%			20.6%	<b>PHF = 0.95</b>		38.2%			34.4%	

## Traffic Count Data Sheet

Year Counts Taken: **2025**      E-W Street **Cordova Rd.**      Speed Limit (Cordova Rd.)= **30**  
 N-S Street: **Cerrilos Rd.**      Speed Limit (Cerrilos Rd.)= **35**  
Signalized      **8/19/25**

Begin Time	End Time	Eastbound (Cordova Rd.)			Westbound (Cordova Rd.)			Northbound (Cerrilos Rd.)			Southbound (Cerrilos Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
7:00 AM	7:15 AM	0	0	0	15	0	2	0	121	40	11	77	0
7:15 AM	7:30 AM	0	0	0	30	0	9	0	126	33	18	102	0
7:30 AM	7:45 AM	0	0	0	26	0	17	0	202	76	14	167	0
7:45 AM	8:00 AM	0	0	0	51	0	17	0	221	92	55	168	0
8:00 AM	8:15 AM	0	0	0	74	0	11	0	185	69	45	203	0
8:15 AM	8:30 AM	0	0	0	66	0	11	0	185	60	26	156	0
8:30 AM	8:45 AM	0	0	0	38	0	10	0	180	72	10	149	0
8:45 AM	9:00 AM	0	0	0	56	0	7	0	207	65	14	154	0

<b>Peak Hour Vol. (AM)</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>217</b>	<b>0</b>	<b>56</b>	<b>0</b>	<b>793</b>	<b>297</b>	<b>140</b>	<b>694</b>	<b>0</b>
% of Total Traffic	0.0%	0.0%	0.0%	9.9%	0.0%	2.5%	0.0%	36.1%	13.5%	6.4%	31.6%	0.0%
% Directional		0.0%			12.4%	<b>PHF = 0.91</b>		49.6%			38.0%	

Begin Time	End Time	Eastbound (Cordova Rd.)			Westbound (Cordova Rd.)			Northbound (Cerrilos Rd.)			Southbound (Cerrilos Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
4:00 PM	4:15 PM	0	0	0	85	0	9	0	199	57	15	247	0
4:15 PM	4:30 PM	0	0	0	98	0	10	0	178	62	15	248	0
4:30 PM	4:45 PM	0	0	0	85	0	13	0	166	43	13	271	0
4:45 PM	5:00 PM	0	0	0	74	0	15	0	199	49	11	220	0
5:00 PM	5:15 PM	0	0	0	91	0	11	0	200	56	23	267	0
5:15 PM	5:30 PM	0	0	0	88	0	10	0	180	49	13	279	0
5:30 PM	5:45 PM	0	0	0	72	0	9	0	198	50	21	227	0
5:45 PM	6:00 PM	0	0	0	70	0	15	0	196	62	5	200	0

<b>Peak Hour Vol. (PM)</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>338</b>	<b>0</b>	<b>49</b>	<b>0</b>	<b>745</b>	<b>197</b>	<b>60</b>	<b>1037</b>	<b>0</b>
% of Total Traffic	0.0%	0.0%	0.0%	15.4%	0.0%	2.2%	0.0%	33.9%	9.0%	2.7%	47.2%	0.0%
% Directional		0.0%			17.6%	<b>PHF = 0.94</b>		42.9%			49.9%	

## Traffic Count Data Sheet

Year Counts Taken: **2025**      E-W Street **NM School for the Deaf NE Driveway**      Speed Limit (NM School for the Deaf NE Dr **25**  
 N-S Street: **Cerrilos Rd.**      Speed Limit (Cerrilos Rd.)=**35**  
Signalized 8/19/25

Begin Time	End Time	EB (NMSFTD NE Dwy)			WB (NMSFTD NE Dwy)			Northbound (Cerrilos Rd.)			Southbound (Cerrilos Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
7:00 AM	7:15 AM	0	0	0	0	0	0	2	120	0	0	88	1
7:15 AM	7:30 AM	0	0	1	0	0	0	3	131	0	0	124	0
7:30 AM	7:45 AM	0	0	1	0	0	0	4	198	0	0	187	1
7:45 AM	8:00 AM	0	0	0	0	0	0	5	239	0	0	218	3
8:00 AM	8:15 AM	0	0	5	0	0	0	4	183	0	0	249	0
8:15 AM	8:30 AM	0	0	2	0	0	0	5	194	0	0	171	0
8:30 AM	8:45 AM	1	0	3	0	0	0	2	191	0	0	153	1
8:45 AM	9:00 AM	0	0	0	0	0	0	0	205	0	0	168	1

<b>Peak Hour Vol. (AM)</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>18</b>	<b>814</b>	<b>0</b>	<b>0</b>	<b>825</b>	<b>4</b>
% of Total Traffic	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	1.1%	48.8%	0.0%	0.0%	49.4%	0.2%
% Directional		0.5%			0.0%	<b>PHF = 0.90</b>		49.9%			49.7%	

Begin Time	End Time	EB (NMSFTD NE Dwy)			WB (NMSFTD NE Dwy)			Northbound (Cerrilos Rd.)			Southbound (Cerrilos Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
4:00 PM	4:15 PM	1	0	3	0	0	0	1	213	0	0	252	1
4:15 PM	4:30 PM	2	0	4	0	0	0	1	186	0	0	255	0
4:30 PM	4:45 PM	2	0	2	0	0	0	0	187	0	0	283	1
4:45 PM	5:00 PM	0	0	2	0	0	0	2	208	0	0	212	0
5:00 PM	5:15 PM	1	0	4	0	0	0	0	209	0	0	283	0
5:15 PM	5:30 PM	0	0	0	0	0	0	0	188	0	0	286	0
5:30 PM	5:45 PM	0	0	1	0	0	0	2	214	0	0	251	0
5:45 PM	6:00 PM	0	0	1	0	0	0	0	199	0	0	202	0

<b>Peak Hour Vol. (PM)</b>	<b>3</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>792</b>	<b>0</b>	<b>0</b>	<b>1064</b>	<b>1</b>
% of Total Traffic	0.2%	0.0%	0.5%	0.0%	0.0%	0.0%	0.1%	47.5%	0.0%	0.0%	63.8%	0.1%
% Directional		0.7%			0.0%	<b>PHF = 0.94</b>		47.6%			63.8%	

### Traffic Count Data Sheet

Year Counts Taken: **2025**      E-W Street **NM School for the Deaf SW Driveway** Speed Limit (NM School for the Deaf SW Driveway)= **25**  
 N-S Street: **Cerrilos Rd.**      Speed Limit (Cerrilos Rd.)= **35**  
Signalized 8/19/25

Begin Time	End Time	Eastbound (NMSFTD SW Dwy)			Westbound (NMSFTD SW Dwy)			Northbound (Cerrilos Rd.)			Southbound (Cerrilos Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
7:00 AM	7:15 AM	0	0	0	0	0	0	0	123	0	0	88	0
7:15 AM	7:30 AM	0	0	1	0	0	0	6	132	0	0	121	0
7:30 AM	7:45 AM	0	0	3	0	0	0	16	203	0	0	181	7
7:45 AM	8:00 AM	0	0	6	0	0	0	11	241	0	0	215	1
8:00 AM	8:15 AM	1	0	4	0	0	0	5	186	0	0	249	3
8:15 AM	8:30 AM	0	0	2	0	0	0	3	198	0	0	172	0
8:30 AM	8:45 AM	2	0	2	0	0	0	4	190	0	0	156	1
8:45 AM	9:00 AM	0	0	1	0	0	0	1	212	0	0	168	0

<b>Peak Hour Vol. (AM)</b>	<b>1</b>	<b>0</b>	<b>15</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>35</b>	<b>828</b>	<b>0</b>	<b>0</b>	<b>817</b>	<b>11</b>
% of Total Traffic	0.1%	0.0%	0.9%	0.0%	0.0%	0.0%	2.1%	48.5%	0.0%	0.0%	47.9%	0.6%
% Directional		0.9%			0.0%	<b>PHF = 0.90</b>		50.6%			48.5%	

Begin Time	End Time	Eastbound (NMSFTD SW Dwy)			Westbound (NMSFTD SW Dwy)			Northbound (Cerrilos Rd.)			Southbound (Cerrilos Rd.)		
		L	T	R	L	T	R	L	T	R	L	T	R
4:00 PM	4:15 PM	1	0	6	0	0	0	1	213	0	0	251	2
4:15 PM	4:30 PM	1	0	7	0	0	0	2	187	0	0	256	0
4:30 PM	4:45 PM	0	0	4	0	0	0	2	179	0	0	282	0
4:45 PM	5:00 PM	0	0	4	0	0	0	5	210	0	0	216	1
5:00 PM	5:15 PM	2	0	8	0	0	0	1	208	0	0	282	2
5:15 PM	5:30 PM	1	0	2	0	0	0	2	185	0	0	286	0
5:30 PM	5:45 PM	2	0	2	0	0	0	1	213	0	0	248	3
5:45 PM	6:00 PM	0	0	2	0	0	0	2	199	0	0	201	1

<b>Peak Hour Vol. (PM)</b>	<b>5</b>	<b>0</b>	<b>16</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9</b>	<b>816</b>	<b>0</b>	<b>0</b>	<b>1032</b>	<b>6</b>
% of Total Traffic	0.3%	0.0%	0.9%	0.0%	0.0%	0.0%	0.5%	47.8%	0.0%	0.0%	60.5%	0.4%
% Directional		1.2%			0.0%	<b>PHF = 0.94</b>		48.3%			60.8%	

### Traffic Count Data Sheet (Bicycles / Pedestrians)

Year Counts Taken: **2025**

E-W Street: **Cerrillos Rd.**  
 N-S Street: **St Francis Rd.**

Speed Limit (Cerrillos Rd.)= **35** MPH  
 Speed Limit (St Francis Rd.)= **35** MPH  
**8/19/25**

Signalized

Begin Time	End Time	Eastbound (Cerrillos Rd.)				Westbound (Cerrillos Rd.)				Northbound (St Francis Rd.)				Southbound (St Francis Rd.)				
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	
7:00 AM	7:15 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
7:15 AM	7:30 AM	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	2
7:30 AM	7:45 AM	0	0	0	2	0	0	0	4	0	0	0	1	0	0	0	0	3
7:45 AM	8:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	1
8:00 AM	8:15 AM	0	0	0	0	1	0	0	1	0	0	0	1	0	0	0	0	0
8:15 AM	8:30 AM	1	1	0	2	0	0	0	4	0	0	0	0	0	0	0	0	2
8:30 AM	8:45 AM	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	0	1
8:45 AM	9:00 AM	0	0	0	3	0	0	0	0	0	0	0	1	0	0	0	0	1
<b>AM Peak Hour Volumes</b>		<b>1</b>	<b>1</b>	<b>0</b>	<b>5</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>4</b>

Begin Time	End Time	Eastbound (Cerrillos Rd.)				Westbound (Cerrillos Rd.)				Northbound (St Francis Rd.)				Southbound (St Francis Rd.)				
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	
4:00 PM	4:15 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	4:30 PM	0	0	0	3	0	0	0	2	0	0	0	1	0	0	0	0	1
4:30 PM	4:45 PM	0	0	0	2	0	0	0	1	0	1	0	1	0	0	0	0	6
4:45 PM	5:00 PM	0	0	0	3	0	0	0	2	0	0	0	3	0	0	0	0	1
5:00 PM	5:15 PM	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3
5:15 PM	5:30 PM	0	0	0	3	0	0	0	6	0	0	0	2	0	0	0	0	1
5:30 PM	5:45 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	3
5:45 PM	6:00 PM	0	0	0	3	0	0	0	1	0	0	0	0	0	0	0	0	2
<b>PM Peak Hour Volumes</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>10</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>11</b>

### Traffic Count Data Sheet (Bicycles / Pedestrians)

Year Counts Taken: **2025**

E-W Street: **Cordova Rd.**  
 N-S Street: **Cerrilos Rd.**

Speed Limit (Cordova Rd.)= **30** MPH  
 Speed Limit (Cerrilos Rd.)= **35** MPH  
**8/19/25**

Signalized

Begin Time	End Time	Eastbound (Cordova Rd.)				Westbound (Cordova Rd.)				Northbound (Cerrilos Rd.)				Southbound (Cerrilos Rd.)				
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	
7:00 AM	7:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0
7:15 AM	7:30 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	7:45 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0
7:45 AM	8:00 AM	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	4
8:00 AM	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0
8:45 AM	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>AM Peak Hour Volumes</b>		0	0	0	0	2	0	0	2	0	1	0	0	0	0	0	0	4

Begin Time	End Time	Eastbound (Cordova Rd.)				Westbound (Cordova Rd.)				Northbound (Cerrilos Rd.)				Southbound (Cerrilos Rd.)				
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	
4:00 PM	4:15 PM	0	0	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0
4:15 PM	4:30 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0
4:30 PM	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	5:00 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0
5:00 PM	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0
5:45 PM	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>PM Peak Hour Volumes</b>		0	0	0	0	0	0	0	6	0	1	0	0	0	0	0	0	0

### Traffic Count Data Sheet (Bicycles / Pedestrians)

Year Counts Taken: **2025**      E-W Street: **NM School for the Deaf NE Driveway**      Speed Limit (NM School for the Deaf NE Driveway) **25** MPH  
 N-S Street: **Cerrilos Rd.**      Signalized      Speed Limit (Cerrilos Rd.)=**35** MPH  
8/19/25

Begin Time	End Time	EB (NM School for the Deaf NE Driveway)				WB (NM School for the Deaf NE Driveway)				Northbound (Cerrilos Rd.)				Southbound (Cerrilos Rd.)			
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians
7:00 AM	7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	7:45 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	8:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	8:30 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0
8:30 AM	8:45 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
AM Peak Hour Volumes		0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0

Begin Time	End Time	EB (NM School for the Deaf NE Driveway)				WB (NM School for the Deaf NE Driveway)				Northbound (Cerrilos Rd.)				Southbound (Cerrilos Rd.)			
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians
4:00 PM	4:15 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	5:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0
5:00 PM	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	5:45 PM	0	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM Peak Hour Volumes		0	0	0	5	0	0	0	0	0	1	0	0	0	0	0	0

### Traffic Count Data Sheet (Bicycles / Pedestrians)

Year Counts Taken: **2025**      E-W Street: **NM School for the Deaf SW Driveway**      Speed Limit (NM School for the Deaf SW Driveway): **25** MPH  
 N-S Street: **Cerrilos Rd.**      Signalized      Speed Limit (Cerrilos Rd.): **35** MPH  
8/19/25

Begin Time	End Time	EB (NM School for the Deaf SW Driveway)				WB (NM School for the Deaf SW Driveway)				Northbound (Cerrilos Rd.)				Southbound (Cerrilos Rd.)			
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians
7:00 AM	7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	7:45 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	8:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	8:30 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0
8:30 AM	8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>AM Peak Hour Volumes</b>		0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0

Begin Time	End Time	EB (NM School for the Deaf SW Driveway)				WB (NM School for the Deaf SW Driveway)				Northbound (Cerrilos Rd.)				Southbound (Cerrilos Rd.)			
		L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians	L	T	R	Pedestrians
4:00 PM	4:15 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	5:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0
5:00 PM	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	5:30 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	5:45 PM	0	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>PM Peak Hour Volumes</b>		0	0	0	6	0	0	0	0	0	1	0	0	0	0	0	0

City of Santa Fe



MOVING TRAFFIC FORWARD

St. Francis & Cerrillos - St. Francis @ Cerrillos - Econolite Type - EOS

**Configuration Cabinet Settings**

**Cabinet Type (MM) 1-1-1**

Cabinet Type: TS2-1

Channel	1	2	3	4
BIU Terminal & Facility	X	X		
BIU Detector	X			
Critical BIU				

Enable SDLC      Yes  
 Stop Time:  
 Latch 3 Critical      Yes  
 Errors:

**Load Switch Configuration (MM) 1-1-2**

Channel	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Channel Type	.	V	V	V	.	V	V	V	P	P	P	P	O	O	O	O
Phase/Overlap	0	2	3	4	0	6	7	8	2	4	6	8	1	2	3	4

**Color Check Enable (MM) 1-1-3**

Enable Color Check: Yes

Channel	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Red		X	X	X		X	X	X	X	X	X	X		X	X	
Yellow		X	X	X		X	X	X						X	X	
Green		X	X	X		X	X	X	X	X	X	X		X	X	

**Monitor Programming (MM) 1-1-4**

Channel Can Serve With Channel	
Channel 1	Channel 2
2	6
2	9
2	11
2	15
3	7
3	8
3	12
3	14
3	15
4	7
4	8
4	10
4	12
4	14
4	15

**Secondary Stations/Tests (MM) 1-1-6**

The Cabinet Type does not support this feature.

6	9
6	11
6	15
7	10
7	15
8	10
8	12
8	14
9	11
9	15
10	12
10	14
10	15
11	15
12	14

**Phase and Overlap Descriptions**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Approach	N	S	E	W	S	N	W	E	N	N	N	N	N	N	N	N
Movement	L	T	L	T	L	T	L	T								
Associated PED																
Overlap	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
Approach	N	N	N	N	N	N	N	N	N	N	N	N	N	N	N	N
Movement																



MOVING TRAFFIC FORWARD

St. Francis & Cerrillos - St. Francis @ Cerrillos - Econolite Type - EOS

Controller Timing Plan (MM) 2-1

Plan 1 - ""

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Direction	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
2-1-1	Minimum Green															
Delay Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	0	10	3	10	0	10	3	10	5	5	5	5	5	5	5	5
Bk Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CS Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Variable Initial															
Lane	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False
Act B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sec/Act	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Max Int	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-2	Vehicle Passage															
Vehicle Ext	0.0	4.0	2.0	2.0	2.0	4.0	2.0	2.0	3.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Ext 2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Volume Occupancy															
Time B4	0	10	10	10	0	10	10	10	0	0	0	0	0	0	0	0
Cars Wt	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STPTDuc	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
TTReduc	0	5	5	5	0	5	5	5	0	0	0	0	0	0	0	0
Min Gap	0.0	3.0	1.5	3.0	0.0	3.0	1.5	3.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-3	Max Green Data															
Max1	0	45	25	35	0	45	25	35	35	35	35	35	35	35	35	35
Max2	0	65	35	55	0	65	35	55	40	40	40	40	40	40	40	40
Max3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Dynamic Max															
DYM Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dym Step	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-4	Pedestrian															
Delay Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Walk	0	7	0	7	0	7	0	7	0	10	0	10	0	10	0	10
Ped Clear	0	30	0	30	0	38	0	24	0	16	0	16	0	16	0	16
	Alternate															
Walk2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Pedestrian Carry Over															
Ped CO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Max Extension															
Walk Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Hawk															

Ingress Phase	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Travel Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-5	Clearance															
Pre-Clear	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	4.0	4.0	4.0	0.0	4.0	4.0	4.0	4.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Red Clear	0.0	3.0	3.0	3.0	0.0	3.0	3.0	3.0	3.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Red Revert	0.0	2.0	2.0	2.0	0.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
	Max Extension															
Red Max	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Hawk Flash															
Yellow	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
Red Delay	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 1 - ""Continued**

**Phase Recall (MM) 2-1-6**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Lock Detector																
Vehicle Recall		X				X										
Ped Recall																
Max Recall																
Soft Recall																
No Rest																

**Overlap (MM) 2-1-7**

Phase	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
	Leading															
Adv. Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Adv. Ped	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Delay FYA	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Trailing															
Lag Green	0.0	2.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	0.0	2.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

**Phase Outputs (MM) 2-1-8**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
	Double Serve															
Dbl Serv Ph.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 2 - ""**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Direction	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
2-1-1	Minimum Green															
Delay Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Bk Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CS Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Variable Initial															

Lane	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False
Act B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sec/Act	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Max Int	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-2	Vehicle Passage															
Vehicle Ext	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Ext 2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Volume Occupancy															
Time B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Cars Wt	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STPTDuc	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
TTReduc	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Gap	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-3	Max Green Data															
Max1	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35
Max2	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40
Max3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Dynamic Max															
DYM Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dym Step	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-4	Pedestrian															
Delay Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Walk	0	10	0	10	0	10	0	10	0	10	0	10	0	10	0	10
Ped Clear	0	16	0	16	0	16	0	16	0	16	0	16	0	16	0	16
	Alternate															
Walk2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Pedestrian Carry Over															
Ped CO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Max Extension															
Walk Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Hawk															
Ingress Phase	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Travel Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-5	Clearance															
Pre-Clear	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Red Clear	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Red Revert	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
	Max Extension															
Red Max	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Hawk Flash															
Yellow	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
Red Delay	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 2 - ""Continued**

**Phase Recall (MM) 2-1-6**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Lock Detector																
Vehicle Recall																

Ped Recall																	
Max Recall																	
Soft Recall																	
No Rest																	

**Overlap (MM) 2-1-7**

Phase	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
Leading																
Adv. Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Adv. Ped	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Delay FYA	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Trailing																
Lag Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

**Phase Outputs (MM) 2-1-8**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Double Serve																
Dbl Serv Ph.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 3 - ""**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Direction	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
2-1-1 Minimum Green																
Delay Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Bk Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CS Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Variable Initial																
Lane	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False
Act B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sec/Act	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Max Int	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-2 Vehicle Passage																
Vehicle Ext	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Ext 2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Volume Occupancy																
Time B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Cars Wt	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STPTDuc	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
TTReduc	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Gap	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-3 Max Green Data																
Max1	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35
Max2	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40
Max3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dynamic Max																
DYM Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dym Step	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

2-1-4	Pedestrian															
Delay Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Walk	0	10	0	10	0	10	0	10	0	10	0	10	0	10	0	10
Ped Clear	0	16	0	16	0	16	0	16	0	16	0	16	0	16	0	16
	Alternate															
Walk2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Pedestrian Carry Over															
Ped CO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Max Extension															
Walk Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Hawk															
Ingress Phase	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Travel Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-5	Clearance															
Pre-Clear	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Red Clear	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Red Revert	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
	Max Extension															
Red Max	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Hawk Flash															
Yellow	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
Red Delay	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 3 - ""Continued**

**Phase Recall (MM) 2-1-6**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Lock Detector																
Vehicle Recall																
Ped Recall																
Max Recall																
Soft Recall																
No Rest																

**Overlap (MM) 2-1-7**

Phase	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
Leading																
Adv. Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Adv. Ped	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Delay FYA	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Trailing																
Lag Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

**Phase Outputs (MM) 2-1-8**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Double Serve																



MOVING TRAFFIC FORWARD

St. Francis & Cerrillos - St. Francis @ Cerrillos - Econolite Type - EOS

**Time Base Event Plan**  
**Event Plan (MM) 5-2**

**Event Plan - 1 - Event Type: "Coord"**

Cycle Length: 120    Offset Value: 110s    Actuated Coord: Yes    Splits In:    Seconds    Offsets In:    Seconds

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Description	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
Split	0	60	24	36	0	60	32	28	0	0	0	0	0	0	0	0

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Coord Phase		X				X										
Fixed Force Off																
Adaptive Split																
Veh Ext 2																
Vehicle Recall			X				X									
Walk 2																
Pedestrian Recall																
Max Recall																
Max 2																
Max 3																
CS Inhibit																
Omit Phase																

Dwell/Add Time: 0  
 Timing Plan: 0  
 Sequence: 3  
 Actuated Walk Rest: No  
 Phase Reservice: No  
 Max Select: MAX 2  
 Max Transition: 0  
 Ring Group Offset Disp: 0

Ring	1	2	3	4
Ring Split Ext	0	0	0	0
Split Sum	120s	120s	0s	0s

	1	2	3	4
Ring Manual Permissive Period	15	0	0	0

Veh Perm 1: 0    Veh Perm 2: 0  
 Veh Perm 2 Disp: 0  
 Veh Perm 2: 0

SCP Strategy Plan: 0    Veh Detector Plan: 0  
 SCP Detector Plan: 0    Veh Det Diag Plan: 0  
 Override Sys: No    Ped Det Diag Plan: 0  
 Backup Prevent Plan: 0    Det Log: None

Outputs	1	2	3	4	5	6	7	8
Coord Patt Spec Func Outputs								
Spec Func (1-8)								
Aux Func (1-3)								

Exit Option: Off :

Statement	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	
LP 1-25	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 26-50	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 51-75	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 76-100	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

**Event Plan - 2 - Event Type: "Coord"**

Cycle Length: 120    Offset Value: 110s    Actuated Coord: Yes    Splits In:    Seconds    Offsets In:    Seconds

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Description	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
Split	0	60	24	36	0	60	32	28	0	0	0	0	0	0	0	0

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Coord Phase		X				X										
Fixed Force Off																
Adaptive Split																
Veh Ext 2																
Vehicle Recall				X				X								
Walk 2																
Pedestrian Recall																
Max Recall																
Max 2																
Max 3																
CS Inhibit																
Omit Phase																

Dwell/Add Time: 0  
 Timing Plan: 0  
 Sequence: 3  
 Actuated Walk Rest: No  
 Phase Reservice: No  
 Max Select: MAX 2  
 Max Transition: 0  
 Ring Group Offset Disp: 0

Ring	1	2	3	4
Ring Split Ext	0	0	0	0
Split Sum	120s	120s	0s	0s

	1	2	3	4
Ring Manual Permissive Period	15	0	0	0

Veh Perm 1: 0    Veh Perm 2 Disp: 0  
 Veh Perm 2: 0

SCP Strategy Plan: 0    Veh Detector Plan: 0  
 SCP Detector Plan: 0    Veh Det Diag Plan: 0  
 Override Sys: No    Ped Det Diag Plan: 0  
 Backup Prevent Plan: 0    Det Log: None  
 Exit Option: Off :

Outputs	1	2	3	4	5	6	7	8
Coord Patt Spec Func Outputs								
Spec Func (1-8)								
Aux Func (1-3)								

Statement	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
LP 1-25	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 26-50	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 51-75	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 76-100	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

**Event Plan - 3 - Event Type: "Coord"**

135                      55s                      Yes                      Seconds                      Seconds

Cycle Length:                      Offset Value:                      Actuated Coord:                      Splits In:                      Offsets In:

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Description	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
Split	0	69	32	34	0	69	32	34	0	0	0	0	0	0	0	0

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Coord Phase		X				X										
Fixed Force Off																
Adaptive Split																
Veh Ext 2																
Vehicle Recall			X				X									
Walk 2																
Pedestrian Recall																
Max Recall																
Max 2																
Max 3																
CS Inhibit																
Omit Phase																

Dwell/Add Time: 0  
 Timing Plan: 0  
 Sequence: 1  
 Actuated Walk Rest: No  
 Phase Reservice: No  
 Max Select: MAX 2  
 Max Transition: 0  
 Ring Group Offset Disp: 0

Ring	1	2	3	4
Ring Split Ext	0	0	0	0
Split Sum	135s	135s	0s	0s

	1	2	3	4
Ring Manual Permissive Period	15	0	0	0

Veh Perm 1: 0      Veh Perm 2 Disp: 0  
 Veh Perm 2: 0

SCP Strategy Plan: 0      Veh Detector Plan: 0  
 SCP Detector Plan: 0      Veh Det Diag Plan: 0  
 Override Sys: No      Ped Det Diag Plan: 0  
 Backup Prevent Plan: 0      Det Log: None  
 Exit Option: Off :

Outputs	1	2	3	4	5	6	7	8
Coord Patt Spec								
Func Outputs								
Spec Func (1-8)								
Aux Func (1-3)								

Statement	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
LP 1-25	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 26-50	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 51-75	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 76-100	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

## City of Santa Fe



MOVING TRAFFIC FORWARD

St. Francis &amp; Cerrillos - St. Francis @ Cerrillos - Econolite Type - EOS

**Time Base Day Plan/Schedule****Day Plan (MM) 5-3****Day Plan #1 - "1"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	2	11:00
4	2	18:30
5	4	20:30
6	5	22:00

**Day Plan #2 - "2"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	1	07:00
4	4	09:00
5	2	11:00
6	2	13:30
7	3	15:00
8	2	18:30
9	4	20:30
10	5	22:00

**Day Plan #6 - "6"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	1	07:00
4	4	09:00
5	2	11:00
6	3	13:30
7	3	15:00
8	2	18:30
9	4	20:30
10	5	22:00

**Day Plan #7 - "7"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	3	09:30
4	2	18:30

**Schedule (MM) 5-4**

**Schedule Number - 1**

Day Plan No.: 1

Month	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
	X	X	X	X	X	X	X	X	X	X	X	X

Day (DOW)	SUN	MON	TUE	WED	THU	FRI	SAT
	X						

Day (DOM)	1	2	3	4	5	6	7	8	9	10	11
	X	X	X	X	X	X	X	X	X	X	X
	12	13	14	15	16	17	18	19	20	21	22
	X	X	X	X	X	X	X	X	X	X	X
	23	24	25	26	27	28	29	30	31		
	X	X	X	X	X	X	X	X	X		

**Schedule Number - 2**

Day Plan No.: 2

Month	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
	X	X	X	X	X	X	X	X	X	X	X	X

Day (DOW)	SUN	MON	TUE	WED	THU	FRI	SAT
		X	X	X	X		

Day (DOM)	1	2	3	4	5	6	7	8	9	10	11
	X	X	X	X	X	X	X	X	X	X	X
	12	13	14	15	16	17	18	19	20	21	22
	X	X	X	X	X	X	X	X	X	X	X
	23	24	25	26	27	28	29	30	31		
	X	X	X	X	X	X	X	X	X		

**Schedule Number - 6**

Day Plan No.: 6

Month	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
	X	X	X	X	X	X	X	X	X	X	X	X

Day (DOW)	SUN	MON	TUE	WED	THU	FRI	SAT
						X	

Day (DOM)	1	2	3	4	5	6	7	8	9	10	11
	X	X	X	X	X	X	X	X	X	X	X

City of Santa Fe



MOVING TRAFFIC FORWARD

Cerrillos & Cordova - Cerrillos @ Cordova - Econolite Type - EOS

**Configuration Cabinet Settings**

**Cabinet Type (MM) 1-1-1**

Cabinet Type: TS1

Channel	1	2	3	4
BIU Detector				
Critical BIU				

I/O Mode: 0

**Load Switch Configuration (MM) 1-1-2**

Channel	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Channel Type	V	V	.	V	.	V	.	.	P	P	.	.	.	.	.	.
Phase/Overlap	1	2	0	4	0	6	0	0	2	4	0	0	1	2	3	4

**Color Check Enable (MM) 1-1-3**

The Cabinet Type does not support this feature.

**Monitor Programming (MM) 1-1-4**

The Cabinet Type does not support this feature.

**Secondary Stations/Tests (MM) 1-1-6**

The Cabinet Type does not support this feature.

**Phase and Overlap Descriptions**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Approach	N	S	E	W	S	N	W	E	N	N	N	N	N	N	N	N
Movement	L	T	L	T	L	T	L	T								
Associated PED																
Overlap	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
Approach	N	N	N	N	N	N	N	N	N	N	N	N	N	N	N	N
Movement																



MOVING TRAFFIC FORWARD

Cerrillos & Cordova - Cerrillos @ Cordova - Econolite Type - EOS

Controller Timing Plan (MM) 2-1

Plan 1 - ""

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Direction	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
2-1-1	Minimum Green															
Delay Green	0	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	3	15	0	5	0	15	0	0	5	5	5	5	5	5	5	5
Bk Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CS Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Variable Initial															
Lane	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False
Act B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sec/Act	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Max Int	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-2	Vehicle Passage															
Vehicle Ext	2.0	4.0	0.0	2.0	0.0	4.0	0.0	0.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Ext 2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Volume Occupancy															
Time B4	5	10	0	10	0	10	0	0	0	0	0	0	0	0	0	0
Cars Wt	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STPTDuc	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
TTReduc	5	5	0	5	0	5	0	0	0	0	0	0	0	0	0	0
Min Gap	1.0	3.0	0.0	2.0	0.0	3.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-3	Max Green Data															
Max1	20	45	0	25	0	45	0	0	35	35	35	35	35	35	35	35
Max2	25	65	0	35	0	65	0	0	40	40	40	40	40	40	40	40
Max3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Dynamic Max															
DYM Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dym Step	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-4	Pedestrian															
Delay Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Walk	0	7	0	7	0	0	0	0	0	10	0	10	0	10	0	10
Ped Clear	0	26	0	22	0	0	0	0	0	16	0	16	0	16	0	16
	Alternate															
Walk2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Pedestrian Carry Over															
Ped CO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Max Extension															
Walk Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Hawk															

Ingress Phase	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Travel Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-5	Clearance															
Pre-Clear	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	4.0	4.0	3.5	3.5	4.0	4.0	3.5	3.5	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Red Clear	1.0	2.0	1.0	2.0	1.0	2.0	1.0	2.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Red Revert	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
	Max Extension															
Red Max	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Hawk Flash															
Yellow	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
Red Delay	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 1 - ""Continued**

**Phase Recall (MM) 2-1-6**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Lock Detector																
Vehicle Recall		X				X										
Ped Recall																
Max Recall																
Soft Recall																
No Rest																

**Overlap (MM) 2-1-7**

Phase	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
	Leading															
Adv. Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Adv. Ped	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Delay FYA	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Trailing															
Lag Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

**Phase Outputs (MM) 2-1-8**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
	Double Serve															
Dbl Serv Ph.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 2 - ""**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Direction	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
2-1-1	Minimum Green															
Delay Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Bk Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CS Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Variable Initial															

Lane	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False
Act B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sec/Act	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Max Int	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-2	Vehicle Passage															
Vehicle Ext	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Ext 2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Volume Occupancy															
Time B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Cars Wt	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STPTDuc	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
TTReduc	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Gap	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-3	Max Green Data															
Max1	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35
Max2	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40
Max3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Dynamic Max															
DYM Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dym Step	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-4	Pedestrian															
Delay Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Walk	0	10	0	10	0	10	0	10	0	10	0	10	0	10	0	10
Ped Clear	0	16	0	16	0	16	0	16	0	16	0	16	0	16	0	16
	Alternate															
Walk2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Pedestrian Carry Over															
Ped CO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Max Extension															
Walk Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Hawk															
Ingress Phase	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Travel Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-5	Clearance															
Pre-Clear	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Red Clear	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Red Revert	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
	Max Extension															
Red Max	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Hawk Flash															
Yellow	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
Red Delay	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 2 - ""Continued**

**Phase Recall (MM) 2-1-6**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Lock Detector																
Vehicle Recall																

Ped Recall																	
Max Recall																	
Soft Recall																	
No Rest																	

**Overlap (MM) 2-1-7**

Phase	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
Leading																
Adv. Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Adv. Ped	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Delay FYA	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Trailing																
Lag Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

**Phase Outputs (MM) 2-1-8**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Double Serve																
Dbl Serv Ph.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 3 - ""**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Direction	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
2-1-1	Minimum Green															
Delay Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5
Bk Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
CS Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Variable Initial																
Lane	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False	False
Act B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Sec/Act	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Max Int	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-2	Vehicle Passage															
Vehicle Ext	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Vehicle Ext 2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Volume Occupancy																
Time B4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Cars Wt	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STPTDuc	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
TTReduc	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Gap	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2-1-3	Max Green Data															
Max1	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35	35
Max2	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40	40
Max3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dynamic Max																
DYM Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dym Step	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

2-1-4	Pedestrian															
Delay Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Walk	0	10	0	10	0	10	0	10	0	10	0	10	0	10	0	10
Ped Clear	0	16	0	16	0	16	0	16	0	16	0	16	0	16	0	16
	Alternate															
Walk2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear 2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Pedestrian Carry Over															
Ped CO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Max Extension															
Walk Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Ped Clear Max	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Hawk															
Ingress Phase	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Travel Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2-1-5	Clearance															
Pre-Clear	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Red Clear	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Red Revert	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
	Max Extension															
Red Max	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	Hawk Flash															
Yellow	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
Red Delay	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

**Plan 3 - ""Continued**

**Phase Recall (MM) 2-1-6**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Lock Detector																
Vehicle Recall																
Ped Recall																
Max Recall																
Soft Recall																
No Rest																

**Overlap (MM) 2-1-7**

Phase	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
Leading																
Adv. Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Adv. Ped	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Delay FYA	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Trailing																
Lag Green	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

**Phase Outputs (MM) 2-1-8**

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Double Serve																



MOVING TRAFFIC FORWARD

Cerrillos & Cordova - Cerrillos @ Cordova - Econolite Type - EOS

**Time Base Event Plan**  
**Event Plan (MM) 5-2**

**Event Plan - 1 - Event Type: "Coord"**

Cycle Length: 120    Offset Value: 110s    Actuated Coord: No    Splits In:    Seconds    Offsets In:    Seconds

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Description	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
Split	18	70	0	32	0	88	0	32	0	0	0	0	0	0	0	0

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Coord Phase		X				X										
Fixed Force Off																
Adaptive Split																
Veh Ext 2																
Vehicle Recall																
Walk 2																
Pedestrian Recall																
Max Recall																
Max 2																
Max 3																
CS Inhibit																
Omit Phase																

Dwell/Add Time: 0  
 Timing Plan: 0  
 Sequence: 1  
 Actuated Walk Rest: No  
 Phase Reservice: No  
 Max Select: MAX 2  
 Max Transition: 0  
 Ring Group Offset Disp: 0

Ring	1	2	3	4
Ring Split Ext	0	0	0	0
Split Sum	120s	120s	0s	0s

	1	2	3	4
Ring Manual Permissive Period	0	15	0	0

Veh Perm 1: 0    Veh Perm 2 Disp: 0  
 Veh Perm 2: 0

SCP Strategy Plan: 0    Veh Detector Plan: 0  
 SCP Detector Plan: 0    Veh Det Diag Plan: 0  
 Override Sys: No    Ped Det Diag Plan: 0  
 Backup Prevent Plan: 0    Det Log: None

Outputs	1	2	3	4	5	6	7	8
Coord Patt Spec Func Outputs								
Spec Func (1-8)								
Aux Func (1-3)								

Exit Option: Off :

Statement	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	
LP 1-25	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 26-50	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 51-75	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 76-100	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

**Event Plan - 2 - Event Type: "Coord"**

Cycle Length: 120      Offset Value: 110s      Actuated Coord: No      Splits In:      Seconds      Offsets In:      Seconds

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Description	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
Split	18	70	0	32	0	88	0	32	0	0	0	0	0	0	0	0

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Coord Phase		X				X										
Fixed Force Off																
Adaptive Split																
Veh Ext 2																
Vehicle Recall																
Walk 2																
Pedestrian Recall																
Max Recall																
Max 2																
Max 3																
CS Inhibit																
Omit Phase																

Dwell/Add Time: 0  
 Timing Plan: 0  
 Sequence: 1  
 Actuated Walk Rest: No  
 Phase Reservice: No  
 Max Select: MAX 2  
 Max Transition: 0  
 Ring Group Offset Disp: 0

Ring	1	2	3	4
Ring Split Ext	0	0	0	0
Split Sum	120s	120s	0s	0s

	1	2	3	4
Ring Manual Permissive Period	0	15	0	0

Veh Perm 1: 0      Veh Perm 2: 0  
 Veh Perm 2 Disp: 0  
 Veh Perm 2: 0

SCP Strategy Plan: 0      Veh Detector Plan: 0  
 SCP Detector Plan: 0      Veh Det Diag Plan: 0  
 Override Sys: No      Ped Det Diag Plan: 0  
 Backup Prevent Plan: 0      Det Log: None  
 Exit Option: Off :

Outputs	1	2	3	4	5	6	7	8
Coord Patt Spec Func Outputs								
Spec Func (1-8)								
Aux Func (1-3)								

Statement	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
LP 1-25	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 26-50	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 51-75	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 76-100	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

**Event Plan - 3 - Event Type: "Coord"**

135      65s      No      Seconds      Seconds

Cycle Length:                      Offset Value:                      Actuated Coord:                      Splits In:                      Offsets In:

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Description	N-L	S-T	E-L	W-T	S-L	N-T	W-L	E-T	N	N	N	N	N	N	N	N
Split	18	85	0	32	0	103	0	32	0	0	0	0	0	0	0	0

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Coord Phase		X				X										
Fixed Force Off																
Adaptive Split																
Veh Ext 2																
Vehicle Recall			X													
Walk 2																
Pedestrian Recall																
Max Recall																
Max 2																
Max 3																
CS Inhibit																
Omit Phase																

Dwell/Add Time: 0  
 Timing Plan: 0  
 Sequence: 1  
 Actuated Walk Rest: No  
 Phase Reservice: No  
 Max Select: MAX 2  
 Max Transition: 0  
 Ring Group Offset Disp: 0

Ring	1	2	3	4
Ring Split Ext	0	0	0	0
Split Sum	135s	135s	0s	0s

	1	2	3	4
Ring Manual Permissive Period	0	15	0	0

Veh Perm 1: 0      Veh Perm 2 Disp: 0  
 Veh Perm 2: 0

SCP Strategy Plan: 0      Veh Detector Plan: 0  
 SCP Detector Plan: 0      Veh Det Diag Plan: 0  
 Override Sys: No      Ped Det Diag Plan: 0  
 Backup Prevent Plan: 0      Det Log: None  
 Exit Option: Off :

Outputs	1	2	3	4	5	6	7	8
Coord Patt Spec								
Func Outputs								
Spec Func (1-8)								
Aux Func (1-3)								

Statement	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
LP 1-25	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 26-50	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 51-75	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
LP 76-100	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

## City of Santa Fe



MOVING TRAFFIC FORWARD

Cerrillos &amp; Cordova - Cerrillos @ Cordova - Econolite Type - EOS

**Time Base Day Plan/Schedule****Day Plan (MM) 5-3****Day Plan #1 - "1"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	2	11:00
4	2	18:30
5	4	20:30
6	5	22:00

**Day Plan #2 - "2"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	1	07:00
4	4	09:00
5	2	11:00
6	2	13:30
7	3	15:00
8	2	18:30
9	4	20:30
10	5	22:00

**Day Plan #6 - "6"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	1	07:00
4	4	09:00
5	2	11:00
6	3	13:30
7	3	15:00
8	2	18:30
9	4	20:30
10	5	22:00

**Day Plan #7 - "7"**

Event	Event Plan	Start Time
1	5	00:00
2	4	06:00
3	3	09:30
4	2	18:30

**A-103**

**Schedule (MM) 5-4**

**Schedule Number - 1**

Day Plan No.: 1

Month	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
	X	X	X	X	X	X	X	X	X	X	X	X

Day (DOW)	SUN	MON	TUE	WED	THU	FRI	SAT
	X						

Day (DOM)	1	2	3	4	5	6	7	8	9	10	11
	X	X	X	X	X	X	X	X	X	X	X
	12	13	14	15	16	17	18	19	20	21	22
	X	X	X	X	X	X	X	X	X	X	X
	23	24	25	26	27	28	29	30	31		
	X	X	X	X	X	X	X	X	X		

**Schedule Number - 2**

Day Plan No.: 2

Month	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
	X	X	X	X	X	X	X	X	X	X	X	X

Day (DOW)	SUN	MON	TUE	WED	THU	FRI	SAT
		X	X	X	X		

Day (DOM)	1	2	3	4	5	6	7	8	9	10	11
	X	X	X	X	X	X	X	X	X	X	X
	12	13	14	15	16	17	18	19	20	21	22
	X	X	X	X	X	X	X	X	X	X	X
	23	24	25	26	27	28	29	30	31		
	X	X	X	X	X	X	X	X	X		

**Schedule Number - 6**

Day Plan No.: 6

Month	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
	X	X	X	X	X	X	X	X	X	X	X	X

Day (DOW)	SUN	MON	TUE	WED	THU	FRI	SAT
						X	

Day (DOM)	1	2	3	4	5	6	7	8	9	10	11
	X	X	X	X	X	X	X	X	X	X	X



# TIERRA WEST, LLC

## Traffic Impact Study Scoping Letter

<b>Project Name:</b>	[Proj. 2025080] – AC Marriott Hotel
<b>Location:</b>	Cerrillos Rd. Nouth of Cordova Rd
<b>Project Descr.</b>	Retail / Hotel Uses
<b>Date / Time:</b>	08/12/2025 – 1:30 PM
<b>Attendees:</b>	Leroy Pacheco and Phil Gallegos (City of Santa Fe); Jennifer Jenkins and Margaret Ambrosino (Jenkins-Gavin), Ian Robertson, Rafael Castellanos, Josh Rogers (Titan); Ron Bohannon, Derek Bohannon, Jay Nelson, and Terry Brown (Tierra West, LLC).

### Scope of Report:

<b>Traffic Data Collection:</b>	Collect New Volumes
<b>Peak Hour? / Peak x 15</b>	Peak hour factor
<b>Demand Volumes?</b>	No
<b>Traffic Count Data:</b>	7:00 to 9:00 AM and 4:00 to 6:00 PM
<b>Trip Generation:</b>	ITE Trip Generation, 11 <sup>th</sup> Edition (see attached)
<b>Growth Rates:</b>	Based on Santa Fe MPO AADT data (10 year history)
<b>Minimum Growth Rate:</b>	0.5% (justify growth rate in report)
<b>Implementation Year:</b>	2027
<b>Horizon Year:</b>	2037
<b>Trip Distribution:</b>	Based on City of SF Socioeconomic Data (Employment area wide inversely proportional to the distance of the project from the subarea.
<b>Other Projects:</b>	N/A

### Study Area:

Intersection	Count?	Control	Comment
1 – Cordova Rd. / Cerrillos Rd.	Yes	Signalized	Existing Signal Timing
2 – St. Francis Dr. / Cerrillos Rd.	Yes	Signalized	Existing Signal Timing
3 – Project Access (School for Blind Dwy)	Yes	Unsignalized	

5571 Midway Park Pl. NE Albuquerque, NM 87109  
 (505) 858-3100 fax (505) 858-1118 1 (800) 245-3102  
 tierrawestllc.com

Analysis Conditions Required:

	<b>Condition</b>	<b>Yes / No</b>	<b>Year</b>
	Existing Analysis	No	N/A*
	Implementation Year Analysis (AM/PM)	Yes	2027
	Horizon Year Analysis (AM/PM)	Yes	2037

\* - 2027 NO BUILD Analysis should approximate the Existing Analysis.

Special Considerations:

- 1) Consider pedestrian mobility and the Santa Fe Pick-Up free transit service near the project.
- 2) Address RailRunner trips through intersection in text of report. No analysis necessary.
- 3) NM DOT has project to restrict access on Cerrillos Rd. from St. Michael's to St. Francis. Discuss this project in the TIS.
- 4) Discuss bike facilities near the project in the TIS.

Crash Analysis:      Crash Analysis for 3-years for study area (2021 – 2023)

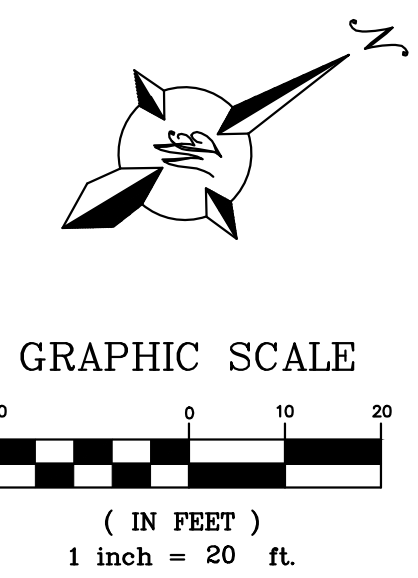
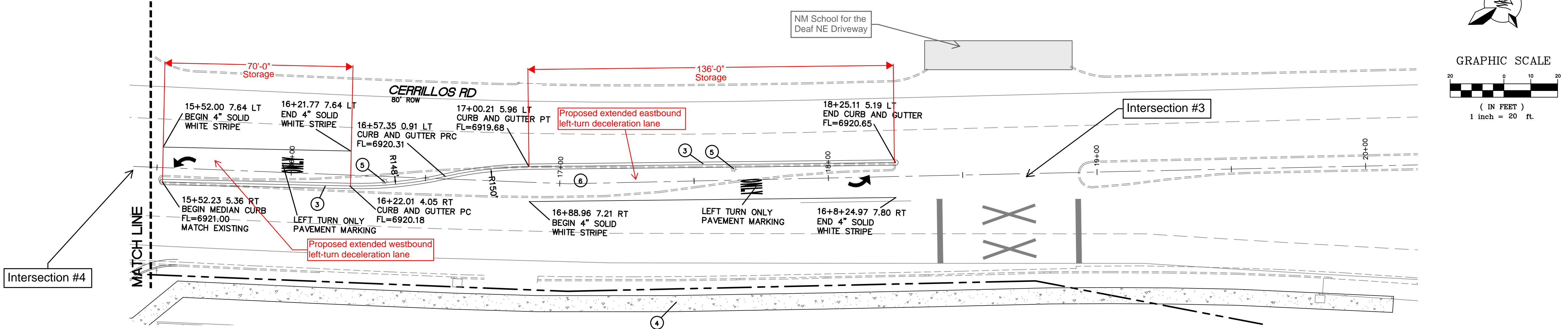
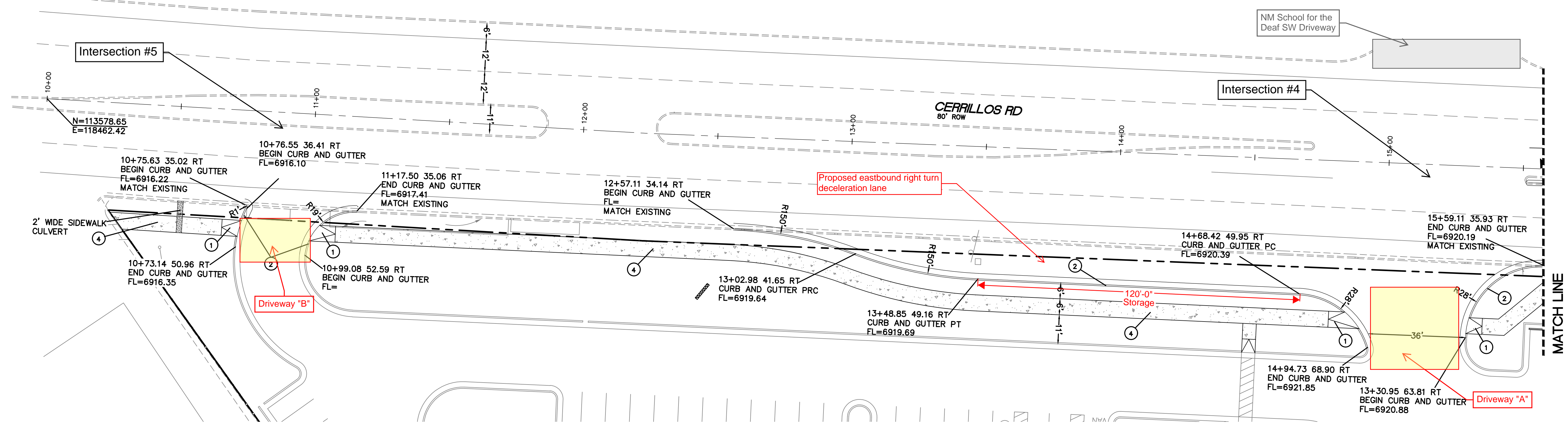
Report Submittals:

<b>To:</b>	<b>Hard Copies</b>	<b>Electronic Copy</b>
City of Santa Fe	0	Yes

<b>Intersection</b>	<b>Agency</b>

Site Plan Received from Developer? **Y** / N

Land Use List Received from Developer? **Y** / N



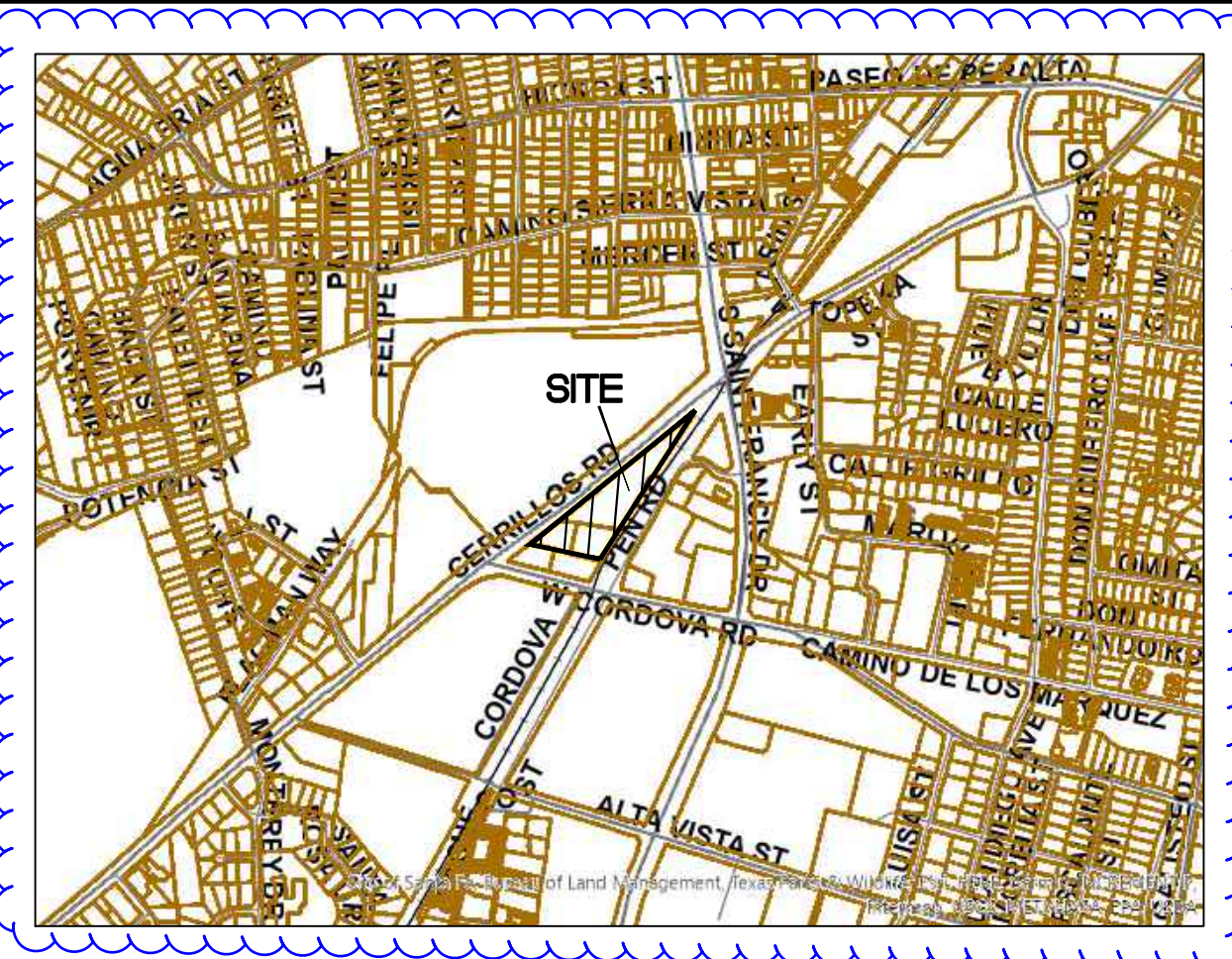
**LEGEND**

	CURB & GUTTER
	BOUNDARY LINE
	BUILDING
	EXISTING CURB & GUTTER
	CONCRETE SIDEWALK

- KEYED NOTES**
- UNIDIRECTIONAL ACCESSIBLE RAMP PER NMDOT DWG 608-001-8
  - CURB AND GUTTER TYPE C PER NMDOT DWG 609-01
  - MOUNTABLE MEDIAN CURB (BACK TO BACK) PER NMDOT DWG 609-01
  - 6" CONCRETE SIDEWALK
  - REMOVE AND SALVAGE EXISTING LIGHT POLES
  - REMOVE AND DISPOSE EXIST MEDIAN CURB AND LANDSCAPE

	<b>1000, 1101, 1103 CERRILLOS RD SANTA FE, NM</b>	DRAWN BY pm
	<b>CERRILLOS RD IMPROVEMENTS</b>	DATE 11-27-25
		DRAWING
		SHEET #
		JOB # 2025080

# AC MARRIOTT HOTEL DEVELOPMENT PLAN 1000, 1101 AND 1103 CERRILLOS RD CASE # 2025-11380



**VICINITY MAP:**

**NOTES:**

1. NO VARIANCES ARE APPROVED FOR THIS DEVELOPMENT PLAN. THE CITY CODE AND APPLICATION CHAPTERS SHALL GOVERN AND APPLY TO THIS DEVELOPMENT PLAN IN ITS ENTIRETY.
2. CITY OF SANTA FE DRAINAGE:
  - a. SUBJECT TO THE APPROVAL OF CITY OF SANTA FE PERMIT AND DEVELOPMENT REVIEW DIVISION STAFF, STORM DRAINAGE AND EROSION/SEDIMENT CONTROL IMPROVEMENTS SHALL BE EXECUTED IN CONJUNCTION WITH THE CONSTRUCTION OF EACH SEGMENT OF ROADS AND UTILITIES. THESE IMPROVEMENTS SHALL BE COMPLETED AND INSPECTED PRIOR TO THE ISSUANCE OF BUILDING PERMITS.
  - b. MAINTENANCE OF PRIVATE DRAINAGE EASEMENTS AND DRAINAGE FACILITIES IS THE RESPONSIBILITY OF THE OWNER. THE CITY OF SANTA FE IS HEREBY GRANTED THE RIGHT TO ACCESS AND INSPECT THESE EASEMENTS AND DRAINAGE FACILITIES AT THE DISCRETION OF THE CITY. THE OWNER AGREES TO INDEMNIFY AND TO HOLD HARMLESS FROM ALL DAMAGE TO PERSONS OR PROPERTY RESULTING FROM THE CITY'S REASONABLE EXERCISE TO THEIR ACCESS AND INSPECTION RIGHT.

**OWNER/DEVELOPER:**

TITAN DEVELOPMENT  
6300 RIVERSIDE PLAZA, SUITE 200  
ALBUQUERQUE, NM 87120  
505-998-0163 IAN ROBERTSON

**SURVEYOR (ALTA/TOPO):**

EDWARD M TRUJILLO  
DAWSON SURVEYS INC  
7505 MALLARD WAY, SUITE A  
SANTA FE, NM  
505-471-6660

**PLANNER:**

JENKINGSGAVIN  
130 GRANT AVE SUITE 101  
SANTA FE, NM 87501  
505-820-7444  
JENKINGSGAVIN.COM JENNIFER JENKINS

**CIVIL ENGINEER:**

TIERRA WEST,LLC  
5571 MIDWAY PARK PL, NE  
ALBUQUERQUE, NM  
505.858.3100  
TIERRAWESTLLC.COM

**ARCHITECT:**

5G STUDIO COLLABORATIVE, LLC  
1217 MAIN STREET  
DALLAS, TX 75202  
214-670-0050 CHRISTINE ROBBINS-ELROD

**LANDSCAPE:**

YELLOWSTONE LANDSCAPE  
7525 SECOND STREET NW  
ALBUQUERQUE, NM 87107  
505-998-9615 CODY McNALLAN

**MEP:**

BLUM CONSULTING ENGINEERS, INC  
12790 MERIT DRIVE, BUILDING 9, SUITE 700  
DALLAS, TX 75251  
214-373-8222 JAKE MUSICK

SHEET LIST	
SHEET #	SHEET NAME
C-001	COVER SHEET
C-002	EXISTING CONDITIONS
C-003	TOPOGRAPHIC SURVEY
DP-1	DEVELOPMENT PLAN
C-004	CONDITIONS OF APPROVAL

**NOTE**

AT TIME OF CONSTRUCTION, THE CONTRACTOR SHALL ENSURE ADA COMPLIANCE FOR CONSTRUCTION OF ADA ACCESSIBLE FEATURES AND APPURTENANCES, AS DETAILED IN, AND IN ADDITION TO, THE APPROVED CONSTRUCTION PERMIT DOCUMENTS AS REQUIRED. IMPROVEMENTS SHALL COMPLY WITH ICC ANSI A117.1-2009 CHAPTERS 1-5 AND CHAPTER 7, 2015 NEW MEXICO ACCESSIBLE PARKING CHECKLIST, MUTCD, NMDOT PEDESTRIAN ACCESS STANDARDS AND PROWAG, NM STATE STATUTE AND ADMINISTRATIVE CODE AND DOJ REGULATIONS AS APPLICABLE.

<p style="text-align: center;">CITY APPROVALS</p> <p>APPROVED BY THE SANTA FE PLANNING COMMISSION AT THEIR MEETING ON _____</p> <p>CASE # _____</p> <p>PLANNING COMMISSION CHAIRPERSON _____ DATE _____</p> <p>PLANNING COMMISSION SECRETARY _____ DATE _____</p> <p>REVIEWED BY THE CITY OF SANTA FE _____</p> <p>CITY PLANNER _____ DATE _____</p> <p>CITY ENGINEER FOR LAND USE _____ DATE _____</p>	<p style="text-align: center;">DRAINAGE FACILITIES MAINTENANCE</p> <p>ALL DRAINAGE FACILITIES SHALL BE MAINTAINED BY ALL PROPERTY OWNERS. THE PROPERTY OWNERS ALSO HAVE THE RESPONSIBILITY FOR MAINTENANCE OF THE PUBLIC DRAINAGE EASEMENT. INSPECTIONS SHALL BE CONDUCTED BY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF NEW MEXICO OR OTHER QUALIFIED PERSON. INSPECTIONS SHALL BE CONDUCTED ON THE DRAINAGE FACILITIES IN ACCORDANCE WITH THE FOLLOWING SCHEDULE: ON OR ABOUT MARCH 15, ON OR ABOUT SEPTEMBER 15, AND AFTER EACH STORM EVENT OF 1-INCH OR GREATER. MAINTENANCE OF THE DRAINAGE FACILITIES SHALL BE CONDUCTED AND DOCUMENTED BY THE ENGINEER AND THE OWNER. THE OWNER SHALL MAINTAIN A FILE OF THE INSPECTIONS AND REMEDIAL ACTION CONDUCTED ON THE DRAINAGE FACILITIES. WORK SHALL BE CONDUCTED IN ACCORDANCE WITH THE ENGINEERS' RECOMMENDATIONS, AND SHALL INCLUDE, BUT IS NOT LIMITED TO THE FOLLOWING:</p> <ol style="list-style-type: none"> <li>A. FLUSH ALL CULVERTS, DROP INLETS AND DRAINAGE PIPES TO REMOVE SEDIMENT AND VEGETATION THAT PREVENTS OR HINDERS THE FLOW OF STORMWATER IN THE DRAINAGE STRUCTURE(S).</li> <li>B. REMOVE SEDIMENT IN PONDS THAT IS GREATER THAN 6 INCHES IN DEPTH, AND IF NECESSARY, RESEED WITH NATIVE GRASSES AND INSTALL FILTER FABRIC AND 3 TO 4 INCH COBBLE AS DIRECTED BY THE ENGINEER.</li> <li>C. INSPECT FOR SEDIMENTATION IN ALL SWALES, DITCHES, DRAINAGE PONDS AND REMOVE AND STABILIZE AS NECESSARY.</li> <li>D. INSPECT FOR SOIL EROSION AND ALL DRAINAGE PONDS, CUT AND FILL SLOPES, AND REPAIR OR STABILIZE ACCORDINGLY.</li> <li>E. INSPECT THE STRUCTURAL INTEGRITY OF DRAINAGE PONDS, SLOPES, RIP-RAP, GABIONS, DROP INLETS, CULVERTS, ENERGY DISSIPATORS, AND RETAINING WALLS, ROCK PLATING, EROSION CONTROL MATS OR BLANKETS, AND REPAIR OR STABILIZE ACCORDINGLY.</li> </ol>
<p style="text-align: center;">AFFIDAVIT</p> <p>KNOWN ALL MEN BY THESE PRESENTS THAT THE UNDERSIGNED OWNERS HAVE CAUSED THIS FINAL DEVELOPMENT PLAN TO BE PREPARED. ALL THAT APPEARS ON THIS PLAN IS MADE WITH THE FREE CONSENT AND IN ACCORDANCE WITH THE DESIRES OF THE OWNER.</p> <p>_____ OWNER'S PRINTED NAME</p> <p>_____ OWNER'S SIGNATURE</p> <p>THE FOREGOING WAS SWORN, ACKNOWLEDGED, AND SUBSCRIBED BEFORE ME BY _____ THIS _____ DAY OF _____, 20____.</p> <p>_____ NOTARY PUBLIC</p> <p style="text-align: right;">MY COMMISSION EXPIRES _____</p>	<p style="text-align: center;">DRAINAGE FACILITIES DESIGN NOTE</p> <p>ALL STORM WATER DETENTION/RETENTION AREAS SHALL DRAIN WITHIN 24 HOURS OF A STORM EVENT AS PER ARTICLE 14-8.2 (D)(4)(C)(II)</p> <p style="text-align: center;">GENERAL NOTES</p> <ol style="list-style-type: none"> <li>A. SOLID WASTE COLLECTION WILL BE PROVIDED BY SF SOLID WASTE COLLECTION.</li> <li>B. EXTERIOR LUMINARIES SHALL COMPLY WITH CHAPTER 14-8.9 SFCC. (WALL RACKS SHALL BE SHIELDED)</li> <li>C. THE SITE SHALL COMPLY WITH INTERNATIONAL FIRE CODE (IFC) 2021.</li> <li>D. FIRE DEPARTMENT ACCESS SHALL COMPLY WITH IFC 2021.</li> <li>E. THIS SITE SHALL HAVE A WATER SUPPLY THAT MEETS FIRE FLOW AND HAVE A DISTANCE TO THE NEAREST HYDRANT TO MEET IFC REQUIREMENTS.</li> <li>F. ALL FIRE DEPARTMENT ACCESS SHALL HAVE A GRADE NO GREATER THAN 10%.</li> <li>G. FIRE LANE SURFACE SHALL BE ALL WEATHER AND SUPPORT 75,000 POUNDS.</li> <li>H. THE PROJECT IS SERVED BY A PUBLIC SANITARY SEWER COLLECTION SYSTEM.</li> </ol> <p style="text-align: center;">DUST CONTROL</p> <p>ALL ON-SITE SOIL DISTRIBUTING CONSTRUCTION ACTIVITY SHALL BE ADDRESSED AND PROVIDE MEASURES TO MITIGATE OR CONTROL DUST FROM BEING TRANSPORTED OFFSITE AND POLLUTING NEIGHBORING PROPERTIES. ANY PERSON, OWNER, CONTRACTOR OR OPERATOR WHO CONDUCTS EARTH MOVING AND DUST GENERATING ACTIVITIES IS RESPONSIBLE FOR IMPLEMENTING BEST MANAGEMENT PRACTICES (BMPs) IN ORDER TO MITIGATE OFF PROPERTY TRANSPORT OF FUGITIVE DUST EMISSIONS. A PLAN, OR STORMWATER PREVENTION PLAN (SWPPP) WHEN APPLICABLE, LISTING THE BEST MANAGEMENT PRACTICES (BMPs), SHALL BE PROVIDED TO THE CITY ENGINEER, OR THEIR DESIGNEE FOR REVIEW AND APPROVAL. THE APPROVED BMPs SHALL BE APPLIED TO THE GRADED AND OR DISTURBED SOIL IN ORDER TO STABILIZE THE SITE. THE INITIAL BMP SHALL ADDRESS HOW THE CONTRACTOR WILL MINIMIZE THE AMOUNT OF DISTURBED SOIL, AND HOW THE CONTRACTOR WILL STABILIZE THE DISTURBED SERVICE AREA EXPOSED TO WIND OR VEHICLE TRAFFIC DURING CONSTRUCTION.</p> <p>SEE SHEET C0-101 FOR FULL DUST CONTROL NOTES.</p>
<p style="text-align: center;">MODIFICATIONS</p> <p>SITE PLAN MODIFICATIONS ARE SUBJECT TO APPROVAL BY THE CITY OF SANTA FE AS PART OF THE BUILDING PERMIT PROCESS.</p>	<p style="text-align: center;">GUNNISON'S PRAIRIE DOG NOTE</p> <p>THIS PROJECT SHALL COMPLY WITH THE PROVISIONS OF THE GUNNISON'S PRAIRIE DOG ORDINANCE (ARTICLE 14-8.12).</p> <p style="text-align: center;">NOTARIZED STORMWATER AGREEMENT</p> <p>STORMWATER AGREEMENT: PROPERTY OWNER(S) HEREBY BY AGREE THAT ALL STORMWATER EASEMENTS AND ANY OTHER DRAINAGE AND STORMWATER MANAGEMENT IMPROVEMENTS ARE ON PRIVATE PROPERTY AND WILL BE MAINTAINED AND KEPT FULLY FUNCTIONAL AS ORIGINALLY DESIGNED AND CONSTRUCTED WITHIN PRIVATE PROPERTY BOUNDARIES BY THE PROPERTY OWNER AND SUBSEQUENT HEIRS, ASSIGNS, AND FUTURE OWNERS. THE CITY IS HEREBY GRANTED THE FOLLOWING: (1) ACCESS FOR INSPECTION OF SAID IMPROVEMENTS; (2) IN THE EVENT OF DRAINAGE AND STORMWATER MANAGEMENT IMPROVEMENT MAINTENANCE DEFICIENCY AND AFTER TEN (10) DAYS WRITTEN NOTICE TO THE RESPECTIVE PROPERTY OWNER, TO ENTER AND RESTORE FULL FUNCTIONAL CAPACITY OF THE DRAINAGE AND STORMWATER MANAGEMENT IMPROVEMENTS; AND (3) TO LIEN PROPERTY FOR BOTH DIRECT AND INDIRECT COSTS ASSOCIATED WITH SUCH WORK. BY SIGNATURE AFFIXED TO THIS INSTRUMENT, THE PROPERTY OWNER(S) APPROVE AND AGREE THAT THIS AGREEMENT IS BINDING PERPETUALLY, RUNNING WITH THE LAND, ON PRESENT AND FUTURE OWNERS, HEIRS, AND ASSIGNS.</p> <p>_____ OWNERS PRINTED NAME</p> <p>_____ OWNERS SIGNATURE</p> <p>THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS DAY OF _____, 20____.</p> <p>_____ NOTARY PUBLIC</p> <p style="text-align: right;">MY COMMISSION EXPIRES _____</p>

COUNTY OF SANTA FE } SS  
STATE OF NEW MEXICO }

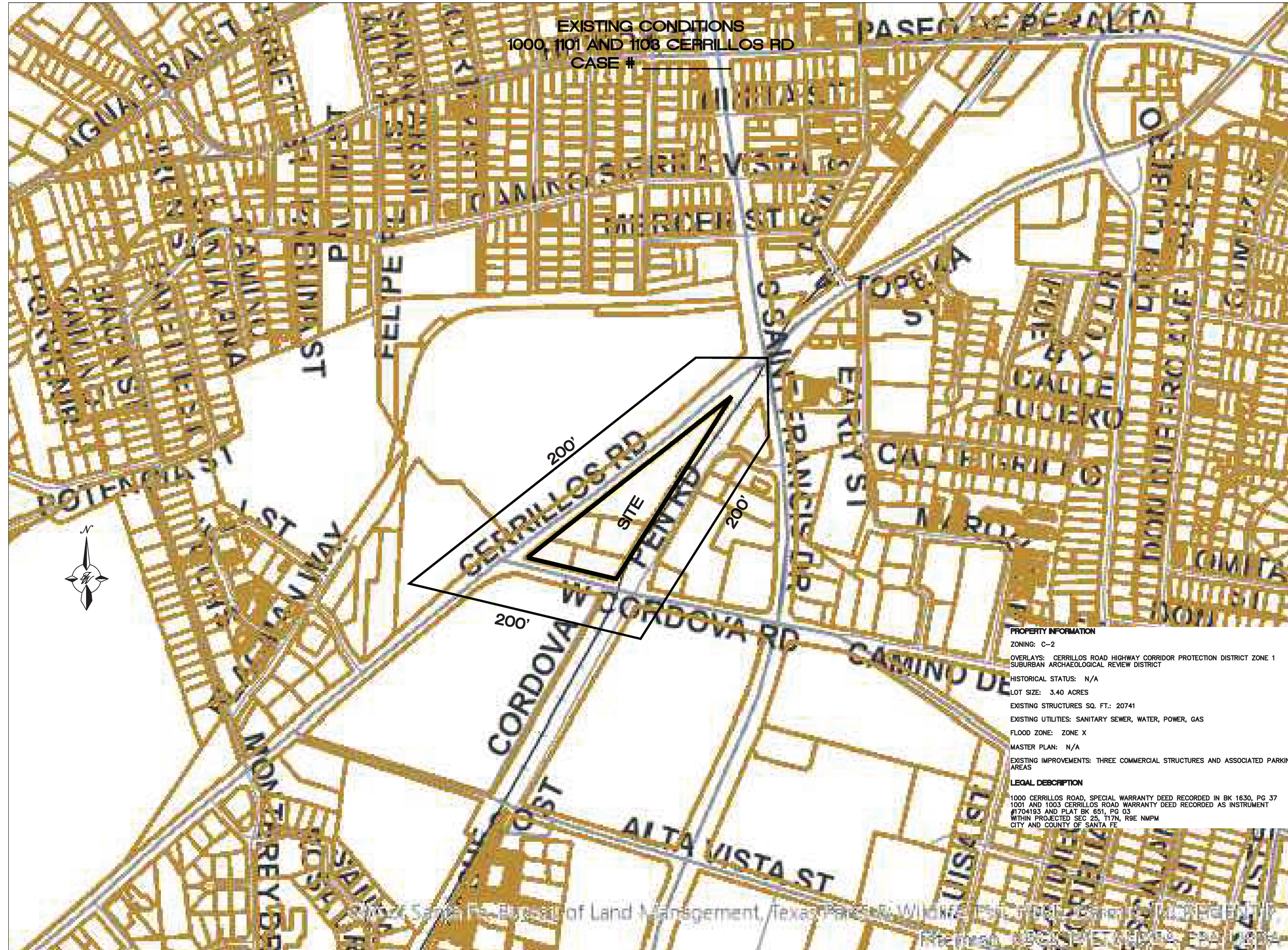
I HEREBY CERTIFY THAT THIS INSTRUMENT# \_\_\_\_\_ WAS FILED FOR RECORD ON THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ A.D. 20\_\_\_\_, AT \_\_\_\_\_ O'CLOCK \_\_\_\_\_ M., AND WAS DULY RECORDED IN BOOK \_\_\_\_\_ PAGE \_\_\_\_\_ OF THE RECORDS OF SANTA FE COUNTY.

WITNESS MY HAND AND SEAL OF OFFICE  
COUNTY CLERK, SANTA FE COUNTY, NM

DEPUTY

<p>ENGINEER'S SEAL</p> <p>RONALD R. BOHANNAN P.E. #7868</p>	<p>JURISDICTION: CITY OF SANTA FE</p> <p>PROJECT NAME: AC MARRIOTT HOTEL</p> <p>DRAWING TYPE: COVER SHEET</p> <p>DRAWING DATE: 12-2-25</p>
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**EXISTING CONDITIONS**  
**1000, 1101 AND 1103 CERRILLOS RD**  
**CASE # \_\_\_\_\_**



**PROPERTY INFORMATION**  
 ZONING: C-2  
 OVERLAYS: CERRILLOS ROAD HIGHWAY CORRIDOR PROTECTION DISTRICT ZONE 1  
 SUBURBAN ARCHAEOLOGICAL REVIEW DISTRICT  
 HISTORICAL STATUS: N/A  
 LOT SIZE: 3.40 ACRES  
 EXISTING STRUCTURES SQ. FT.: 20741  
 EXISTING UTILITIES: SANITARY SEWER, WATER, POWER, GAS  
 FLOOD ZONE: ZONE X  
 MASTER PLAN: N/A  
 EXISTING IMPROVEMENTS: THREE COMMERCIAL STRUCTURES AND ASSOCIATED PARKING AREAS

**LEGAL DESCRIPTION**  
 1000 CERRILLOS ROAD, SPECIAL WARRANTY DEED RECORDED IN BK 1630, PG 37  
 1001 AND 1003 CERRILLOS ROAD WARRANTY DEED RECORDED AS INSTRUMENT  
 #1704193 AND PLAT BK 651, PG 03  
 WITHIN PROJECTED SEC 25, T17N, R9E NMPM  
 CITY AND COUNTY OF SANTA FE

**NOTES:**

FOR ADDITIONAL INFORMATION AFFECTING THIS LOT, SEE RECORDED PLATS, DOCUMENTS, FOR NOTES, RESTRICTIONS AND EASEMENTS, THIS PROPERTY IS SUBJECT TO ALL VALID RESTRICTIONS, EASEMENTS AND COVENANTS OF RECORD.

THIS IS NOT A BOUNDARY SURVEY OR A RIGHT-OF-WAY SURVEY. APPARENT PROPERTY CORNERS, RIGHT-OF-WAY LINE, OR PROPERTY LINES AS SHOWN ARE DERIVED FROM RECORD SURVEY PLATS, RIGHT-OF-WAY MAPS, OR DEEDS REFERENCED HEREON AND ARE NOT GUARANTEED OR TO BE RELIED ON FOR THE ESTABLISHMENT OF PROPERTY LINES.

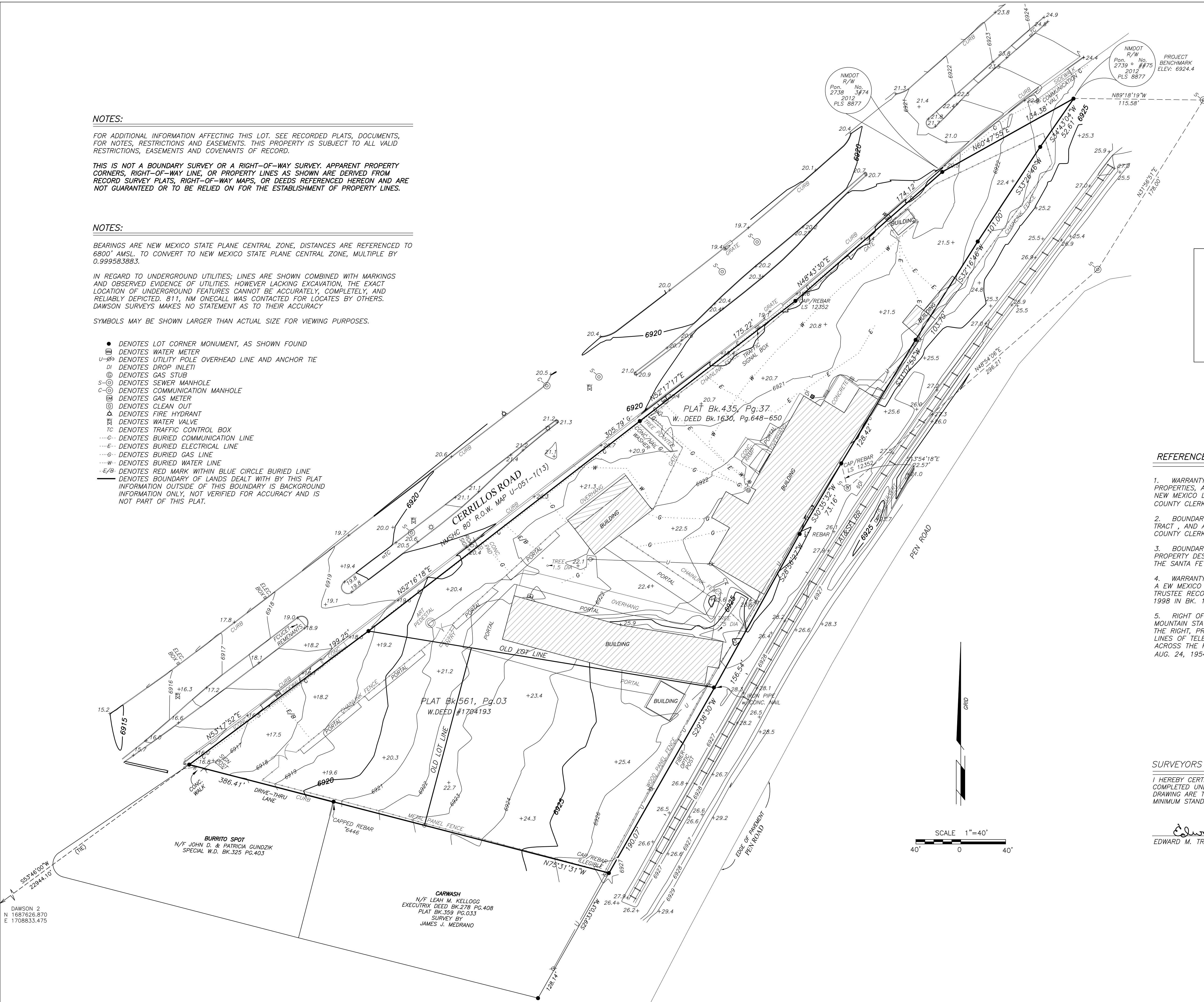
**NOTES:**

BEARINGS ARE NEW MEXICO STATE PLANE CENTRAL ZONE, DISTANCES ARE REFERENCED TO 6800' AMSL. TO CONVERT TO NEW MEXICO STATE PLANE CENTRAL ZONE, MULTIPLY BY 0.999583883.

IN REGARD TO UNDERGROUND UTILITIES, LINES ARE SHOWN COMBINED WITH MARKINGS AND OBSERVED EVIDENCE OF UTILITIES. HOWEVER LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY, AND RELIABLY DEPICTED. 811, NM ONECALL WAS CONTACTED FOR LOCATES BY OTHERS. DAWSON SURVEYS MAKES NO STATEMENT AS TO THEIR ACCURACY

SYMBOLS MAY BE SHOWN LARGER THAN ACTUAL SIZE FOR VIEWING PURPOSES.

- DENOTES LOT CORNER MONUMENT, AS SHOWN FOUND
- ⊕ DENOTES WATER METER
- U-99 DENOTES UTILITY POLE OVERHEAD LINE AND ANCHOR TIE
- DI DENOTES DROP INLET
- ⊙ DENOTES GAS STUB
- S-⊙ DENOTES SEWER MANHOLE
- C-⊙ DENOTES COMMUNICATION MANHOLE
- ⊕ DENOTES GAS METER
- ⊕ DENOTES CLEAN OUT
- ⊕ DENOTES FIRE HYDRANT
- ⊕ DENOTES WATER VALVE
- TC DENOTES TRAFFIC CONTROL BOX
- C--- DENOTES BURIED COMMUNICATION LINE
- E--- DENOTES BURIED ELECTRICAL LINE
- G--- DENOTES BURIED GAS LINE
- W--- DENOTES BURIED WATER LINE
- E/B- DENOTES RED MARK WITHIN BLUE CIRCLE BURIED LINE
- DENOTES BOUNDARY OF LANDS DEALT WITH BY THIS PLAT
- INFORMATION OUTSIDE OF THIS BOUNDARY IS BACKGROUND INFORMATION ONLY, NOT VERIFIED FOR ACCURACY AND IS NOT PART OF THIS PLAT.



TOPOGRAPHIC MAP  
PREPARED FOR  
**TITAN DEVELOPMENT**  
1000 CERRILLOS ROAD, SPECIAL WARRANTY DEED  
RECORDED IN Bk. 1630, Pg. 649 AND  
PLAT Bk. 435, Pg. 37  
1001 AND 1003 CERRILLOS ROAD, WARRANTY DEED  
RECORDED AS INSTRUMENT #1704193, AND  
PLAT Bk. 651, Pg. 03,  
WITHIN  
PROJECTED SEC. 26, T. 17N, R. 9E, N.M.P.M.  
CITY AND COUNTY OF SANTA FE, NEW MEXICO

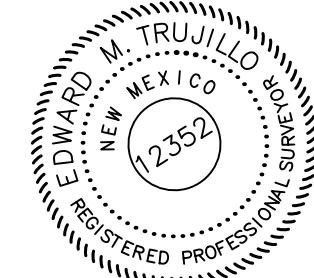
**REFERENCE DOCUMENT:**

1. WARRANTY DEED, 1001 CERRILLOS ROAD AND 1003 CERRILLOS ROAD FROM: JT PROPERTIES, A NEW MEXICO LIMITED LIABILITY COMPANY, TO: MGT INVESTMENTS, LLC, A NEW MEXICO LIMITED LIABILITY COMPANY, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK ON MAY 1, 2013 AS INSTRUMENT #1704193.
2. BOUNDARY SURVEY PLAT PREPARED FOR LOS SECOS, LLC. SHOWING A 0.56 ACRES± TRACT, AND A 0.77 ACRES± TRACT, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK ON JUNE 4, 2003 IN PLAT Bk. 561, Pg. 03.
3. BOUNDARY SURVEY PLAT PREPARED FOR GILLIAM IRREVOCABLE TRUST SHOWING PROPERTY DESCRIBED IN WARRANTY DEED Bk. 1630, Pg. 648, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK ON FEBRUARY 15, 2000 IN PLAT Bk. 435, Pg. 37.
4. WARRANTY DEED, 1000 CERRILLOS ROAD FROM: SANTA FE RENTALS AND SALES, INC., A NEW MEXICO CORPORATION, TO: GILLIAM IRREVOCABLE TRUST, W. THOMAS KELLAHIN, TRUSTEE RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK ON SEPTEMBER 15, 1998 IN Bk. 1540, PG. 351-353.
5. RIGHT OF WAY FROM: ESTATE OF JOSEPH BRYNE, J. ELMO TIPTON TRUSTEE TO: THE MOUNTAIN STATE TELEPHONE AND TELEGRAPH CO. AND PUBLIC SERVICE COMPANY OF N.M. THE RIGHT, PRIVILEGE AND AUTHORITY TO CONSTRUCT, OPERATE AND MAINTAIN THEIR LINES OF TELEPHONE, TELEGRAPH AND ELECTRIC POWER, FIXTURES UPON, OVER AND ACROSS THE PROPERTY, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK ON AUG. 24, 1954 AS INSTRUMENT #119,841.

**SURVEYORS CERTIFICATE**

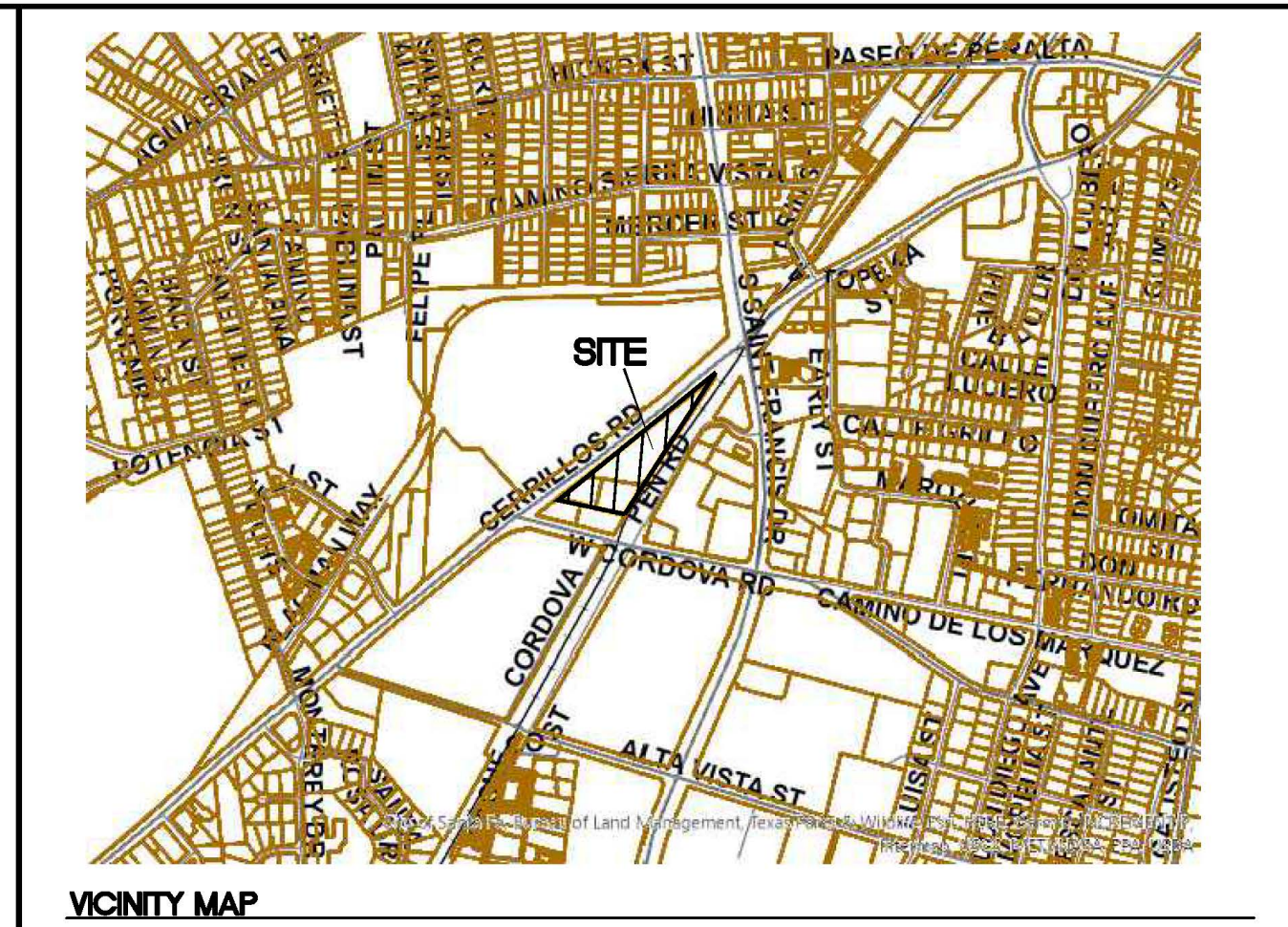
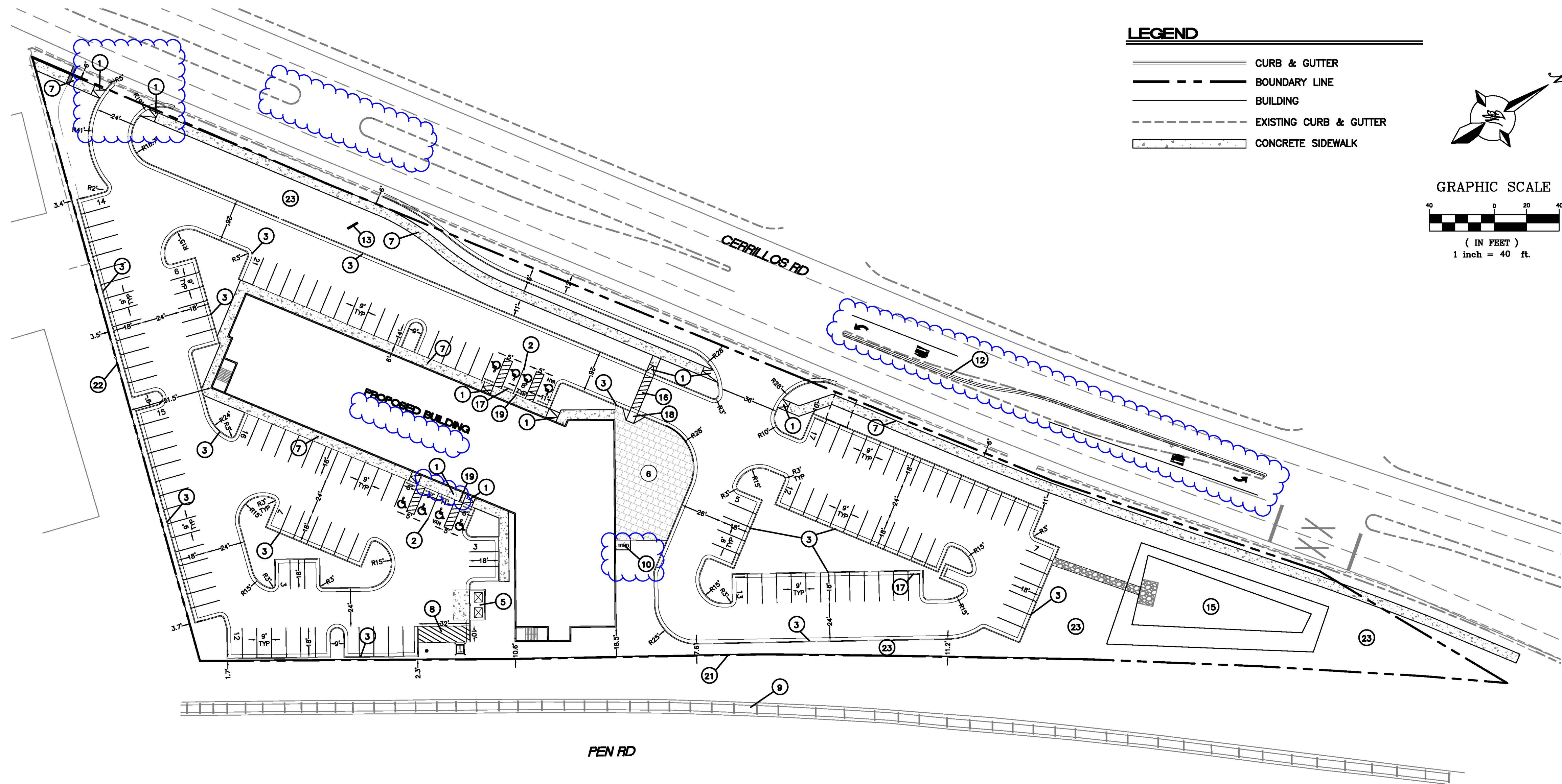
I HEREBY CERTIFY THAT THIS MAP IS A TRUE REPRESENTATION OF A TOPOGRAPHIC SURVEY COMPLETED UNDER MY PERSONAL SUPERVISION IN AUGUST OF 2025. THE SURVEY AND DRAWING ARE TRUE AND CORRECT, TO THE BEST OF MY KNOWLEDGE, AND MEET THE MINIMUM STANDARDS FOR TOPOGRAPHIC SURVEYS IN NEW MEXICO.

*Edward M. Trujillo* SEPT. 03, 2025  
EDWARD M. TRUJILLO, N.M.P.L.S. #12352 DATE



DAWSON SURVEYS INC.  
PROFESSIONAL LAND SURVEYORS  
7505 MALLARD WAY SUITE A  
SANTA FE, N.M. Tel: (505) 471-6660  
E-mail: dawsonsurveys@gmail.com  
FILE#10890/TOPO DATE: 09/03/2025

**C-003**



**LEGAL DESCRIPTION**  
 1000 CERRILLOS ROAD, SPECIAL WARRANTY DEED RECORDED IN Bk. 1630, Pg.649 AND PLAT Bk. 435, Pg.37, 1001 AND 1003 CERRILLOS ROAD, WARRANTY DEED RECORDED AS INSTRUMENT #1704193, AND PLAT Bk.651, Pg.03,

**SITE DATA**  
 PROPOSED USE HOTEL  
 LOT AREA 148122 SF (3.40 ACRES)  
 ZONING C-2  
 ROOFED AREA 19853 GSF  
 LOT COVERAGE 13.4%  
 MAXIMUM PERMISSIBLE HEIGHT 45'  
 PROPOSED HEIGHT 45'

**BUILDING SETBACK**  
 STREET 15'  
 REAR 10'  
 SIDE 0'  
 GFA 79491 SF

**PARKING REQUIRED** 150 SPACES (1 PER HOTEL ROOM)  
**PARKING PROVIDED** 151 SPACES  
**ACCESSIBLE PARKING REQUIRED** 8 SPACES (2 VAN)  
**ACCESSIBLE SPACES PROVIDED** 8 SPACES (2 VAN)

**BICYCLE SPACES REQUIRED** 6 (1 PER 15 ROOMS OR 6 IF MORE THAN 75 ROOMS)  
**BICYCLE SPACES PROVIDED** 6

**OPEN SPACE REQUIRED** 20% 0.68 AC  
**OPEN SPACE PROVIDED** 25.3% 0.86 AC

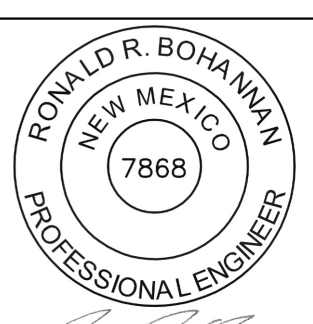
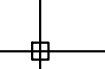
- KEYED NOTES**
- 1 UNIDIRECTIONAL ACCESSIBLE RAMP SEE DETAIL SHEET DET-8
  - 2 ACCESSIBLE PARKING PER ADA REQUIREMENTS (5)
  - 3 STANDARD CURB AND GUTTER, SEE DETAIL SHEET DET-8
  - 4 CONCRETE SIDEWALK
  - 5 DOUBLE DUMPSTER
  - 6 STAMPED CONCRETE, SEE ARCHITECTURAL PLANS FOR PATTERN AND COLOR
  - 7 NEW 6" CONCRETE SIDEWALK PER CITY OF SANTA FE STANDARDS
  - 8 LOADING ZONE
  - 9 EXISTING RAILROAD TRACK
  - 10 BICYCLE RACK (6)
  - 11 SITE LIGHTING
  - 12 MOUNTABLE MEDIAN CURB TYPE A PER NMDOT 609-01-1/1
  - 13 MONUMENT SIGN
  - 14 DIRECTIONAL SIGN
  - 15 DRAINAGE POND
  - 16 6" PEDESTRIAN CROSSWALK
  - 17 CONCRETE WHEEL STOP (TYP)
  - 18 ACCESSIBLE RAMP SEE DETAIL SHEET DET 8
  - 19 ZERO CURB
  - 20 TRANSFORMER
  - 21 8' TALL SCREEN/SOUND ( 560 LF) WALL SEE DETAIL SHEET 11
  - 22 WROUGHT IRON FENCE (375 LF) SEE DETAIL SHEET 11
  - 23 OPEN SPACE

	<b>1000, 1101, 1103 CERRILLOS RD</b> <b>SANTA FE, NM</b>	DRAWN BY pm
	<b>DEVELOPMENT PLAN</b>	DATE 12-2-25
	<b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	DRAWING
RONALD R. BOHANNAN P.E. #7868		SHEET # <b>DP-1</b>
		JOB # 2025080

AC MARRIOTT HOTEL  
 CONDITIONS OF APPROVAL  
 1000, 1101 AND 1103 CERRILLOS RD  
 CASE # 2025-11380

**CONDITIONS OF APPROVAL**

1. AT TIME OF CONSTRUCTION, THE CONTRACTOR SHALL ENSURE ADA COMPLIANCE FOR CONSTRUCTION OF ADA ACCESSIBLE FEATURES AND APPURTENANCES, AS DETAILED IN, AND IN ADDITION TO, THE APPROVED CONSTRUCTION PERMIT DOCUMENTS AS REQUIRED. IMPROVEMENTS SHALL COMPLY WITH ICC ANSI A117.1-2009 CHAPTERS 1-5 AND CHAPTER 7, 2015 NEW MEXICO ACCESSIBLE PARKING CHECKLIST, MUTCD, NMDOT PEDESTRIAN ACCESS STANDARDS AND PROWAG, NM STATE STATUTE AND ADMINISTRATIVE CODE AND DOJ REGULATIONS AS APPLICABLE.
2. AN APPROVED WATER PLAN WILL BE REQUIRED FOR ALL NEW PUBLIC WATER INFRASTRUCTURE AND FIRE SERVICES.
3. AN APPROVED AGREEMENT TO CONSTRUCT AND DEDICATE (ACD) WILL BE REQUIRED WITH THE WATER DIVISION FOR ALL NEW PUBLIC WATER INFRASTRUCTURE AND FIRE SERVICES.
4. A SEPARATE IRRIGATION METER WILL BE REQUIRED IF THE TOTAL LANDSCAPED AREA IS 1,000 SQUARE FEET OR GREATER. AN APPROVED BACKFLOW PREVENTION DEVICE SHALL BE INSTALLED BEYOND THE METER ON ANY NEW IRRIGATION SERVICE.
5. DEVELOPMENT COMPLY WITH THE 2021 INTERNATIONAL FIRE CODE (IFC) AND ADOPTED CITY ORDINANCES UPON BUILDING PERMIT APPROVAL.

 RONALD R. BOHANNAN P.E. #7868	<b>ENGINEER'S SEAL</b>	<b>1000, 1101, 1103 CERRILLOS RD          SANTA FE, NM</b>	<i>DRAWN BY</i> pm
	<b>CONDITIONS OF APPROVAL</b>		<i>DATE</i> 12-2-25
	 <b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL. NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	<i>DRAWING</i>	
	SHEET # <b>C-004</b>	<i>JOB #</i> 2025080	

**AC MARRIOTT HOTEL  
INFRASTRUCTURE PLAN  
1000, 1101 AND 1103 CERRILLOS RD  
CASE # 2025-11380**

**NOTES:**

1. NO VARIANCES ARE APPROVED FOR THIS DEVELOPMENT PLAN. THE CITY CODE AND APPLICATION CHAPTERS SHALL GOVERN AND APPLY TO THIS DEVELOPMENT PLAN IN ITS ENTIRETY.

2. CITY OF SANTA FE DRAINAGE:

a. SUBJECT TO THE APPROVAL OF CITY OF SANTA FE PERMIT AND DEVELOPMENT REVIEW DIVISION STAFF, STORM DRAINAGE AND EROSION/SEDIMENT CONTROL IMPROVEMENTS SHALL BE EXECUTED IN CONJUNCTION WITH THE CONSTRUCTION OF EACH SEGMENT OF ROADS AND UTILITIES. THESE IMPROVEMENTS SHALL BE COMPLETED AND INSPECTED PRIOR TO THE ISSUANCE OF BUILDING PERMITS.

b. MAINTENANCE OF PRIVATE DRAINAGE EASEMENTS AND DRAINAGE FACILITIES IS THE RESPONSIBILITY OF THE OWNER. THE CITY OF SANTA FE IS HEREBY GRANTED THE RIGHT TO ACCESS AND INSPECT THESE EASEMENTS AND DRAINAGE FACILITIES AT THE DISCRETION OF THE CITY. THE OWNER AGREES TO INDEMNIFY AND TO HOLD HARMLESS FROM ALL DAMAGE TO PERSONS OR PROPERTY RESULTING FROM THE CITY'S REASONABLE EXERCISE TO THEIR ACCESS AND INSPECTION RIGHT.

**OWNER/DEVELOPER:**

TITAN DEVELOPMENT  
6300 RIVERSIDE PLAZA, SUITE 200  
ALBUQUERQUE, NM 87120  
505-998-0163 IAN ROBERTSON

**SURVEYOR (ALTA/TOPO):**

EDWARD M TRUJILLO  
DAWSON SURVEYS INC  
7505 MALLARD WAY, SUITE A  
SANTA FE, NM  
505-471-6660

**PLANNER:**

JENKINGGAVIN  
130 GRANT AVE SUITE 101  
SANTA FE, NM 87501  
505-820-7444  
JENKINGGAVIN.COM JENNIFER JENKINS

**CIVIL ENGINEER:**

TIERRA WEST,LLC  
5571 MIDWAY PARK PL, NE  
ALBUQUERQUE, NM  
505.858.3100  
TIERRAWESTLLC.COM

**ARCHITECT:**

5G STUDIO COLLABORATIVE, LLC  
1217 MAIN STREET  
DALLAS, TX 75202  
214-670-0050 CHRISTINE ROBBINS-ELROD

**LANDSCAPE:**

YELLOWSTONE LANDSCAPE  
7525 SECOND STREET NW  
ALBUQUERQUE, NM 87107  
505-998-9615 CODY McNALLEN

**MEP:**

BLUM CONSULTING ENGINEERS, INC  
12790 MERIT DRIVE, BUILDING 9, SUITE 700  
DALLAS, TX 75251  
214-373-8222 JAKE MUSICK

**SHEET LIST**

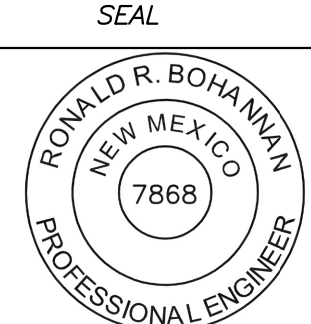
		SHEET #	SHEET NAME
C-001	COVER SHEET		
TR-1	TERRAIN MANAGEMENT PLAN	E1.00	PHOTOMETRIC SITE PLAN
DEM-1	DEMOLITION PLAN	E1.01	LIGHTING FIXTURES CUTSHEET
GR-1	GRADING AND DRAINAGE PLAN		
MU-1	MASTER UTILITY PLAN	A101	OPEN SPACE PLAN
DET-1	CONSTRUCTION DETAILS		
DET-2	CONSTRUCTION DETAILS	A201	LEVEL 1 FLOOR PLAN
DET-3	CONSTRUCTION DETAILS	A202	LEVEL 2 AND 3 FLOOR PLAN
DET-4	CONSTRUCTION DETAILS	A203	LEVEL 4 FLOOR PLAN
PAV-1	PAVING PLAN		
RD-1	CERRILLOS ROAD IMPROVEMENT PLAN	A301	BUILDING ELEVATIONS
RD-2	CERRILLOS ROAD MEDIAN PLAN AND PROFILE	A302	BUILDING ELEVATIONS
FO-1	FIRE PROTECTION PLAN	A303	SIGNAGE
21	WATER PLAN		
LS-00	MATERIAL PLAN OVERALL		
LS-01	MATERIAL PLAN ENLARGEMENT		
LS-02	MATERIAL PLAN ENLARGEMENT		
LS-03	MATERIAL PLAN ENLARGEMENT		
LP-00	PLANTING PLAN OVERALL		
LP-01	PLANTING PLAN ENLARGEMENT		
LP-02	PLANTING PLAN ENLARGEMENT		
LP-03	PLANTING PLAN ENLARGEMENT		
LH-00	IRRIGATION PLAN OVERALL		
LH-01	IRRIGATION PLAN ENLARGEMENT		
LH-02	IRRIGATION PLAN ENLARGEMENT		
LH-03	IRRIGATION PLAN ENLARGEMENT		
LH-04	IRRIGATION DETAILS		

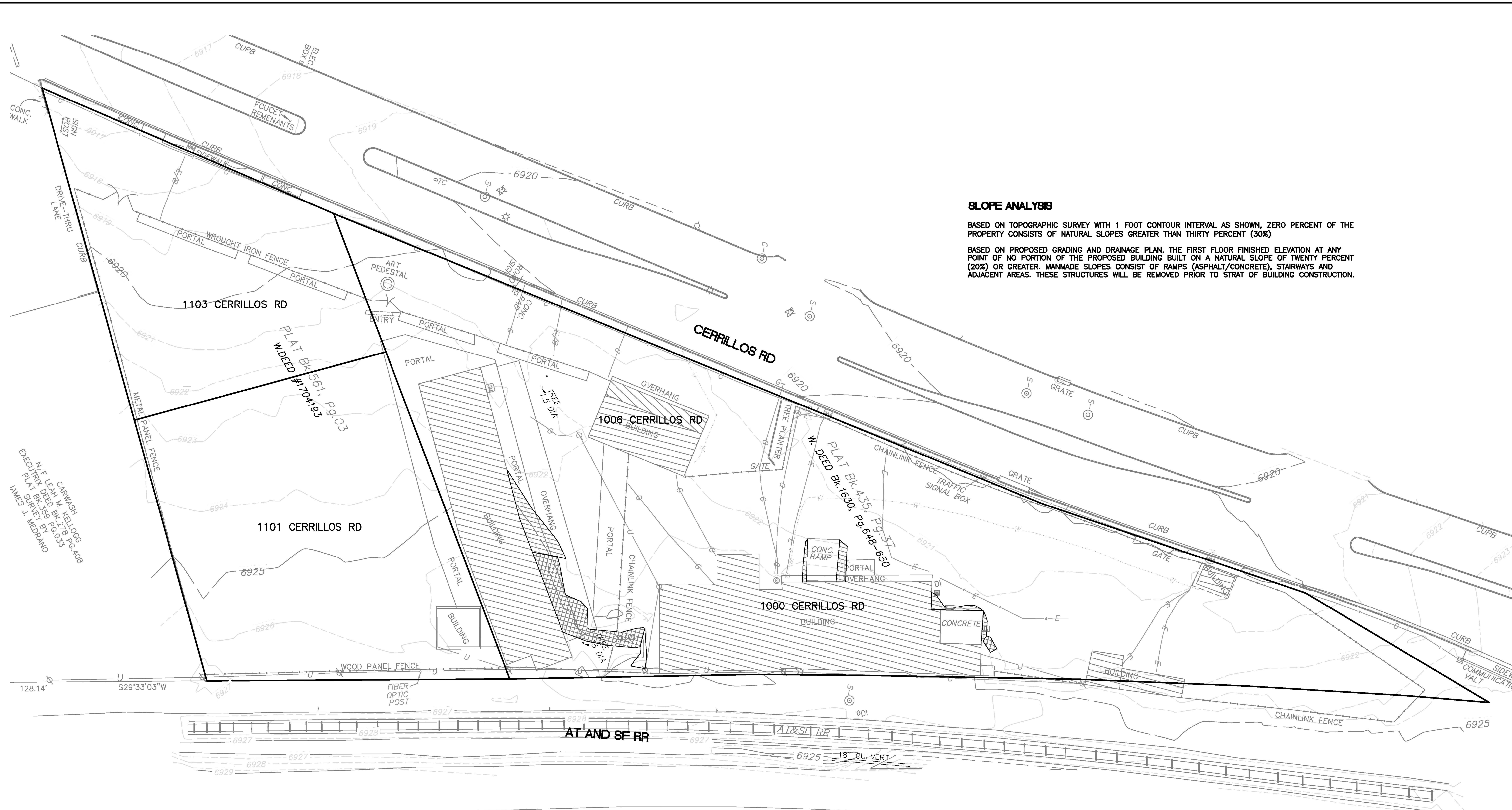
**ENGINEER'S STATEMENT**

I, RONALD BOHANNAN, NEW MEXICO REGISTERED PROFESSIONAL ENGINEER NO. 7868, DO HEREBY STATE THAT I HAVE PERSONALLY MADE AN ON-SITE FIELD INSPECTION OF THE SUBJECT PROPERTY, AND HAVE FOUND THAT NO RECENT GRADING, FILLING, OR CUTTING, HAD TAKEN PLACE ON SAID SITE PRIOR TO THE PREPARATION OF THE TOPOGRAPHY SURVEY SHOWN ON THE PLAN HEREON.

SIGNATURE \_\_\_\_\_

DATE \_\_\_\_\_

	<p><b>JURISDICTION:</b> CITY OF SANTA FE</p> <p><b>PROJECT NAME:</b> AC MARRIOTT HOTEL</p> <p><b>DRAWING TYPE:</b> COVER SHEET</p> <p><b>DRAWING DATE:</b> 12-2-25</p>
<p>12-2-25</p> <p>RONALD R. BOHANNAN P.E. #7868</p>	



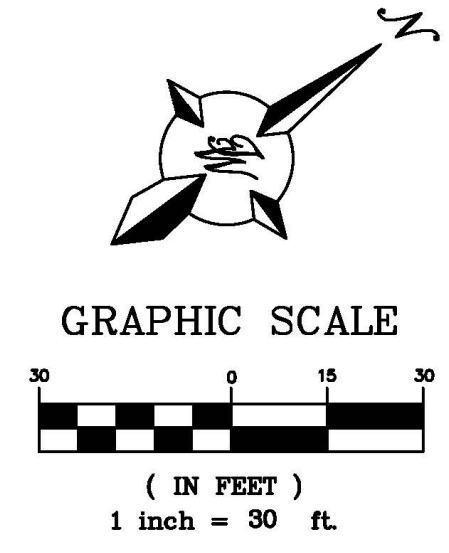
**SLOPE ANALYSIS**

BASED ON TOPOGRAPHIC SURVEY WITH 1 FOOT CONTOUR INTERVAL AS SHOWN, ZERO PERCENT OF THE PROPERTY CONSISTS OF NATURAL SLOPES GREATER THAN THIRTY PERCENT (30%)

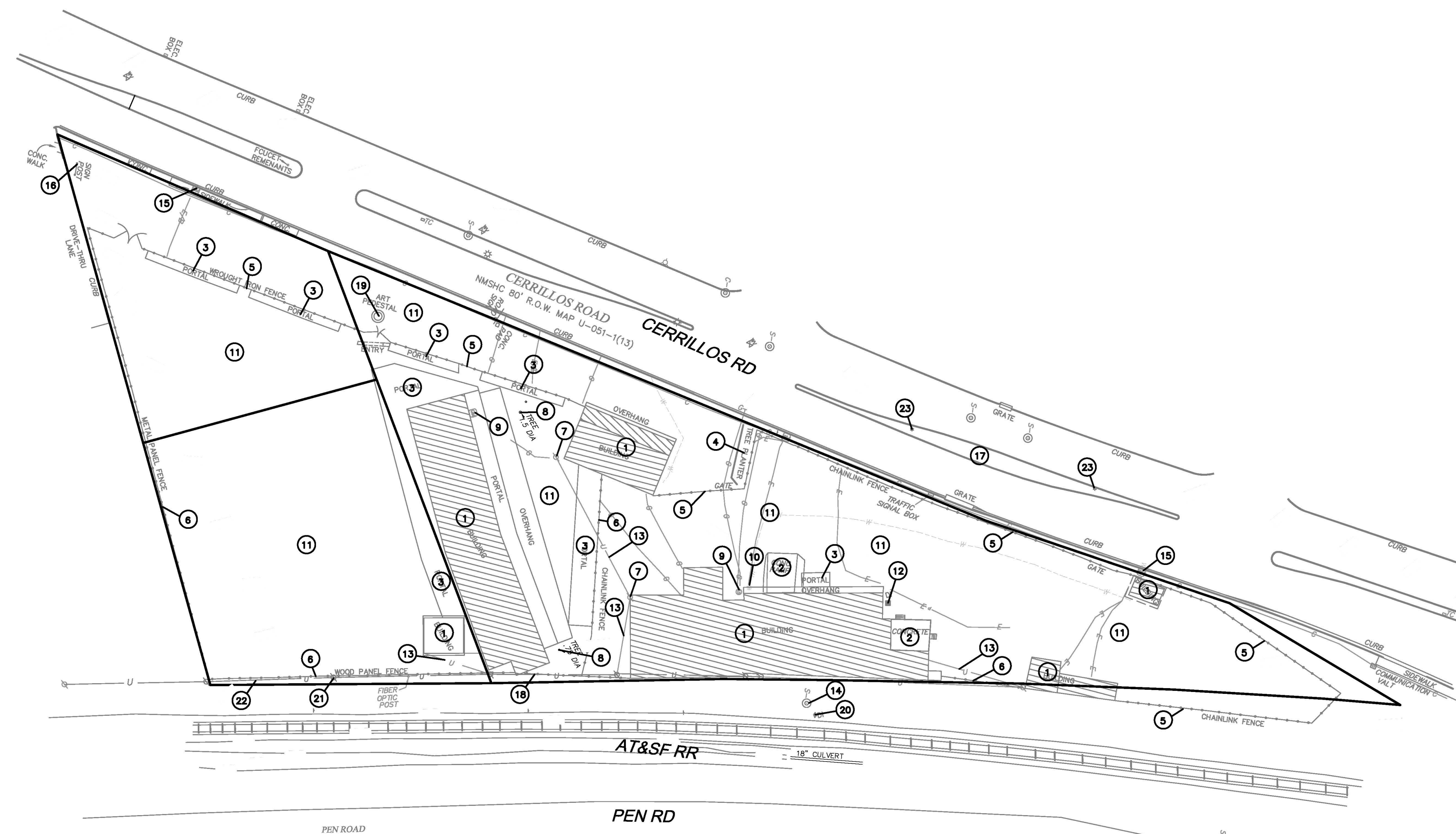
BASED ON PROPOSED GRADING AND DRAINAGE PLAN, THE FIRST FLOOR FINISHED ELEVATION AT ANY POINT OF NO PORTION OF THE PROPOSED BUILDING BUILT ON A NATURAL SLOPE OF TWENTY PERCENT (20%) OR GREATER. MANMADE SLOPES CONSIST OF RAMPS (ASPHALT/CONCRETE), STAIRWAYS AND ADJACENT AREAS. THESE STRUCTURES WILL BE REMOVED PRIOR TO STRAT OF BUILDING CONSTRUCTION.

**LEGEND**

-  AREA OF 30% OR GREATER (MAN MADE/DISTURBED)
-  AREA OF 20% TO 30% (MAN MADE/DISTURBED)



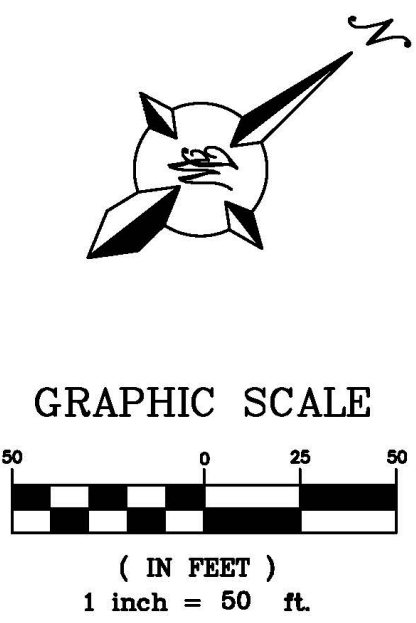
 12-2-25 RONALD R. BOHANNAN P.E. #7868	<b>AC MARRIOTT HOTEL</b> 1000, 1006, 1101 AND 1103 CERRILLOS RD SANTA FE, NM	DRAWN BY pm DATE 12-2-25 DRAWING
	<b>TERRAIN MANAGEMENT PLAN</b>	SHEET # <b>TR-1</b>
 <b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com		JOB # 2025080



- KEYED NOTES**
- 1 REMOVE AND DISPOSE EXISTING BUILDINGS AND APPURTENANCES
  - 2 REMOVE AND DISPOSE EXISTING CONCRETE
  - 3 REMOVE AND DISPOSE EXISTING COVERED AREA
  - 4 REMOVE AND DISPOSE EXISTING PLANTER
  - 5 REMOVE AND DISPOSE EXISTING FENCE AND GATE
  - 6 REMOVE AND DISPOSE EXISTING FENCE
  - 7 REMOVE AND DISPOSE EXISTING POWER POLE
  - 8 REMOVE AND DISPOSE EXISTING TREE
  - 9 REMOVE AND DISPOSE EXISTING GAS METER
  - 10 REMOVE AND DISPOSE EXISTING CLEANOUT
  - 11 REMOVE AND DISPOSE EXISTING ASPHALT AND/OR GRAVEL
  - 12 REMOVE AND DISPOSE EXISTING INLET
  - 13 REMOVE AND DISPOSE EXISTING POWER LINE
  - 14 EXISTING SAS MH TO REMAIN
  - 15 REMOVE AND DISPOSE EXISTING WATER METER
  - 16 REMOVE AND DISPOSE EXISTING SIGN AND POSTS
  - 17 REMOVE AND DISPOSE MEDIAN CURB AND LANDSCAPING
  - 18 REMOVE AND DISPOSE 410 LF EXISTING OVERHEAD POWER LINE AND 4 POWER POLES
  - 19 REMOVE AND DISPOSE EXISTING ART PEDESTAL
  - 20 EXISTING DROP INLET TO REMAIN
  - 21 EXISTING POWER POLE TO REMAIN
  - 22 EXISTING POWER LINE TO REMAIN
  - 23 REMOVE AND SALVAGE EXISTING LIGHTS

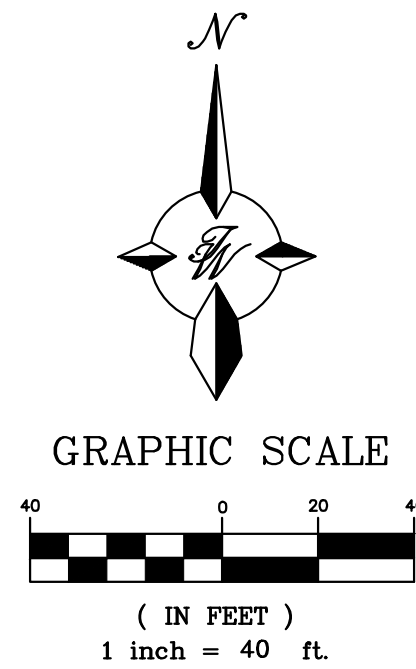
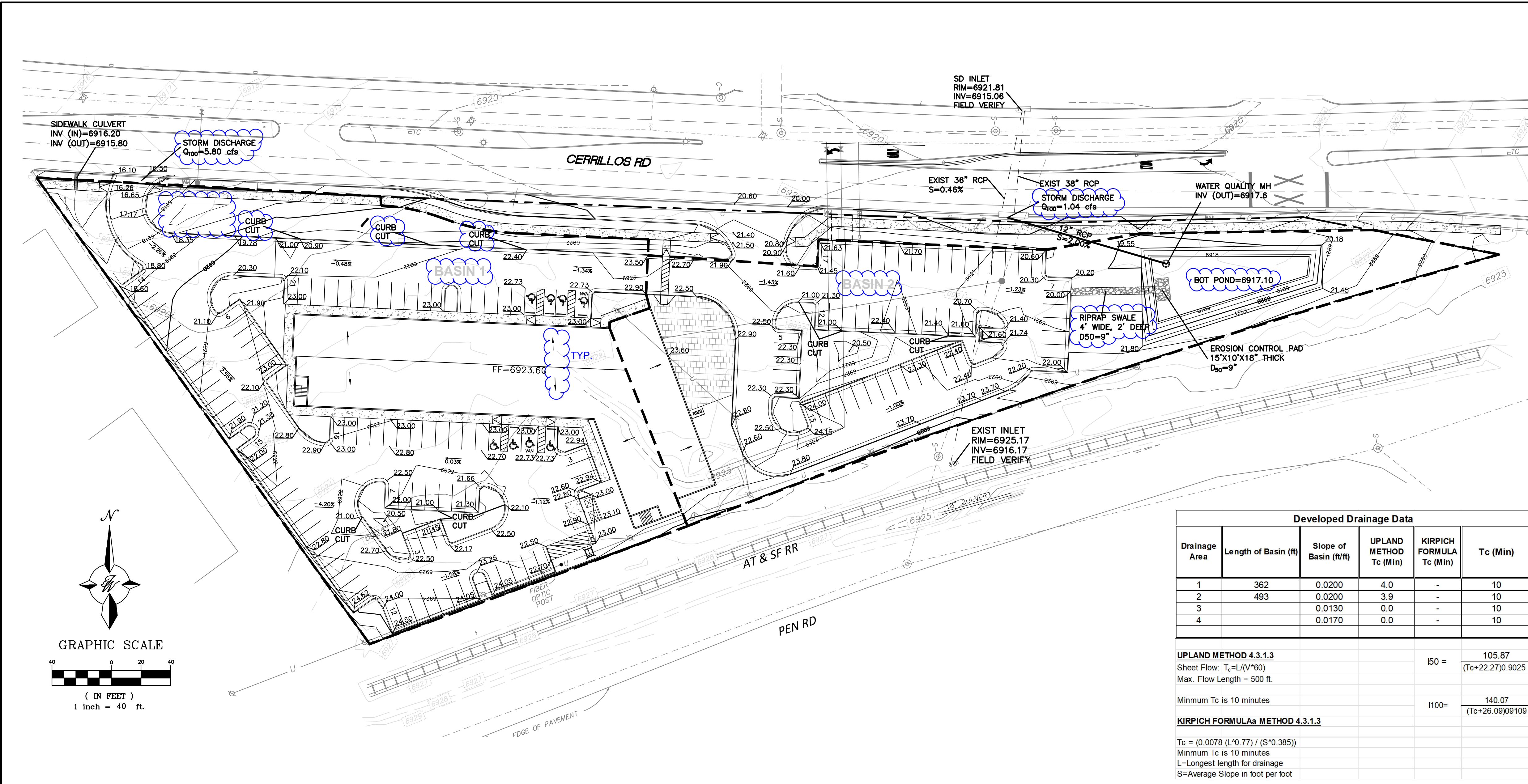
**GENERAL DEMOLITION AND REMOVAL NOTES:**

1. ITEMS OF SITE DEMOLITION SHALL BE GOVERNED BY THE CITY OF SANTA FE PUBLIC WORKS
2. REGULATORY REQUIREMENTS
  - a. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS REQUIRED FOR DEMOLITION, PAYING ALL SPECIFIED FEES.
  - b. CONFORM TO ALL APPLICABLE CODES AND REGULATIONS FOR DEMOLITION, SAFETY OF ADJACENT STRUCTURES, DUST CONTROL, RUNOFF CONTROL AND DISPOSAL OF DEBRIS.
  - c. NOTIFY AFFECTED UTILITY COMPANIES BEFORE STARTING WORK AND COMPLY WITH THEIR REQUIREMENTS.
  - d. DO NOT CLOSE OR OBSTRUCT ROADWAYS AND FIRE HYDRANTS WITHOUT PERMITS.
  - e. CONFORM TO APPLICABLE REGULATORY PROCEDURES WHEN DISCOVERING HAZARDOUS MATERIALS OR CONTAMINATED MATERIALS.
3. DEMOLITION REQUIREMENTS
  - a. CONDUCT DEMOLITION TO MINIMIZE INTERFERENCE WITH ADJACENT ITEMS.
  - b. CEASE OPERATIONS IMMEDIATELY IF ADJACENT STRUCTURES APPEAR TO BE IN DANGER. DO NOT RESUME OPERATIONS UNTIL DIRECTED.
4. DEMOLITION
  - a. IT IS THE INTENT OF THIS PLAN THAT ALL EXISTING BUILDINGS, PAVEMENTS, SIDEWALK, TREES AND OTHER ABOVE GROUND FEATURES WITHIN THE PROJECT LIMITS SHALL BE DEMOLISHED AND REMOVED FROM THE SITE UNLESS OTHERWISE INDICATED.
  - b. SEE THE SITE UTILITY PLAN FOR CONNECTIONS AND NEW LOCATIONS OF THE EXISTING UTILITIES ON THE SITE. COORDINATE ALL RELOCATIONS AND INTERRUPTION OF SERVICE WITH APPROPRIATE UTILITY COMPANY. ALLOW ADEQUATE TIME FOR SCHEDULING.
  - c. ALL FOUNDATIONS SHALL BE REMOVED TO 18" BELOW PROPOSED GRADE. ANY FOUNDATIONS NOT COMPLETELY REMOVED SHALL HAVE ANY PIPES PLUGGED WITH NON-SHRINK GROUT AND THE FLOOR RUBBLIZED.
  - d. BACKFILL AREAS EXCAVATED AND ANY OPEN PITS AND HOLES CAUSED AS A RESULT OF DEMOLITION WITH PIT RUN SAND AND GRAVEL.
  - e. ROUGH GRADE AND COMPACT AREAS AFFECTED BY DEMOLITION TO MAINTAIN SITE GRADES AND CONTOURS. PROVIDE POSITIVE DRAINAGE.
  - f. REMOVE DEMOLISHED MATERIALS FROM SITE AND LEGALLY DISPOSE OF AT A SITE TO BE ARRANGED FOR BY CONTRACTOR.
  - g. DO NOT BURN OR BURY MATERIALS ON SITE. LEAVE SITE IN CLEAN CONDITION.
  - h. EXISTING TREES SHALL BE COMPLETELY REMOVED BELOW EXISTING GROUND ELEVATION INCLUDING ALL ROOTS. STUMPS SHALL NOT BE BURIED ON SITE.
  - i. EXISTING TOPSOIL TO BE REMOVED AND STOCK PILED ON SITE FOR USE IN PLATING SLOPES AS DIRECTED BY OWNER.
5. PROVIDE FULL DEPTH SAW CUTS AT EDGES OF EXISTING PAVEMENT AND SIDEWALK REMOVAL LOCATIONS AS REQUIRED.



**CAUTION**  
 ALL EXISTING UTILITIES SHOWN WERE OBTAINED FROM RESEARCH, AS-BUILTS, SURVEYS OR INFORMATION PROVIDED BY OTHERS. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO CONDUCT ALL NECESSARY FIELD INVESTIGATIONS PRIOR TO AND INCLUDING ANY EXCAVATION, TO DETERMINE THE ACTUAL LOCATION OF UTILITIES AND OTHER IMPROVEMENTS, PRIOR TO STARTING THE WORK. ANY CHANGES FROM THIS PLAN SHALL BE COORDINATED WITH AND APPROVED BY THE ENGINEER.

	<b>1000, 1101, 1103 CERRILLOS RD          SANTA FE, NM</b>	DRAWN BY prn
	<b>DEMOLITION PLAN</b>	DATE 12-2-25
		DRAWING 2025080-DEMO
5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com		SHEET # <b>DEM-1</b>
RONALD R. BOHANNAN P.E. #7868		JOB # 2025080



Developed Drainage Data					
Drainage Area	Length of Basin (ft)	Slope of Basin (ft/ft)	UPLAND METHOD Tc (Min)	KIRPICH FORMULA Tc (Min)	Tc (Min)
1	362	0.0200	4.0	-	10
2	493	0.0200	3.9	-	10
3		0.0130	0.0	-	10
4		0.0170	0.0	-	10

**UPLAND METHOD 4.3.1.3**  
Sheet Flow:  $T_c = L / (V \cdot 60)$   
Max. Flow Length = 500 ft.  
Minimum Tc is 10 minutes

**KIRPICH FORMULA METHOD 4.3.1.3**  
 $T_c = (0.0078 (L^{0.77}) / (S^{0.385}))$   
Minimum Tc is 10 minutes  
L=Longest length for drainage  
S=Average Slope in foot per foot

**VOLUME CALCULATIONS**

AC Hotel  
NORTH POND

Ab - Bottom Of The Pond Surface Area  
At - Top Of The Pond Surface Area  
D - Water Depth  
Dt - Total Pond Depth  
C - Change In Surface Area / Water Depth

Volume =  $Ab \cdot D + 0.5 \cdot C \cdot D^2$   
 $C = (At - Ab) / Dt$

Ab = 2,865.00 B.O.P.= 6917.60  
 At = 4,445.00 T.O.P. = 6919.55  
 Dt = 1.95  
 C = 810.26  
 B Elev. = 6,917.60

ACTUAL ELEV.	DEPTH (FT)	VOLUME (AC-FT)	Q (CFS)
6917.60	0	0	0.000
6918.60	1.00	0.0751	0.697
6919.55	1.95	0.1636	1.042

Orifice Equation  
 $Q = CA \sqrt{2gh}$

C = 0.6  
 Diameter (in) = 5.5  
 Area (ft<sup>2</sup>) = 0.164988156  
 g = 32.2  
 H (ft) = Depth of water above center of orifice  
 Q (CFS) = Flow

**LEGEND**

- CURB & GUTTER
- BOUNDARY LINE
- BUILDING
- PROPOSED HYDRANT
- NEW SD MH
- AREA INLET
- EXISTING SAS MH
- EXISTING GATE VALVE
- EXISTING WATERLINE
- EXISTING SAS
- RETAINING WALL
- WATER BLOCK
- STORM DRAIN
- BASINS

**NOTICE TO CONTRACTORS**

- AN EXCAVATION/CONSTRUCTION PERMIT WILL BE REQUIRED BEFORE BEGINNING ANY WORK WITHIN CITY RIGHT-OF-WAY.
- ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON, SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF SANTA FE SPECIFICATIONS.
- TWO WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT LINE LOCATING SERVICE, 765-1234, FOR LOCATION OF EXISTING UTILITIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL CONNECTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- BACKFILL COMPACTION SHALL BE ACCORDING TO TRAFFIC/STREET USE.
- MAINTENANCE OF THESE FACILITIES SHALL BE THE RESPONSIBILITY OF THE OWNER OF THE PROPERTY SERVED.
- WORK ON ARTERIAL STREETS SHALL BE PERFORMED ON A 24-HOUR BASIS.

**EROSION CONTROL NOTES:**

- CONTRACTOR IS RESPONSIBLE FOR OBTAINING A TOPSOIL DISTURBANCE PERMIT PRIOR TO BEGINNING WORK.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING RUN-OFF ON SITE DURING CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL SEDIMENT THAT GETS INTO EXISTING RIGHT-OF-WAY.
- REPAIR OF DAMAGED FACILITIES AND CLEANUP OF SEDIMENT ACCUMULATIONS ON ADJACENT PROPERTIES AND IN PUBLIC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL EXPOSED EARTH SURFACES MUST BE PROTECTED FROM WIND AND WATER EROSION PRIOR TO FINAL (CITY) ACCEPTANCE OF ANY PROJECT.
- ALL SLOPES NOT STABILIZED AT THE END OF THE PROJECT SHALL BE STABILIZED IN ACCORDANCE WITH COA SPECS OR 3" GRAVEL.

**ENGINEER'S STORMWATER INFRASTRUCTURE CERTIFICATION**

I, THE UNDERSIGNED PROFESSIONAL ENGINEER IN THE STATE OF NEW MEXICO, DO HEREBY CERTIFY THAT THE RECORD INFORMATION SHOWN HEREON IS BASED ON ACTUAL FIELD MEASUREMENTS AND VISUAL INSPECTIONS PERFORMED BY MYSELF OR UNDER MY DIRECT SUPERVISION. I FURTHER CERTIFY THAT THE RECORD CONDITION AS OF \_\_\_\_\_ IS IN SUBSTANTIAL COMPLIANCE WITH THE APPROVED GRADING AND DRAINAGE PLAN PREPARED BY \_\_\_\_\_, DATED \_\_\_\_\_.

SIGNATURE \_\_\_\_\_ NMPE# \_\_\_\_\_ DATE \_\_\_\_\_

Existing Drainage Data										
Drainage Area	Area (Sq. Ft.)	Area (Ac)	Tc (Min)	C-value	I <sub>50yr</sub> (in/hr)	Q <sub>50yr</sub> (cfs)	V <sub>50yr</sub> (ac-ft)	I <sub>100yr</sub> (in/hr)	Q <sub>100yr</sub> (cfs)	V <sub>100yr</sub> (ac-ft)
1	148,023	3.398	10.00	0.83	2.82	7.99	0.82	3.13	8.86	0.936

Proposed Drainage Data										
Drainage Area	Area (Sq. Ft.)	Area (Ac)	Tc (Min)	C-value	I <sub>50yr</sub> (in/hr)	Q <sub>50yr</sub> (cfs)	V <sub>50yr</sub> (ac-ft)	I <sub>100yr</sub> (in/hr)	Q <sub>100yr</sub> (cfs)	V <sub>100yr</sub> (ac-ft)
Basin 1	90,631	2.081	10.00	0.88	2.82	5.23	0.54	3.13	5.80	0.613
Basin 2	57,392	1.318	10.00	0.90	2.82	3.37	0.35	3.13	3.74	0.395
<b>Totals</b>	<b>148,023</b>	<b>3.398</b>				<b>8.6</b>	<b>0.9</b>		<b>9.5</b>	<b>1.008</b>

V<sub>100yr</sub> = ARC/12  
 A = 100% of contributing watershed area in acres  
 R = Rainfall in inches Taken from NOAA Atlas 14, Volume 1, Version 5 for 100-year, 24-hr event = 3.13 inches  
 C = Runoff Factor

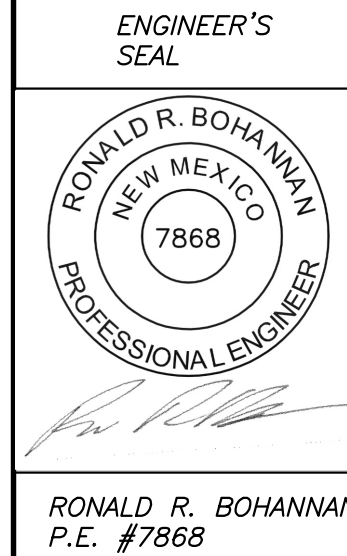
Existing Developed Weighted "C" Values							
Drainage Area	Area (Sq. Ft.)	Area (Ac)	Percent Impervious Surface	Percent Landscaping	C-Value for Impervious Surface	C-value for Landscaping/Gravel	Weighted C-Value
Total Site	148,023	3.398	68%	32%	0.98	0.50	0.83
Total		3.398					

Proposed Developed Weighted "C" Values							
Drainage Area	Area (Sq. Ft.)	Area (Ac)	Percent Impervious Surface	Percent Landscaping	C-Value for Impervious Surface	C-value for Landscaping	Weighted C-Value
Basin 1	90,631	2.081	80%	20%	0.98	0.50	0.88
Basin 2	57,392	1.318	83%	17%	0.98	0.50	0.90
Total		3.398					

**NOTE**  
 ALL PROPOSED GRADES ARE TO FLOWLINE UNLESS OTHERWISE INDICATED  
 ALL PROPOSED SLOPES ARE 3:1 MAXIMUM UNLESS OTHERWISE NOTED  
 RIPRAP: DUMPED, 18" DEPTH W/CLASS 1 NON-WOVEN GEOTEXTILE PER NMDOT SPECS, D50=9"

**CAUTION**  
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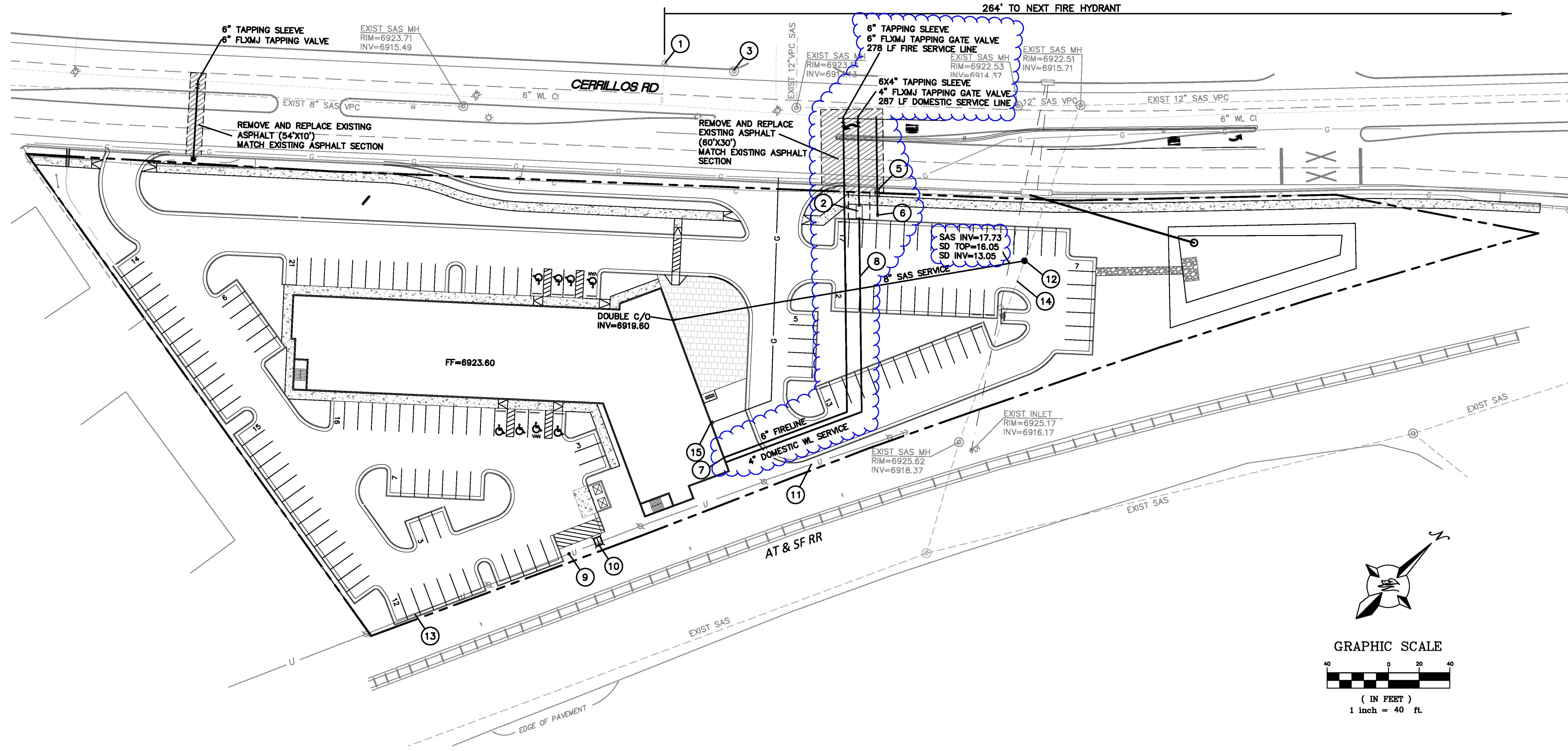


**1000, 1101, 1103 CERRILLOS RD  
SANTA FE, NM**

**GRADING AND DRAINAGE PLAN**

TIERRA WEST, LLC  
 5571 MIDWAY PARK PL NE  
 ALBUQUERQUE, NEW MEXICO 87109  
 (505) 858-3100  
 www.tierrawestllc.com

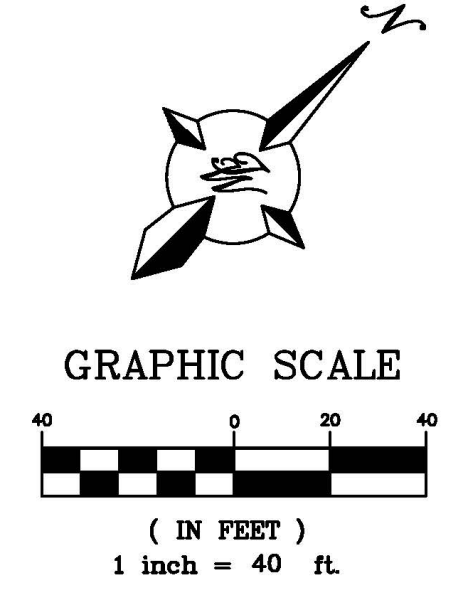
DRAWN BY pm  
 DATE 12-2-25  
 DRAWING  
 SHEET # GR-1  
 JOB # 2025080



**LEGEND**

	CURB & GUTTER
	BOUNDARY LINE
	BUILDING
	EXISTING CURB & GUTTER
	EXISTING SAS MH
	EXISTING GATE VALVE
	EXISTING WATERLINE
	EXISTING SAS
	8" SAS
	8" WL
	SANITARY SEWER LINE
	WATERLINE
	PROPOSED HYDRANT
	NEW SAS MH
	NEW WATER VALVE
	NEW SINGLE WATER METER
	SAWCUT LINE
	EXISTING GAS LINE
	NEW GAS LINE

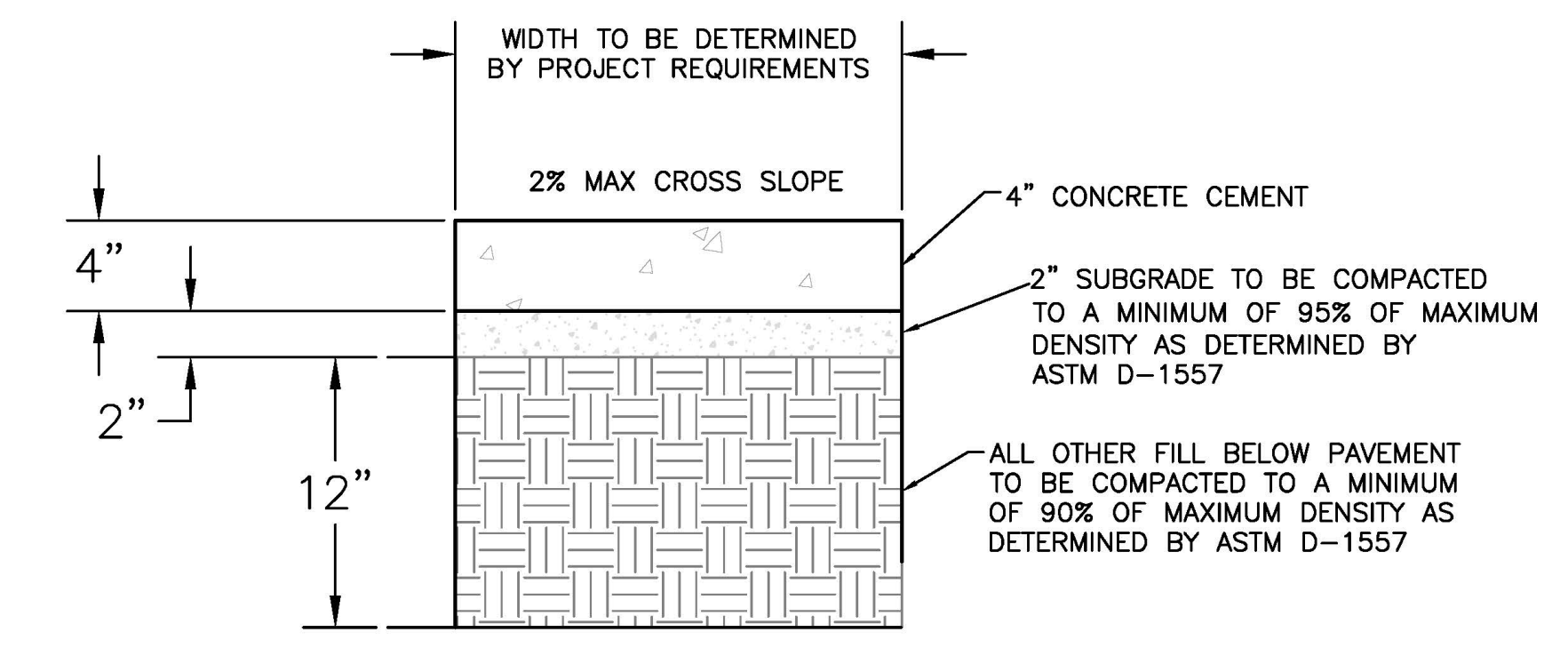
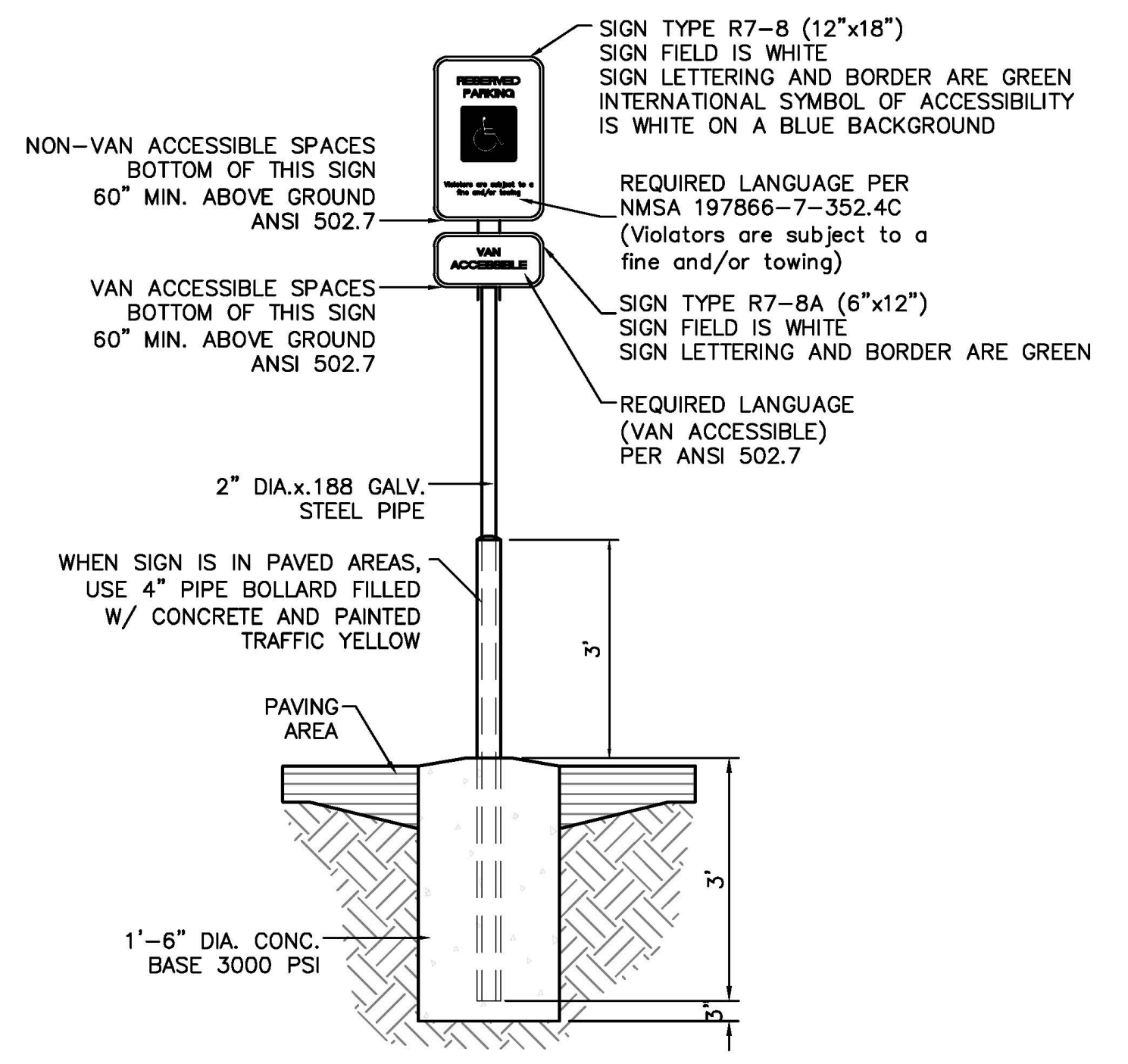
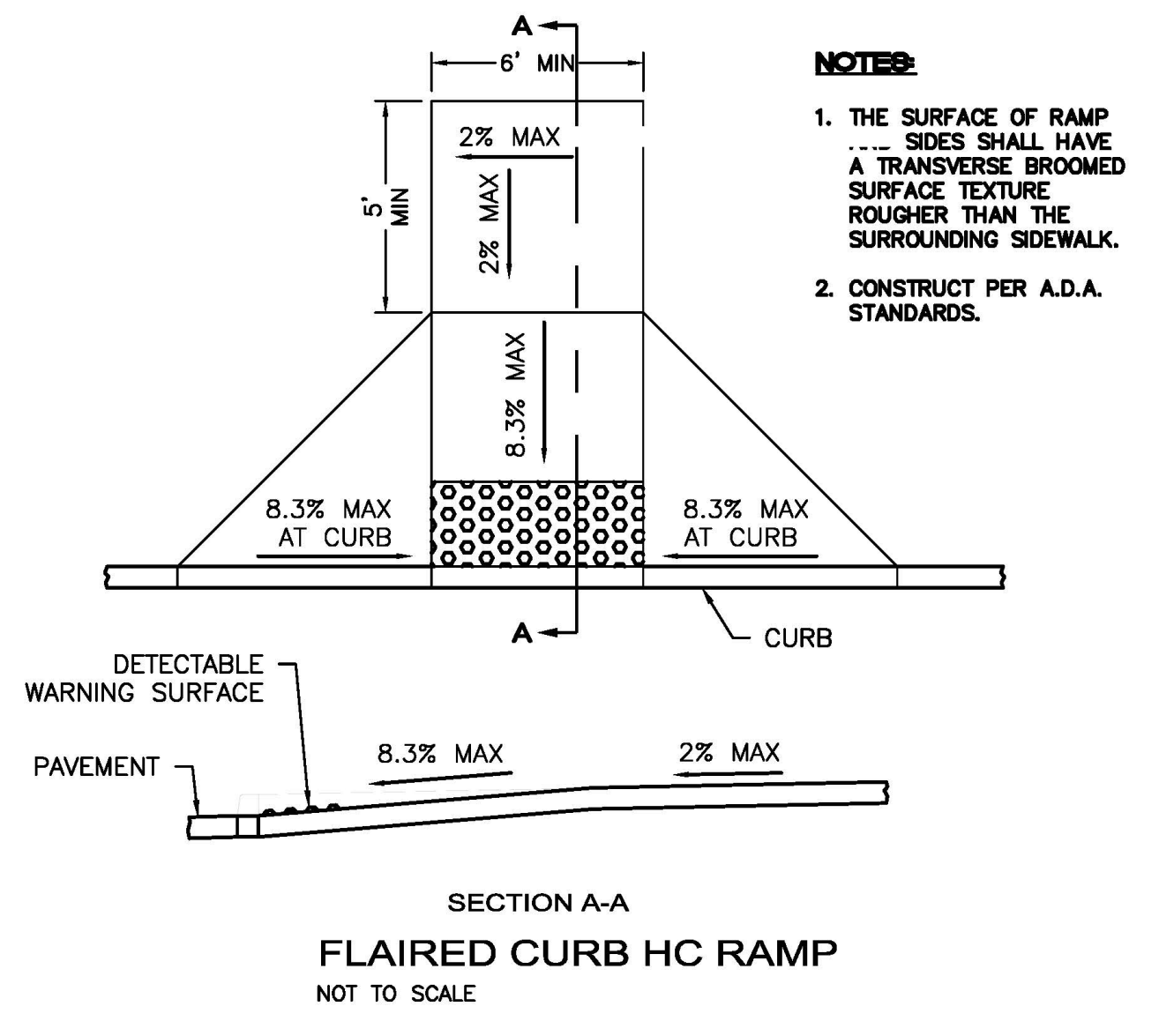
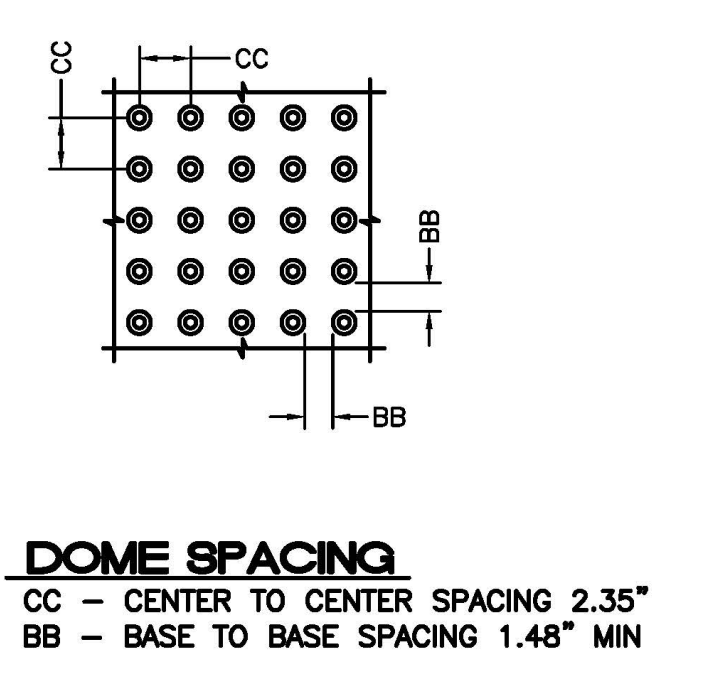
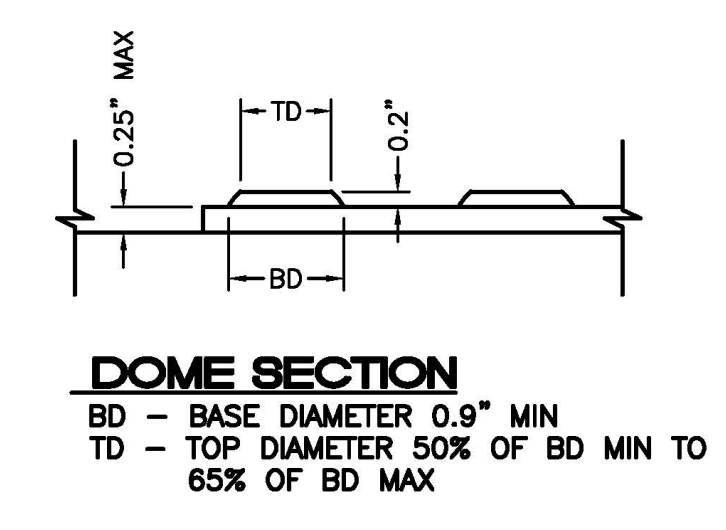
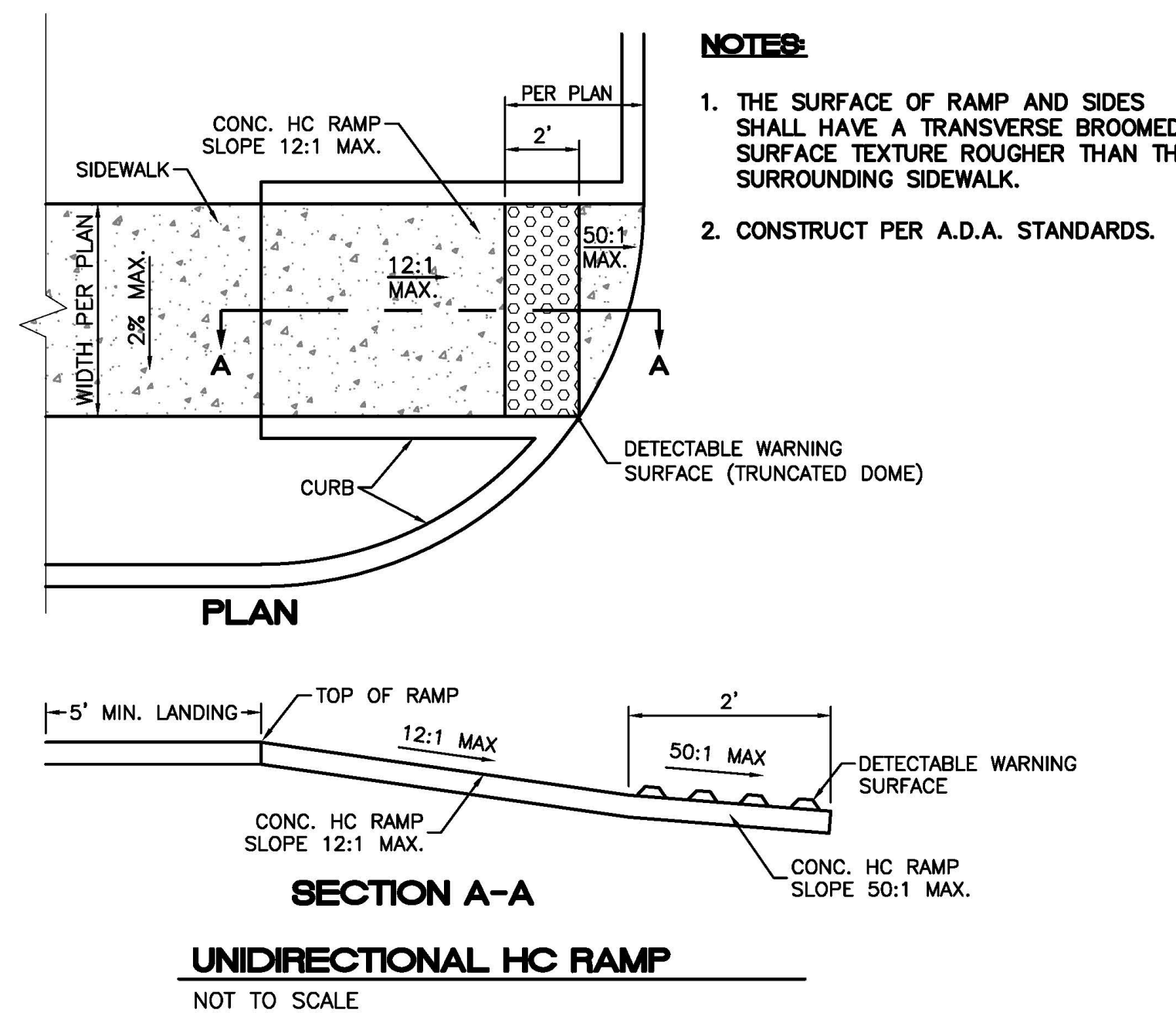
- GENERAL UTILITY NOTES:**
- ALL WATER AND SEWER UTILITY WORK TO BE DONE IN ACCORDANCE WITH CITY OF SANTA FE STANDARDS AND SPECIFICATIONS LATEST EDITION.
  - 4' MINIMUM BURY REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
  - REFERENCE PLUMBING PLANS FOR WATER LINE RISER LOCATIONS. BACKFLOW PREVENTOR LOCATED INSIDE BUILDING UNLESS OTHERWISE NOTED ON THIS PLAN
  - CLEAN OUTS ARE TO BE BUILT PER UNIFORM PLUMBING CODE STANDARDS AT LOCATIONS INDICATED AND AT MID RUN IF LONGER THAN 100'
  - ALL PLUMBING PIPE MATERIAL TO BE USED PER UPC.
  - FIRE LINE AND DOMESTIC WATERLINE MUST HAVE BACKFLOW PREVENTORS PER UPC. TO BE PROVIDED IN THE BUILDING UNLESS OTHERWISE NOTED ON THIS PLAN
  - ALL EX. SD INLETS AND MH'S SHALL HAVE CONCRETE COLLARS POURED AND BE ADJUSTED TO FINISHED GRADE.
  - ALL EXCAVATION, TRENCHING AND SHORING ACTIVITIES MUST BE CARRIED-OUT IN ACCORDANCE WITH OSHA 29 CFR 1926.650 SUBPART P.
  - ALL UTILITY DISTANCES SHOWN ARE FOR REFERENCE ONLY.
  - PNM HAS NUMEROUS ELECTRIC FACILITIES AT THIS SITE CURRENTLY SERVING EXISTING CUSTOMERS. THE APPLICANT SHALL COORDINATE WITH PNM REGARDING THESE EXISTING FACILITIES. ANY RELOCATION, CHANGES OR REALIGNMENT OF EXISTING ELECTRIC UTILITIES WILL BE THE DEVELOPER EXPENSE. IN SOME CASES, RELOCATION OR CHANGES TO EXISTING FACILITIES MAY NOT BE FEASIBLE DUE TO PHYSICAL, USE OR SAFETY CLEARANCE CONSTRAINTS.
  - PNM WILL REVIEW ALL TECHNICAL NEEDS, ISSUES AND SAFETY CLEARANCES FOR ITS ELECTRIC POWER SYSTEMS. ANY EXISTING AND PROPOSED PUBLIC UTILITY EASEMENTS SHALL BE INDICATED ON THE SITE PLAN UTILITY SHEET PNM'S STANDARD FOR PUBLIC UTILITY EASEMENTS IS 10 FEET IN WIDTH TO ENSURE ADEQUATE, SAFE CLEARANCES.
  - SCREENING SHALL BE DESIGNED TO ALLOW FOR ACCESS TO UTILITY FACILITIES. IT IS NECESSARY TO PROVIDE ADEQUATE CLEARANCE OF TEN FEET SURROUNDING ALL GROUND-MOUNTED UTILITIES FOR SAFE OPERATION, MAINTENANCE AND REPAIR PURPOSES.



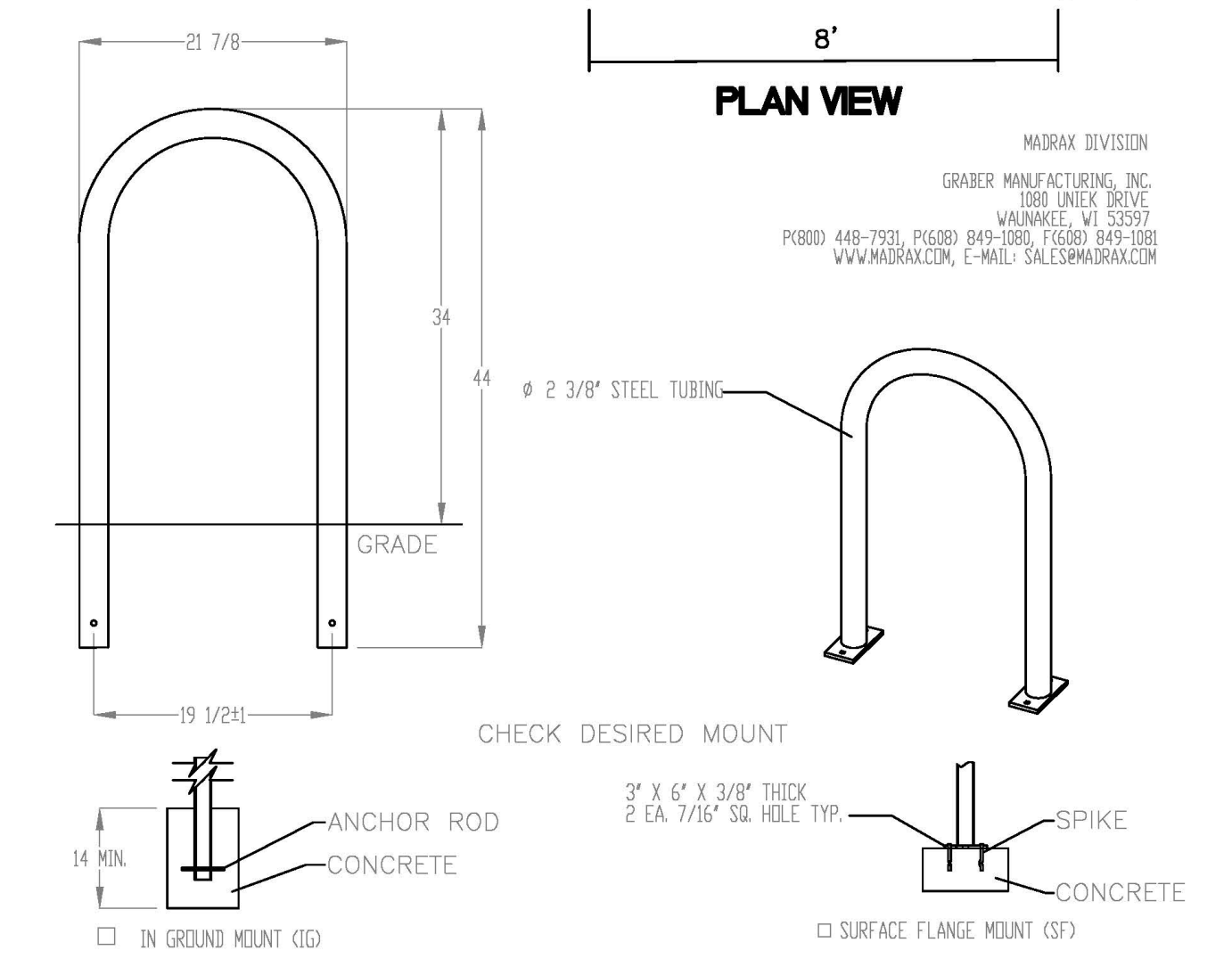
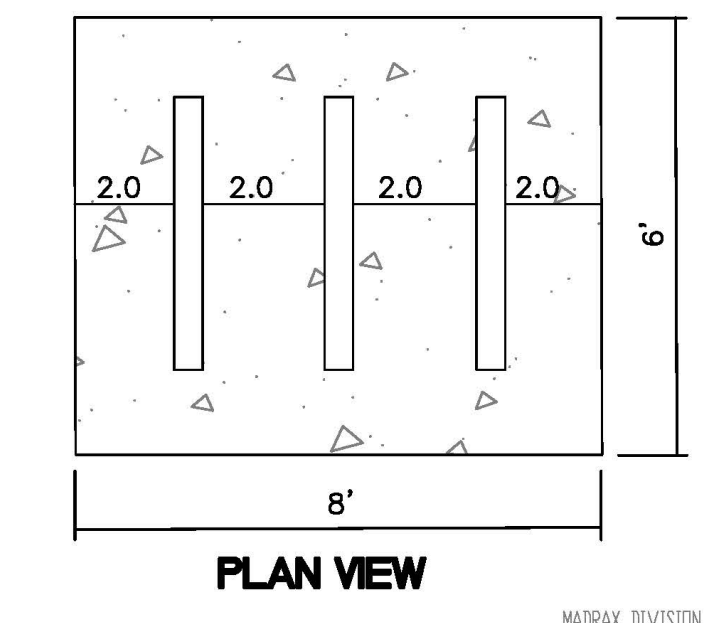
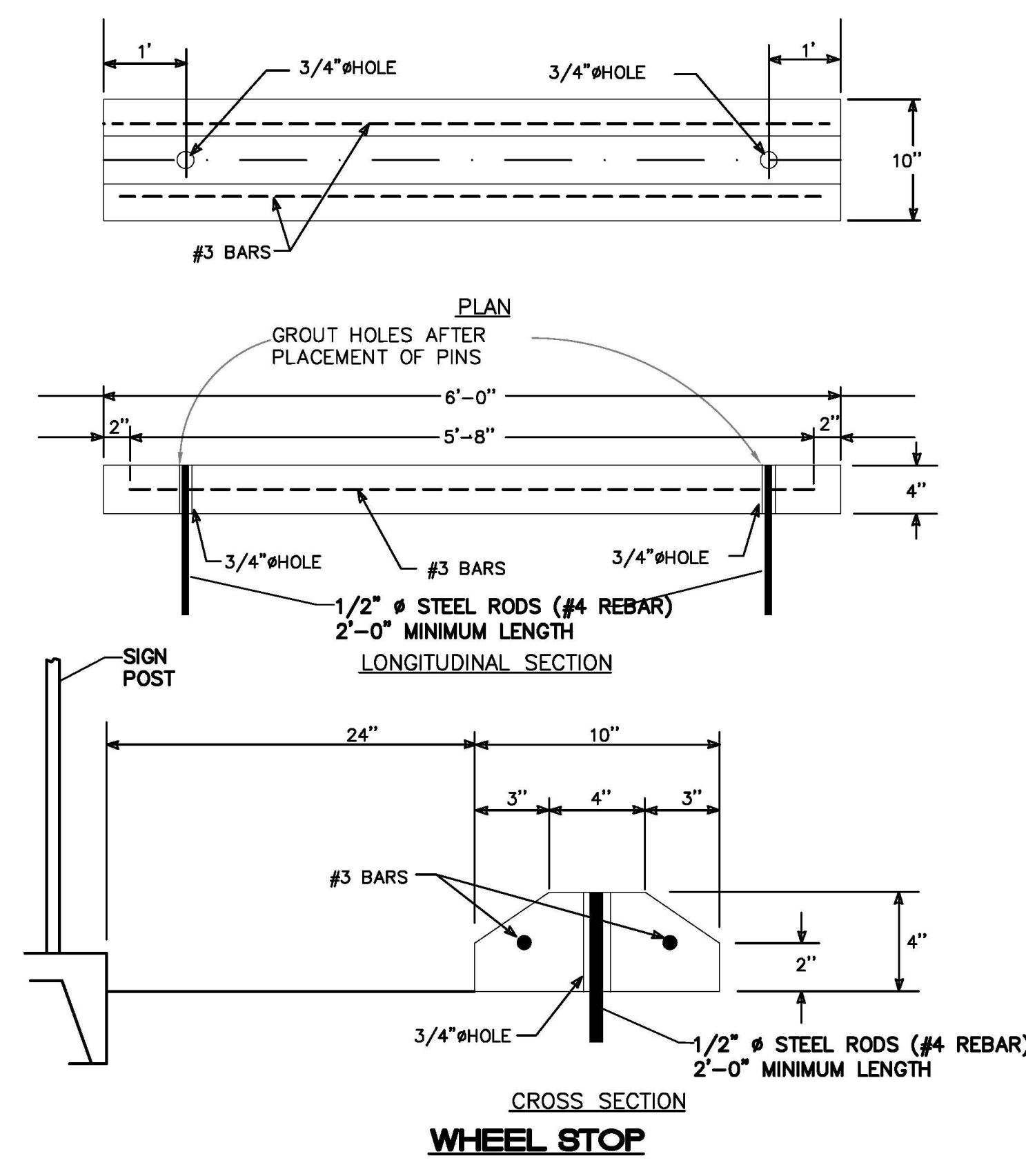
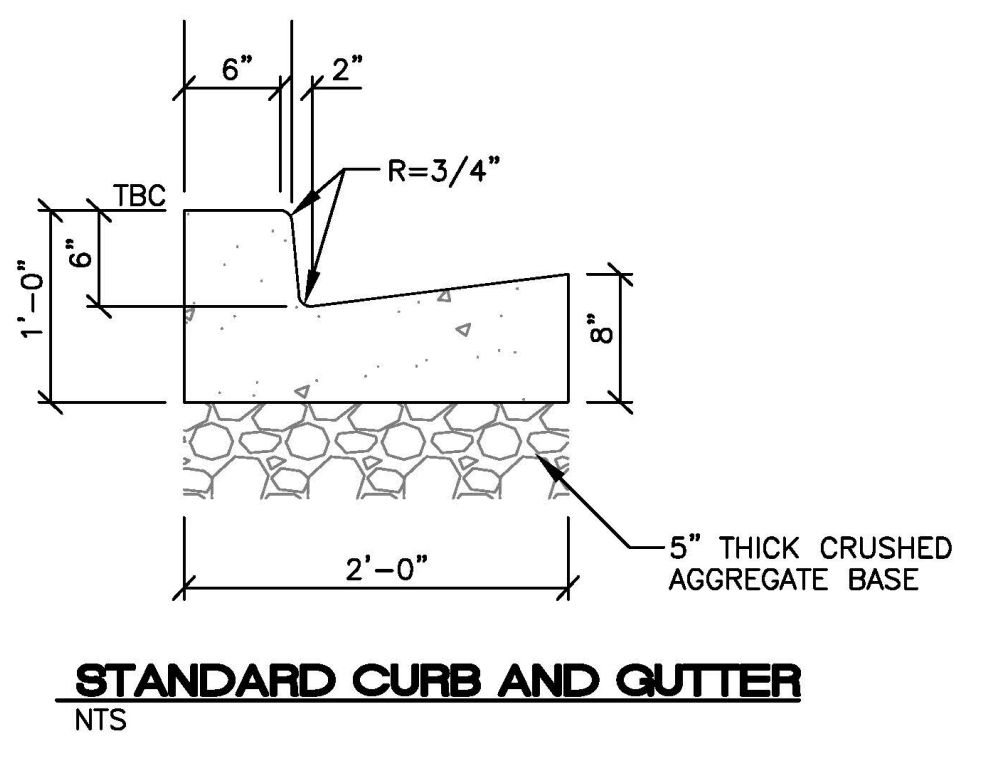
- KEYED NOTES**
- EXISTING FIRE HYDRANT (PUBLIC)
  - 4" DOMESTIC SERVICE METER AND VAULT
  - EXISTING UTILITY VAULT
  - EXISTING 10" SAS SDR26, S=1.75%
  - 1" IRRIGATION METER/SERVICE
  - BACKFLOW PREVENTER (IRRIGATION) REFERENCE LANDSCAPE IRRIGATION PLAN SHEETS LI-101 AND LI-102 FOR CONTINUATION
  - 6" BACKFLOW PREVENTER LOCATED INSIDE BUILDING
  - 4" DOMESTIC SERVICE WL
  - NEW POWER POLE W/SERVICE RISER
  - TRANSFORMER
  - REMOVE 410 LF EXISTING OVERHEAD POWER LINE AND 4 POWER POLES
  - NEW 4" SAS MANHOLE RIM=6920.72 INV=6917.90 EXIST INVERT TO BE FIELD VERIFIED
  - EXISTING OVERHEAD POWER LINE TO REMAIN
  - EXISTING 10" VPC SAS
  - GAS METER
  - PROPOSED FIRE HYDRANT, 6"x12" TEE, 6" GATE VALVE

**CAUTION**  
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	<b>1000, 1101, 1103 CERRILLOS RD          SANTA FE, NM</b>	DRAWN BY prn
	<b>MASTER UTILITY PLAN</b>	DATE 12-2-25
	<b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	DRAWING
	12-2-25 RONALD R. BOHANNAN P.E. #7868	SHEET # <b>MU-1</b>



**CONCRETE SIDEWALK SECTION**  
ALL SIDEWALKS SHALL BE PER NMDOT STANDARDS AND BE ADA COMPLIANT



PRODUCT: 1238-10CSF  
DESCRIPTION: BIKE RACK  
DATE: 10-4-18  
ENG: SMC  
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	<b>1000, 1101, 1103 CERRILLOS RD</b> <b>SANTA FE, NM</b>	DRAWN BY pm DATE 12-2-25
	<b>CONSTRUCTION DETAILS</b>	DRAWING 2025080-DET
<b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	SHEET # <b>DET-1</b>	JOB # 2025080

**F.H. NUMBERS**  
EXAMPLE:  
(CITY OF S.F.) [0808]  
(S.F. COUNTY) [2035C]

**KEYED NOTES:**  
 1. FIRE HYDRANT  
 2. 6" DUCTILE IRON POLY WRAPPED PIPE  
 3. 1/2 CUBIC YARD 3/8" GRAVEL DRAIN W/ 10 MIL. PLASTIC SHEET  
 4. 6" MJ RETAINER AND 6" HARNESS (IF REQUIRED)  
 5. 6" MJ x FL GATE VALVE & BOX  
 6. TEE: MJ RUN W/ 6" FLANGE OUTLET

**CONSTRUCTION NOTES:**  
 A- FIRE HYDRANT SHALL NOT BE INSTALLED NEAR ANY STRUCTURES AND SHALL HAVE A MINIMUM 36" CLEAR SPACE IN ALL DIRECTIONS.  
 B- LOCATE HYDRANT 18" BEHIND BACK OF CURB UNLESS OTHERWISE SHOWN ON PLANS OR SPECIFICALLY DIRECTED BY CSDPVD HYDRANT TO BE SET PLUMB AND AT PROPER ELEVATION (ELEVATION PROVIDED BY DEVELOPER) HYDRANTS INSTALLED AS PART OF A NEW DEVELOPMENT/EXTENSION SHALL BE A STANDARD 5' BURY HYDRANT AND SHALL BE SET TO FINISHED GRADE WITHOUT THE USE OF HYDRANT EXTENSIONS (IF AN ADJUSTMENT IS REQUIRED, RE-LAY THE WATER MAIN AND FIRE HYDRANT LEG OR INSTALL RESTRAINED OFFSET AS DIRECTED BY CSDPVD)  
 C- WEEP HOLE MUST HAVE 1/4" PLUG REMOVED AND BE FREE DRAINING INTO GRAVEL DRAIN. USE FULLY RESTRAINED JOINTS FROM THE FIRE HYDRANT TO THE FLANGED CONNECTION ON VALVE.  
 D- FIRE HYDRANT SHALL BE PAINTED AND REFLECTIVE NUMBERS INSTALLED AFTER INSTALLATION.

**CITY OF SANTA FE WATER DIVISION  
SANTA FE, NEW MEXICO**

**FIRE HYDRANT**

DATE: 08-2008  
SCALE: N/A  
07

**VALVE INSTALLATION**

**VALVE BOX INSTALLATION**

**NEW PAVING**

**EXISTING PAVING**

**UNPAVED AREAS**

**PROTECTION OF VALVE BOXES**

**LEGEND:**  
 - FINISH PAVING COURSE  
 - EXISTING PAVEMENT  
 - 3000 P.S.I. CONCRETE  
 - 1" ST. PAVING COURSE  
 - SUBGRADE

**LEGEND:**  
 ← THRUST DIRECTION  
 L R PIPE LENGTH REQUIRING RESTRAINTS  
 RESTRAINED JOINT  
 UNRESTRAINED JOINT

**CITY OF SANTA FE WATER DIVISION  
SANTA FE, NEW MEXICO**

**VALVE & VALVE BOX INSTALLATION**

DATE: 08-2008  
SCALE: N/A  
08

**VERTICAL OFFSETS**

**HORIZONTAL BENDS**

**TEE**

**REDUCER**

**LEGEND:**  
 ← THRUST DIRECTION  
 L R PIPE LENGTH REQUIRING RESTRAINTS  
 RESTRAINED JOINT  
 UNRESTRAINED JOINT

**CITY OF SANTA FE WATER DIVISION  
SANTA FE, NEW MEXICO**

**JOINT RESTRAINT DETAIL**

DATE: 08-2008  
SCALE: N/A  
10B

**USE MECHANICALLY RESTRAINED FITTINGS & PIPE JOINTS FOR THRUST RESTRAINT UNLESS CONCRETE BLOCKING IS SPECIFICALLY CALLED FOR BY SDWC.**

**THRUST BLOCK SIZING TABLE**

PIPE DIAMETER	TYPE "A" BENDS									TYPE "B" FITTINGS					
	11-14"			22-12"			45°			90°			A	B	C
4"	12	12	12	12	12	12	12	12	12	12	12	12	12	12	12
6"	12	12	12	12	12	12	12	18	12	18	12	24	12	24	12
8"	12	12	12	12	18	12	18	24	12	24	30	18	18	30	12
10"	12	12	12	12	24	12	24	30	12	24	42	18	24	36	18
12"	12	18	12	18	24	12	24	36	18	32	48	24	24	42	18
14"	12	24	12	18	36	18	30	42	24	36	60	24	30	48	24
16"	18	24	24	24	36	24	30	40	24	42	66	24	36	54	24
18"	18	30	24	24	40	24	36	54	24	48	72	24	42	60	24
20"	20	30	24	30	42	24	42	60	24	54	80	24	42	76	24
24"	24	36	24	36	54	24	48	72	24	66	96	30	54	80	30

**CITY OF SANTA FE WATER DIVISION  
SANTA FE, NEW MEXICO**

**CONCRETE THRUST BLOCKING**

DATE: 08-2008  
SCALE: N/A  
13

**JOINT RESTRAINT TABLE PVC**

**THRUST RESTRAINT TABLE - PVC**

**TABLE:** This table is based on the ESDU Non-Matching Joint Calculations program with the following input criteria: 3.5 ft depth of bury (conservative), trench type 4, PVC material, 150 psi pipe, 1.50 ft safety factor.

**Minimum Restraint Length:** A minimum restrained length of 5 ft from the fitting joint is recommended. Vertical offsets shall be completely restrained between the top vertical fitting and the bottom vertical fitting.

**FJO:** Fitting Joint Only - This includes at least a 1' length of restrained pipe beyond the fitting joint.

**Vertical Offset:** Use 11-1/2" bends or 22-1/2" bends wherever possible due to the shorter restrained length requirements.

**Cap/Fittings:** Concrete blocking may be required by CSDPVD on a case-by-case basis in addition to mechanical restraints.

**Pipe Lengths:** Piping shall be laid out to minimize pipe joint near fittings. Wherever possible, full 20 ft pipe lengths shall be utilized when connecting to fittings or valves. No pipe bell joints shall be used where fittings are less than 20 ft apart.

DIAMETER (IN.)	VERTICAL BENDS			HORIZONTAL BENDS			CAPS / PLUGS / VALVES			TEES		
	Run	Branch	Run	Run	Branch	Run	Run	Branch	Run	Branch	Run	Branch
11-1/4"	12	12	12	12	12	12	12	12	12	12	12	12
12"	12	12	12	12	12	12	12	12	12	12	12	12
14"	12	12	12	12	12	12	12	12	12	12	12	12
16"	12	12	12	12	12	12	12	12	12	12	12	12
18"	12	12	12	12	12	12	12	12	12	12	12	12
20"	12	12	12	12	12	12	12	12	12	12	12	12
24"	12	12	12	12	12	12	12	12	12	12	12	12

**CITY OF SANTA FE WATER DIVISION  
SANTA FE, NEW MEXICO**

DATE: 08-2008  
SCALE: N/A  
10C

**TYPICAL FIRE SERVICE**

**ITEM:**  
 1. TEE OR TAPPING SLEEVE WITH FLG. OUTLET (4" MIN.)  
 2. GATE VALVE FLG. x MJ (4" MIN.)  
 3. RESTRAINED DIP-PW (4" MIN.)  
 4. REDUCED PRESSURE ZONE BACKFLOW PREVENTER W/ DETECTOR ASSEMBLY  
 5. CUSTOMER PIPING (REDUCER/TAPPED CAP FOR PIPING LESS THAN 4")  
 6. HEATED ENCLOSURE

**CITY OF SANTA FE WATER DIVISION  
SANTA FE, NEW MEXICO**

DATE: 08-2008  
SCALE: N/A  
32

**ENGINEER'S SEAL**

**RONALD R. BOHANNAN**  
NEW MEXICO  
PROFESSIONAL ENGINEER  
7868

**1000, 1101, 1103 CERRILLOS RD  
SANTA FE, NM**

**CONSTRUCTION DETAILS**

**TIERRA WEST, LLC**  
5571 MIDWAY PARK PL NE  
ALBUQUERQUE, NEW MEXICO 87109  
(505) 858-3100  
www.tierrawestllc.com

**DRAWN BY**  
pm

**DATE**  
12-2-25

**DRAWING**  
2025080-DET

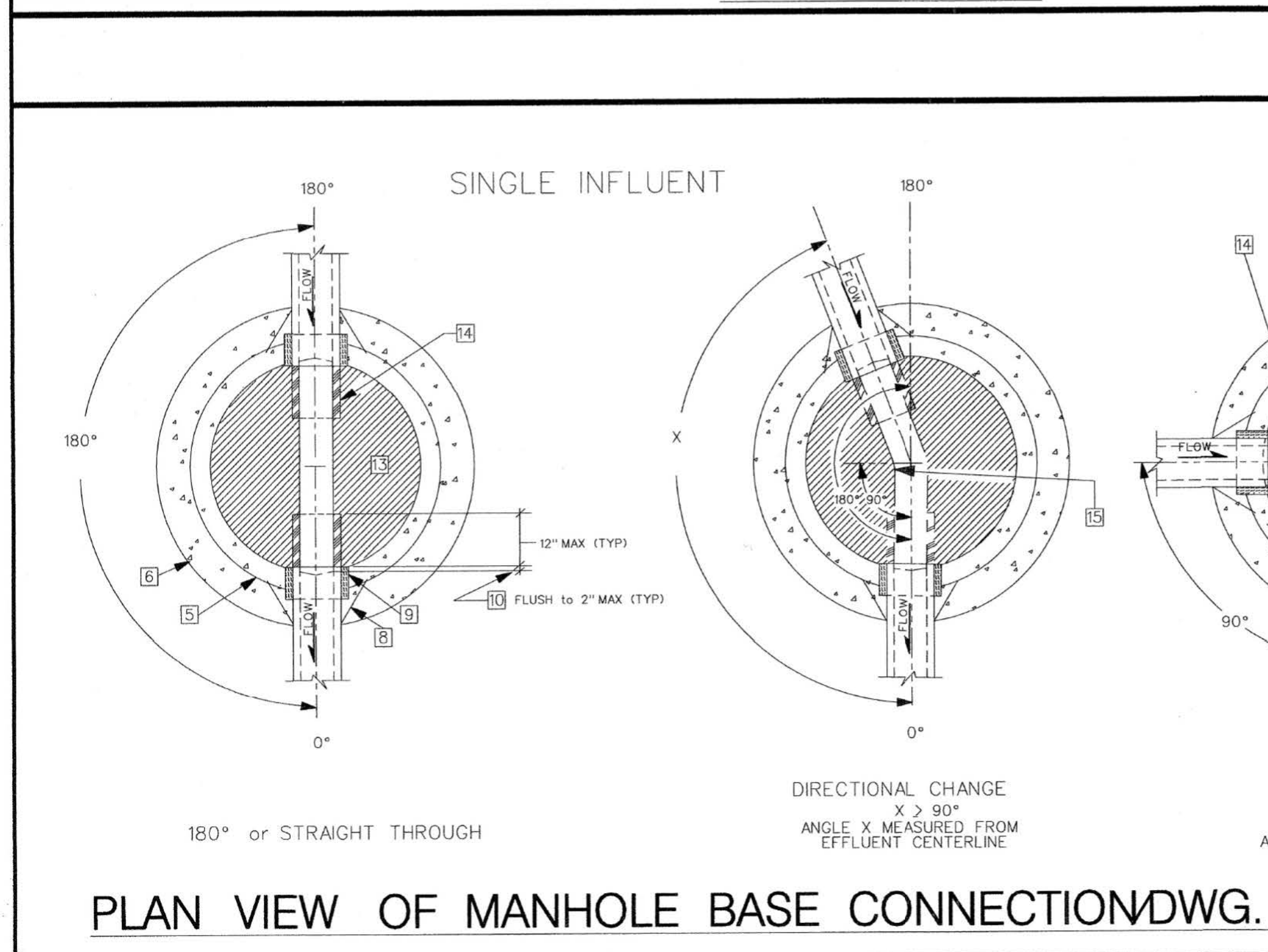
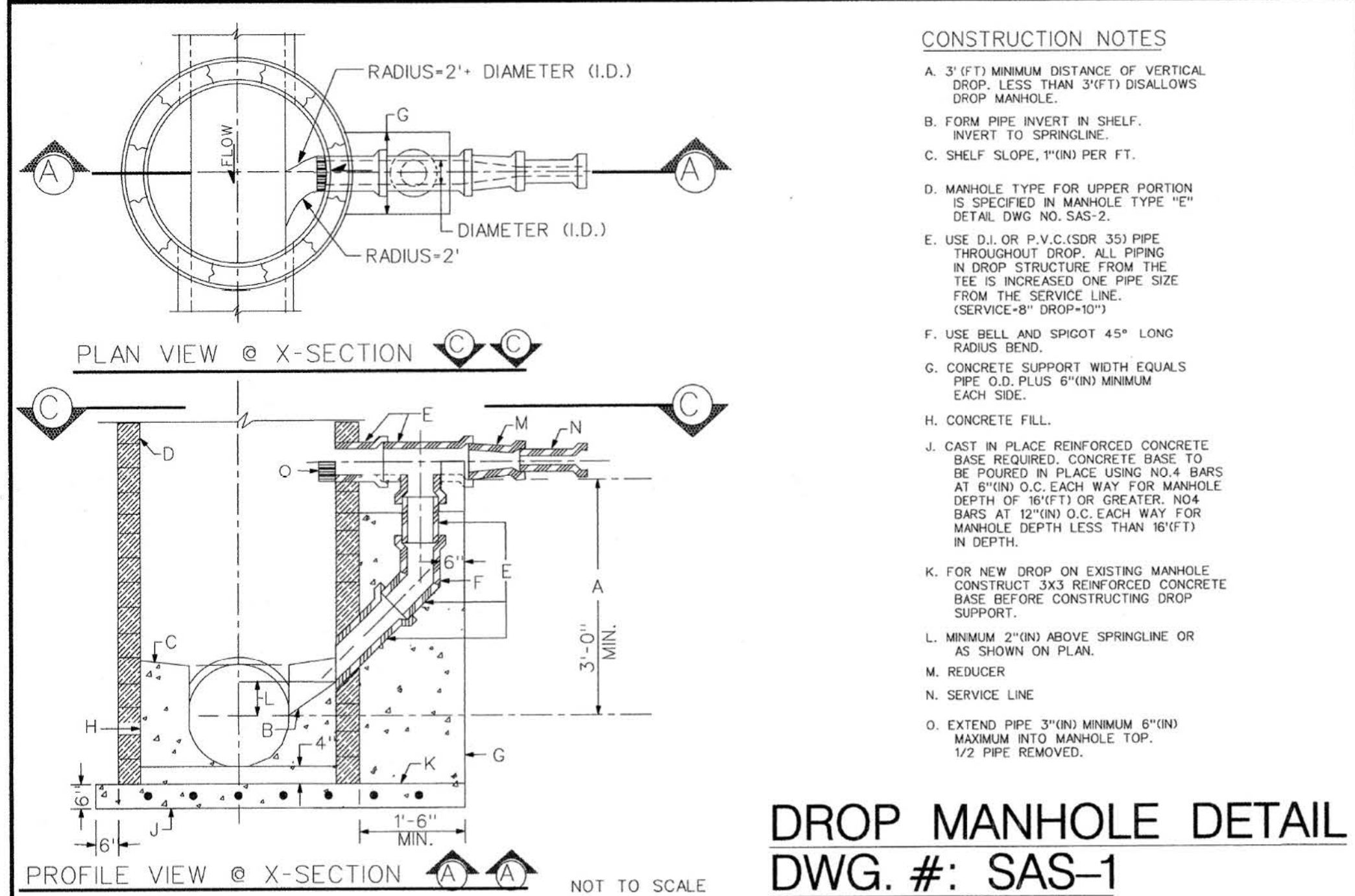
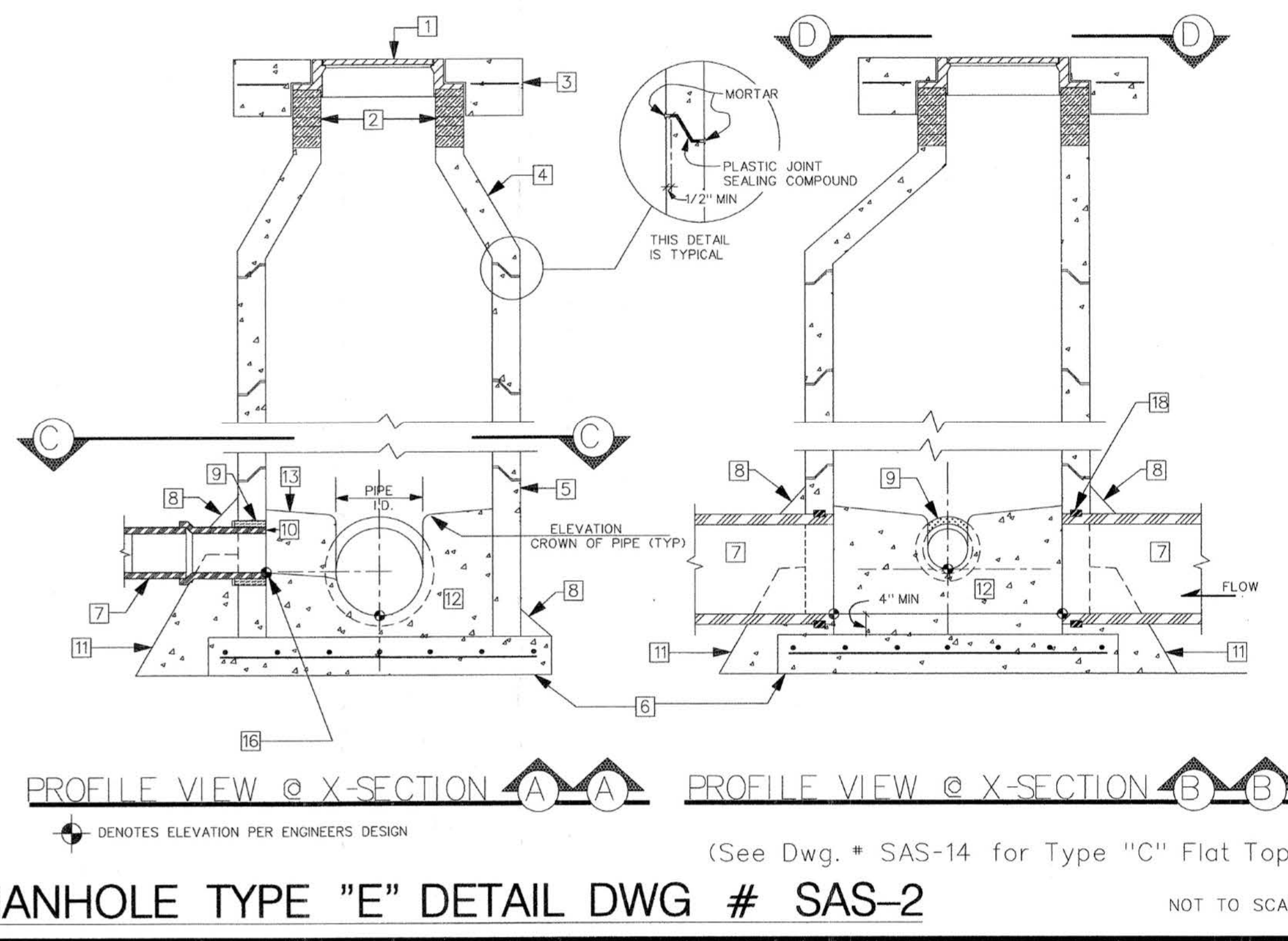
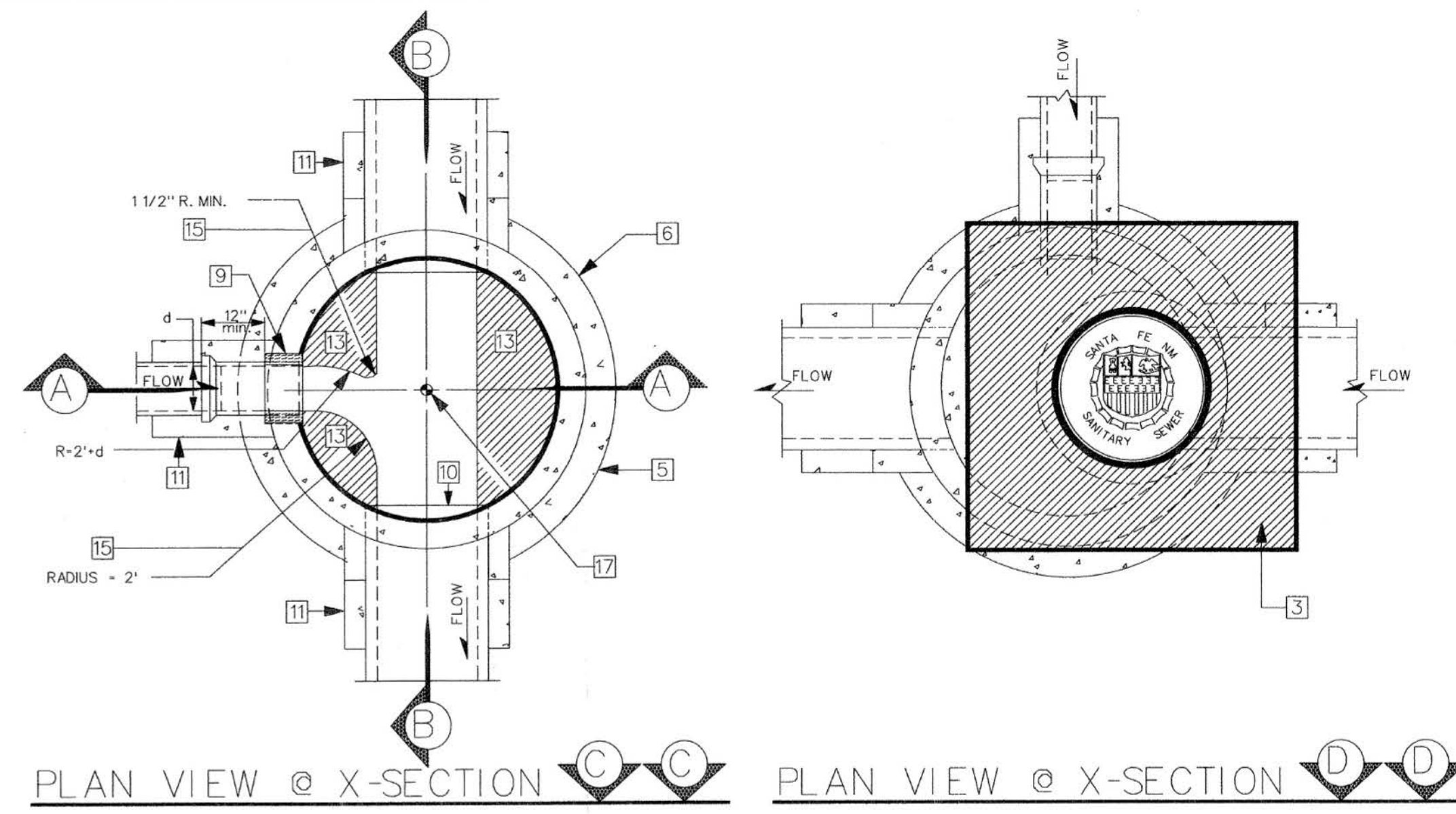
**SHEET #**  
DET-2

**JOB #**  
2025080

DATE: 12-2-25  
SCALE: N/A  
RONALD R. BOHANNAN  
P.E. #7868

# LEGEND

ITEM	DESCRIPTION
1	MANHOLE FRAME & COVER, refer to manhole frame and cover detail Dwg. No. SAS-4
2	CONCRETE ADJUSTMENT RINGS or CONCRETE BRICK, refer to concrete adjustment detail Dwg. No. SAS-5
3	CONCRETE COLLAR, refer to concrete collar detail Dwg. No. SAS-6
4	PRECAST REINFORCED CONCRETE RISER, CONE or FLAT TOP, with 5"(in) wall thickness, refer to general note CM-2
5	PRECAST REINFORCED CONCRETE BASE RISER, with suitable sized openings, refer to general note CM-2A
6	CONCRETE BASE, refer to concrete base detail Dwg. No. SAS-7
7	SEWER PIPE, refer to general note CM-1
8	6"(in) GROUT FILLET, on upper half of pipe and around base
9	ADAPTER, MANHOLE, refer to manhole adapter detail Dwg. No. SAS-8
10	PIPE PENETRATION INTO MANHOLE, refer to manhole adapter detail Dwg. No. SAS-8
11	PIPE SUPPORT, CONCRETE, shall extend out-side of manhole a maximum of 18"(in) to bell of first joint and shall cradle pipe half pipe
12	CONCRETE FILL, 3000 p.s.i., refer to general note CR-6
13	SHELF, to be 9"(in) minimum width with 1"(in) per 1'-0" slope, from crown of pipe
14	CUT UPPER HALF OF PIPE, after manhole has been completed and inspected by engineer
15	HAND FORMED CHANNELS, shall be on a uniform radius and shall not hold water
16	INVERT ELEVATIONS OF LATERAL LINES, shall be the same as the springline elevation of the sewer main, where possible
17	CHANGE SLOPE OF PIPE, at center of manhole
18	APPROVED WATER STOP, to be with type of pipe



# GENERAL NOTES

- CONSTRUCTION REQUIREMENTS**
- CR-1 MATERIALS AND WORK: CURRENT NEW MEXICO STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (referred to as NM APWA) WITH MODIFICATIONS NOTED BY THE CITY OF SANTA FE.
  - CR-2 APPROVED PLANS: USE PLANS BEARING THE OFFICIAL SEAL OF THE DESIGN ENGINEER AND BEARING THE APPROVAL SIGNATURE OF THE CITY WATER QUALITY DIVISION OR APPROVED REPRESENTATIVE. CONSTRUCTION PERFORMED WITHOUT APPROVED PLANS WILL BE REJECTED.
  - CR-3 SEWER HOOK-UP PERMIT: OBTAIN PERMITS FOR THE PROJECT BEFORE COMMENCING ANY SEWER CONSTRUCTION. CONSTRUCTION PERFORMED WITHOUT OBTAINING PERMITS SHALL BE REJECTED. A CONSTRUCTION PLAN SHALL INDICATE THE CLASS OF BEDDING TO BE USED. CHANGE OF BEDDING MAY REQUIRE A CHANGE IN PIPE CLASSIFICATION OR WALL THICKNESS.
  - CR-4 SUBSTITUTIONS OR CHANGES: ALL SUBSTITUTIONS OR CHANGES MUST BE APPROVED BY THE CITY WATER QUALITY DIVISION AND MATERIAL SPECIFICATIONS REPRESENTATIVE PRIOR TO CONSTRUCTION. ALL SUBSTITUTIONS OR CHANGES MUST BE SUBMITTED BY THE DESIGN ENGINEER TO THE CITY WATER QUALITY DIVISION OR APPROVED REPRESENTATIVE. WHERE APPROPRIATE, SUBMITTALS MUST INCLUDE FABRICATION DRAWINGS, WORKING DRAWINGS AND MATERIAL SPECIFICATIONS OR TEST DATA TO JUSTIFY SUBSTITUTIONS OR CHANGES. DESIGN ENGINEER SHALL AUTHORIZE ANY DRAWINGS FOR SUBSTITUTIONS AND CHANGES AND SUBMIT THEM TO THE CITY WATER QUALITY DIVISION FOR APPROVAL. UNAUTHORIZED SUBMITTALS WILL BE REJECTED.
  - CR-5 MANUFACTURER'S CERTIFICATES: WHEN CERTIFICATES OF COMPLIANCE AND TEST REPORTS ARE REQUIRED FOR MATERIALS, DOCUMENTS SHALL BE SUBMITTED TO THE CITY WATER QUALITY DIVISION OR APPROVED REPRESENTATIVE AT THE TIME OF MATERIALS DELIVERY TO THE JOBSITE.
  - CR-6 CONTRACTOR REQUIREMENTS: CONTRACTOR PERFORMING WORK ON PUBLIC SEWER LINES SHALL BE A LICENSED UTILITY CONTRACTOR.
- INSTALLATION**
- I-1 LAYING PIPE: AS PER SECTION 900, NM APWA; PIPE SHALL BE PLACED AND BEDDED IN A FROST FREE TRENCH. GASKET SHALL BE FULLY SEATED AND NOT SLIPPED. PIPE SHALL BE LAD THROUGH MANHOLE LOCATIONS ON STRAIGHT AND UP TO 22 1/2 DEGREE DEFLECTIONS.
    - A. IF PIPE TRENCH INSTALLATION CONFIGURATION EXCEEDS THE LIMITS OF NM APWA STANDARDS, SECTION 700, OR AS DEFINED ON THE CONSTRUCTION PLANS, THE DESIGN ENGINEER WILL SPECIFY THE NEW PIPE CLASSIFICATION OR WALL THICKNESS.
    - B. TYPE I TRENCH CONFIGURATION IS NORMALLY USED WHEN TRENCH DEPTHS ARE 8'(FT.) OR LESS. TYPE II TRENCH CONFIGURATION IS NORMALLY USED WHEN TRENCH DEPTHS ARE 8'(FT.) AND OVER, DEPENDING ON SOIL CONDITIONS. REFER TO NM APWA STANDARDS SECTION 700.
  - I-2 MANHOLE CONSTRUCTION:
    - A. BASE:
      1. CAST IN PLACE; ON UNDISTURBED FROST FREE SUBGRADE
      2. PRECAST UNIT; ON PEA GRAVEL WITH COMPLETE EVEN BEARING
    - B. PRECAST BARREL:
      1. JOINTS: FILL COMPLETELY WITH NON-SHRINK GROUT AND TROWEL
      2. MANHOLE ADAPTOR: INSTALL OVER PVC PIPE AND FILL IN PENETRATION WITH NON-SHRINK GROUT.
      3. CAST IN PLACE BASES: SHALL ACHIEVE A MINIMUM OF 2500 PSI COMPRESSIVE STRENGTH BEFORE SETTING PRECAST BARREL SECTIONS.
  - I-3 EXCAVATION AND BACKFILL: AS PER SECTION 700, NM APWA; SATURATION BY FLOODING OR JETTING METHODS IS NOT PERMITTED WITHOUT A SOILS ENGINEERING REPORT RECOMMENDING THESE METHODS. MECHANICAL OR VIBRATORY COMPACTION SHALL NOT BE USED ON THE BEDDING AND 12"(in) OF INITIAL BACKFILL. COMPACTION SHALL BE DETERMINED PER AASHTO T-180.

- CONSTRUCTION MATERIALS**
- CM-1 SEWER PIPE (CERTIFICATES REQUIRED)
    - A. VITRIFIED CLAY: REFER TO SECTION 125, NM APWA FOR EXTRA STRENGTH VCP.
    - B. PLASTIC (PVC): REFER TO SECTION 121, NM APWA, AS MODIFIED BY THE CITY:
      1. 4" THRU 10" (60) DIAMETER, ASTM D-3034 OR ASTM F-789 PIPE, MINIMUM PS-46 STRENGTH, SDR-35 OR EQUAL.
      2. LARGER THAN 10" (IN) DIAMETER: ASTM F 679 VOL. 08.04.
    - C. HDPE PIPE PER ASTM D-1248 CLASS B WHEN APPROVED BY WATER QUALITY DIVISION ENGINEER.
    - D. PVC RESTRAINED JOINTS: SERIES 1350 OR SERIES 1390 FOR COUPLINGS PRODUCED BY UNI-FLANGE CORPORATION. COUPLINGS WITH NYLON SPLINE, MARKED AS "YELLOWLINE" AND PRODUCED BY CERTANTEED CORPORATION, OR APPROVED EQUAL.
    - E. MANHOLE ADAPTERS: ASBESTOS CEMENT (AC) MANHOLE ADAPTERS, OR AC/PVC ADAPTER COUPLINGS.
    - F. BUILDING SERVICE STUBS: CAST IRON DWV, PVC SCH. 40 DWV.
    - G. SEWER CONNECTIONS:
      1. VCP PIPE: FACTORY TEE FITTINGS: SECTION 125 NM APWA
      2. PVC: CAST IRON BODIES TAPPING SADDLE WITH STAINLESS STEEL TENSION STRAP AND FITTINGS; FOWLER "QUICK-ON" (GENCO, HERSEY "PIONEER", OR APPROVED EQUAL.
    - H. SOIL CLASSIFICATION: THE UNIFIED SOIL CLASSIFICATION SYSTEM PER ASTM D 2487 TABLE 701.3.5 NM APWA.
  - CM-2 MANHOLES:
    - A. CONCRETE MANHOLES: PRECAST REINFORCED CONCRETE RISERS, REDUCING CONES, AND ADJUSTMENT RINGS PER ASTM C 478 VOL. 04.05. BASES MAY BE FIELD PLACED CONCRETE OR PRECAST CONCRETE PER ASTM C 478 VOL. 04.05 (CERTIFICATES REQUIRED). CRACKED OR VISIBLY DEFECTIVE UNITS WILL BE REJECTED.
    - B. PIPE PENETRATIONS: PRECAST UNITS SHALL HAVE SUITABLE SIZED OPENINGS CAST INTO BARREL AT PROPER ANGLES FOR PIPE AND MANHOLE ADAPTERS.
    - C. MANHOLE STEPS: REFER TO SECTION 920.47 NM APWA. POLYPROPYLENE ENCASED GRADE 60 STEEL BY M.A. INC. OR APPROVED EQUAL: 14"(in) WIDE, 16"(in) MAXIMUM SPACING.
    - D. FRAMES AND COVERS:
      1. CASTING: SHALL CONFORM TO SECTION 160, 161 & 162, NM APWA CLASS 306. (CERTIFICATES AND SHOP DRAWINGS REQUIRED)
      2. MINIMUM COVER WEIGHT: 165 POUNDS
      3. MINIMUM COMBINED WEIGHT: 365 POUNDS +/- 5%
      4. BEARING SURFACES: SHALL BE MATCHED FOR A FIRM NON ROCKING SEAT BETWEEN FRAME AND COVER. MINIMUM SEATING WIDTH: 7/8"(in.)
      5. COATING: NONE.
      6. COVERS LETTERINGS: SANTA FE, N.M. SANITARY SEWER
      7. CASTINGS: CAST MANUFACTURER AND MODEL NUMBER ON FRAME AND COVER.
      8. CASTINGS TOLERANCE: +/- 1/16"(in.) PER FOOT OF OVERALL DIMENSION. OUT OF ROUND CASTINGS AND LOOSE FITTING UNITS WILL BE REJECTED IN THE FIELD.
  - CM-3 CONCRETE ENCASEMENT:
    - A. REQUIREMENTS:
      1. WHEN THE PIPE COVER IS 36" (in) OR LESS.
      2. WHEN VITRIFIED CLAY CROSSES AN ARROYO.
      3. WHEN A WATER LINE PASSES BELOW OR LESS THAN 18" (in) ABOVE THE EXISTING SEWER LINE.
      4. WHEN A PARALLEL WATER LINE IS LESS THAN 10'(ft.) HORIZONTALLY AND LESS THAN 2'(ft.) ABOVE THE SEWER LINE.
      5. THE SEWER LINE SHALL BE ENCASED IN CONCRETE 6"(in.) THICK AS DETAILED, AND EXTEND AT LEAST 10'(ft.) ON EACH SIDE OF THE WATER LINE.
- FIELD QUALITY CONTROL**
- FOC-1 TESTING AND INSPECTION:
    - A. SUPERVISION: CONDUCTED BY DESIGN ENGINEER.
    - B. CERTIFICATION: DESIGN ENGINEER SHALL CERTIFY THAT THE PROJECT HAS BEEN COMPLETED IN ACCORDANCE TO PLANS & SPECIFICATIONS AND SHALL SUBMIT A CERTIFICATION OF COMPLIANCE STATEMENT WITH STAMP AND SIGNATURE.
    - C. EQUIPMENT AND ASSISTANCE: PROVIDED BY CONTRACTOR.
  - FOC-2 LINE AND GRADE: ALLOWABLE TOLERANCE BETWEEN STRUCTURES FROM DESIGN:
    - A. LINE: 0.20 FOOT
    - B. GRADE: 0.02 FOOT; PIPE SHALL NOT HOLD BACK ANY WATER.
  - FOC-3 LEAKAGE TEST: AIR TEST REQUIRED; REFER TO SECTION 901.7 NM APWA.
  - FOC-4 TELEVISION INSPECTION: CONTRACTOR SHALL PROVIDE A CERTIFIED CCTV SEWERLINE INSPECTION AND RECORD TAPES AT HIS OWN EXPENSE.
  - FOC-5 ALL CONNECTIONS TO EXISTING MANHOLES INCLUDES REHABILITATING THE TIE IN MANHOLE TO MEET THESE STANDARD CONSTRUCTION DETAILS.

CONTRACT NO. DACA47-03-D-0012 TASK ORDER NO.3

CITY OF SANTA FE  
WATER QUALITY DIVISION

TITLE: SANITARY SEWER  
STANDARD CONSTRUCTION DETAILS - A

DATE: JULY 1992

DRAWN BY: G. CHAVEZ

CADD REVISION BY: G. CHAVEZ

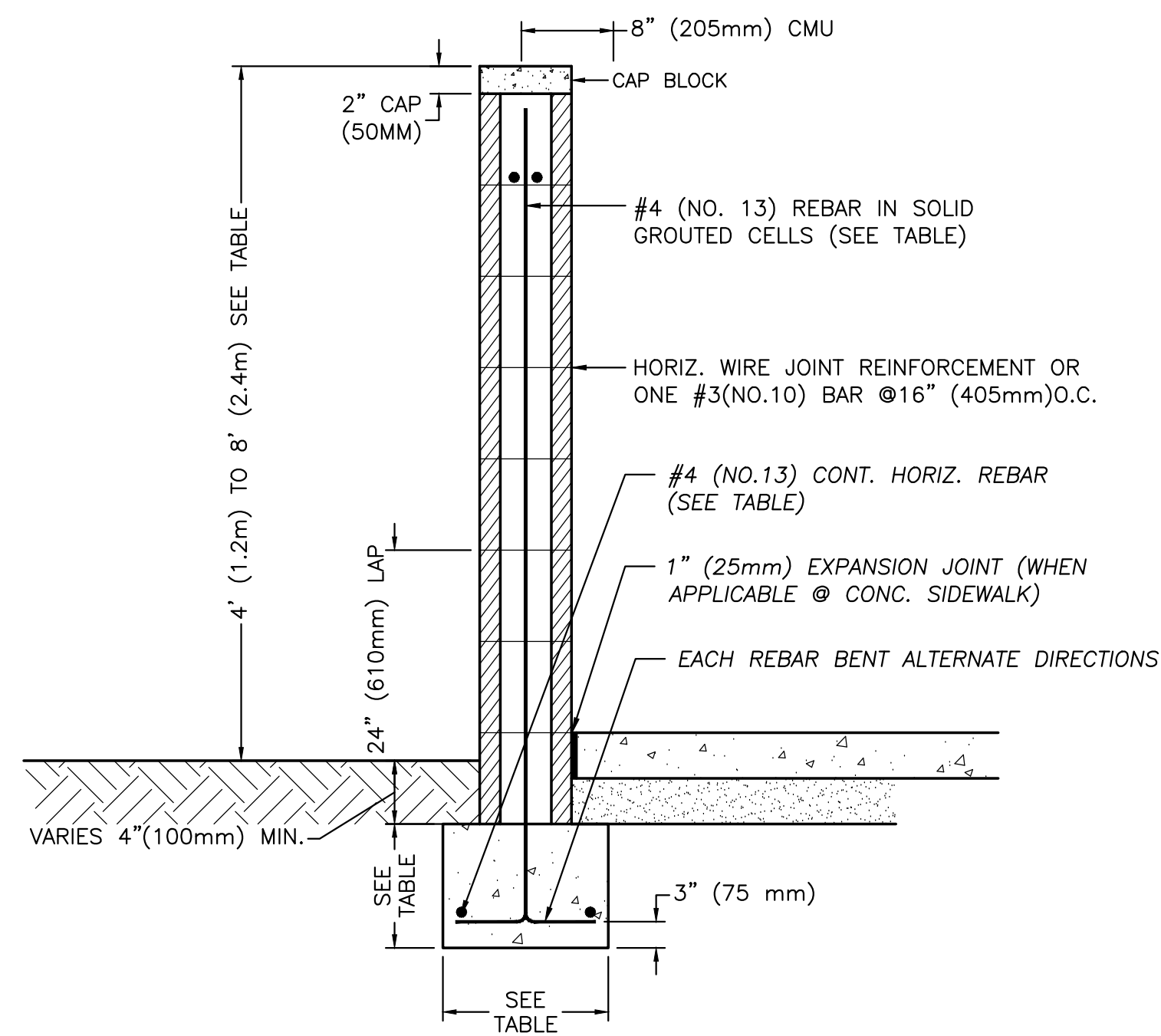
APPROVED BY: E. BROWN

REVISIONS

8-3-92	
12-14-92	
11-16-94	

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SHEET 3 OF 12

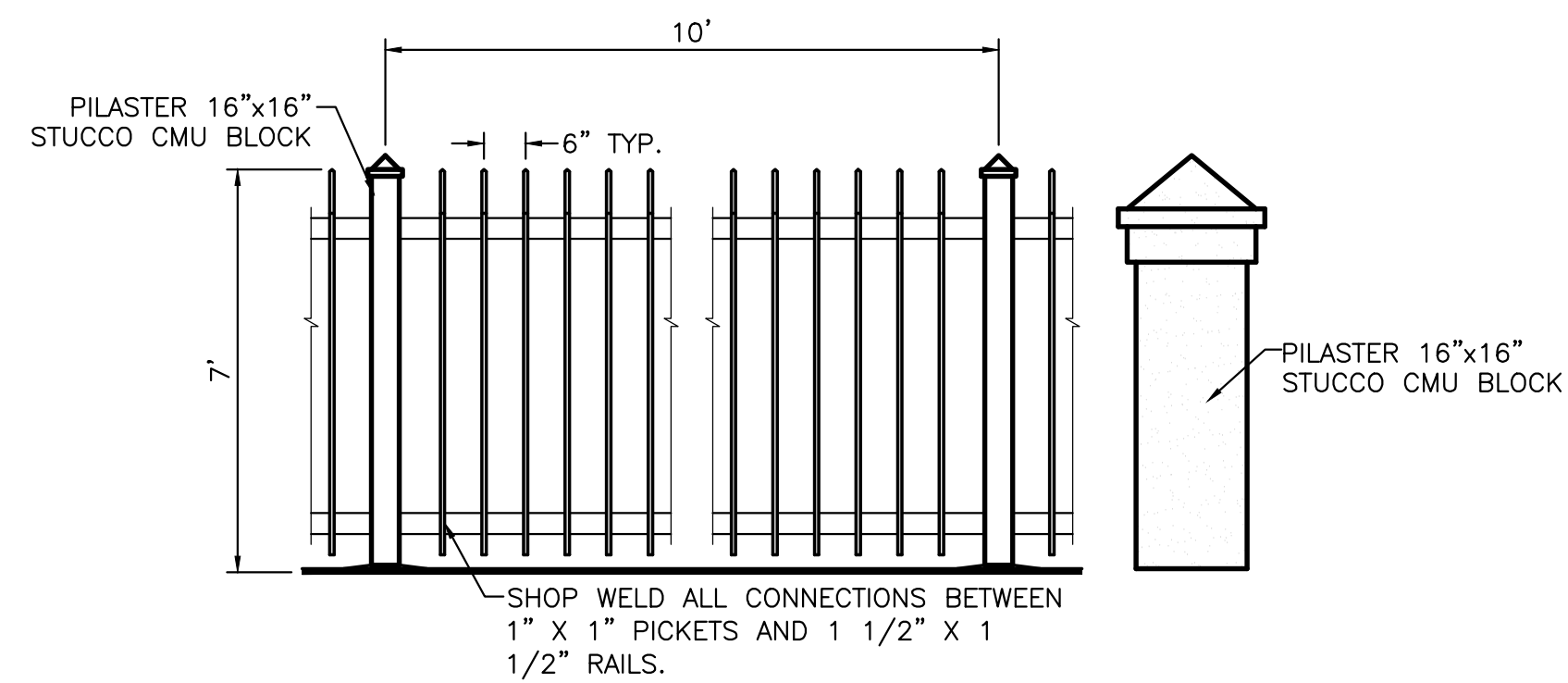


**SCREEN WALL SECTION**  
NTS

**NOTES:**

- FOOTING TO BE UNDISTURBED SOIL OR IN COMPACTED SOIL @ 95%
- CONCRETE TO BE 2500 PSI (17.2 MPa) (MIN.)
- REBAR TO BE GRADED 40
- 8" (205 mm) CMU TO BE fm= 1350 PSI (9.3 MPa)
- COLOR AND TEXTURE OF BLOCK TO BE AS SPECIFIED ON PLANS.
- DESIGN WIND PRESSURE IS 15psf (718 pa) AND SOIL PRESSURE 1000 psi (6.9 MPa) MORTAR/GROUT, TYPE S Fm=1800 psi (12.4 MPa)

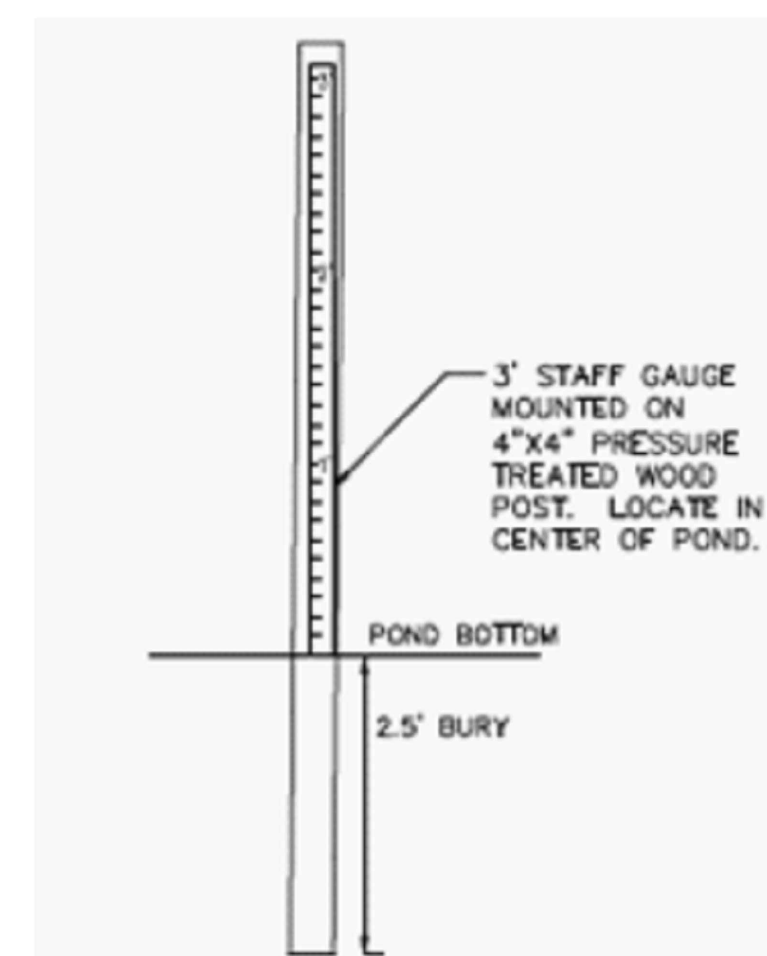
WALL HEIGHT (MAX.)	VERT. BAR SPACING	FOOTING WIDTH	FOOTING DEPTH	FOOTING HORIZ. BAR
4'-0" (1.2m)	48" (1.2m)	14" (355mm)	8" (205mm)	2 #4(NO. 13)
4'-8" (1.4m)	48" (1.2m)	16" (405mm)	10" (255mm)	2 #4(NO. 13)
5'-4" (1.6m)	48" (1.2m)	18" (460mm)	12" (305mm)	2 #4(NO. 13)
6'-0" (1.8m)	48" (1.2m)	20" (510mm)	12" (305mm)	3 #4(NO. 13)
6'-8" (2.0m)	32" (810mm)	22" (550mm)	12" (305mm)	3 #4(NO. 13)
7'-4" (2.2m)	32" (810mm)	24" (610mm)	12" (305mm)	3 #4(NO. 13)
8'-0" (2.4m)	32" (810mm)	26" (660mm)	15" (380mm)	4 #4(NO. 13)



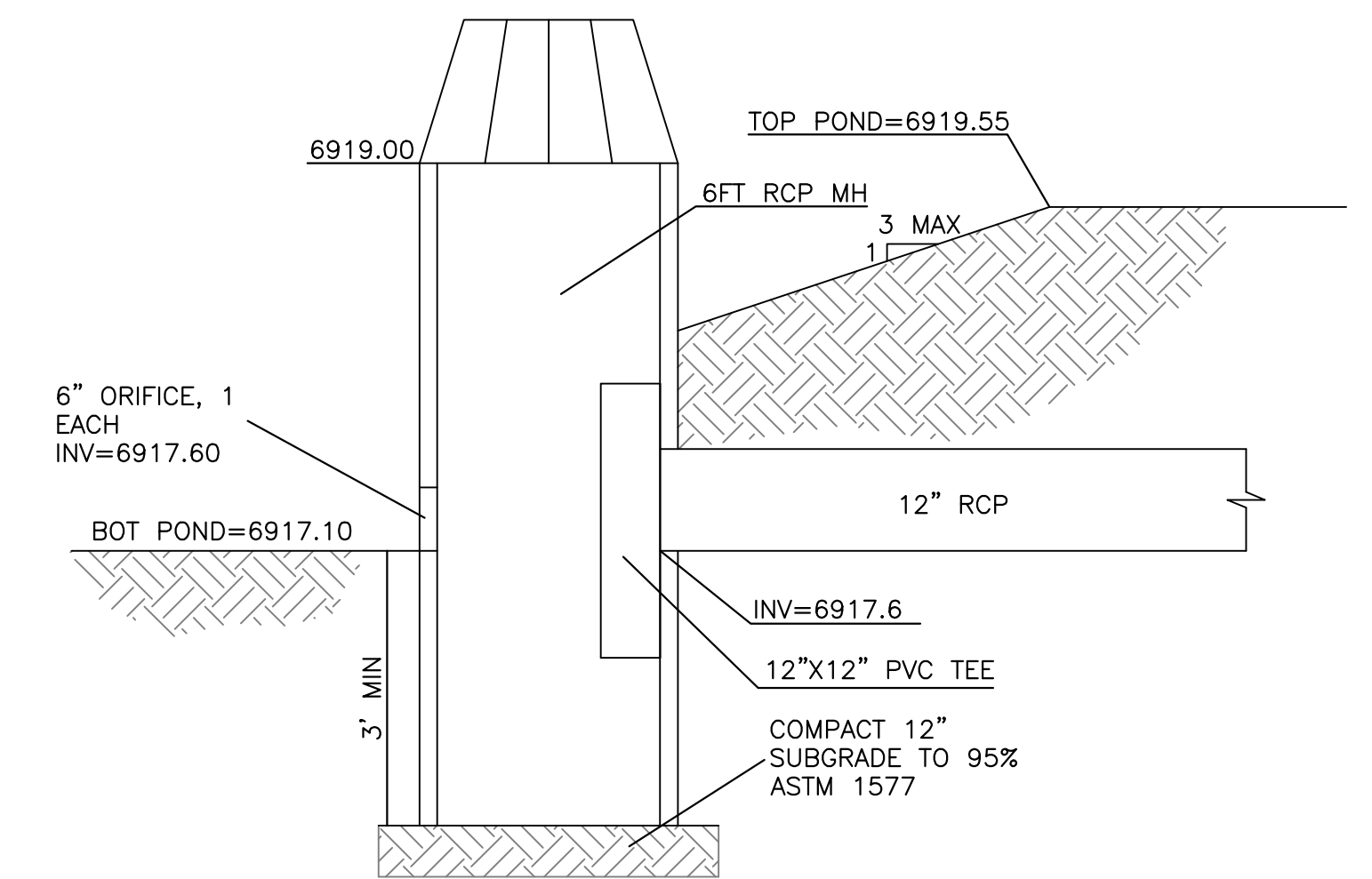
**WROUGHT IRON FENCE DETAIL**  
NTS

**GUNNISON'S PRAIRIE DOG NOTE**

A. THE PROJECT SHALL COMPLY WITH THE PROVISIONS OF THE GUNNISON'S PRAIRIE DOG ORDINANCE (ARTICLE 14-8.12).

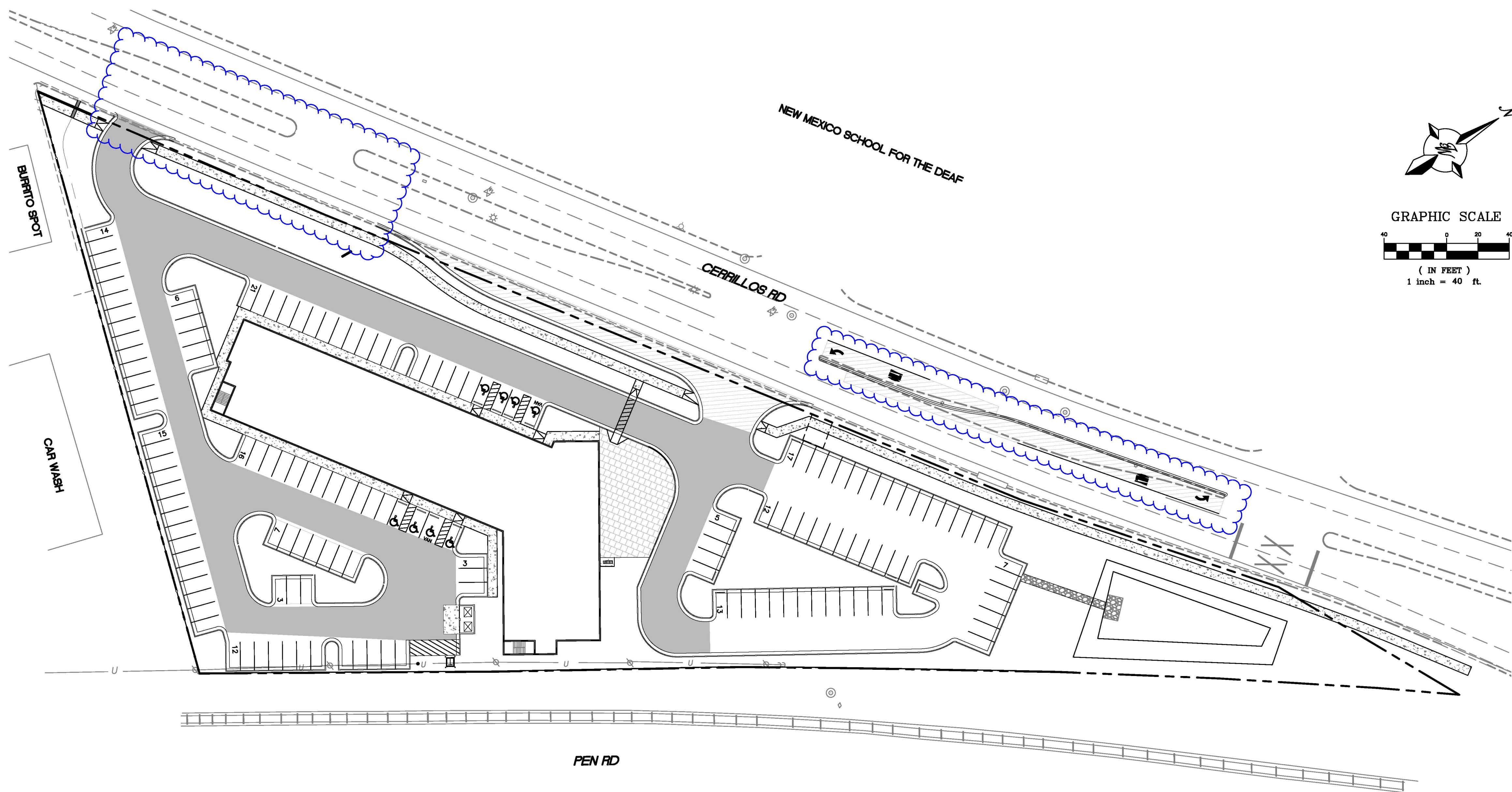


**POST & STAFF GAUGE**  
NTS



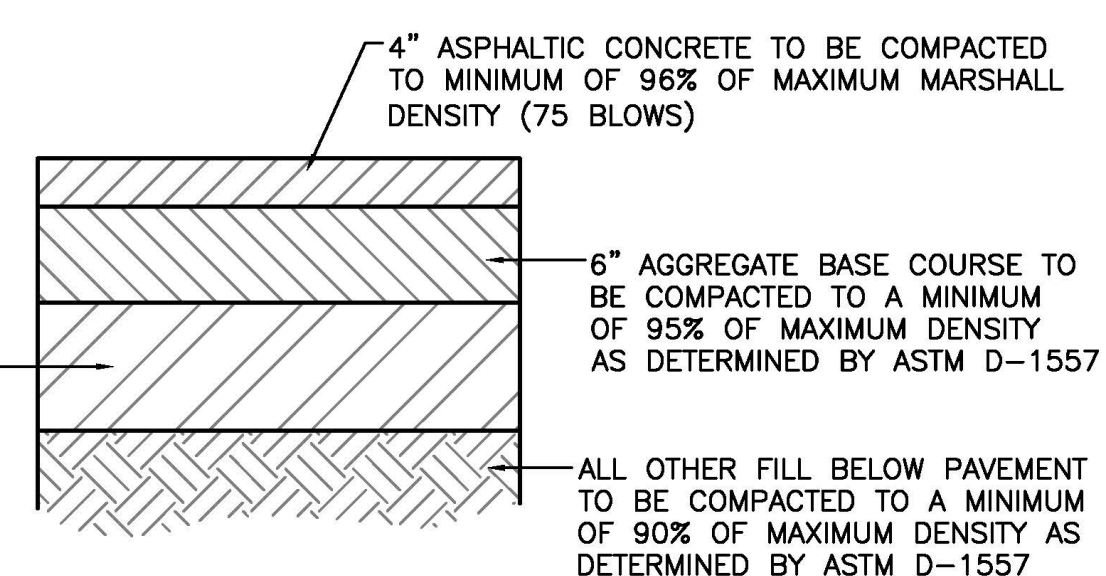
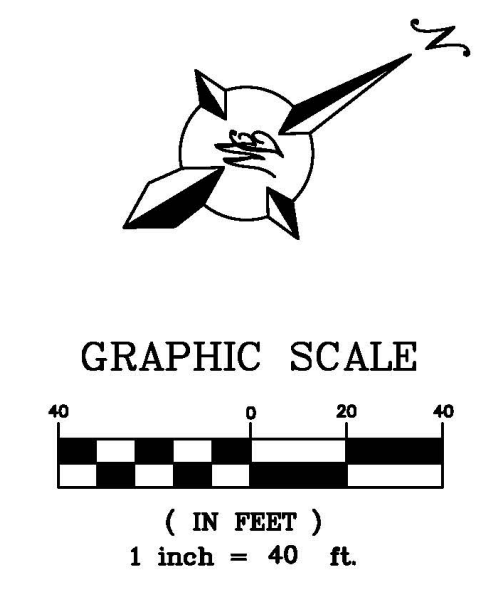
**RISER DETAIL**

	<b>1000, 1101, 1103 CERRILLOS RD</b> <b>SANTA FE, NM</b>	DRAWN BY pm
	<b>CONSTRUCTION DETAILS</b>	DATE 12-2-25
	5571 MIDWAY PARK PL. NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	DRAWING 2025080-DET
		SHEET # <b>DET-4</b>
RONALD R. BOHANNAN P.E. #7868		JOB # 2025080

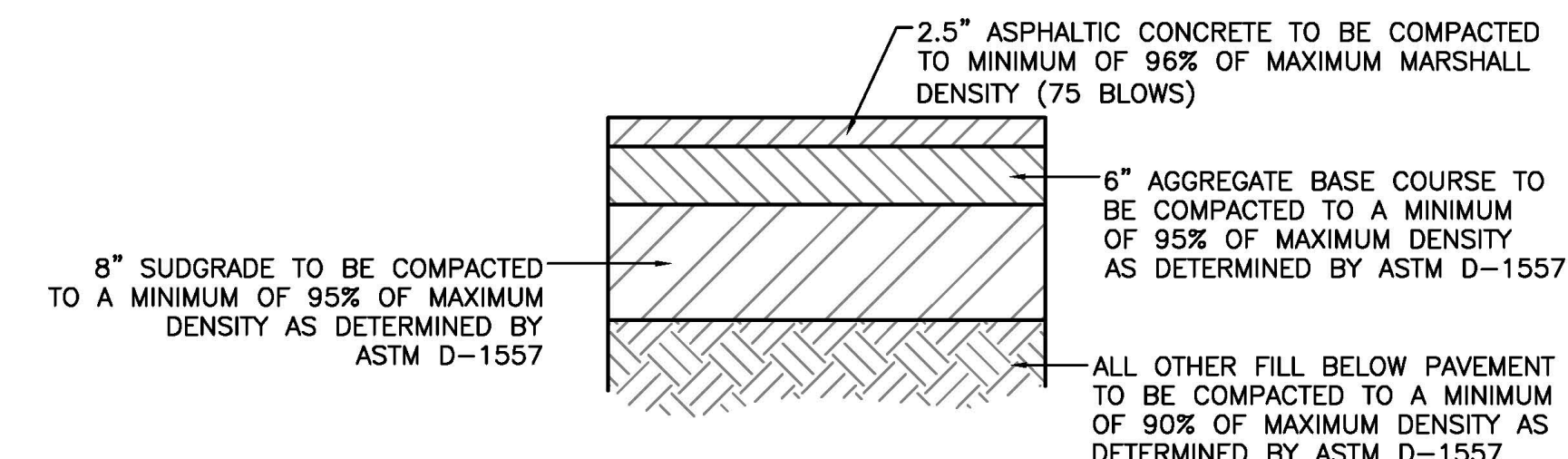


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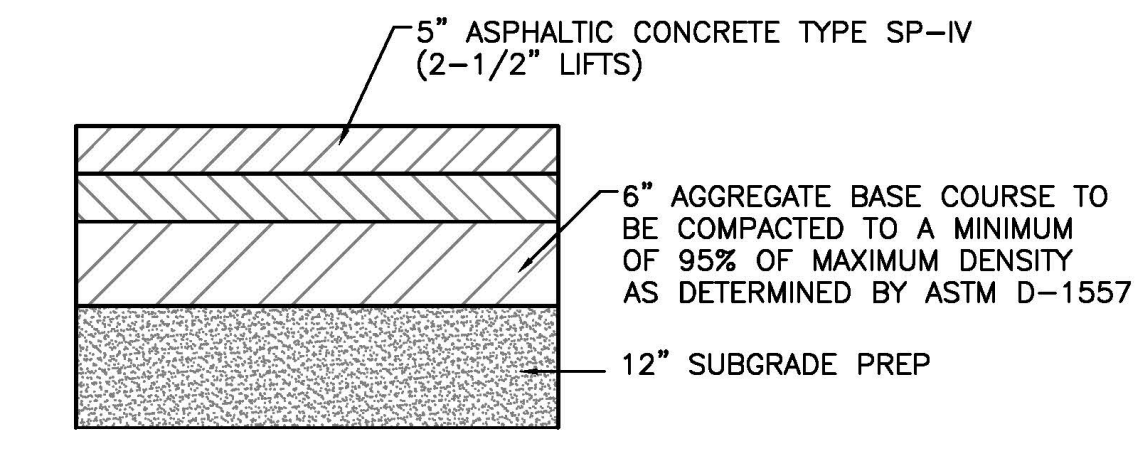
	CURB & GUTTER
	BOUNDARY LINE
	BUILDING
	EXISTING CURB & GUTTER
	CONCRETE SIDEWALK
	LIGHT DUTY PAVING
	HEAVY DUTY PAVING
	RURAL PAVING



**AUTOMOBILE DRIVEWAYS AND AREAS SUBJECT TO SEMI-TRUCKS TYPICAL PAVING SECTION**  
1" = 1'



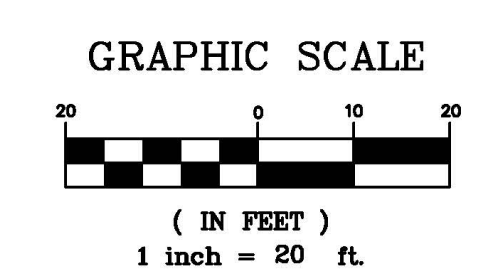
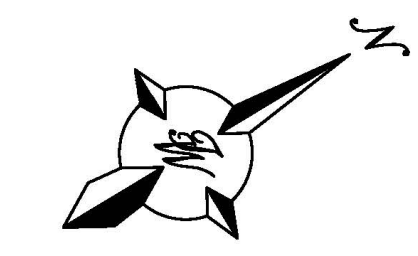
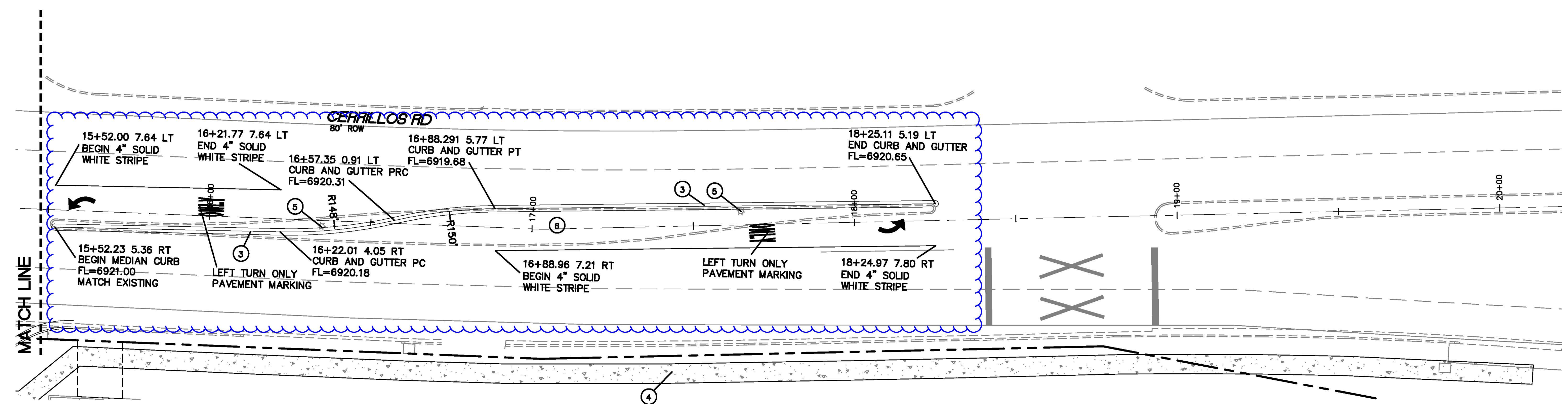
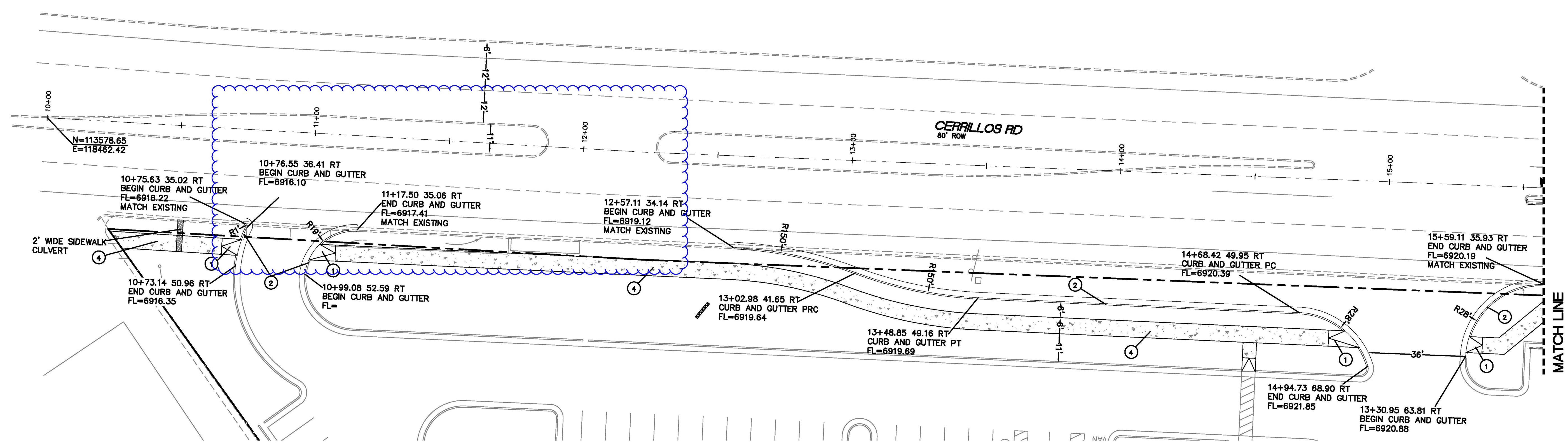
**AUTOMOBILE PARKING TYPICAL PAVING SECTION**  
1" = 1'



**RURAL PAVING TYPICAL SECTION**  
1" = 1'

**NOTE**  
PAVING SECTIONS SHALL BE CONFIRMED WITH RECOMMENDATION PROVIDED IN THE GEOTECHNICAL REPORT PREPARED BY TERRACON (9-14-22)

	<b>1000, 1101, 1103 CERRILLOS RD</b> <b>SANTA FE, NM</b>	DRAWN BY prm
	<b>PAVING PLAN</b>	DATE 12-2-25
	<b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	DRAWING
	12-2-25 RONALD R. BOHANNAN P.E. #7868	SHEET # <b>PAV-1</b>



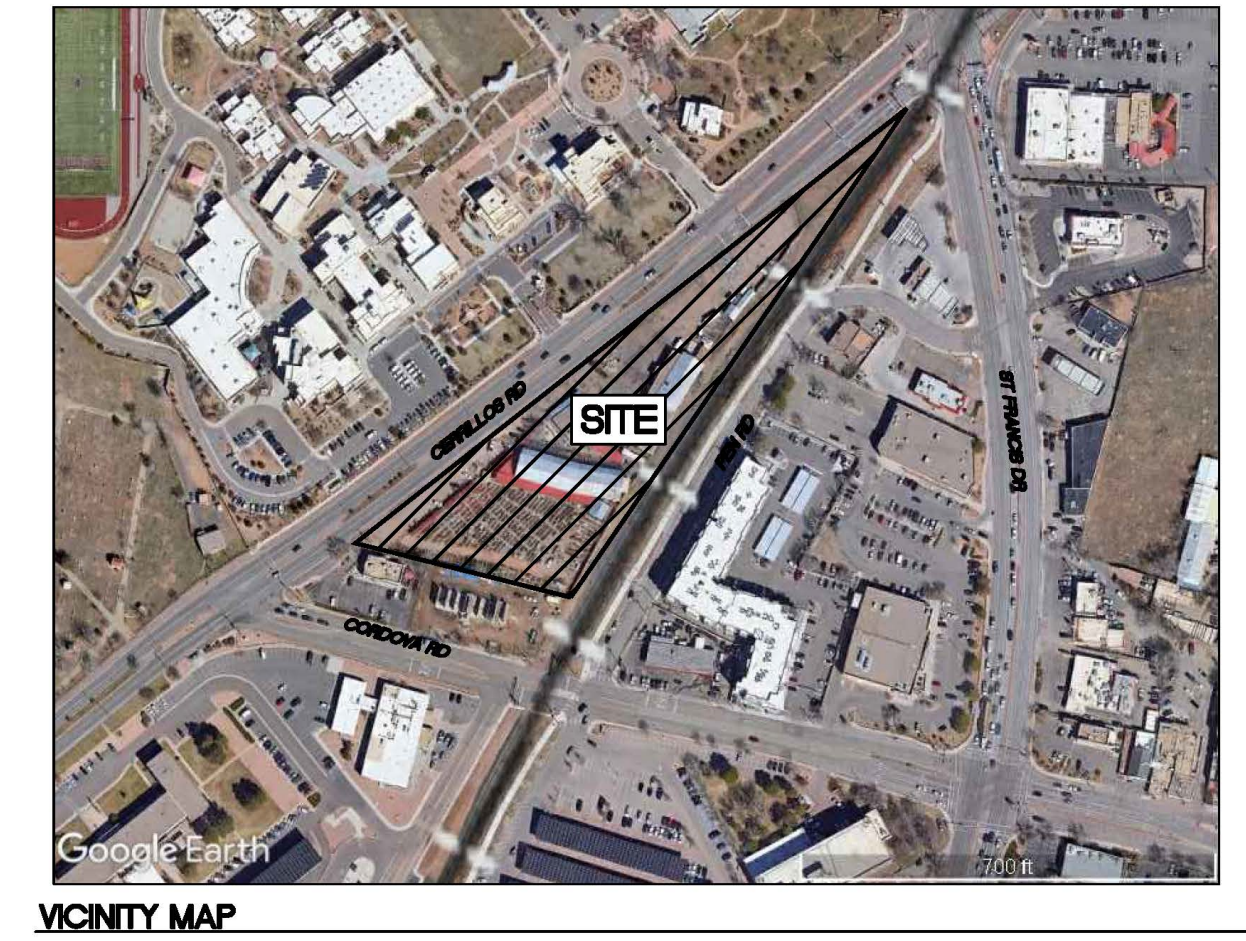
**LEGEND**

	CURB & GUTTER
	BOUNDARY LINE
	BUILDING
	EXISTING CURB & GUTTER
	CONCRETE SIDEWALK

- KEYED NOTES**
- ① UNIDIRECTIONAL ACCESSIBLE RAMP PER NMDOT DWG 608-001-5
  - ② CURB AND GUTTER TYPE C PER NMDOT DWG 608-01
  - ③ MOUNTABLE MEDIAN CURB (BACK TO BACK) PER NMDOT DWG 608-01
  - ④ 6" CONCRETE SIDEWALK
  - ⑤ REMOVE AND SALVAGE EXISTING LIGHT POLES
  - ⑥ REMOVE AND DISPOSE EXIST MEDIAN CURB AND LANDSCAPE

	<b>1000, 1101, 1103 CERRILLOS RD</b> <b>SANTA FE, NM</b>	DRAWN BY pm
	<b>CERRILLOS RD IMPROVEMENTS</b>	DATE 12-2-25
	<b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	DRAWING
RONALD R. BOHANNAN P.E. #7868	SHEET # <b>RD-1</b>	JOB # 2025080





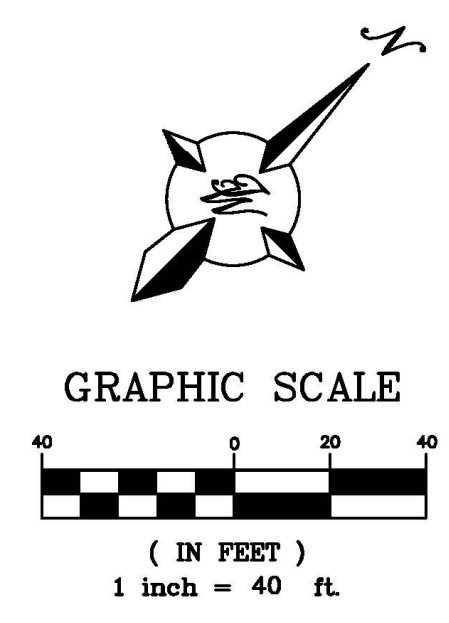
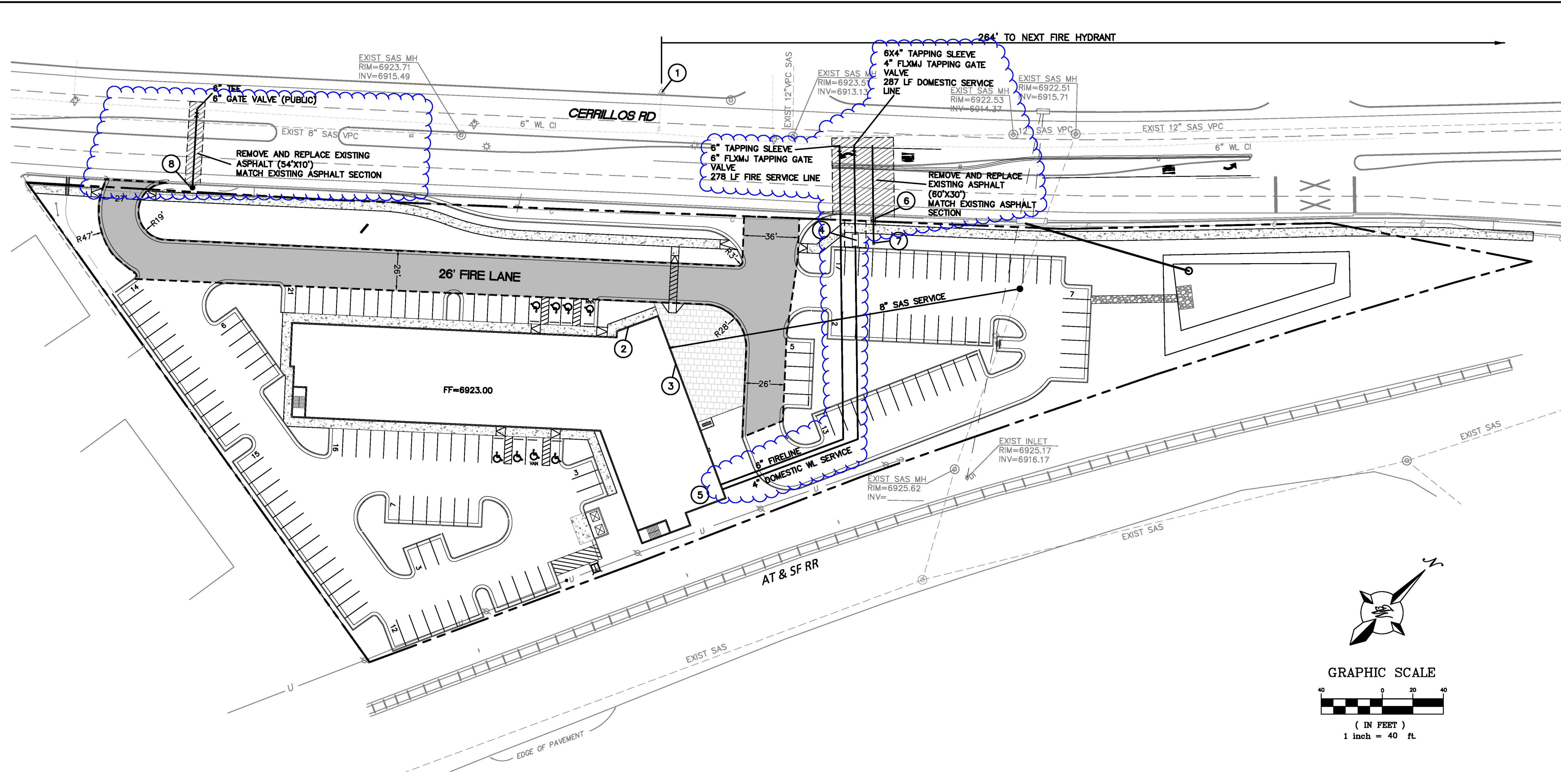
**LEGAL DESCRIPTION:**  
 1000 CERRILLOS ROAD, SPECIAL WARRANTY DEED RECORDRD IN Bk. 1630, Pg.649 AND PLAT Bk. 435, Pg.37, 1001 AND 1003 CERRILLOS ROAD, WARRANTY DEED RECORDED AS INSTRUMENT #1704193, AND PLAT Bk.651, Pg.03,

**SANTA FE HOTEL**  
 1000, 1101 AND 1103 CERRILLOS RD  
 SANTA FE, NM

**HOTEL**  
 OCCUPANCY TYPE R-1  
 LOT AREA 148122 SF (3.40 AC)  
 BUILDING SIZE 19853 GSF  
 BUILDING OVERALL AREA 79491 GSF

**CONSTRUCTION TYPE TYPE V-A**  
 FULLY SPRINKLED  
 MAX BUILDING HEIGHT 45'

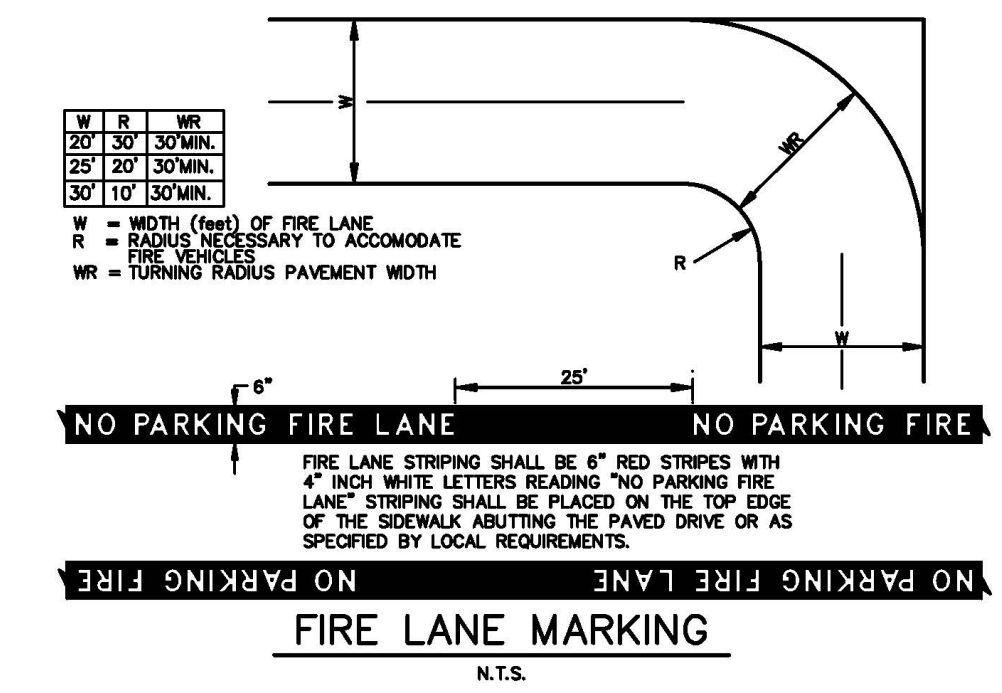
- NOTES**
- ON SITE FIRE LANE ROAD SHALL BE MARKED ON BOTH SIDES, AS DIRECTED BY FIRE MARSHALL.
  - FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE AND SHALL HAVE AN ALL WEATHER DRIVING SURFACE CAPABLE OF SUPPORTING THE IMPOSED LOAD OF FIRE APPARATUS WEIGHING AT LEAST 75000 LBS
  - KEY BOX (KNOX BOX) SHALL BE MOUNTED BETWEEN 4 AND 6 FEET ABOVE GRADE AND SHALL BE ILLUMINATED.
  - MECHANICAL PENTHOUSE AND ROOF SCREENS MAY EXCEED MAXIMUM BUILDING HEIGHT LISTED ABOVE BY 8 FEET



**LEGEND**

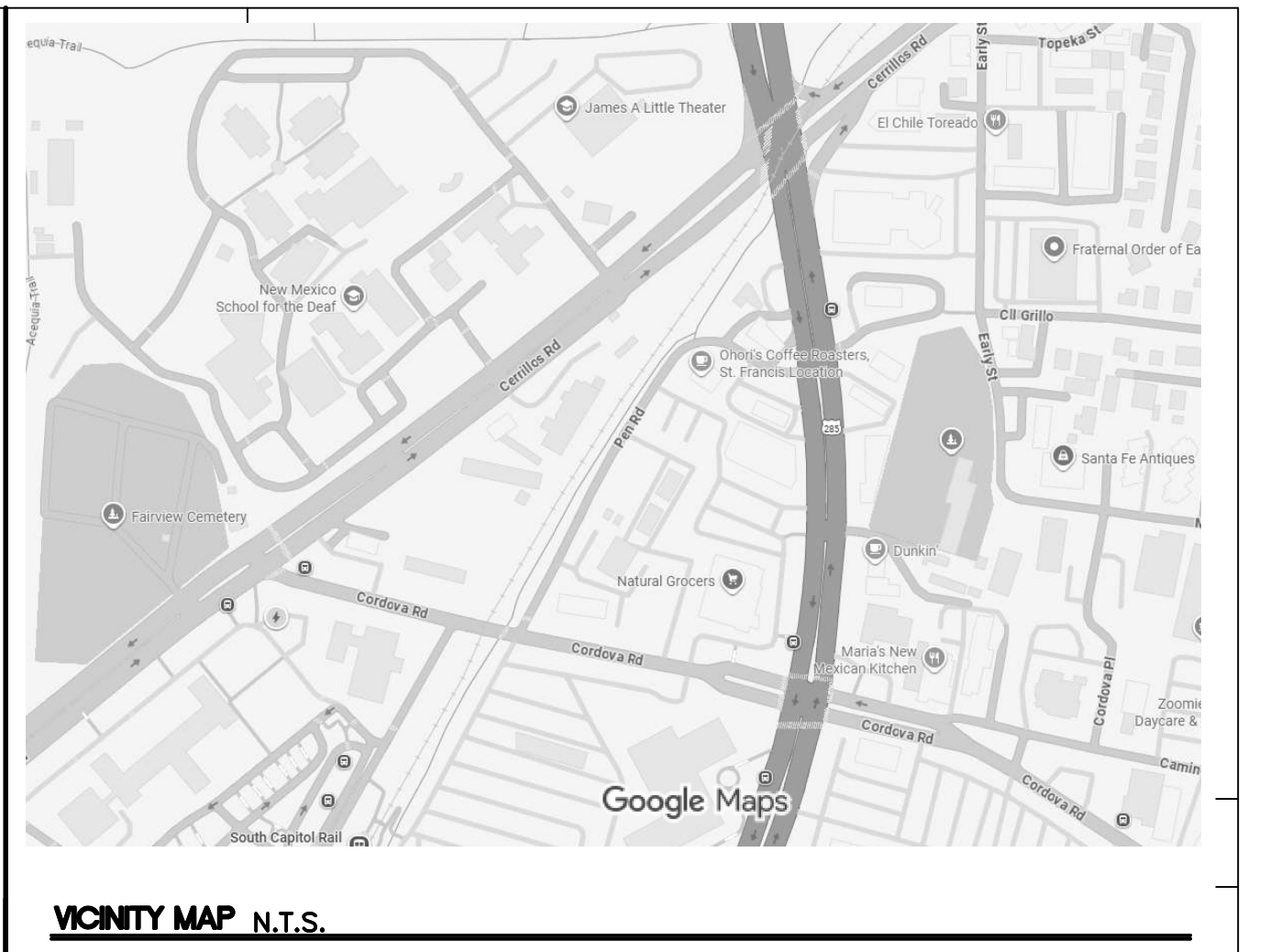
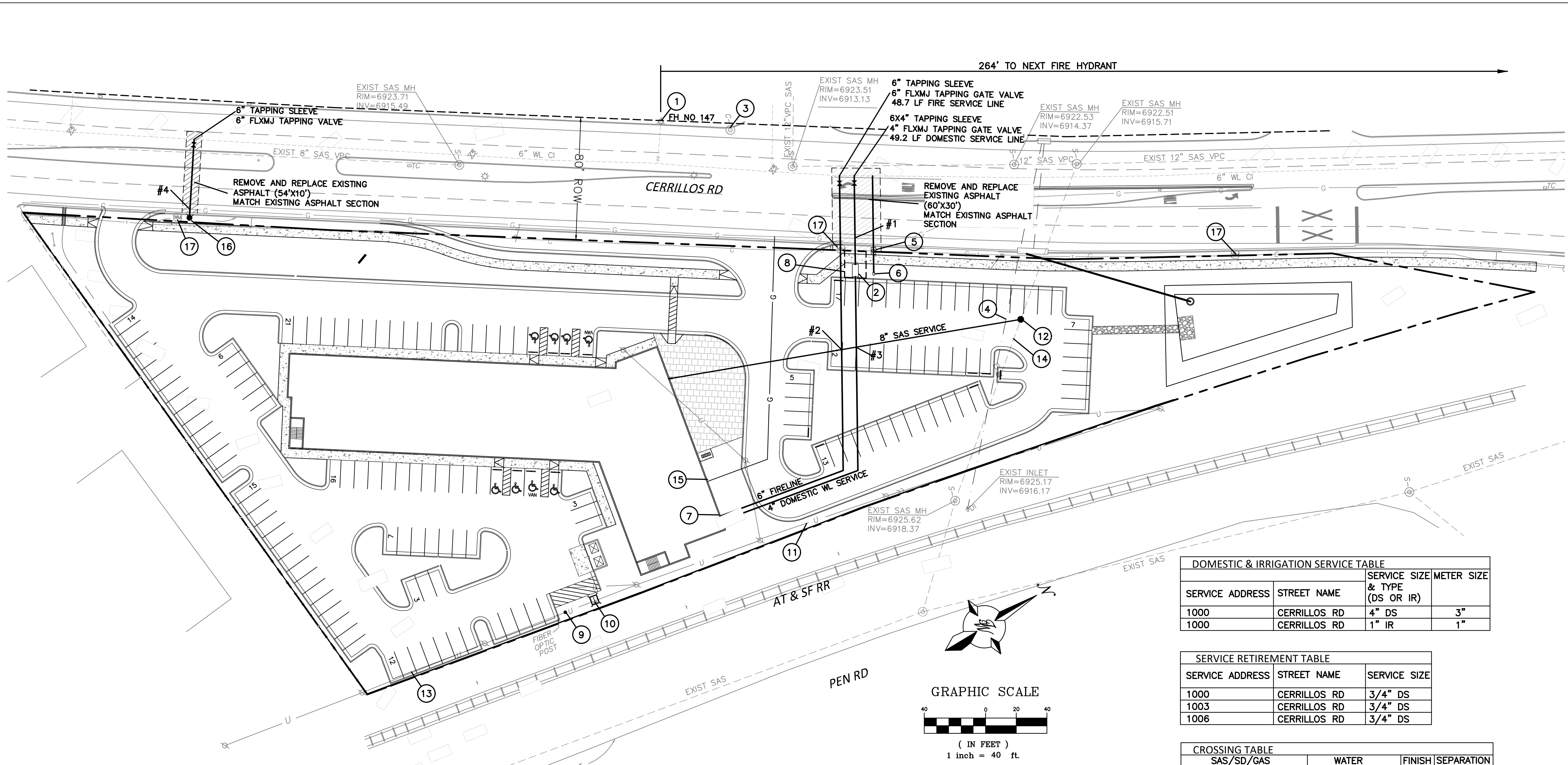
—	CURB & GUTTER
- - -	BOUNDARY LINE
—	BUILDING
- - -	EXISTING CURB & GUTTER
⊙	EXISTING SAS MH
⊗	EXISTING GATE VALVE
—	EXISTING WATERLINE
—	EXISTING SAS
—	8" SAS
—	8" WL
—	WATERLINE
●	PROPOSED HYDRANT
●	NEW SAS MH
⊗	NEW WATER VALVE
⊙	NEW SINGLE WATER METER
- - -	SAWCUT LINE
- - -	FIRE LANE

- KEYED NOTES**
- EXISTING FIRE HYDRANT (PUBLIC)
  - BUILDING ADDRESS
  - KNOX BOX
  - 3" WATER METER AND VAULT
  - 8" BACKFLOW PREVENTER INSIDE BUILDING
  - 1" IRRIGATION METER
  - BACKFLOW PREVENTER (IRRIGATION)
  - NEW FIRE HYDRANT



**CAUTION**  
 ALL EXISTING UTILITIES SHOWN WERE OBTAINED FROM RESEARCH, AS-BUILTS, SURVEYS OR INFORMATION PROVIDED BY OTHERS. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO CONDUCT ALL NECESSARY FIELD INVESTIGATIONS PRIOR TO AND INCLUDING ANY EXCAVATION, TO DETERMINE THE ACTUAL LOCATION OF UTILITIES AND OTHER IMPROVEMENTS, PRIOR TO STARTING THE WORK. ANY CHANGES FROM THIS PLAN SHALL BE COORDINATED WITH AND APPROVED BY THE ENGINEER.

	<b>1000, 1101, 1103 CERRILLOS RD</b> <b>SANTA FE, NM</b>	DRAWN BY prm DATE 12-2-25 DRAWING
	<b>FIRE PROTECTION PLAN</b>	SHEET # <b>FO-1</b>
	<b>TIERRA WEST, LLC</b> 5571 MIDWAY PARK PL NE ALBUQUERQUE, NEW MEXICO 87109 (505) 858-3100 www.tierrawestllc.com	JOB # 2025080



NOTE:  
ALL WATERLINES SHALL BE CONSTRUCTED OF POLY WRAPPED DUCTILE IRON

NOTE:  
PIPE JOINTS SHALL BE MECHANICALLY RESTRAINED PER UPDATED COSF WATER DIVISION DETAIL 10A. SEE COSF WATER DIVISION STANDARD DETAIL 10B FOR DIAGRAMMATIC REPRESENTATION OF JOINT RESTRAINT LENGTHS

UTILITY CONTACT INFORMATION  
CITY OF SANTA FE DEPARTMENT OF PUBLIC UTILITIES—WATER DIVISION  
  
CITY OF SANTA FE DEPARTMENT OF PUBLIC UTILITIES—WASTE WATER DIVISION  
**GARY MARTINEZ (505) 670-0480**  
  
NEW MEXICO GAS COMPANY  
STEVE LARRANGA (505) 412-2904

RECORD DRAWING  
  
THIS RECORD DRAWING HAS BEEN PREPARED BASED ON INFORMATION FURNISHED BY OTHERS. THE ARCHITECT/ENGINEER HAS NOT VERIFIED THE ACCURACY OR COMPLETENESS OF THIS INFORMATION AND SHALL NOT BE RESPONSIBLE FOR ERRORS OR OMISSIONS INCORPORATED INTO THIS DRAWING AS A RESULT FIELD VERIFICATION IS REQUIRED BEFORE USING INFORMATION REPRESENTED BY THIS DRAWING.  
  
ENGINEER \_\_\_\_\_ PE \_\_\_\_\_ DATE \_\_\_\_\_

DOMESTIC & IRRIGATION SERVICE TABLE			
SERVICE ADDRESS	STREET NAME	SERVICE SIZE & TYPE (DS OR IR)	METER SIZE
1000	CERRILLOS RD	4" DS	3"
1000	CERRILLOS RD	1" IR	1"

SERVICE RETIREMENT TABLE		
SERVICE ADDRESS	STREET NAME	SERVICE SIZE
1000	CERRILLOS RD	3/4" DS
1003	CERRILLOS RD	3/4" DS
1006	CERRILLOS RD	3/4" DS

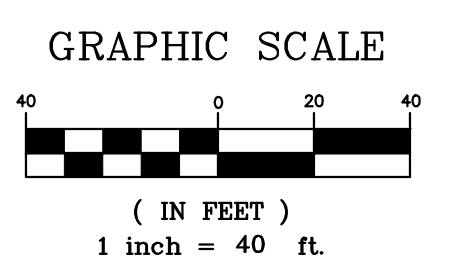
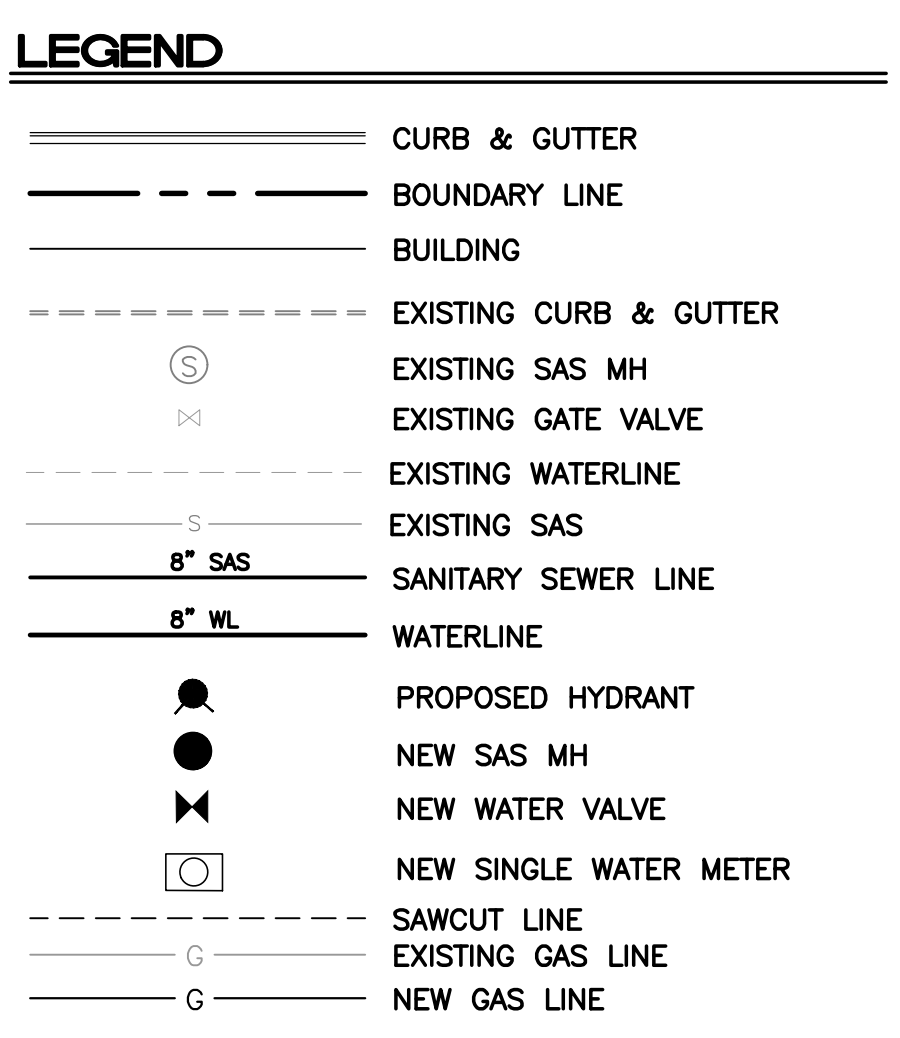
CROSSING TABLE							
SAS/SD/GAS				WATER			
NO.	SIZE	INV	TOP	SIZE	INV	TOP	SEPARATION
1	6" GAS	6914.74	6915.24	4"	6916.99	6917.24	6920.24 1.75'
2	6" SAS	6918.80	6919.30	6"	6916.50	6917.00	6921.00 1.80
3	6" SAS	6918.73	6919.23	4"	6916.67	6917.00	6921.00 1.73
4	6" GAS	6911.71	6912.21	6"	6913.71	6914.21	6917.21 1.50'

FIRE DESIGN TABLE			
SERVICE SIZE	REQUIRED FLOW* GPM	RESIDUAL PRESSURE	BUILDING(S) SERVED
6"	500 GPM	100 psi	HOTEL

— ALL FIRE SERVICES SHALL BE EQUIPPED WITH A BACKFLOW PREVENTION DEVICE MEETING THE REQUIREMENTS OF THE CoSF DESIGN CRITERIA, LATEST VERSION. REQUIRED FLOW RATE SHALL NOT EXCEED MANUFACTURE'S RATED CAPACITY FOR BACKFLOW PREVENTION DEVICE TO BE USED.  
 — TESTING, INSPECTION, AND MAINTENANCE OF PRIVATE FIRE HYDRANTS, BACKFLOW PREVENTERS AND OTHER FIRE SERVICE APPURTENANCES SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER AND SHALL COMPLY WITH ALL CoSF, SFFD, AND NFPA REQUIREMENTS.  
 \* REQUIRED FLOW RATE  
 — FOR FIRE SERVICE LINES SERVING ONLY AUTOMATIC FIRE SPRINKLER SYSTEMS, INCLUDE ONLY THE REQUIRED FLOW RATE OF SPRINKLER SYSTEM TO BE SERVED.  
 — FOR FIRE SERVICE LINES THAT WILL BE SERVING PRIVATE FIRE HYDRANTS (WITH OR WITHOUT AUTOMATIC FIRE SPRINKLER SYSTEMS), INCLUDE THE REQUIRED FLOW RATE FOR THE SITE AS DETERMINED BY APPENDIX B OF THE INTERNATIONAL FIRE CODE, LATEST VERSION ADOPTED BY THE CITY OF SANTA FE FIRE DEPARTMENT (SFFD).  
 \*\* RESIDUAL PRESSURE LISTED SHALL BE CALCULATED AT THE REQUIRED FLOW RATE AT THE POINT OF CONNECTION TO THE MAIN AS DETERMINED BY THE CoSF'S WATER MODEL OR FIELD TEST. RESIDUAL PRESSURES LESS THAN 20 PSI ARE NOT PERMITTED.

- KEYED NOTES**
- EXISTING FIRE HYDRANT (PUBLIC)
  - 3" DOMESTIC SERVICE METER AND VAULT SEE CoSFWD DETAIL 18
  - EXISTING UTILITY VAULT
  - EXISTING 36" SD RCP
  - 1" IRRIGATION METER/SERVICE PER CoSFWD DETAIL NO 03 (PEAK DESIGN FLOW=9.8 GPM)
  - 1" BACKFLOW PREVENTER (IRRIGATION) REFERENCE LANDSCAPE IRRIGATION PLAN SHEETS LI-101 AND LI-102 FOR CONTINUATION
  - 6" REDUCED PRESSURE PRINCIPLE BACKFLOW PREVENTER LOCATED INSIDE BUILDING PER CoSFWD STANDARDS
  - 15" WATERLINE EASEMENT
  - NEW POWER POLE W/SERVICE RISER
  - TRANSFORMER
  - REMOVE 410 LF EXISTING OVERHEAD POWER LINE AND 4 POWER POLES
  - NEW 4' SAS MANHOLE RIM=6920.72 INV=6917.00
  - EXISTING OVERHEAD POWER LINE TO REMAIN
  - EXISTING 10" VPC SAS
  - GAS METER
  - NEW FIRE HYDRANT, 6" TAPPING SLEEVE, 6" GATE VALVE
  - RETIRED WATER SERVICE, CUT AND CAP AT MAIN CLOSE CORP. REMOVE METER CAN AND RETURN METER TO CoSFWD

- CITY OF SANTA FE WATER DIVISION GENERAL NOTES
- CONTRACTOR SHALL NOTIFY THE CITY OF SANTA FE WATER DIVISION (CoSFWD) FIVE (5) DAYS PRIOR TO COMMENCEMENT OF WORK.
  - CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE CoSFWD CONSTRUCTION STANDARDS AND SPECIFICATIONS.
  - ALL EASEMENTS SHALL BE DEDICATED, CLEARED, GRADED AND STAKED PRIOR TO WATER LINE INSTALLATION.
  - ALL STREETS SHALL BE CUT TO WITHIN ±6" OF FINAL GRADE PRIOR TO WATER LINE INSTALLATION.
  - LOT CORNERS SHALL BE STAKED PRIOR TO SERVICE LINE INSTALLATION. CURB, GUTTER AND DRIVEWAY APRON SHALL BE INSTALLED PRIOR TO SERVICE LINE INSTALLATION UNLESS OTHERWISE APPROVED, IN WRITING, BY CoSFWD.
  - CONTRACTOR (DEVELOPER) SHALL PROVIDE CONSTRUCTION STAKING UTILIZING THE APPROPRIATE RIGHT-OF-WAY MAPS, SIGNED PLATS AND CoSFWD DRAWINGS.
  - MATERIAL SUBMITTALS SHALL BE APPROVED BY CoSFWD PRIOR TO CONSTRUCTION.
  - CONTACT NEW MEXICO ONE CALL AT 811 TWO (2) WORKING DAYS IN ADVANCE OF CONSTRUCTION FOR UTILITY SPOTS.
  - PRESSURE REGULATORS SHALL BE INSTALLED ON ALL SERVICES DOWNSTREAM FROM THE METER.
  - 4 FEET COVER TO TOP OF PIPE SHALL BE MAINTAINED ON ALL WATER MAINS AND SERVICES.
  - CONTRACTOR SHALL SUBMIT AS-BUILT CONSTRUCTION PACKET WITHIN FIVE (5) DAYS OF COMPLETION OF CONSTRUCTION INCLUDING: VALVE TIES, AS-BUILT DRAWINGS (INCLUDING, BUT NOT LIMITED TO: FITTING-TO-FITTING MEASUREMENTS, SERVICE-TO-SERVICE MEASUREMENTS, CENTER OF MAIN TO CENTER OF SERVICE MEASUREMENTS, LENGTH OF MAIN INSTALLED, FITTINGS INSTALLED, ETC.) AND POTABILITY RESULTS.
  - ALL VALVE BOXES SHALL BE BROUGHT UP TO GRADE OF THE FINAL PAVING MATERIAL SO THAT THE CONCRETE IS EXPOSED.
  - FIRE HYDRANTS SHALL BE NUMBERED USING REFLECTIVE NUMERALS. THE REFLECTIVE NUMERALS SHALL BE OBTAINED BY THE CONTRACTOR FOR THE CoSFWD FIELD REPRESENTATIVE AT THE TIME THE NOTICE TO PROCEED (NTP) IS ISSUED. NUMBERS SHALL BE LEGIBLE FROM THE ROAD. PRIOR TO INSTALLING NUMBERS, FIRE HYDRANTS SHALL BE PAINTED.
  - A MECHANICAL RESTRAINT SYSTEM SHALL BE UTILIZED ON FITTINGS AND PIPING FOR THRUST RESTRAINT. CONCRETE THRUST BLOCKING SHALL BE USED ONLY FOR SPECIAL CONDITIONS (E.G. CAPS WHERE MAIN WILL BE EXTENDED IN THE FUTURE) AS SPECIFICALLY APPROVED BY CoSFWD.
  - ANY FIELD CHANGES TO THESE PLANS REQUIRE APPROVAL OF BOTH THE DESIGN ENGINEER AND CoSFWD.
  - WORK ON CoSFWD FACILITIES SHALL NOT BEGIN UNTIL CoSFWD HAS ISSUED A NTP TO THE APPROVED UTILITY CONTRACTOR.
  - ANY AND ALL EASEMENTS FOR CITY-OWNED INFRASTRUCTURE SHALL BE OBTAINED, VERIFIED AND RECORDED BY DEVELOPER BEFORE PROJECT COMMENCEMENT. CONTRACTOR SHALL VERIFY THE EASEMENT IS IN PLACE BEFORE INSTALLING ANY INFRASTRUCTURE.



**TIERRA WEST, LLC**  
5571 MIDWAY PARK PL NE  
ALBUQUERQUE, NEW MEXICO 87109  
(505) 858-3100  
www.tierrawestllc.com

2025080-WATER

**AC MARRIOTT HOTEL**  
1000, 1101 AND 1103 CERRILLOS RD, SANTA FE, NM

**PUBLIC WATER PLAN**

PLAT RECORDING INFORMATION	APPROVED	INSPECTOR
BOOK _____	CITY OF SANTA FE WATER DIVISION DATE _____	DATE: _____
PAGE _____	SANTA FE FIRE DEPARTMENT DATE _____	2025
FILE DATE _____	AERIAL TOWNSHIP RANGE SECTION WORK ORDER NO.	<b>21</b>
	I-17 SEC25,T17N,R9E # _____	(1 OF 1)

LANDSCAPE DATA			
AREAS (SF)		REQUIRED	PROVIDED
OPEN SPACE		29,621	48,455
PONDING AREA			5,774
OPEN SPACE PLANTING		REQUIRED	PROVIDED
TREE	OPEN SPACE	ONE TREE PER 500 SF OF REQUIRED OPEN SPACE	59
	PONDING AREA	MINIMUM 25% EVERGREEN	15
		ONE TREE PER 500 SF OF PONDING AREA	12
SHRUB	OPEN SPACE	TWO SHRUBS PER 500 SF OF REQUIRED OPEN SPACE	118
	PONDING AREA	MINIMUM 25% EVERGREEN	30
		THREE SHRUBS PER 500 SF OF PONDING AREA	35
STREET TREE		REQUIRED	PROVIDED
ONE TREE PER 30 LF ON AVERAGE		-	-
799 LF		27	27
PARKING LOT		REQUIRED	PROVIDED
ONE TREE PER 25 LF ON AVERAGE IF ABUTS OR OCCUPIES A STREET YARD		-	-
MINIMUM 10 SF OF LANDSCAPE AREA PER PARKING SPACE		-	-
152 PARKING SPACE		1520	12,094
ONE TREE PER 90 SF OF LANDSCAPE AREA (1920 SF/90)		17	17
COOL SEASON TURF		REQUIRED	PROVIDED
MAXIMUM 1,000 SQ. FT. OR 3% OF REQUIRED OPEN SPACE (WHICHEVER IS GREATER)		-	945
FENCE/WALL SCREENING		REQUIRED	PROVIDED
WALLS AND FENCES SHALL BE LANDSCAPED WITH PLANT MATERIAL THAT IS PREDOMINANTLY THORNY OR OTHER BARRIER PLANTINGS THAT WILL COVER A MINIMUM OF 75% OF THE FACE OF THE FENCE OR WALL AT MATURITY		5057	5164
TOTALS		REQUIRED	PROVIDED
TREES	2" CAL OR 6' HT.	59	71
SHRUBS	5 GAL	153	279

WATER LEVEL MEASURING DEVICE TO BE LOCATED AT THE LOW POINT OF THE POND WITH ZERO SET AT TOP OF FINISHED GRADE

### WATER BUDGET CALCULATIONS

ALL SITE LANDSCAPING

YEAR 1 AND YEAR 2

DRIP IRRIGATION TO TREES AND SHRUBS  
4 MONTHS AT 1 RUN TIME PER MONTH  
4 MONTHS AT 1 RUN TIME PER WEEK  
APPROX. 90 RUN CYCLES PER YEAR

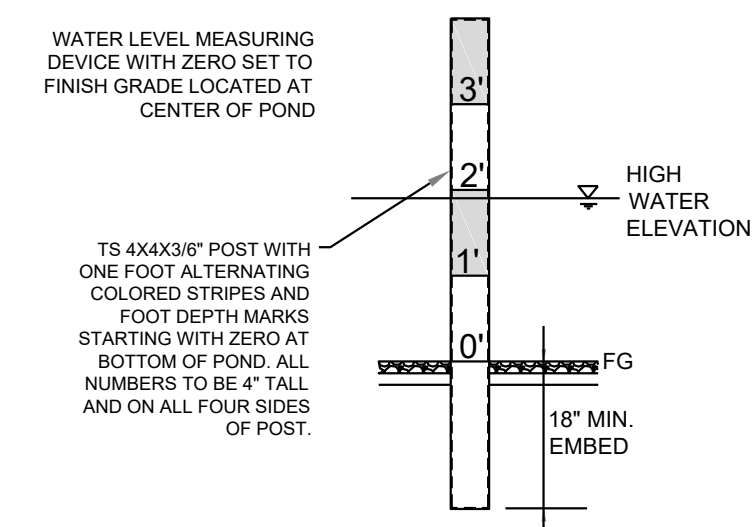
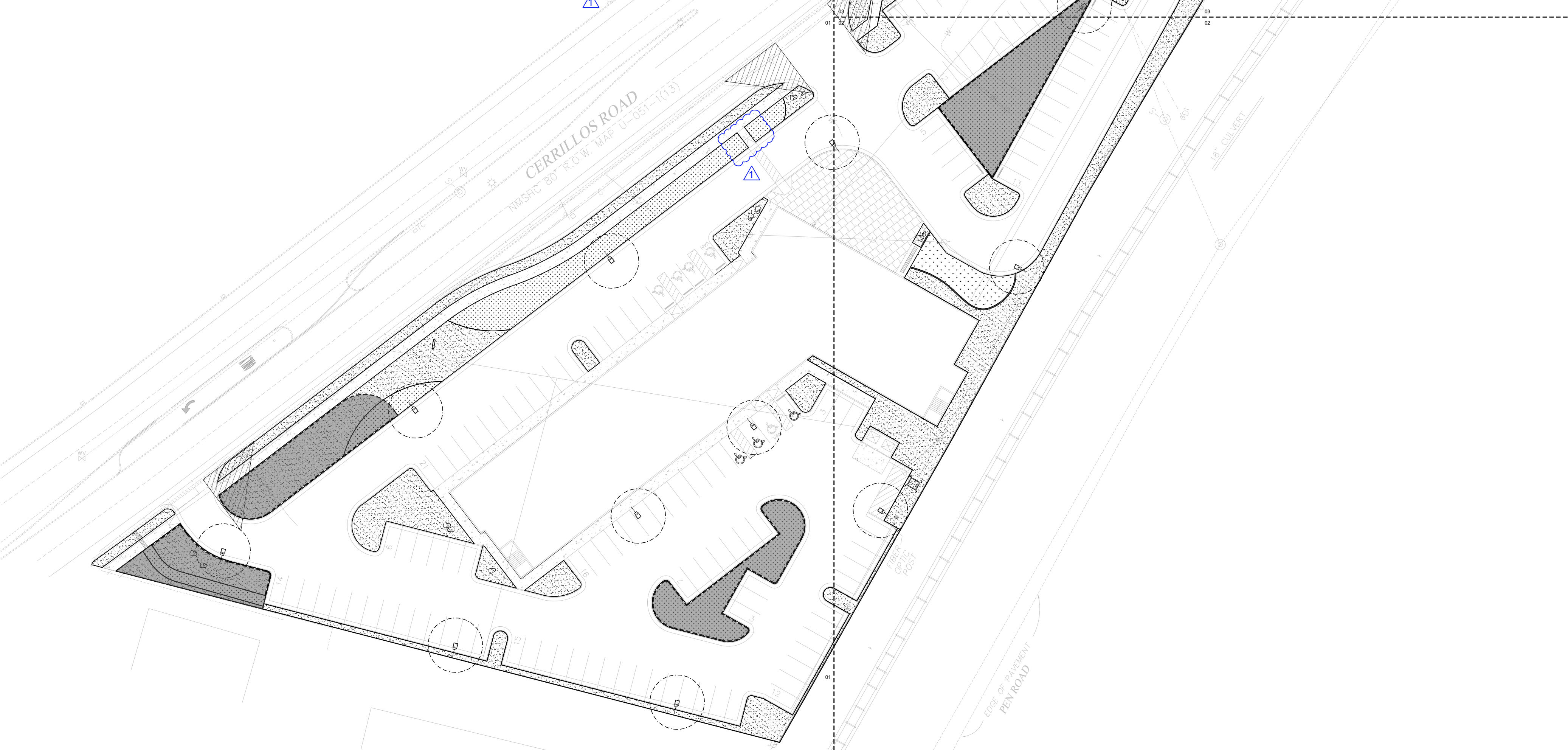
71 TREES = 426 GALLONS PER RUN CYCLE  
365 SHRUBS = 730 GALLONS PER RUN CYCLE  
1156 GALLONS PER RUN CYCLE X 90 RUN TIMES = 104040 GPY / 0.32 ACRE FT.  
SPRAY IRRIGATION TO LAWN  
945 SF X 3' PER YEAR = 21207.27 GPY / 0.07 ACRE FT.  
TOTAL WATER USE PER YEAR = 125247.27 GPY / 0.38 ACRE FT.

YEAR 3 AND BEYOND

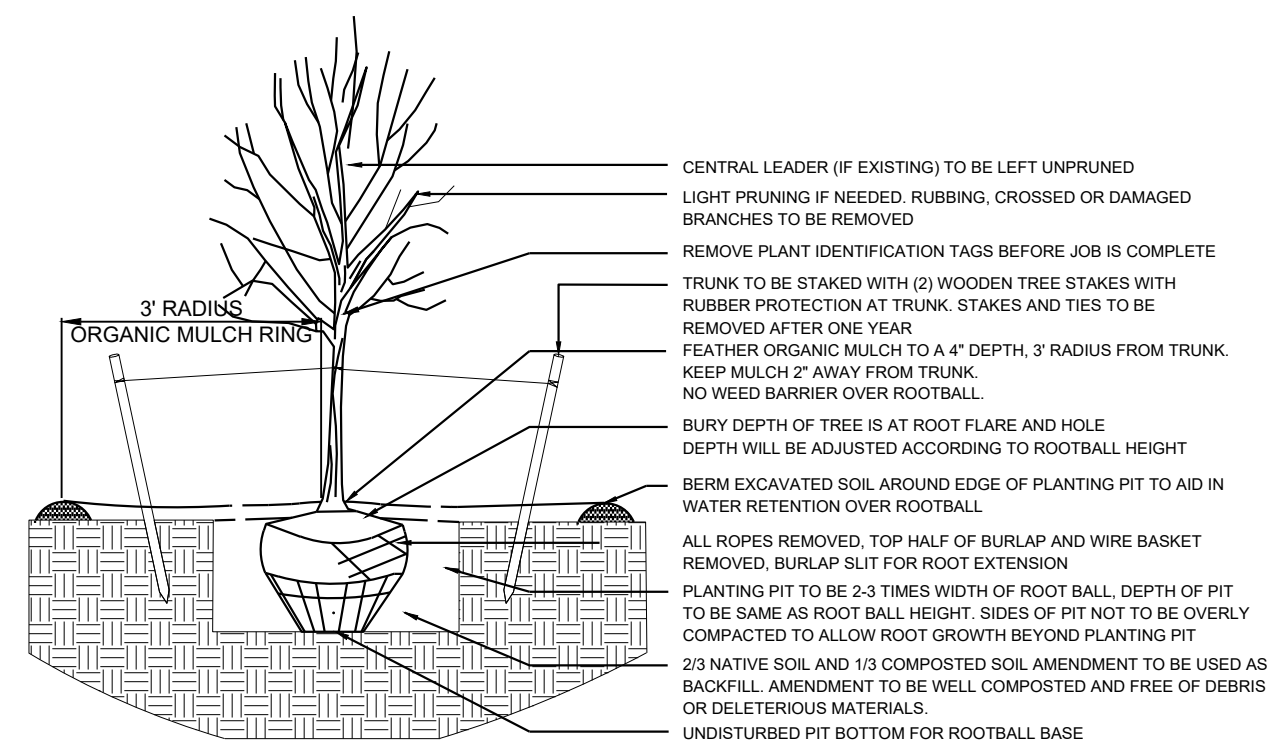
DRIP IRRIGATION TO TREES AND SHRUBS  
4 MONTHS AT 1 RUN TIME PER MONTH  
4 MONTHS AT 1 RUN TIME PER WEEK  
APPROX. 77 RUN CYCLES PER YEAR

71 TREES = 426 GALLONS PER RUN CYCLE  
365 SHRUBS = 730 GALLONS PER RUN CYCLE  
1156 GALLONS PER RUN CYCLE X 77 RUN TIMES = 89012 GPY / 0.27 ACRE FT.  
SPRAY IRRIGATION TO LAWN  
945 SF X 3' PER YEAR = 21207.27 GPY / 0.07 ACRE FT.  
TOTAL WATER USE PER YEAR = 110219.27 GPY / 0.34 ACRE FT.

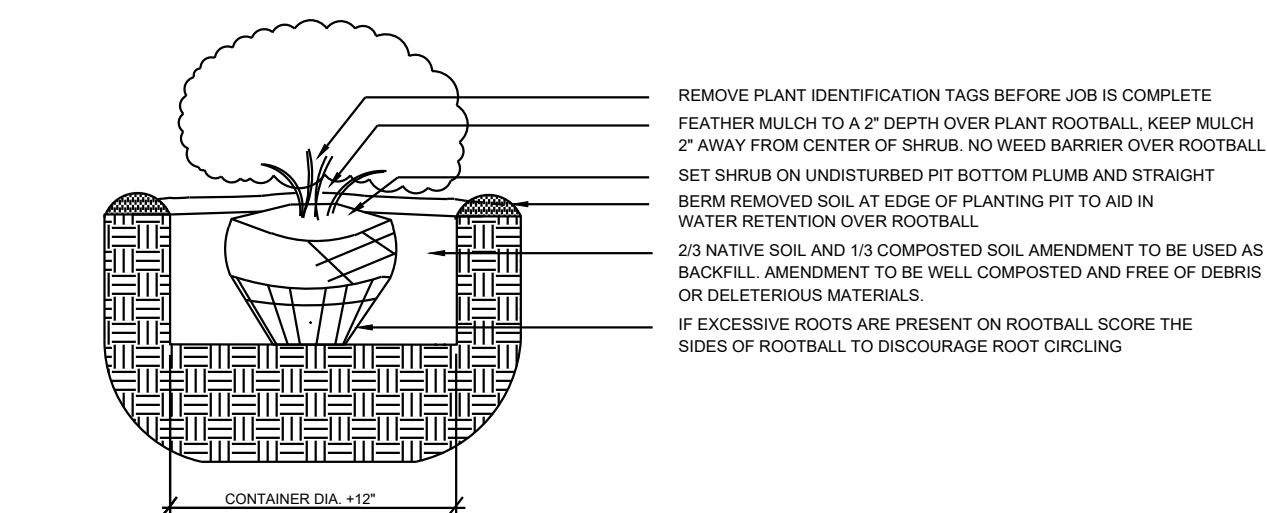
NOTE: THIS WATER BUDGET IS APPROXIMATE AND IS TO BE USED AS A GUIDELINE AND ADJUSTED ACCORDING TO SITE CONDITIONS



1 WATER LEVEL MEASURING DEVICE N.T.S.



2 TREE PLANTING DETAIL N.T.S.



3 SHRUB PLANTING DETAIL N.T.S.

### LANDSCAPE NOTES

- ALL GRAVEL MULCHES TO BE A 2-INCHES MINIMUM THICKNESS OVER WEED BARRIER.
- FILTER FABRIC UNDER ALL GRAVEL MULCHES AND RIP RAP ROCK. MULCH TO BE ONE HALF INCH BELOW CONCRETE AND TWO INCHES BELOW TOP OF CURB AND NOTED ON LANDSCAPE PLAN.
- PLANTS TO BE IRRIGATED WITH AUTOMATIC DRIP IRRIGATION SYSTEM PROTECTED WITH BACKFLOW PREVENTION DEVICE (SEE DETAILS).
- EXISTING LANDSCAPE AREAS OUTSIDE PROJECT AREA TO BE PROTECTED DURING CONSTRUCTION.
- STREET TREES PROVIDED AT APPROX. 30' ON CENTER.
- PROPOSED COOL SEASON SOD AREA WILL BE FOR RECREATIONAL USE. SOD USED WILL BE THE SIEMPRE VERDE FESCUE MIX FROM EVERGREEN TURF.
- TURF GRASS SOD OR TURF GRASS SEED MIXES CONTAIN NO MORE THAN TWENTY-FIVE PERCENT KENTUCKY BLUEGRASS PER 14-8.4(F)(3)(A).
- PLANTS LOCATED IN BOTTOM THIRD OF POND AREAS ARE ADAPTED TO PERIODS OF INUNDATION.
- NO OBJECTS, WALLS, FENCES, PARKED VEHICLES OR VEGETATION SHALL BE PLACED WITHIN CLEAR-SIGHT TRIANGLES BETWEEN 3' AND 6' HEIGHT.
- STREET TREES WILL BE INSTALLED SO AS TO NOT OBSTRUCT STREET SIGNS.
- STREET TREES TO BE LOCATED/ADJUSTED TO BE A MINIMUM OF 15' FROM STREET LIGHT STANDARDS AND FIRE HYDRANTS PER ZONING SECTION 14-8.4(G)(3)(e)(f).
- ALL PROJECT STREET TREES SHALL BE IRRIGATED AND MAINTAINED BY THE DEVELOPER.
- NO TREES SHALL BE SITUATED WITHIN 15 FEET OF A WATERMAIN OR WASTEWATER MAIN PER CITY UTILITY CODES.
- ALL PROJECT STREET TREES SHALL BE IRRIGATED AND MAINTAINED BY THE DEVELOPER.
- A LANDSCAPE IRRIGATION PLAN BY A QUALIFIED IRRIGATION DESIGNER PER COSF CODE CHAPTER 14-8.4(E) WATER HARVESTING AND IRRIGATION STANDARDS AND COSF LANDSCAPE IRRIGATION DESIGN STANDARDS TO BE PROVIDED.
- THIS PLAN INCORPORATES PASSIVE WATER HARVESTING TECHNIQUES FOR LANDSCAPE IRRIGATION WHEREVER MADE POSSIBLE BY THE GRADING AND DRAINAGE PLAN. DETENTION AND RETENTION PONDS ARE INTENDED AS INTEGRATED LANDSCAPE FEATURES RATHER THAN SINGLE USE FLOOD CONTROL FEATURES.
- TRANSITION LINE BETWEEN EXCAVATION AND NATIVE SOIL SHALL BE SMOOTHLY RAKED, CREATING A CLEAN CONSISTENT GRADE. EXISTING GRADE BELOW NATIVE TREES AND SHRUBS SHALL REMAIN UNDISTURBED THROUGHOUT THE DRIPLINE OF THE PLANT MATERIAL. NO MOUNDING OF SOIL, FILL DIRT, ORGANIC OR INORGANIC DEBRIS SHALL BE ABANDONED UNDER NATIVE PLANT MATERIAL CANOPIES.
- ALL DISTURBED AREAS DUE TO CONSTRUCTION AND NOT PART OF THE LANDSCAPE PLAN SHALL BE REVEGETATED AND IRRIGATED INCLUDING SPOIL PILES AND STOCKPILES OF ANY MATERIAL. GRASS SEED MIX SHALL BE DRYLAND BLEND NATIVE GRASS MIXTURE FROM PLANTS OF THE SOUTHWEST OR EQUAL. THE SEED RATE SHALL BE 2 LBS. PER 1,000 SF.
- AREAS OF NATURAL GRAVEL, COBBLE, FRACTURED STONE, AND/OR BOULDER FIELDS SHALL BE RESTORED TO THE NATURAL STATE THAT EXISTED PRIOR TO CONSTRUCTION. GRADE OUT ANY DAMAGE TO THE NATURAL TERRAIN PRIOR TO THE RE-APPLICATION OF NATIVE STONE MATERIAL.
- TRANSITION LINE BETWEEN EXCAVATION AND NATIVE SOIL SHALL BE SMOOTHLY RAKED, CREATING A CLEAN CONSISTENT GRADE. EXISTING GRADE BELOW NATIVE TREES AND SHRUBS SHALL REMAIN UNDISTURBED THROUGHOUT THE DRIPLINE OF THE PLANT MATERIAL. NO MOUNDING OF SOIL, FILL DIRT, ORGANIC OR INORGANIC DEBRIS SHALL BE ABANDONED UNDER NATIVE PLANT MATERIAL CANOPIES.

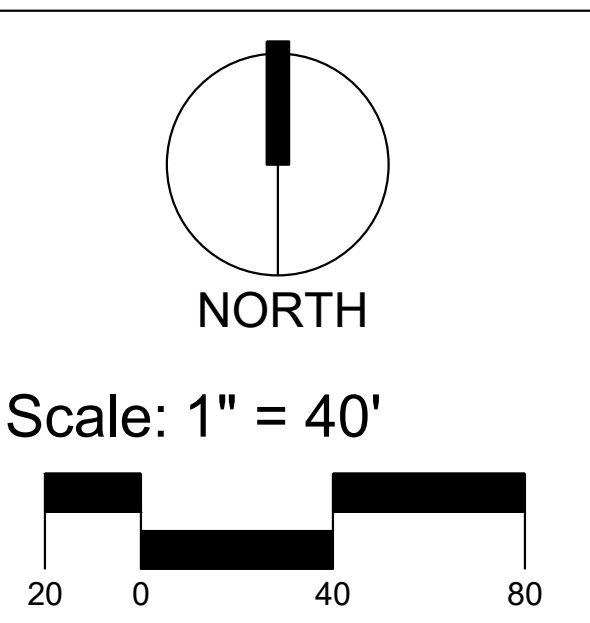
- ALL PLANTS SHALL BE COVERED BY A 1-YEAR WARRANTY FROM POINT OF SUBSTANTIAL COMPLETION, PROVIDED THAT PROPER MAINTENANCE HAS BEEN PERFORMED ON LANDSCAPE.
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- 14-8.4(F)(2)(j) THE OWNER SHALL PROPERLY MAINTAIN ALL MATERIALS AND INSTALLATION REQUIRED BY THIS SECTION 14-8.4 INCLUDING PROPER PRUNING, SOIL TESTING, FERTILIZING, AND WEEDING.
- ALL REMOVED SIGNIFICANT TREES SHALL BE REPLACED TREE FOR TREE.
- NUMBER OF 2" CALIPER DECIDUOUS AND 8' OR TALLER EVERGREEN SIGNIFICANT TREES REQUIRED BY CODE MUST BE MAINTAINED.
- 14-8.4(F)(5)(a)(ii) SIGNIFICANT TREES THAT ARE TO BE PRESERVED OR RELOCATED SHALL BE HEALTHY AND FREE FROM SERIOUS INSECT OR PARASITE INFESTATION. CITY STAFF SHALL INSPECT SIGNIFICANT TREES PRIOR TO REMOVALS FOR TREE HEALTH. CITY STAFF TO COMPLETE AN INSPECTION PRIOR TO ANY TREE REMOVALS. INSPECTION DATE SHALL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.
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- THE RP MUST BE INSTALLED A MINIMUM DISTANCE OF FIVE (5) FEET FROM THE METER SERVICE. BACKFLOW PREVENTERS SHALL BE PLACED OUTSIDE OF STREET RIGHT OF WAY. BACKFLOW PREVENTERS SHALL NOT BE LOCATED WITHIN THE PLANTER STRIP WITHOUT WATER DIVISION WRITTEN APPROVAL.

**YELLOWSTONE**  
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Date: 10/14/2025  
Revisions:  
12/01/2025

Drawn by: PL  
Reviewed by: CM

**AC by Marriott Hotel**  
1000, 1101 & 1103 Cerrillos Road  
Santa Fe, New Mexico



Sheet Title:  
**Material Plan Overall**

Sheet Number:  
**LS-00**







LANDSCAPE DATA			
AREAS (SF)		REQUIRED	PROVIDED
OPEN SPACE		29,621	48,455
PONDING AREA			5,774
OPEN SPACE PLANTING		REQUIRED	PROVIDED
TREE	OPEN SPACE	ONE TREE PER 500 SF OF REQUIRED OPEN SPACE	59
	PONDING AREA	MINIMUM 25% EVERGREEN	15
		ONE TREE PER 500 SF OF PONDING AREA	12
SHRUB	OPEN SPACE	TWO SHRUBS PER 500 SF OF REQUIRED OPEN SPACE	118
	PONDING AREA	MINIMUM 25% EVERGREEN	30
		THREE SHRUBS PER 500 SF OF PONDING AREA	35
STREET TREE		REQUIRED	PROVIDED
ONE TREE PER 30 LF ON AVERAGE		-	-
799 LF		27	27
PARKING LOT		REQUIRED	PROVIDED
ONE TREE PER 25 LF ON AVERAGE IF ABUTS OR OCCUPIES A STREET YARD		-	-
MINIMUM 10 SF OF LANDSCAPE AREA PER PARKING SPACE		-	-
152 PARKING SPACE		1520	12,094
ONE TREE PER 90 SF OF LANDSCAPE AREA (1920 SF/90)		17	17
COOL SEASON TURF		REQUIRED	PROVIDED
MAXIMUM 1,000 SQ. FT. OR 3% OF REQUIRED OPEN SPACE (WHICHEVER IS GREATER)		-	945
FENCE/WALL SCREENING		REQUIRED	PROVIDED
WALLS AND FENCES SHALL BE LANDSCAPED WITH PLANT MATERIAL THAT IS PREDOMINANTLY THORNY OR OTHER BARRIER PLANTINGS THAT WILL COVER A MINIMUM OF 75% OF THE FACE OF THE FENCE OR WALL AT MATURITY		5057	5164
TOTALS		REQUIRED	PROVIDED
TREES	2" CAL OR 6' HT.	59	71
SHRUBS	5 GAL	153	279

WATER LEVEL MEASURING DEVICE TO BE LOCATED AT THE LOW POINT OF THE POND WITH ZERO SET AT TOP OF FINISHED GRADE

### WATER BUDGET CALCULATIONS

ALL SITE LANDSCAPING

YEAR 1 AND YEAR 2

DRIP IRRIGATION TO TREES AND SHRUBS  
4 MONTHS AT 1 RUN TIME PER MONTH  
4 MONTHS AT 1 RUN TIME PER WEEK  
APPROX. 90 RUN CYCLES PER YEAR

71 TREES = 426 GALLONS PER RUN CYCLE  
365 SHRUBS = 730 GALLONS PER RUN CYCLE  
1156 GALLONS PER RUN CYCLE X 90 RUN TIMES = 104040 GPY / 0.32 ACRE FT.  
SPRAY IRRIGATION TO LAWN  
945 SF X 3" PER YEAR = 21207.27 GPY / 0.07 ACRE FT.  
TOTAL WATER USE PER YEAR = 125247.27 GPY / 0.38 ACRE FT.

YEAR 3 AND BEYOND

DRIP IRRIGATION TO TREES AND SHRUBS  
4 MONTHS AT 1 RUN TIME PER MONTH  
4 MONTHS AT 1 RUN TIME PER WEEK  
APPROX. 77 RUN CYCLES PER YEAR

71 TREES = 426 GALLONS PER RUN CYCLE  
365 SHRUBS = 730 GALLONS PER RUN CYCLE  
1156 GALLONS PER RUN CYCLE X 77 RUN TIMES = 89012 GPY / 0.27 ACRE FT.  
SPRAY IRRIGATION TO LAWN  
945 SF X 3" PER YEAR = 21207.27 GPY / 0.07 ACRE FT.  
TOTAL WATER USE PER YEAR = 110219.27 GPY / 0.34 ACRE FT.

NOTE: THIS WATER BUDGET IS APPROXIMATE AND IS TO BE USED AS A GUIDELINE AND ADJUSTED ACCORDING TO SITE CONDITIONS



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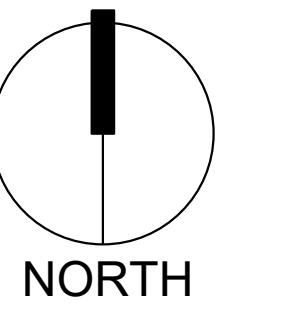
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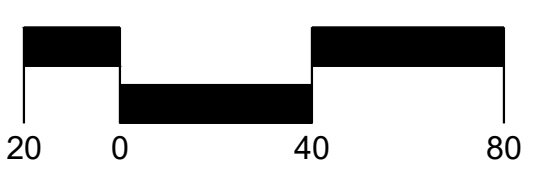
Drawn by: PL

Reviewed by: CM

**AC by Marriott Hotel**  
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Scale: 1" = 40'



Sheet Title:

**Planting Plan Overall**

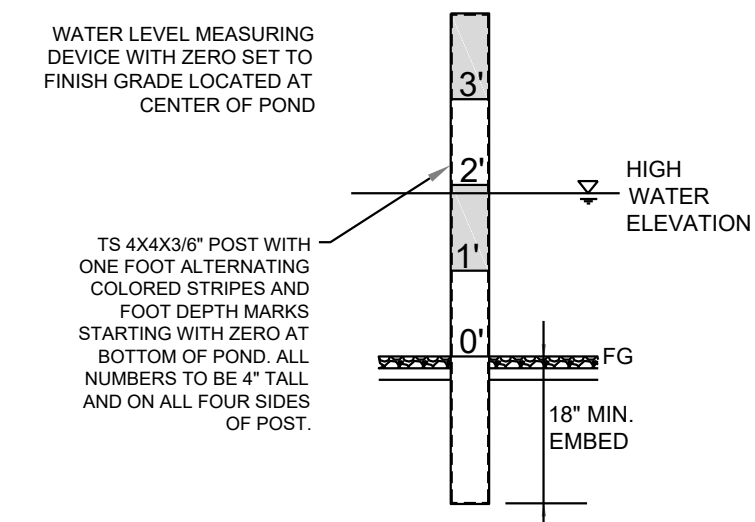
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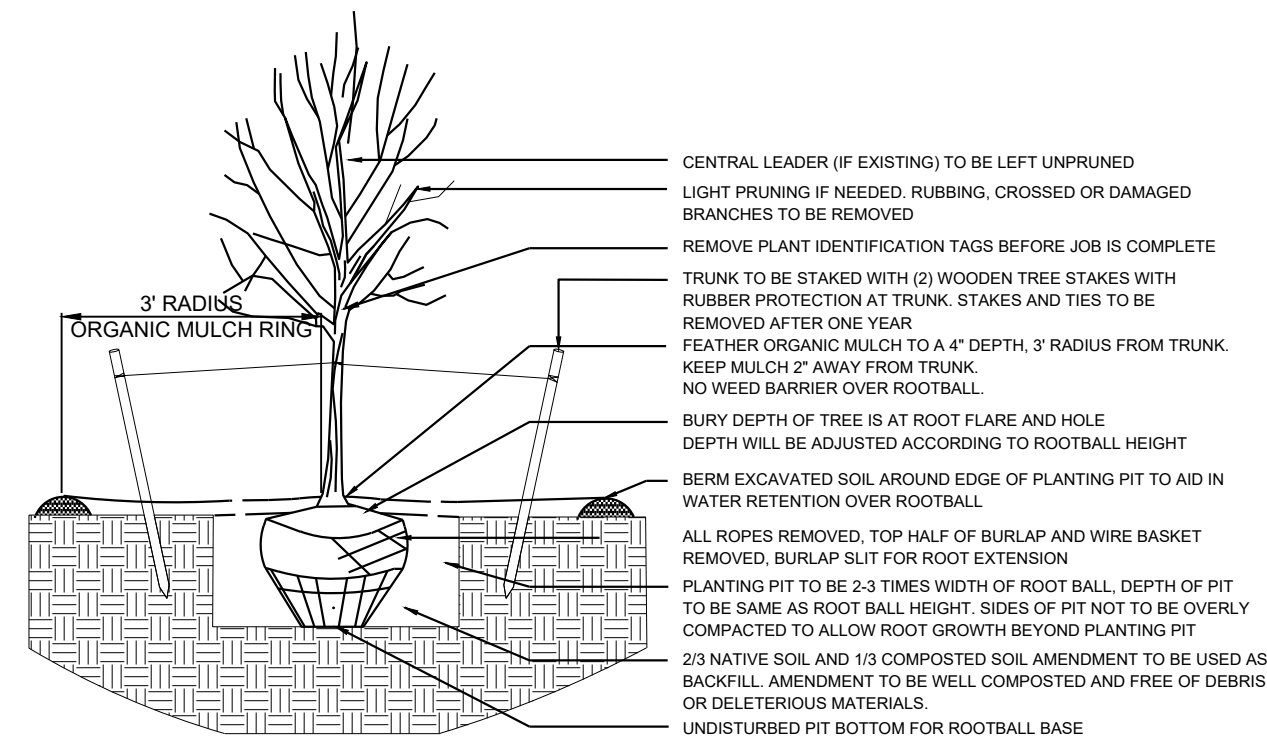
### LANDSCAPE NOTES

- ALL GRAVEL MULCHES TO BE A 2-INCHES MINIMUM THICKNESS OVER WEED BARRIER.
- FILTER FABRIC UNDER ALL GRAVEL MULCHES AND RIP RAP ROCK. MULCH TO BE ONE HALF INCH BELOW CONCRETE AND TWO INCHES BELOW TOP OF CURB AND NOTED ON LANDSCAPE PLAN.
- PLANTS TO BE IRRIGATED WITH AUTOMATIC DRIP IRRIGATION SYSTEM PROTECTED WITH BACKFLOW PREVENTION DEVICE (SEE DETAILS).
- EXISTING LANDSCAPE AREAS OUTSIDE PROJECT AREA TO BE PROTECTED DURING CONSTRUCTION.
- STREET TREES PROVIDED AT APPROX. 30' ON CENTER.
- PROPOSED COOL SEASON SOD AREA WILL BE FOR RECREATIONAL USE. SOD USED WILL BE THE SIEMPRE VERDE FESCUE MIX FROM EVERGREEN TURF.
- TURF GRASS SOD OR TURF GRASS SEED MIXES CONTAIN NO MORE THAN TWENTY-FIVE PERCENT KENTUCKY BLUEGRASS PER 14-8.4(F)(3)(A).
- PLANTS LOCATED IN BOTTOM THIRD OF POND AREAS ARE ADAPTED TO PERIODS OF INUNDATION.
- NO OBJECTS, WALLS, FENCES, PARKED VEHICLES OR VEGETATION SHALL BE PLACED WITHIN CLEAR-SIGHT TRIANGLES BETWEEN 3' AND 6' HEIGHT.
- STREET TREES WILL BE INSTALLED SO AS TO NOT OBSTRUCT STREET SIGNS.
- STREET TREES TO BE LOCATED/ADJUSTED TO BE A MINIMUM OF 15' FROM STREET LIGHT STANDARDS AND FIRE HYDRANTS PER ZONING SECTION 14-8.4(G)(3)(e)(f).
- ALL PROJECT STREET TREES SHALL BE IRRIGATED AND MAINTAINED BY THE DEVELOPER.
- NO TREES SHALL BE SITUATED WITHIN 15 FEET OF A WATERMAIN OR WASTEWATER MAIN PER CITY UTILITY CODES.
- ALL PROJECT STREET TREES SHALL BE IRRIGATED AND MAINTAINED BY THE DEVELOPER.
- A LANDSCAPE IRRIGATION PLAN BY A QUALIFIED IRRIGATION DESIGNER PER COSF CODE CHAPTER 14-8.4(E) WATER HARVESTING AND IRRIGATION STANDARDS AND COSF LANDSCAPE IRRIGATION DESIGN STANDARDS TO BE PROVIDED.
- THIS PLAN INCORPORATES PASSIVE WATER HARVESTING TECHNIQUES FOR LANDSCAPE IRRIGATION WHEREVER MADE POSSIBLE BY THE GRADING AND DRAINAGE PLAN. DETENTION AND RETENTION PONDS ARE INTENDED AS INTEGRATED LANDSCAPE FEATURES RATHER THAN SINGLE USE FLOOD CONTROL FEATURES.
- TRANSITION LINE BETWEEN EXCAVATION AND NATIVE SOIL SHALL BE SMOOTHLY RAKED, CREATING A CLEAN CONSISTENT GRADE. EXISTING GRADE BELOW NATIVE TREES AND SHRUBS SHALL REMAIN UNDISTURBED THROUGHOUT THE DRIPLINE OF THE PLANT MATERIAL. NO MOUNDING OF SOIL, FILL DIRT, ORGANIC OR INORGANIC DEBRIS SHALL BE ABANDONED UNDER NATIVE PLANT MATERIAL CANOPIES.
- ALL DISTURBED AREAS DUE TO CONSTRUCTION AND NOT PART OF THE LANDSCAPE PLAN SHALL BE REVEGETATED AND IRRIGATED INCLUDING SPOIL PILES AND STOCKPILES OF ANY MATERIAL. GRASS SEED MIX SHALL BE DRYLAND BLEND NATIVE GRASS MIXTURE FROM PLANTS OF THE SOUTHWEST OR EQUAL. THE SEED RATE SHALL BE 2 LBS. PER 1,000 SF.
- AREAS OF NATURAL GRAVEL, COBBLE, FRACTURED STONE, AND/OR BOULDER FIELDS SHALL BE RESTORED TO THE NATURAL STATE THAT EXISTED PRIOR TO CONSTRUCTION. GRADE OUT ANY DAMAGE TO THE NATURAL TERRAIN PRIOR TO THE RE-APPLICATION OF NATIVE STONE MATERIAL.
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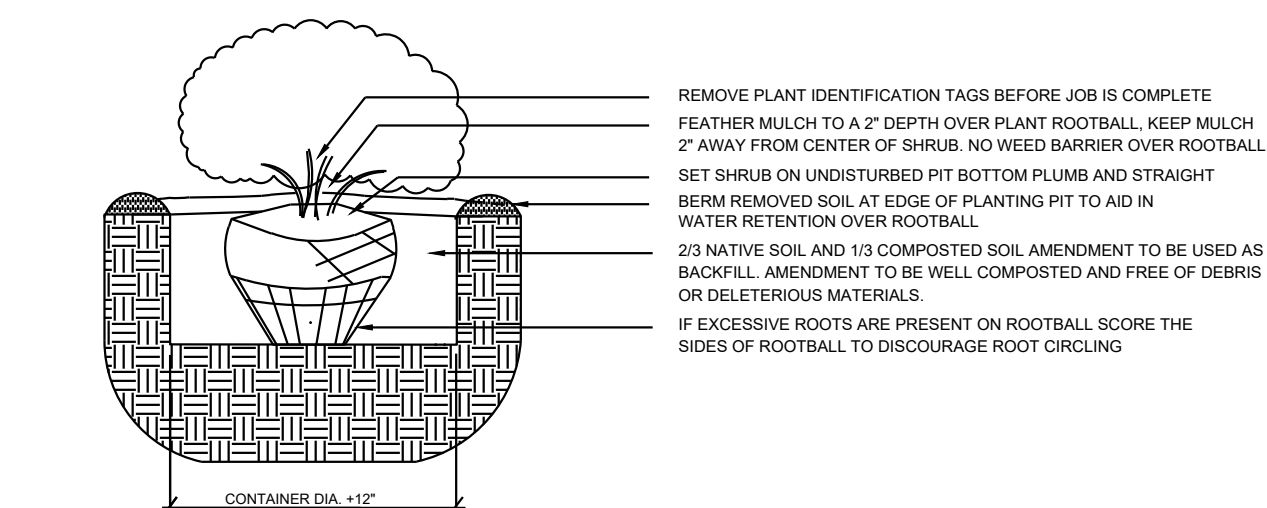
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1 WATER LEVEL MEASURING DEVICE N.T.S.



2 TREE PLANTING DETAIL N.T.S.



3 SHRUB PLANTING DETAIL N.T.S.

2/20/2025 10:00 AM - 11:00 AM - 12/01/2025 10:00 AM - 11:00 AM - 12/01/2025 10:00 AM - 11:00 AM









# WATER BUDGET CALCULATIONS

ALL SITE LANDSCAPING

## YEAR 1 AND YEAR 2

DRIP IRRIGATION TO TREES AND SHRUBS

4 MONTHS AT 1 RUN TIME PER MONTH

4 MONTHS AT 1 RUN TIME PER WEEK

APPROX 90 RUN TIMES PER YEAR

71 TREES	=	426 GALLONS PER RUN CYCLE			
365 SHRUBS	=	730 GALLONS PER RUN CYCLE			
1156 GALLONS PER RUN CYCLE X 90 RUN TIMES	=	104040	GPY	/	0.32 ACRE FT.

SPRAY IRRIGATION TO LAWN

945 SF X 3' PER YEAR	=	21207.27	GPY	/	0.07 ACRE FT.
TOTAL WATER USE PER YEAR	=	125247.27	GPY	/	0.38 ACRE FT.

## YEAR 3 AND BEYOND

DRIP IRRIGATION TO TREES AND SHRUBS

4 MONTHS AT 1 RUN TIME PER MONTH

4 MONTHS AT 1 RUN TIME PER WEEK

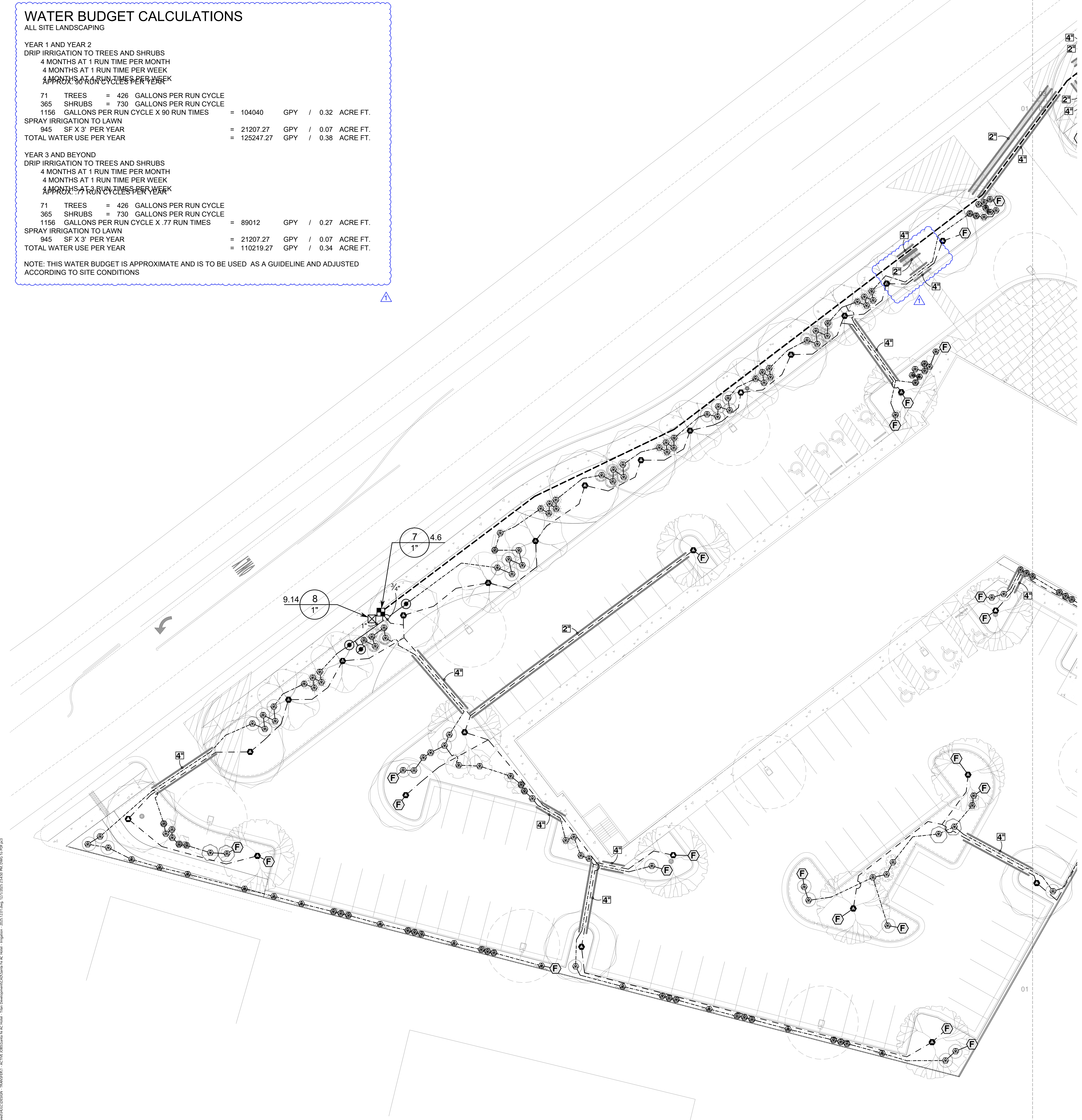
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NOTE: THIS WATER BUDGET IS APPROXIMATE AND IS TO BE USED AS A GUIDELINE AND ADJUSTED ACCORDING TO SITE CONDITIONS

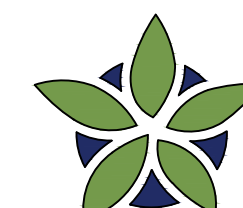


## Irrigation Schedule

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY
	RAIN BIRD R-VAN18 - TURF ROTARY 1804-SAM-P45, 13FT-18FT, 45-270 DEGREES AND 360 DEGREES, HAND ADJUSTABLE MULTI-STREAM ROTARY W/1800 TURF SPRAY BODY ON 4IN. POP-UP, WITH CHECK VALVE AND 45 PSI IN-STEM PRESSURE REGULATOR, 1/2IN. NPT FEMALE THREADED INLET.	7
	RAIN BIRD R-VAN24 - TURF ROTARY 1804-SAM-P45, 17FT-24FT, 45-270 DEGREES AND 360 DEGREES, HAND ADJUSTABLE MULTI-STREAM ROTARY W/1800 TURF SPRAY BODY ON 4IN. POP-UP, WITH CHECK VALVE AND 45 PSI IN-STEM PRESSURE REGULATOR, 1/2IN. NPT FEMALE THREADED INLET.	2
	DRIP CONTROL VALVE - SHRUB - L-M RAIN BIRD XCZ-100-PRF, MEDIUM FLOW DRIP CONTROL KIT, 3 1IN. DV VALVE, 1IN. PRESSURE REGULATING FILTER, 40PSI PRESSURE REGULATOR, 5 GPM-20 GPM.	3
	DRIP CONTROL VALVE - SHRUB - VL-L RAIN BIRD XCZ-100-LC, WIDE FLOW DRIP CONTROL KIT, FOR LIGHT COMMERCIAL USES, 1IN. PEB VALVE, WITH 1IN. PRESSURE REGULATING 40PSI BASKET FILTER, 0.3-20 GPM.	1
	DRIP CONTROL VALVE - TREE - L-M RAIN BIRD XCZ-100-PRF, MEDIUM FLOW DRIP CONTROL KIT, 3 1IN. DV VALVE, 1IN. PRESSURE REGULATING FILTER, 40PSI PRESSURE REGULATOR, 3 GPM-15 GPM.	3
	TRANSITION FITTING PIPE TRANSITION POINT FROM PVC LATERAL TO DRIP TUBING WITH RISER IN 6IN. DRIP BOX.	3
	FLUSH CAP NDS CEP900	43
	DRIP EMITTER - SHRUB - L-M (2) RB XB20-PC DRIP EMITTERS	306
	DRIP EMITTER - SHRUB - VL-L (2) RB XB20-PC DRIP EMITTERS	59
	DRIP EMITTER - TREE (6) RB XB20-PC DRIP EMITTERS	71
	CONTROL VALVE RAIN BIRD PEB, 1IN., 1-1/2IN., 2IN. PLASTIC INDUSTRIAL VALVES, LOW FLOW OPERATING CAPABILITY, GLOBE CONFIGURATION.	1
	MAIN SHUT OFF VALVE SPEARS SCH80 PVC COMPACT BALL VALVE	1
	MASTER VALVE 1" RAIN BIRD PEB, 1IN., 1-1/2IN., 2IN. PLASTIC INDUSTRIAL MASTER VALVES, LOW FLOW OPERATING CAPABILITY, GLOBE CONFIGURATION.	1
	REDUCED PRESSURE BACKFLOW PREVENTER 1" FEBCO 825Y	1
	CONTROLLER - RAIN BIRD ESP-2WIRE 50 STATION 2-WIRE, INDOOR/ OUTDOOR CONTROLLER W/ DECODER AUTO-ADDRESS, FOR RESIDENTIAL OR LIGHT COMMERCIAL USE. LNK WIFI MODULE AND FLOW SENSOR READY. USE WITH 2W-1 SINGLE STATION DECODERS AND STANDARD DIRECT BURIAL WIRE. LOCATION TO BE DETERMINED.	1
	FLOW SENSOR CREATIVE SENSOR TECHNOLOGY FSI-T10-001 1IN. PVC TEE TYPE FLOW SENSOR W/SOCKET ENDS, CUSTOM MOUNTING TEE AND ULTRA-LIGHTWEIGHT IMPELLER ENHANCES LOW FLOW MEASUREMENT. 2 WIRE DIGITAL OUTPUT COMPATIBLE W/ALL IRRIGATION CONTROLLERS. FLOW RANGE: .86 GPM - 52 GPM.	1
	WATER METER 1"	1
	IRRIGATION LATERAL LINE: PVC SCHEDULE 40	
	IRRIGATION LATERAL LINE: A940 DRIP POLYLINE - SHRUB - L-M	
	IRRIGATION LATERAL LINE: A940RDS DRIP POLYLINE - TREE	
	IRRIGATION LATERAL LINE: A940YS DRIP POLYLINE - SHRUB - VL-L	
	IRRIGATION MAINLINE: PVC SCHEDULE 40	
	PIPE SLEEVE: PVC CLASS 200 SDR 21	
	PIPE SLEEVE: PVC CLASS 200 SDR 21 - WIRE	
	Valve Callout # - Valve Number # - Valve Flow # - Valve Size	

## VALVE SCHEDULE

NUMBER	MODEL	SIZE	TYPE	GPM
1	DRIP CONTROL VALVE - SHRUB - VL-L	1"	DRIP EMITTER	3.94
2	DRIP CONTROL VALVE - TREE - L-M	1"	DRIP EMITTER	5.6
3	DRIP CONTROL VALVE - SHRUB - L-M	1"	DRIP EMITTER	4.8
4	CONTROL VALVE	1"	TURF ROTARY	7.88
5	DRIP CONTROL VALVE - SHRUB - L-M	1"	DRIP EMITTER	6.47
6	DRIP CONTROL VALVE - TREE - L-M	1"	DRIP EMITTER	4
7	DRIP CONTROL VALVE - TREE - L-M	1"	DRIP EMITTER	4.6
8	DRIP CONTROL VALVE - SHRUB - L-M	1"	DRIP EMITTER	9.14



**YELLOWSTONE**  
LANDSCAPE

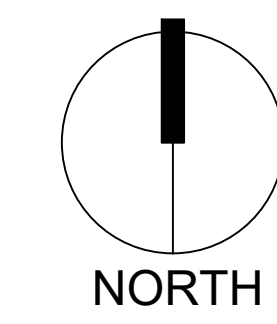
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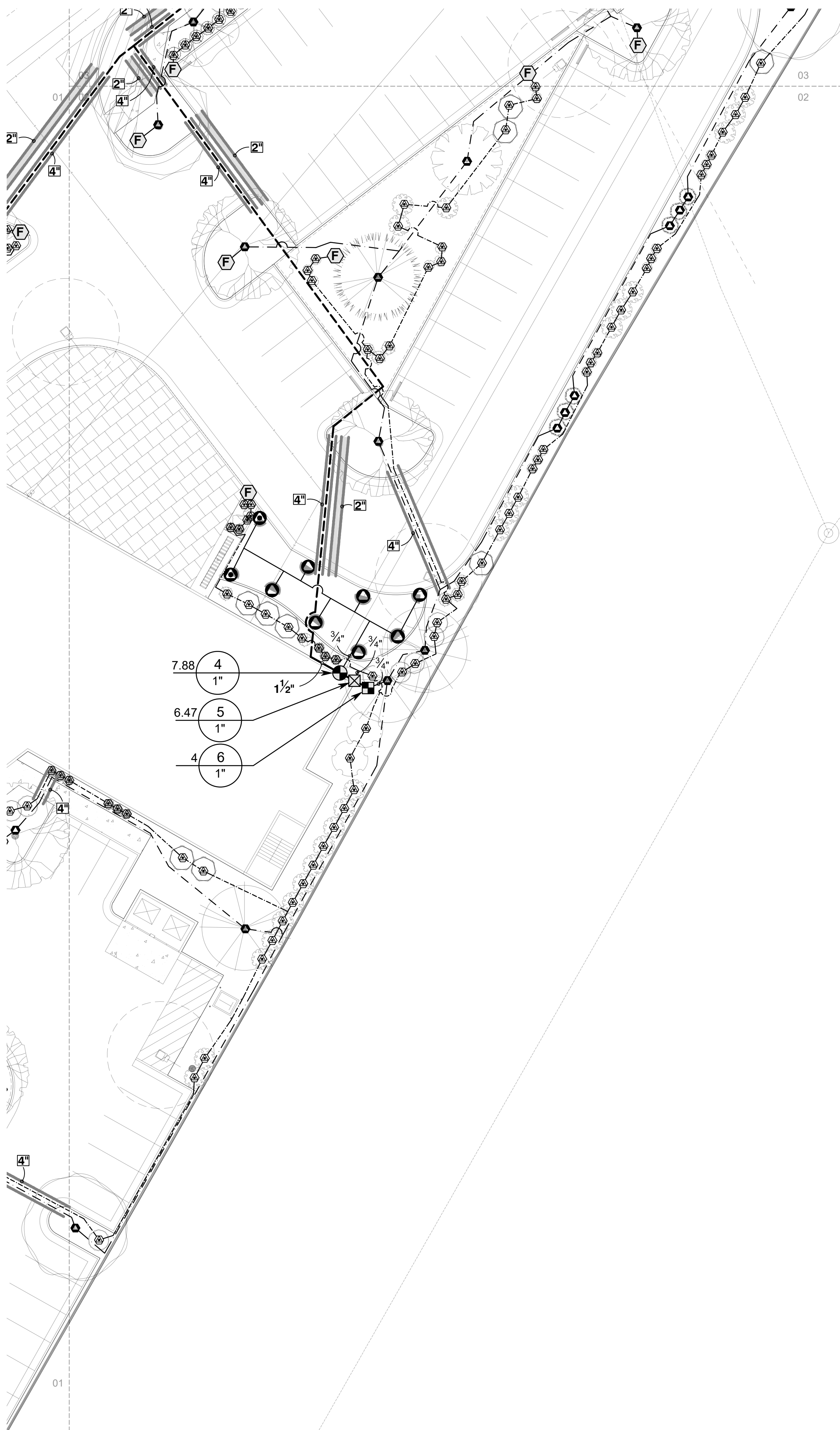


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Sheet Title:  
**Irrigation Plan Enlargement**

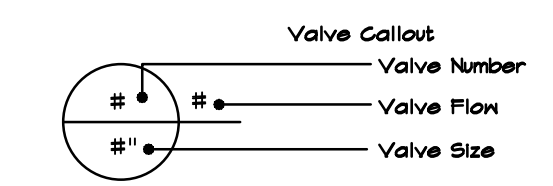
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### Irrigation Schedule

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY
	RAIN BIRD R-VAN18 - TURF ROTARY 1804-SAM-P45, 13FT-18FT, 45-270 DEGREES AND 360 DEGREES, HAND ADJUSTABLE MULTI-STREAM ROTARY W/1800 TURF SPRAY BODY ON 4IN. POP-UP, WITH CHECK VALVE AND 45 PSI IN-STEM PRESSURE REGULATOR, 1/2IN. NPT FEMALE THREADED INLET.	7
	RAIN BIRD R-VAN24 - TURF ROTARY 1804-SAM-P45, 17FT-24FT, 45-270 DEGREES AND 360 DEGREES, HAND ADJUSTABLE MULTI-STREAM ROTARY W/1800 TURF SPRAY BODY ON 4IN. POP-UP, WITH CHECK VALVE AND 45 PSI IN-STEM PRESSURE REGULATOR, 1/2IN. NPT FEMALE THREADED INLET.	2
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY
	DRIP CONTROL VALVE - SHRUB - L-M RAIN BIRD XCZ-100-PRF, MEDIUM FLOW DRIP CONTROL KIT, 3 1IN. DV VALVE, 1IN. PRESSURE REGULATING FILTER, 40PSI PRESSURE REGULATOR, 5 GPM-20 GPM.	3
	DRIP CONTROL VALVE - SHRUB - VL-L RAIN BIRD XCZ-100-LC, WIDE FLOW DRIP CONTROL KIT, FOR LIGHT COMMERCIAL USES, 1IN. PEB VALVE, WITH 1IN. PRESSURE REGULATING 40PSI BASKET FILTER, 0.3-20 GPM.	1
	DRIP CONTROL VALVE - TREE - L-M RAIN BIRD XCZ-100-PRF, MEDIUM FLOW DRIP CONTROL KIT, 3 1IN. DV VALVE, 1IN. PRESSURE REGULATING FILTER, 40PSI PRESSURE REGULATOR, 3 GPM-15 GPM.	3
	TRANSITION FITTING PIPE TRANSITION POINT FROM PVC LATERAL TO DRIP TUBING WITH RISER IN 6IN. DRIP BOX.	3
	FLUSH CAP NDS CEP900	43
	DRIP EMITTER - SHRUB - L-M (2) RB XB20-PC DRIP EMITTERS	306
	DRIP EMITTER - SHRUB - VL-L (2) RB XB20-PC DRIP EMITTERS	59
	DRIP EMITTER - TREE (6) RB XB20-PC DRIP EMITTERS	71
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY
	CONTROL VALVE RAIN BIRD PEB, 1IN., 1-1/2IN., 2IN. PLASTIC INDUSTRIAL VALVES, LOW FLOW OPERATING CAPABILITY, GLOBE CONFIGURATION.	1
	MAIN SHUT OFF VALVE SPEARS SCH80 PVC COMPACT BALL VALVE	1
	MASTER VALVE 1" RAIN BIRD PEB, 1IN., 1-1/2IN., 2IN. PLASTIC INDUSTRIAL MASTER VALVES, LOW FLOW OPERATING CAPABILITY, GLOBE CONFIGURATION.	1
	REDUCED PRESSURE BACKFLOW PREVENTER 1" FEBCO 825Y	1
	CONTROLLER - RAIN BIRD ESP-2WIRE 50 STATION 2-WIRE, INDOOR/ OUTDOOR CONTROLLER W/ DECODER AUTO-ADDRESS, FOR RESIDENTIAL OR LIGHT COMMERCIAL USE. LNK WIFI MODULE AND FLOW SENSOR READY. USE WITH 2W-1 SINGLE STATION DECODERS AND STANDARD DIRECT BURIAL WIRE. LOCATION TO BE DETERMINED.	1
	FLOW SENSOR CREATIVE SENSOR TECHNOLOGY FSI-T10-001 1IN. PVC TEE TYPE FLOW SENSOR W/SOCKET ENDS, CUSTOM MOUNTING TEE AND ULTRA-LIGHTWEIGHT IMPELLER ENHANCES LOW FLOW MEASUREMENT. 2 WIRE DIGITAL OUTPUT COMPATIBLE W/ALL IRRIGATION CONTROLLERS. FLOW RANGE: .86 GPM - 52 GPM.	1
	WATER METER 1"	1
	IRRIGATION LATERAL LINE: PVC SCHEDULE 40	
	IRRIGATION LATERAL LINE: A940 DRIP POLYLINE - SHRUB - L-M	
	IRRIGATION LATERAL LINE: A940RDS DRIP POLYLINE - TREE	
	IRRIGATION LATERAL LINE: A940YS DRIP POLYLINE - SHRUB - VL-L	
	IRRIGATION MAINLINE: PVC SCHEDULE 40	
	PIPE SLEEVE: PVC CLASS 200 SDR 21	
	PIPE SLEEVE: PVC CLASS 200 SDR 21 - WIRE	



### VALVE SCHEDULE

NUMBER	MODEL	SIZE	TYPE	GPM
1	DRIP CONTROL VALVE - SHRUB - VL-L	1"	DRIP EMITTER	3.94
2	DRIP CONTROL VALVE - TREE - L-M	1"	DRIP EMITTER	5.6
3	DRIP CONTROL VALVE - SHRUB - L-M	1"	DRIP EMITTER	4.8
4	CONTROL VALVE	1"	TURF ROTARY	7.88
5	DRIP CONTROL VALVE - SHRUB - L-M	1"	DRIP EMITTER	6.47
6	DRIP CONTROL VALVE - TREE - L-M	1"	DRIP EMITTER	4
7	DRIP CONTROL VALVE - TREE - L-M	1"	DRIP EMITTER	4.6
8	DRIP CONTROL VALVE - SHRUB - L-M	1"	DRIP EMITTER	9.14

### WATER BUDGET CALCULATIONS

ALL SITE LANDSCAPING

YEAR 1 AND YEAR 2

DRIP IRRIGATION TO TREES AND SHRUBS  
4 MONTHS AT 1 RUN TIME PER MONTH  
4 MONTHS AT 1 RUN TIME PER WEEK  
4 MONTHS AT 3 RUN TIMES PER WEEK  
APPROX. 90 RUN CYCLES PER YEAR

71 TREES	= 426 GALLONS PER RUN CYCLE			
370 SHRUBS	= 740 GALLONS PER RUN CYCLE			
1166 GALLONS PER RUN CYCLE X 90 RUN TIMES	= 104940	GPY	/	0.32 ACRE FT.
SPRAY IRRIGATION TO LAWN				
945 SF X 3" PER YEAR	= 21207.27	GPY	/	0.07 ACRE FT.
TOTAL WATER USE PER YEAR	= 126147.27	GPY	/	0.39 ACRE FT.

YEAR 3 AND BEYOND

DRIP IRRIGATION TO TREES AND SHRUBS  
4 MONTHS AT 1 RUN TIME PER MONTH  
4 MONTHS AT 1 RUN TIME PER WEEK  
4 MONTHS AT 3 RUN TIMES PER WEEK  
APPROX. 77 RUN CYCLES PER YEAR

71 TREES	= 426 GALLONS PER RUN CYCLE			
370 SHRUBS	= 740 GALLONS PER RUN CYCLE			
1166 GALLONS PER RUN CYCLE X .77 RUN TIMES	= 89782	GPY	/	0.28 ACRE FT.
SPRAY IRRIGATION TO LAWN				
945 SF X 3" PER YEAR	= 21207.27	GPY	/	0.07 ACRE FT.
TOTAL WATER USE PER YEAR	= 110989.27	GPY	/	0.34 ACRE FT.

NOTE: THIS WATER BUDGET IS APPROXIMATE AND IS TO BE USED AS A GUIDELINE AND ADJUSTED ACCORDING TO SITE CONDITIONS

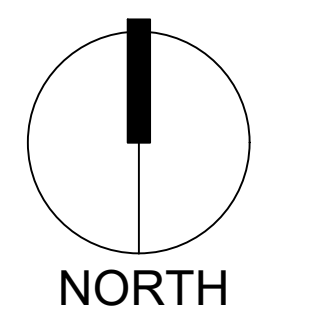
**YELLOWSTONE**  
LANDSCAPE

www.yellowstonelandscape.com  
PO Box 10597  
Albuquerque, NM 87184  
505.898.9615  
design@yellowstonelandscape.com



Date: 10/14/2025  
Revisions:  
 12/01/2025  
  
  
  
  
 Drawn by: PL  
Reviewed by: CM

**AC by Marriott Hotel**  
1000, 1101 & 1103 Cerrillos Road  
Santa Fe, New Mexico

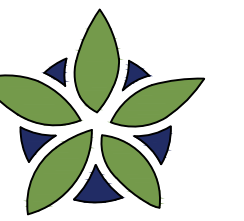


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Sheet Title:  
**Irrigation Plan  
Enlargement**

Sheet Number:  
**LI-02**





**YELLOWSTONE**  
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design@yellowstonelandscape.com



Date: 10/14/2025  
Revisions:  
▲ 12/01/2025  
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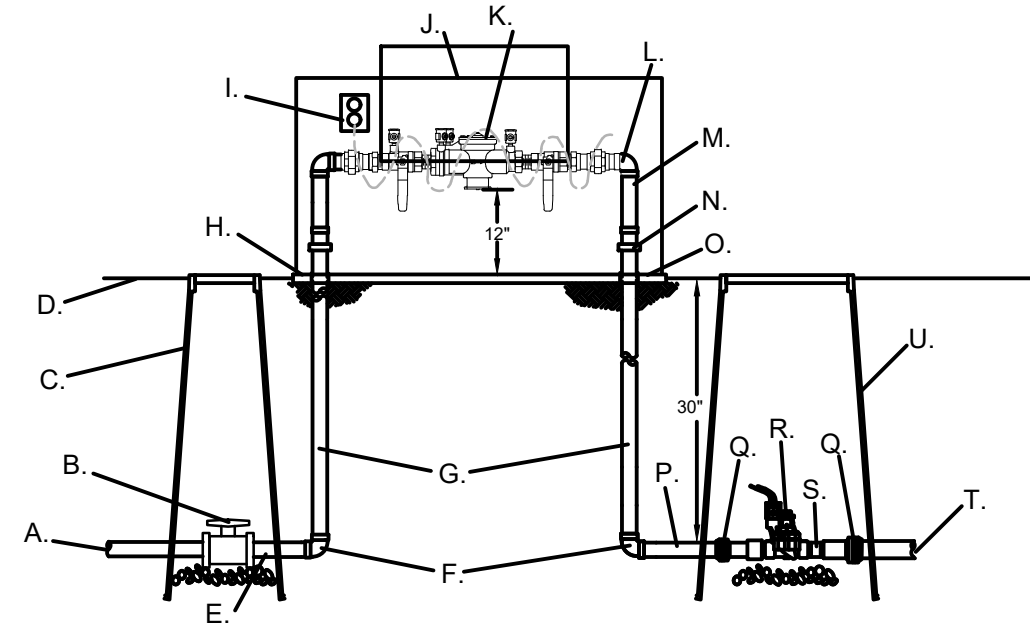
Drawn by: PL  
Reviewed by: CM

**AC by Marriott Hotel**  
1000, 1101 & 1103 Cerrillos Road  
Santa Fe, New Mexico

Sheet Title:  
**Irrigation  
Details**

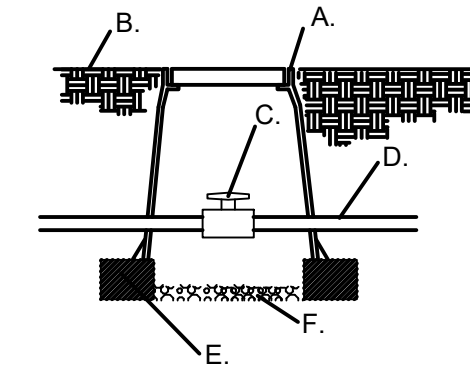
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**LI-04**

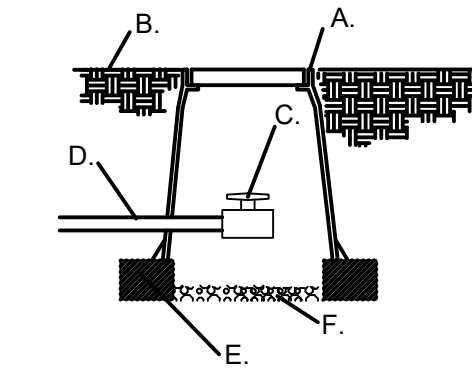


- A. IRRIGATION SUPPLY LINE
- B. SCH. 80 UTILITY BALL VALVE
- C. APPLIED ENGINEERING PRODUCTS 910 ROUND VALVE BOX WITH EXTENSIONS AS NEEDED
- D. FINISH GRADE
- E. SCH. 80 NIPPLE
- F. GAL. 90°
- G. GAL. 30" NIPPLE
- H. 4" THICK CONCRETE PAD (6" WIDER THAN ASSEMBLY ON ALL SIDES)
- I. OUTLET FOR HEAT TAPE PROVIDED BY OTHERS
- J. HOT BOX (HEATED ENCLOSURE)
- K. REDUCED PRESSURE BACKFLOW PREVENTER
- L. GAL. STREET 90°
- M. GAL. NIPPLE (TYP.)
- N. GAL. UNION (TYP.)
- O. PVC SLEEVE THROUGH CONCRETE (TYP.)
- P. SCH. 80 NIPPLE
- Q. SCH. 80 TRUE UNION
- R. MASTER VALVE
- S. SLIP X THREAD MALE ADAPTER
- T. SCH. 40 PVC MASTERLINE
- U. APPLIED ENGINEERING PRODUCTS 1320 VALVE BOX WITH EXTENSIONS AS NEEDED

NOTE: INSTALL BACKFLOW PREVENTER AS REQUIRED BY LOCAL CODES AND HEALTH DEPARTMENT. VERIFY LOCAL REQUIREMENTS PRIOR TO INSTALLATION.



- A. APPLIED ENGINEERING PRODUCTS 910 ROUND VALVE BOX WITH EXTENSIONS AS NEEDED
- B. FINISH GRADE
- C. SCH. 80 UTILITY BALL VALVE
- D. MAINLINE/24" BURY
- E. 8" X 8" X 16" CMU CONCRETE BLOCK
- F. 4" LAYER OF 3/4" GRAVEL



- A. APPLIED ENGINEERING PRODUCTS 910 ROUND VALVE BOX WITH EXTENSIONS AS NEEDED
- B. FINISH GRADE
- C. SCH. 80 UTILITY BALL VALVE
- D. MAINLINE/24" BURY
- E. 8" X 8" X 16" CMU CONCRETE BLOCK
- F. 3/4" GRAVEL SUMP UNDER MANUAL DRAIN TO BE A MINIMUM OF 12" DEEP

1 RP BACKFLOW/MASTER VALVE DETAIL

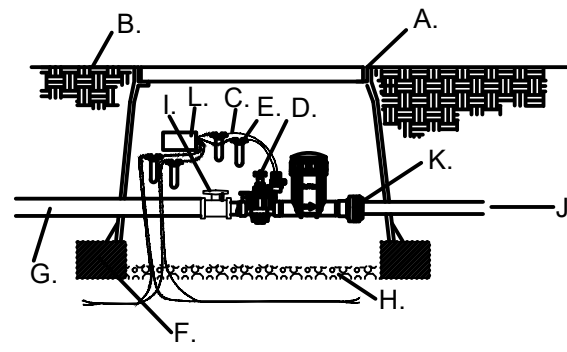
N.T.S.

2 MAINLINE ISOLATION VALVE DETAIL

N.T.S.

3 MANUAL DRAIN VALVE DETAIL

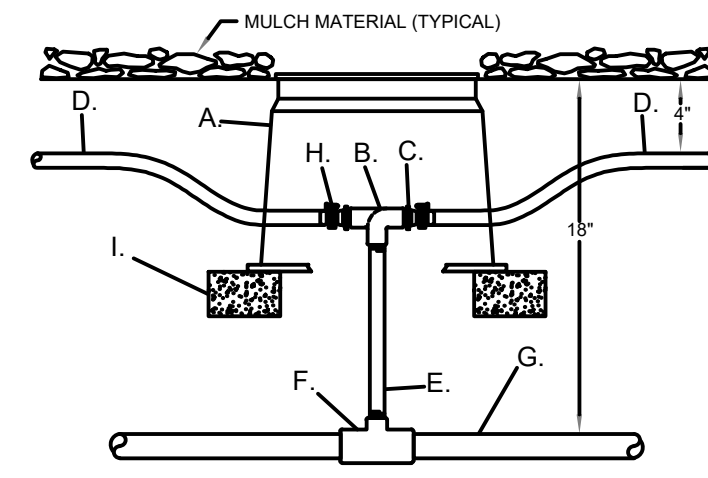
N.T.S.



- A. APPLIED ENGINEERING PRODUCTS 1320 VALVE BOX WITH EXTENSIONS AS NEEDED
- B. FINISH GRADE
- C. SOLENOID WIRE
- D. RAIN BIRD XEZ CONTROL ZONE KIT WITH BASKET FILTER, SEE SCHEDULE
- E. DRY SPLICE CONNECTOR OR EQUAL
- F. 8" X 8" X 16" CMU CONCRETE BLOCK
- G. MASTERLINE/24" BURY
- H. 4" LAYER OF 3/4" GRAVEL
- I. SCH. 80 TRUE UNION BALL VALVE
- J. LATERAL LINE, SEE PLAN.
- K. SCH. 80 UNION
- L. FIELD DECODER, SEE SCHEDULE.

4 DRIP VALVE WITH FILTER & PRESSURE REGULATOR FOR 2-WIRE IRRIGATION

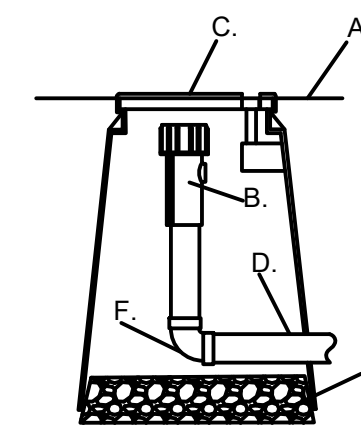
N.T.S.



- A. 6" ROUND BOX WITH LID
- B. SCH.40 PVC THREADED TEE OR THREADED ELL (REFER TO PLAN) - 3/4"
- C. BARBED DRIP INSERT FITTING (TYPICAL) (3/4" MIPT x 3/4" BARB)
- D. 3/4" POLY DRIP TUBING - MAXIMUM RUN OF 150' IN EACH DIRECTION
- E. SCH.80 PVC NIPPLE - 3/4" x 6" (OR LENGTH AS REQUIRED)
- F. SCH.40 PVC LATERAL FITTING WITH 3/4" THREADED OUTLET (SEE PLAN FOR SIZE)
- G. LATERAL PIPING (SEE PLAN FOR SIZE AND TYPE)
- H. 1" STAINLESS STEEL HOSE CLAMP (TYPICAL) - SECURE TUBING TO INSERT FITTING
- I. SOLID BRICK (2 BRICKS REQUIRED)

5 PVC TO POLY TRANSITION

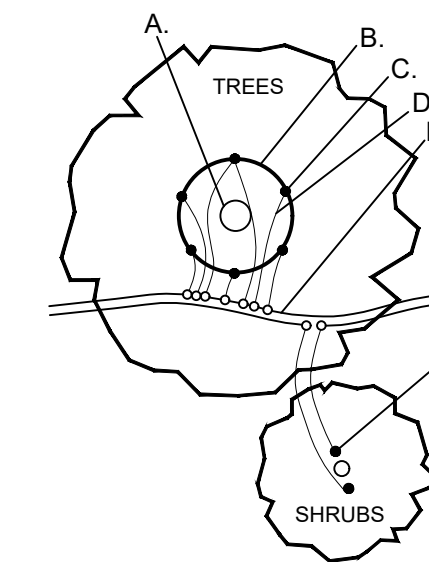
N.T.S.



- A. FINISH GRADE
- B. END FLUSH CAP
- C. 6" ROUND BOX WITH LID
- D. 3/4" POLY DRIP PIPE
- E. 2" DEPTH OF GRAVEL
- F. 90° FITTING

6 END FLUSH CAP

N.T.S.

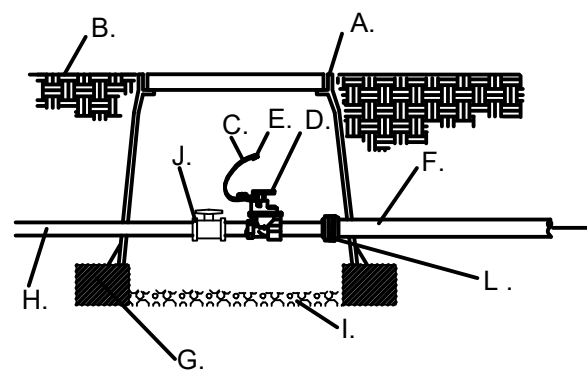


- A. TREE TRUNK/ ROOT CROWN
- B. 24" CIRCLE FROM TRUNK
- C. EMITTERS
- D. 1/4" DISTRIBUTION LINE
- E. 3/4" POLYETHYLENE DRIPLINE
- F. EMITTER PLACED AT THE EDGE OF THE ROOT BALL

NOTE:INSTALL EMITTERS ABOVE THE ROOT BALL IF PLANT IS INSTALLED ON A SLOPE.

7 EMITTER PLACEMENT DETAIL

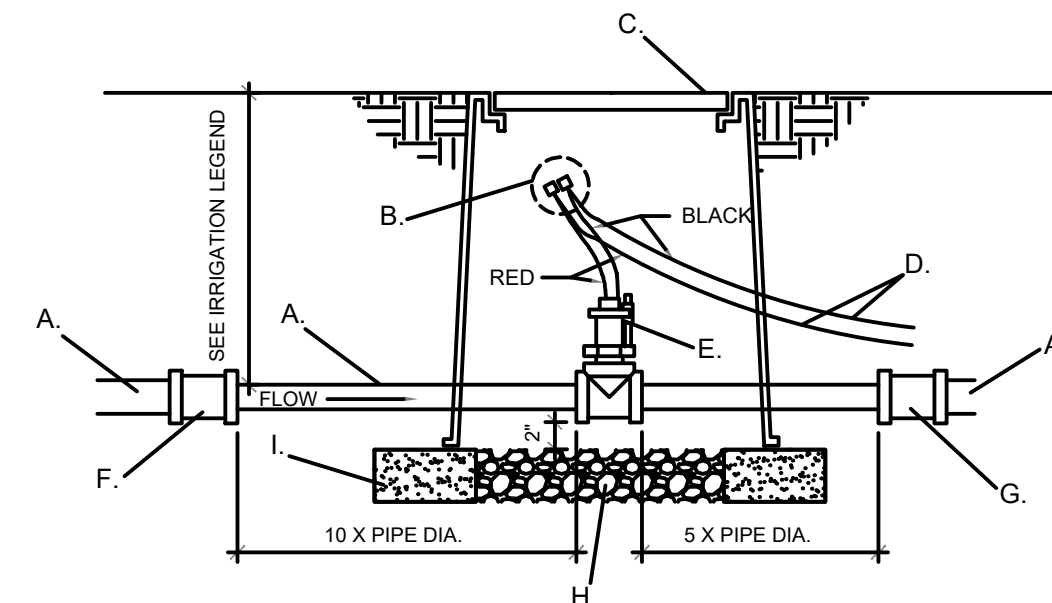
N.T.S.



- A. APPLIED ENGINEERING PRODUCTS 1320 VALVE BOX WITH EXTENSIONS AS NEEDED
- B. FINISH GRADE
- C. 24" WIRE LOOP
- D. AUTOMATIC VALVE, SEE IRRIGATION LEGEND
- E. DRY SPLICE CONNECTOR OR EQUAL
- F. LATERAL LINE/24" BURY
- G. 8" X 8" X 16" CMU CONCRETE BLOCK
- H. MASTERLINE/24" BURY
- I. 4" LAYER OF 3/4" GRAVEL
- J. SCH. 80 TRUE UNION BALL VALVE
- K. SCH. 40 PVC LATERAL LINE
- L. SCH. 80 UNION

8 AUTOMATIC VALVE

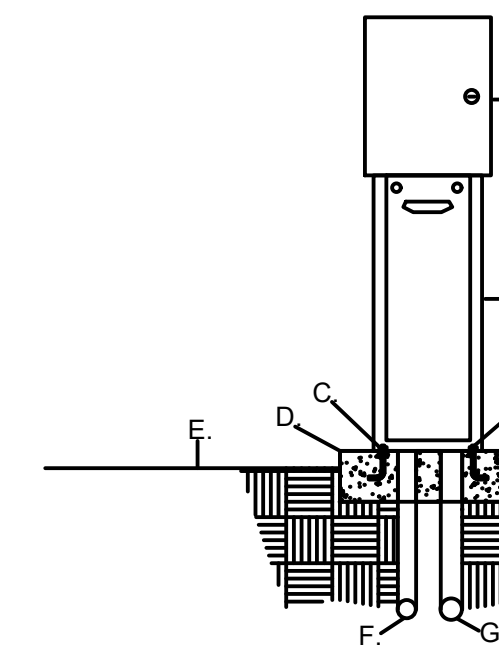
N.T.S.



- A. PVC IRRIGATION MAINLINE, SEE PLAN.
- B. WATERPROOF CONNECTIONS.
- C. APPLIED ENGINEERING PRODUCTS MODEL 1320 VALVE BOX WITH EXTENSIONS AS REQUIRED AND FLUSH BOLT-DOWN COVER.
- D. SHIELDED CABLE CONNECTED TO IRRIGATION CONTROLLER - SEE IRRIGATION LEGEND.
- E. FLOW SENSOR - SEE IRRIGATION LEGEND.
- F. SIZE OR DIRECTION CHANGE AT DISTANCE EQUAL TO 10X PIPE DIAMETER FROM FLOW SENSOR.
- G. SIZE OR DIRECTION CHANGE AT DISTANCE EQUAL TO 5X PIPE DIAMETER FROM FLOW SENSOR.
- H. 1" DIAMETER WASHED ROCK, 8" DEPTH.
- I. 8"X8"X16" SOLID CMU BLOCK - 4 PER VALVE BOX.

9 FLOW SENSOR

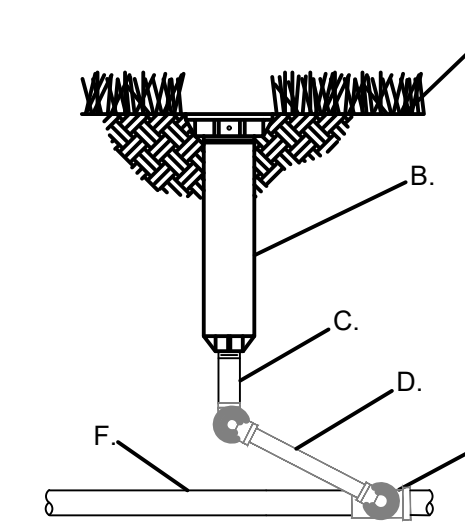
N.T.S.



- A. AUTOMATIC CONTROLLER, SEE IRRIGATION LEGEND
- B. PEDESTAL (SAME AS CONTROLLER MANUFACTURER)
- C. 1/2" X 6" ANCHOR BOLTS, FOUR PER PEDESTAL
- D. 3000 PSI CONCRETE PAD, 18" X 24" X 8" THICK
- E. FINISH GRADE
- F. 3/4" RIGID PVC SWEEP ELL (120 VOLT WIRE)
- G. 2" RIGID PVC SWEEP ELL (24 VOLT WIRE)

10 PEDESTAL MOUNTED AUTOMATIC CONTROLLER

N.T.S.



- A. FINISH GRADE
- B. SEE PLAN FOR HEAD TYPE
- C. PVC SCH 40 NIPPLE
- D. HUNTER SWING JOINT
- E. PVC SCH 40 TEE
- F. PVC LATERAL LINE

11 ROTOR/ROTARY HEAD

N.T.S.



Cooper Poles Square Straight Steel Anchor Bolt Construction - SSS-AB

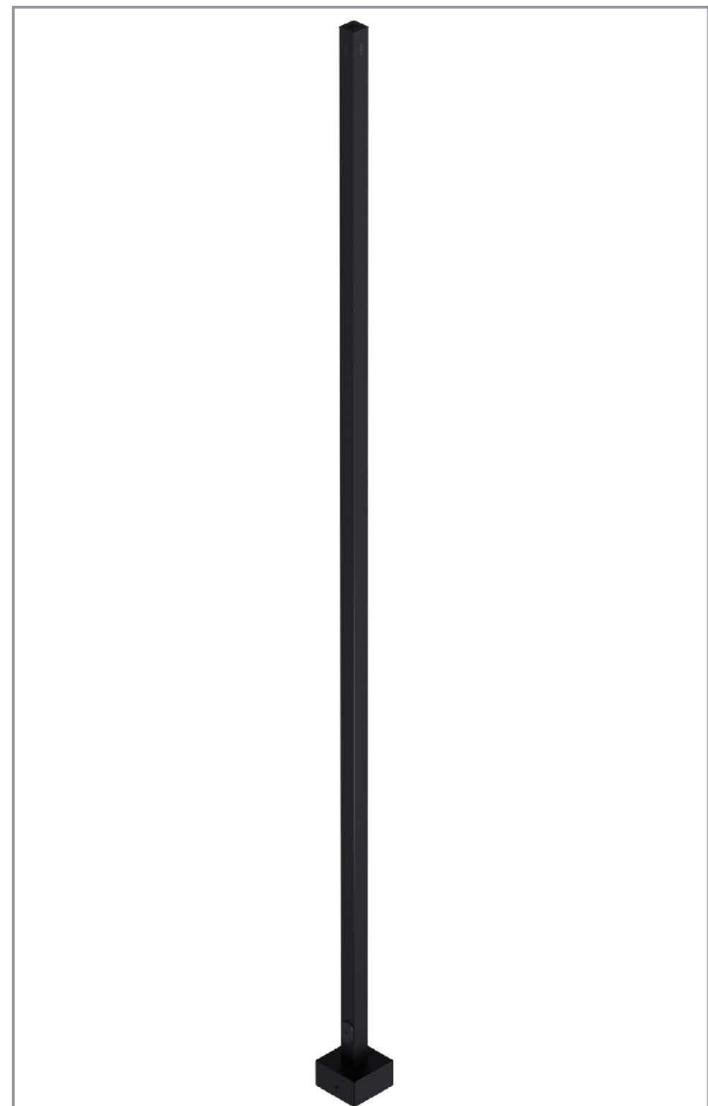
Ordering Information Table with columns: Domestic Preference, Mounting Height(H), Shaft Size-Wall Thickness, and Installation Type.

Mounting Types Table with columns: Mounting Types, Prep/Finish, and Options (Add as Suffix).

NOTES: 1. Only product configurations with this prefix are built to be compliant with the Buy American Act of 1933 (BAA) or the Buy America Build America Act (BABA).

Accessories (Order Separately) Table listing various options like BCP-SSS-xxxxx, HCP-SSS-xxxxx, and DRAL-PLUS-KIT-xxxx.

Project, Catalog #, Type, Prepared by, Notes, Date table.



Cooper Poles Square Straight Steel - SSS

Anchor Base Installation, Product Certifications & Features (listing IESNA, AASHTO, etc.), Base Cover, Handhole, and Finish details.

Quick Facts: Weldable grade carbon steel, One-year limited warranty, Fully galvanized anchor bolts & template included. Includes Interactive Menu and Domestic Preferences.

Project, Catalog #, Type, Prepared by, Notes, Date table.



McGraw-Edison GALN Galleon II

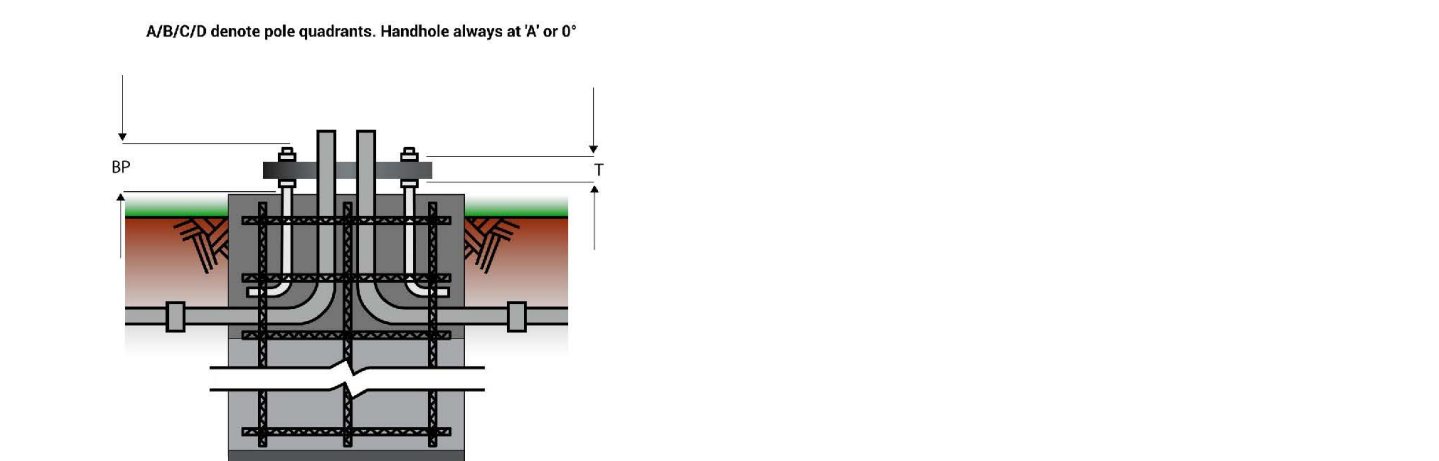
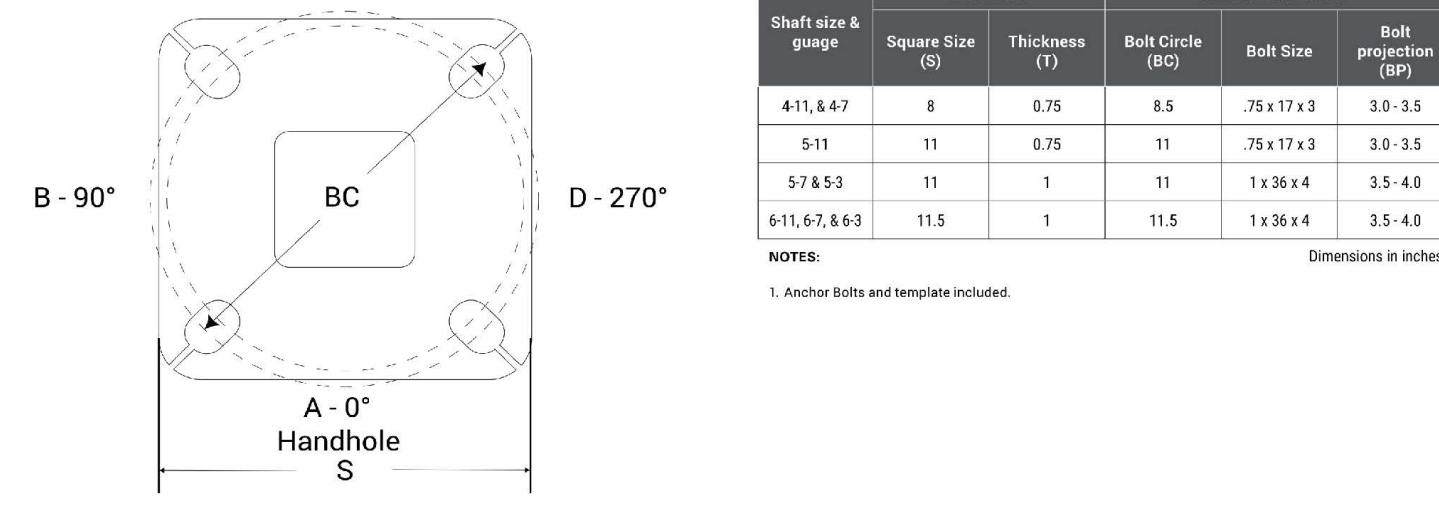
Area / Site Luminaire, Product Features (listing BAA, TAA, etc.), Product Certifications, and Interactive Menu.

Quick Facts: Lumen packages range from 3,300 - 102,700 (33W - 658W), 22 optical distributions, Efficacy up to 178 LPW. Includes Connected Systems and Dimensional Details.

Dimensional Details Table with columns: Number of Light Squares, Width 'A', Housing Length 'B', Weight with Standard or Opt Arm, and EPA with Standard or Opt Arm.

Cooper Poles Square Straight Steel Anchor Bolt Construction - SSS-AB

Mounting Plate Details Table with columns: Shaft size & gauge, Square Size, Thickness (T), Bolt Circle (BC), Bolt Size, and Bolt Spacing (BP).



NOTE: If custom base option required, please specify BMDxxxx option and provide drawing. Approval required, additional lead times may apply. Premium metal base cover included.

McGraw-Edison GALN Galleon II

Ordering Information Table with columns: Product Family, Light Source, Square Count, Lumen Output, Color Temperature, Voltage, Distribution, Mounting, and Finish.

Options (Add as Suffix) and Accessories (Order Separately) tables listing various configurations and add-ons.

NOTES: 1. Customer is responsible for engineering analysis of contents page and fixture compatibility for all applications. Refer to us for more information. 2. Single-light configurations available. Refer to www.mcgraw-edison.com for more information.

BLUM CONSULTING ENGINEERS, INC. 12790 Merit Drive, Suite 700 Dallas, Texas 75251 www.blumeng.com

5GSTUDIO COLLABORATIVE. OWNER/DEVELOPER: Titan Development, 6300 Riverside Plaza, Ste 200 Albuquerque, New Mexico 87120.

ARCHITECT: 5G Studio Collaborative, LLC., 1217 Main Street Dallas, TX 75202. CIVIL ENGINEER: Tierra West, LLC, 5571 Midway Park PI NE Albuquerque, New Mexico 87109.

LANDSCAPE ARCHITECT: Yellowstone Landscape, 7525 Second Street NW Albuquerque, New Mexico 87107. MEP: Blum Consulting Engineers, Inc., 12790 Merit Drive, Building 9, Suite 700 Dallas, Texas 75251.

Revision table with columns: REV, DATE, ISSUE TITLE. Shows a revision on 10/14/25 for DEVELOPMENT PLAN.

Professional Engineer seal for Scott Swan, New Mexico, License No. 22823.

AC BY MARRIOTT HOTEL DEVELOPMENT PLAN 1000, 1101 & 1103 CERRILLOS RD SANTA FE, NM 87505 LIGHTING FIXTURE CUTSHEETS



**OWNER/DEVELOPER**  
Tilan Development  
6300 Riverside Plaza, Ste 200  
Albuquerque, New Mexico 87120  
(505-998-0163 Ian Robertson)

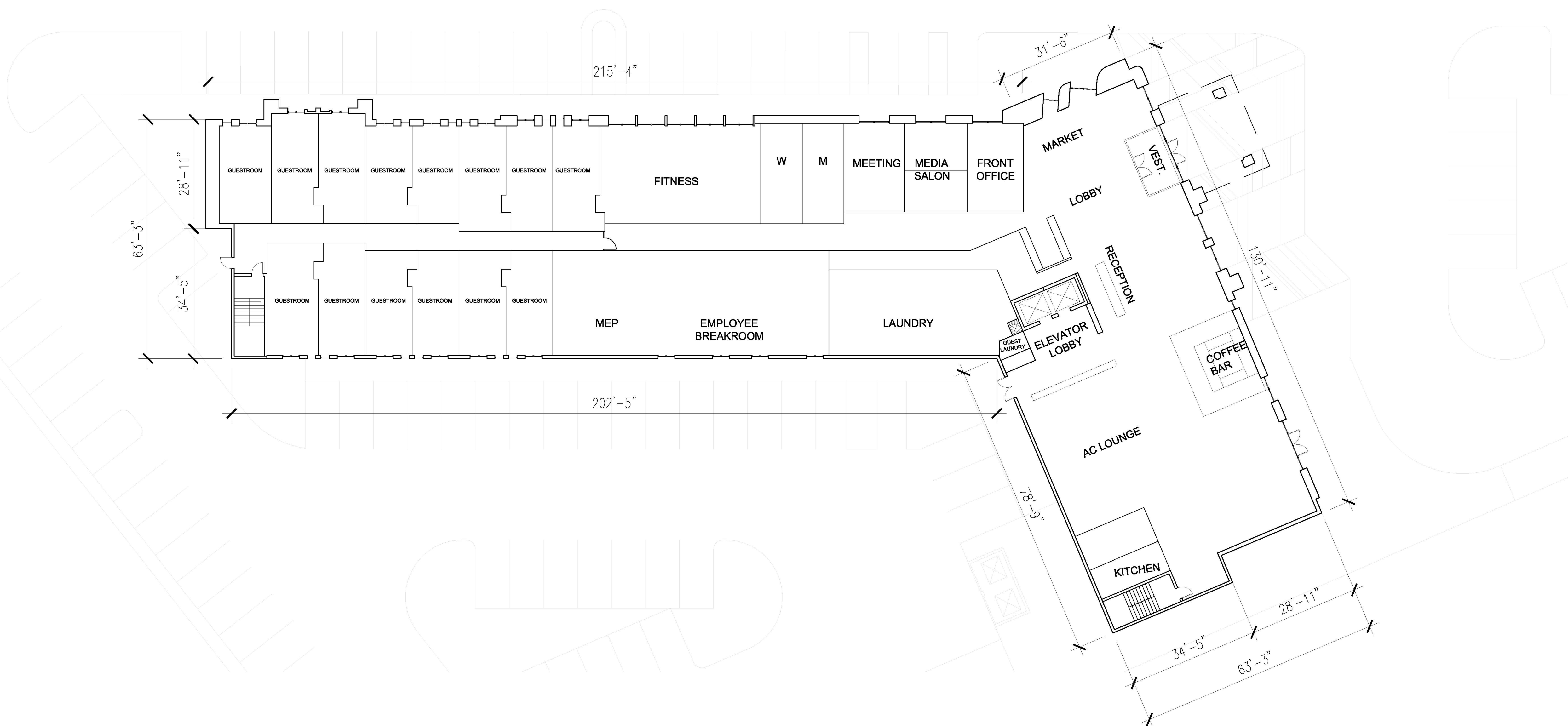
**ARCHITECT**  
5G Studio Collaborative, LLC.  
1217 Main Street  
Dallas, TX 75202  
(214-670-0050 Christine Robbins-Elrod)

**CIVIL ENGINEER**  
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(505-858-3100 Ronald R. Bohannon)

**CONSULTANT**  
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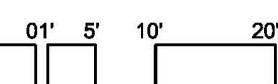
**LANDSCAPE ARCHITECT**  
Yellowstone Landscape  
7525 Second Street NW  
Albuquerque, New Mexico 87107  
(505-998-9615 Cody McNaillen)

**MEP**  
Blum Consulting Engineers, Inc.  
12790 Merit Drive, Building 9, Suite 700  
Dallas, Texas 75251  
(214-373-8222 Jake Musick)



**01 FIRST FLOOR PLAN**

SCALE 1/16"=1'-0"



REV.	DATE	ISSUE TITLE
1	12/01/25	DEVELOPMENT PLAN REV 1



12.01.25

AC BY MARRIOTT HOTEL  
DEVELOPMENT PLAN  
1000, 1101 & 1103 CERRILLOS RD  
SANTA FE, NM 87505

FIRST FLOOR PLAN

250008  
PROJECT NUMBER

A201  
SHEET NUMBER









